



**UNION INTERNATIONALE
MOTONAUTIQUE**

**STATUTES - STATUTS
BY LAWS - R.O.I.**

AS FROM 2005

TABLE DES MATIERES

STATUTS

CHAPITRE	PAGE
I. DENOMINATION / OBJET/ DUREE/ SIEGE SOCIAL	2
II. CONDITION D'ADMISSION, DE DEMISSION OU DU PATRIMOINE	3
III. ADMINISTRATION DE L'ASSOCIATION	7
IV. SURVEILLANCE DE L'ASSOCIATION	11
V. RESSOURCES ANNUELLES	12
VI. MODIFICATION AUX STATUTS	12
VII. DISSOLUTION - LIQUIDATION - DEVOLUTION DU PATRIMOINE	13

INDEX**STATUTES**

<u>CHAPITRE</u>	<u>PAGE</u>
I. NAME - OBJECT - HEADQUARTERS	2
II. ADMITTANCE RESIGNATION OR EXCLUSION OF MEMBERS	3
III. ADMINISTRATION OF THE ASSOCIATION	7
IV. CONTROL OF THE ASSOCIATION	11
V. ANNUAL INCOME	12
VI. MODIFICATION OF STATUTES	12
VII. DISSOLUTION - WINDING-UP - DEVOLUTION OF ASSETS AND LIABILITIES	13

I. DENOMINATION - OBJET - DUREE - SIEGE SOCIAL

ARTICLE 1

L'Association Internationale dénommée Union Internationale Motonautique, en abrégé U.I.M., constituée à Bruxelles et transférée à Monaco à la date d'avril 1988 et fonctionnant dans le cadre de la loi n°1072 du 27.06.84 pour une durée illimitée une Association Internationale à but scientifique, pédagogique et sportif dénommée Union Internationale Motonautique, en abrégé U.I.M., régie par les principes généraux du droit applicables aux contrats et obligations, les dispositions des présents statuts et le règlement d'ordre intérieur.

ARTICLE 2

L'association a pour objet toute étude, examen, recherche scientifique et technique et pédagogique se rapportant au domaine du motonautisme, de la navigation tant en mer que dans les eaux intérieures et la recherche des meilleurs moyens de promouvoir l'établissement d'écoles de formation, pour initier la jeunesse aux activités motonautiques, de la coordination et de la comparaison des travaux effectués et des résultats obtenus dans le monde entier par l'intermédiaire de ses membres quelle qu'en soit la nationalité.

Pour réaliser son objet, elle a recours à des personnes hautement qualifiées dans les pays représentés à U.I.M., ainsi qu'à des spécialistes dans les diverses branches de la construction navale des petites et moyennes unités dans tous domaines, qu'il s'agisse de coques, d'objets d'équipement ou de moyens de propulsion, sans en rien excepter.

Les recherches seront poussées jusqu'aux études pratiques faites dans des conditions se rapprochant de l'utilisation des prototypes étudiés.

A cet effet et pour rendre comparables les résultats obtenus de par le monde, elle réglemente l'établissement de records et codifie la course et autres épreuves sportives et épreuves de plaisance ayant pour objet de faire bénéficier tous les intéressés du résultat de ses études et recherches.

I. NAME - OBJECT - HEADQUARTERS

ARTICLE 1

The International Association which is named the Union Internationale Motonautique (U.I.M.) was established in Brussels and was transferred to Monaco in April, 1988, where it functions in conformity with Law Number 1072, dated June 27, 1984, for an unlimited period. An International Association with a scientific, educational and sports aim, the Union Internationale Motonautique (U.I.M.) is governed by the general principles of the law applicable to contracts and obligations, by the provisions contained in these Statutes and by the By-laws of the Association.

ARTICLE 2

The object of the Association is the study, examination and scientific or educational research of matters related to motorboating and navigation, at sea and on inland waters, as well as a quest for the best means to promote the establishment of training schools and to introduce youth to motonautical activities. The Association functions as an intermediary to co-ordinate and compare the work performed and the results achieved throughout the world by its members of all nationalities.

To accomplish this purpose, the Association uses the assistance of highly qualified people in the countries represented by the U.I.M. and of specialists in the many realms of small and medium-sized boat building, including hulls, equipment and propulsion means.

Research includes practical tests carried out under conditions close to those in which the prototypes under study are intended to be used.

In order to obtain comparable results from all parts of the world, the Association establishes rules for speed records and for racing as well as for sports and for pleasure navigation events, all of which are aimed at providing everyone concerned with the advantages of the results obtained from the studies and research.

ARTICLE 3

Le siège social est actuellement situé à Monaco. Il peut être fixé en un point quelconque du territoire de la Principauté par décision du Conseil (Conseil d'Administration).

II. CONDITION D'ADMISSION, DE DEMISSION OU D'EXCLUSION

ARTICLE 4

L'association comprend des membres :

effectifs	4.1
correspondants	4.2
associés	4.3
adhérents	4.4
d'honneur	4.5
honoraire	4.6

Les demandes d'admission doivent être adressées au Conseil de l'U.I.M. Elles comportent l'adhésion aux statuts et règlements. L'admission est proposée à l'A.G. par le Conseil.

4.1 Membres effectifs

Peuvent être membres effectifs, les fédérations de chaque pays affilié et reconnues comme Autorité Nationale par l'U.I.M. Seuls les membres effectifs ont droit de vote aux assemblées. Il ne peut y avoir qu'un membre effectif par pays affilié.

Si l'Autorité Nationale d'un pays ne désire pas régir un secteur sportif particulier de l'U.I.M., une autre organisation du même pays peut alors devenir un membre de l'U.I.M. pour ledit secteur sportif exclusivement.

ARTICLE 3

The Headquarters are presently located in Monaco. They can be located in any place on the territory of the Principality by a decision of the Council (Board of Administrators).

II. ADMITTANCE, RESIGNATION OR EXCLUSION OF MEMBERS

ARTICLE 4

The Association is composed of the following members:

Full members	4.1
Corresponding members	4.2
Associate members	4.3
Adherent members	4.4
Members of Honour	4.5
Honorary Members	4.6

Requests for admission must be sent to the Council of U.I.M. They imply a full compliance with the Statutes and By-Laws. Requests are submitted to the G.A. by the Council for approval.

4.1 Full Members

Are allowed to become full members the Federations of each affiliated country acknowledged as a National Authority of U.I.M. Only full members are entitled to vote at the assemblies. There can be only one full member per affiliated country.

If a N.A. in a country does not want to govern an approved special sports sector of the U.I.M., then a different organisation in that country can become a member of the U.I.M. for that special sports sector only.

Les droits de vote de cette organisation et de ses membres sont alors limités au seul secteur sportif qu'ils représentent.

Pour être admise à l'U.I.M. une Autorité Nationale doit adresser une demande, signée par le Président ou un deuxième dirigeant accrédité, au Conseil de l'U.I.M., en accompagnant la demande de deux exemplaires des statuts en français ou anglais et dans la langue nationale, ainsi qu'une liste du comité et les noms des sociétés affiliées. La demande d'admission doit être soumise à l'Assemblée Générale, dont la décision est prise à la majorité des deux tiers des membres présents ou représentés. L'admission peut se faire à titre provisoire, par le Conseil, cette admission devant obligatoirement être ratifiée par l'Assemblée Générale. Pendant la période transitoire, le nouveau membre n'a pas de droit de vote.

4.2 Membres correspondants

Dans le but de rallier à l'U.I.M. les pays où il n'existe pas de Fédération, ni d'Autorité Nationale, mais où les clubs isolés font cependant preuve d'activité, il est créé une catégorie de Membres Correspondants.

Les Membres Correspondants peuvent assister aux Assemblées Générales, mais ils n'y ont pas droit de vote. L'admission se fait comme pour les membres effectifs. Le Conseil peut nommer provisoirement des Membres Correspondants, sous réserve de ratification par l'Assemblée Générale la plus proche. Les Membres Correspondants s'engagent à faire auprès des groupements motonautiques de leur pays, les démarches nécessaires en vue de la formation d'une Fédération Nationale.

Les nouveaux membres sont Membres Correspondants pour une période d'un an. Le Conseil décidera de la catégorie de leurs cotisations au moment où ils deviennent Membres Effectifs.

Un nouveau Membre Correspondant de l'U.I.M. ne peut obtenir le statut de Membre Permanent que s'il a une activité internationale (organisation d'épreuves internationales notamment).

Au cas où une Autorité Nationale, Membre Correspondant ou Membre Permanent de l'U.I.M., n'aurait eu aucune activité internationale depuis plus de deux ans, le Conseil serait alors en droit de nommer ou d'accepter un nouveau Membre Correspondant.

The voting rights of that organisation and its members are limited solely to the special sports sector, which they represent.

To be admitted in U.I.M., a National Authority has to send in a request, duly signed by its President or a second substantiated executive, to the Council of U.I.M., with two copies of its Statutes in French or English and in the national language and a list of the members of the committee and the names of the affiliated associations. The request must be submitted to the General Assembly, whose decision is to be carried by a majority of two thirds of the members present. The Council may admit a member temporarily, this admission having to be ratified by the General Assembly. During the transitory period, the new member has no right to speak and vote.

4.2 Corresponding Members

In order to provide the countries where there is neither Federation nor National Authority, but in which isolated clubs are proving quite active, with a chance to join U.I.M., a category of Corresponding Members has been set-up. The Corresponding Members are allowed to attend the General Assembly but have no right to vote. The provisions for admission are the same as those governing Full Members. The Council may admit Corresponding Members on a temporary basis, subject to ratification by the following General Assembly. Corresponding Members agree to take, with motorboating groups and association in their countries, the action required with a view to setting-up a National Federation.

New members are corresponding members for one year and the Council will decide on their category membership fees when they become full members.

A new corresponding member of the U.I.M. can only become a full member if they are active at international level (i.e. organising international races).

For a National Authority being correspondent or full member without international activity for more than 2 years, the Council has the right to appoint or accept another corresponding member in order to make international activities possible.

4.3 Membres associés

Le Conseil accepte, comme Membres Associés, des personnes physiques s'intéressant au motonautisme. Les fédérations ou groupements sont exclus. La qualité de Membre Associé ne confère aucun droit et n'entraîne aucune responsabilité dans l'administration de l'U.I.M.

4.4 Membres adhérents

Sur proposition du Conseil, l'A.G. accepte des Membres Adhérents, parmi lesquels peuvent figurer des firmes commerciales, des personnes ou des groupements exclus dans les articles 4.1, 4.2 ,4.3.

4.5 Membres d'honneur

Sur proposition du Conseil, l'Assemblée Générale peut octroyer à des personnalités particulièrement distinguées le titre de Membre d'honneur de l'U.I.M.

4.6 Membres honoraires

Sur proposition du Conseil, l'Assemblée Générale peut octroyer, à des personnalités particulièrement distinguées, le titre honoraire de leur fonction au service du motonautisme ou de l'U.I.M., ou encore pour services éminents rendus à cette dernière.

4.7 Action judiciaire

4.7.1

Toute Autorité Nationale qui, pour une question sportive, intenterait une action judiciaire contre l'U.I.M., sera automatiquement, immédiatement et définitivement suspendue avec déchéance de tous ses droits et prérogatives comme membre de l'U.I.M.

4.7.2

Tout club ou personne, soumis(e) à l'Autorité Nationale qui, pour une question sportive, intenterait une action judiciaire contre l'U.I.M., doit être suspendu (e) par son Autorité Nationale.

4.3 Associate Members

The Council admits, as Associate Members, individuals interested in motorboating. Federations and clubs are excluded from this category. The quality of Associate Members involves no right whatsoever nor any responsibility in U.I.M. administration.

4.4 Adherent Members

Upon a proposal of the Council, the G.A. accepts Adherent Members which may include business companies, individuals or groups excluded from 4.1, 4.2, and 4.3.

4.5 Members of Honour

Upon a proposal by the Council, the General Assembly may grant the title of U.I.M. Member of Honour to particularly eminent persons.

4.6 Honorary Members

Upon a proposal by the Council, the General Assembly may grant to particularly eminent persons the honorary title of their function for their service to motorboating or U.I.M. or for specific services to the latter.

4.7 Legal action

4.7.1

Any National Authority who, with reference to a question of sports, should bring a legal action against the U.I.M. will automatically, immediately and definitively be excluded as a member of the U.I.M. with forfeiture of all rights and privileges.

4.7.2

Any club or person affiliated to a National Authority, who would bring any legal action with reference to a question of sport against the U.I.M. will be suspended by its National Authority.

ARTICLE 5 - COTISATION

Les Membres Effectifs, Correspondants, Associés et Adhérents payent une cotisation annuelle dont le montant est fixé annuellement par l'Assemblée Générale. Tout membre qui pour une raison quelconque, cesse de faire partie de l'U.I.M. n'a aucun droit sur l'actif social.

ARTICLE 6 - DÉMISSION ET EXCLUSION

La qualité de membre se perd par démission (6.1) suspension (6.2) exclusion (6.3)

6.1

La démission volontaire comme membre effectif ne peut se faire que par lettre recommandée à la fin de l'année avec préavis de 6 mois. Passé ce délai les cotisations restent dues pour l'exercice suivant. Les lettres de démission doivent être signées par le Président et le Secrétaire Général de l'Autorité Nationale en cause.

6.2

La suspension d'une Autorité Nationale peut être prononcée par décision du Conseil à l'unanimité de ses membres ou par l'Assemblée Générale statuant à la majorité de 75% (soixante-quinze pour cent) de ses membres, dans les cas suivants :

- En cas de non-paiement des montants dus à l'U.I.M.
- En cas de non-paiement des cotisations annuelles dues à l'U.I.M. depuis deux années et plus
- En cas de non-respect des dispositions contenues dans les Statuts et le Règlement d'Ordre Intérieur de l'U.I.M.

6.3

L'exclusion d'une Autorité Nationale peut être prononcée par résolution de l'Assemblée Générale de l'U.I.M. statuant à la majorité de 75% (soixante-quinze pour cent) de ses membres, dans les cas suivants :

ARTICLE 5 - CONTRIBUTIONS

Full Members, Corresponding Members, Associate Members and Adherent Members pay an annual fee, the amount of which is fixed annually by the General Assembly. Any member who leaves U.I.M. for any reason whatsoever, has no right regarding the Association's assets.

ARTICLE 6 - RESIGNATION AND EXCLUSION

Membership can be lost through resignation (6.1), suspension (6.2), exclusion (6.3).

6.1

A full member may resign voluntarily by registered letter sent at the end of the calendar year, with a six-month previous notice. After such term, fees remain due for the following financial year. Resignation letter must be duly signed by the President and the Secretary General of the National Authority concerned.

6.2

A National Authority may be suspended from membership by a unanimous decision of the Council or a 75 % (seventy-five per cent) majority decision of a General Assembly for the following reasons :

- Non payment of any liabilities to the U.I.M.
- Becoming two years or more in arrears with any annual membership fee due to the U.I.M.
- Failure to comply with the provisions of the U.I.M. Statutes or the By Laws appertaining thereto.

6.3

A National Authority may be expelled from membership on a resolution passed by 75 % (seventy-five per cent) majority of the U.I.M. in General Assembly for the following reasons :

- En cas de refus de payer les montants dus à l'U.I.M.
- En cas de non-paiement de cotisations dues à l'U.I.M. depuis quatre ans et plus
- En cas de refus de respecter les dispositions contenues dans les Statuts et le Règlement d'Ordre Intérieur de l'U.I.M.

6.4.

L'Autorité Nationale incriminée doit être invitée à se défendre devant l'Assemblée Générale avant que l'exclusion ne soit prononcée. L'exclusion est définitive. La suspension est réversible.

6.5

Toute proposition de suspension ou d'exclusion doit obligatoirement figurer à l'ordre du jour de la réunion de l'Assemblée Générale ou du Conseil.

III. ADMINISTRATION DE L'ASSOCIATION

ARTICLE 7 - ASSEMBLÉE GÉNÉRALE

7.1

L'Assemblée Générale possède des pouvoirs absolus, permettant la réalisation de l'objet de l'Association. Elle se compose des membres effectifs. Chaque membre a droit à une voix.

7.2

L'Assemblée Générale se réunit de plein droit chaque année la troisième semaine pleine du mois d'octobre, sous la présidence du Président de l'U.I.M., au siège social ou en tout lieu indiqué dans la convocation. Cette dernière doit être envoyée par le Secrétariat au minimum deux mois avant la date prévue. L'Assemblée Générale peut, en outre être convoquée à la demande du Président ou par 30 % du nombre total d'Autorités Nationales affiliées, avec droit de vote.

- Refusal to pay any liabilities due to the U.I.M.
- Non payment of any membership fees due to the U.I.M. for 4 (four) or more years.
- Refusal to comply with the provisions of the U.I.M. Statutes or the By Laws appertaining thereto.

6.4.

The full member concerned must be invited to defend itself at the General Assembly before the exclusion is carried. The exclusion is final. The suspension is reversible.

6.5

Any proposal for suspension, or expulsion has to be included in the agenda of the General Assembly and the Council.

III. ADMINISTRATION OF THE ASSOCIATION

ARTICLE 7 - GENERAL ASSEMBLY

7.1

The General Assembly has full powers to meet the objectives of the Association. It is composed of full members. Each member has one vote.

7.2

The General Assembly meets every year on the third full week of October under the chairmanship of the President of the U.I.M., at the headquarters or in any place mentioned in the notice of the meeting. This notice must be sent by the Secretariat at least two months before the dates fixed. Moreover, the General Assembly may be convened upon a request by the President or by 30 % of the total number of affiliated National Authorities with right of vote.

7.3

Sauf dans les cas prévus par les présents statuts, les résolutions sont prises à la majorité simple des membres effectifs présents, et elles sont portées à la connaissance de tous les membres effectifs et correspondants.

7.4

Il ne peut être statué sur aucun objet ou sujet non porté à l'ordre du jour, sauf sur résolution prise à la majorité des deux tiers des membres présents.

7.5

Un règlement d'ordre intérieur sera élaboré par le Conseil et ratifié par l'Assemblée Générale à la majorité des deux tiers des membres effectifs présents. Il doit être appliqué par tous.

7.6

Les résolutions de l'Assemblée Générale sont enregistrées par ordinateur. Les fichiers de sauvegarde, sous enveloppe scellée signée par le Président et le Secrétaire Général, et sont conservés dans le coffre-fort de l'UIM. Les résolutions sont tenues à la disposition des membres.

7.7.

Pour délibérer valablement, l'Assemblée Générale doit être composée de 33 % au moins des membres effectifs de l'Association présents.

Si cette condition n'est pas remplie, l'Assemblée Générale est convoquée à nouveau et les délibérations sont valables quel que soit le nombre de membres présents; elles ne peuvent cependant porter que sur les points mis à l'ordre du jour de la première réunion.

ARTICLE 8 - ADMINISTRATION

8.1

L'Union Internationale Motonautique est administrée par un Conseil de six personnes au moins, dont une doit être domiciliée à Monaco. Sur

7.3

Except in the cases provided for in these Statutes, resolutions will be carried by a simple majority of full members present and will be disseminated to all full and corresponding members.

7.4

No decision may be made on a matter which is not included in the agenda, except in case of a request put forward by a two-third majority of the members present.

7.5

A by-law will be drawn-up by the Council and ratified by the General Assembly with a two-third majority of members present. It must be complied-with by all.

7.6

The resolutions of the General Assembly are registered ***by computer. The back up files are kept, in a sealed envelope signed by the President and the Secretary General, in the UIM safe. These resolutions are at the disposal of the members.***

7.7

To speak and vote validly, the General Assembly must be composed of at least 33 % of the full members of the Association present.

Should this condition fail to be fulfilled, the General Assembly would be convened again and its discussions and votes would then be valid whatever the number of members present. Such discussions and votes, however, should then only deal with the matters included in the agenda of the first meeting.

ARTICLE 8 - ADMINISTRATION

8.1

Union Internationale Motonautique is administered by a Council composed of six persons at least, one of whom must be domiciled in

proposition des Autorités Nationales, le Président du Conseil et les Administrateurs sont élus par l'Assemblée Générale. La durée de leur mandat est de quatre ans, un quart des membres du Conseil est renouvelé chaque année.

Les Administrateurs sont rééligibles. Ils sont révocables par l'Assemblée Générale statuant à la majorité des deux tiers des membres effectifs présents.

8.2

Le Conseil élit dans son sein le premier et le deuxième Vice-Président et un Trésorier. Le Conseil nomme un Secrétaire Général qui sera rémunéré par l'U.I.M. Les Présidents des Commissions sont membres du Conseil avec droit de vote.

Le Secrétaire Général est le chef de l'Administration et du personnel de l'U.I.M., il est chargé notamment de la conservation des résolutions et décisions de l'Assemblée Générale et du Conseil. Il est chargé de veiller à leur application. Il s'occupe des contacts au nom de l'U.I.M. Le Président, Secrétaire Général et le consultant juridique sont membres de tous les Comités de l'U.I.M. ex officio et sans droit de vote.

8.3

Le Conseil se réunit au moins avant et après l'Assemblée Générale et sur convocation spéciale chaque fois qu'une réunion s'avère nécessaire. Ses résolutions sont prises à la majorité simple des administrateurs présents, en cas de parité, la voix du président ou de celui qui le remplace est prépondérante.

8.4

Le Conseil a tous les pouvoirs de gestion et de d'administration sous réserve des attributions de l'Assemblée Générale.

8.5

Tous les actes qui engagent l'Association sont, signés par le Président avec le premier ou le deuxième Vice-Président ou un autre administrateur, qui n'ont pas à justifier des pouvoirs vis-à-vis des tiers.

Monaco. Upon a proposal by National Authorities, the President of the Council and the Administrators are elected by the General Assembly. The duration of their mandate is four years, 25 % of the members of the Council being re-eligible every year.

Administrators are re-eligible. They are subject to dismissal by the General Assembly with a two-third majority of the full members, present.

8.2

The Council elects among its members a Senior Vice President and Vice-President and a Treasurer. The Council nominates an Secretary General who will be remunerated by the U.I.M. The Presidents of the Commissions are members of the Council with a right to vote.

The Secretary General is the head of U.I.M. administration and staff. He namely has to record the resolutions and decisions of the General Assembly and the Council. He has to see to their application. He takes all contacts on behalf of U.I.M. The President and the Secretary General and the legal consultant are members of all U.I.M. bodies, with no right to vote.

8.3

The Council meets at least prior and after the General Assembly and upon special notices whenever a meeting is required. Its resolutions are taken by the simple majority of the administrators present, and in case of parity the vote by the President or the person replacing him prevails.

8.4

The Council has all powers of management and administration, subject to competence provided by the General Assembly.

8.5

All acts binding the Association must be signed by the President with the Senior Vice-President or Vice-President or another Administrator. They do not have to justify their power of attorney to third parties.

8.6

Les actions judiciaires, tant en demandant qu'en défendant sont suivies, poursuites et diligence, du Conseil représenté par son président, par le premier ou le deuxième Vice-Président ou par un Administrateur désigné. Le choix doit être acté au procès-verbal.

8.7

Le Comité exécutif du Conseil est composé de :

- Président de l'U.I.M.
- Premier Vice-Président
- Deuxième Vice-Président
- Trésorier
- ***Consultant juridique***

Les fonctions des différents membres sont décrites in extenso dans le règlement d'ordre intérieur.

8.8

Le Conseil peut déléguer les pouvoirs qu'il juge nécessaires et convenables à un ou plusieurs de ses membres pour un ou plusieurs objets bien déterminés. Cette délégation temporaire de pouvoirs doit figurer chaque fois au procès verbal du Conseil qui a pris la décision. Cette décision doit être motivée et limitée dans le temps.

8.9

Les décisions du Conseil sont prises à la majorité simple des votes valablement exprimés sauf dispositions spéciales des statuts ou du règlement d'ordre intérieur. Les procurations ne sont pas admises.

ARTICLE 9 - BUDGET ET COMPTES

L'exercice social est clôturé chaque année le trente et un décembre. Le Conseil est tenu de soumettre tous les ans à l'approbation de

8.6

Prosecutions before a court of justice, both as a plaintiff or as a defendant, legal actions and proceedings are taken and followed-up by the Council represented by its President, by the Senior Vice-President or Vice-President, by an Administrator appointed in this purpose. The choice must be recorded in the minutes.

8.7

The Executive Committee of the Council is composed of :

- the President of U.I.M.
- the Senior Vice-President
- the Vice-President
- the Treasurer
- ***the Legal Consultant***

The powers of the various members are referred to in details in the by-laws.

8.8

The Council may delegate the powers it deems necessary and proper to one or several of its members for one or several well-defined matters. Each time this provisional delegation of powers must be stated in the minutes of the Council meeting that made the decision. This decision must state the reasons on which it is based and must have a deadline.

8.9

The Council decisions are taken by the simple majority of the valid votes except where otherwise expressly provided in the statutes or by-laws. The proxy is not accepted.

ARTICLE 9 - BUDGET AND ACCOUNTS

The financial year ends every year on the thirty-first of December. Every year, the Council has to submit the accounts of the past financial year

l'Assemblée Générale le bilan de l'exercice écoulé et le budget de l'exercice suivant.

IV. SURVEILLANCE DE L'ASSOCIATION

ARTICLE 10

Conformément à la loi Monégasque n° 1072 (art.12) du 27 juin 1984 le Président via le Secrétariat est tenu, dans le mois, de déclarer au Secrétariat Général du Ministère d'Etat qui en délivrera récépissé :

1. tout changement d'adresse du siège social ;
2. toute modification dans la composition du Conseil d'Administration ainsi que dans les fonctions de ses membres ;
3. toute acquisition ou aliénation de locaux et immeubles ;
4. toute décision de l'Assemblée Générale modifiant les statuts ;
5. toute décision de l'Assemblée Générale comportant dissolution volontaire de l'Association.

ARTICLE 11

Conformément à la loi Monégasque n° 1072 du 27 juin 1984, les administrateurs sont tenus de publier au Journal de Monaco un avis mentionnant :

1. La dénomination, l'objet et l'adresse du siège social ;
2. Toutes les modifications affectant ces mentions ;
3. La décision comportant dissolution de l'Association.

La publication doit être faite dans le mois qui suit soit la publication de l'Arrêté Ministériel d'autorisation, soit le prononcé de la dissolution.

(balance sheet) and the budget of the following financial year to the approval of the General Assembly.

IV. CONTROL OF THE ASSOCIATION

ARTICLE 10

In conformity with Article 12 in law n° 1072 of Monaco dated June the 27th, 1984, the President - via the Secretariat - has to deliver to the Secretariat General of the State Ministry within the month, the following documents :

1. Notice of any change in the address of the headquarters ;
2. Notice of any change in the composition of the Board of Administrators as well as in the function of its members ;
3. Notice of any purchase or alienation of premises and real estate ;
4. Notice of any decision by the General Assembly amending the Statutes;
5. Notice of any decision by the General Assembly involving the voluntary dissolution of the Association of which the Ministry will acknowledge receipt.

ARTICLE 11

In conformity with the Law n° 1072 dated June 27, 1984 of Monaco, the Administrators have to publish in Journal de Monaco a notice containing:

1. The name, the object and the address of the headquarters ;
2. All the amendments to the above-mentioned items ;
3. The decision providing for the dissolution of the Association.

Such notice must be published within the month that follows either the publication of the Departmental Authorisation Decree or the decision of the dissolution.

ARTICLE 12

Conformément à la loi n° 1072 du 27 juin 1984, les Administrateurs doivent conserver **les minutes** de toutes les discussions et les votes des organes de l'association ainsi que les récépissés et autorisations administratives (voir article 7.6). **Les minutes conservées au Secrétariat** doivent être présentées à toute requête des Autorités Administratives ou Judiciaires.

V. RESSOURCES ANNUELLES

ARTICLE 13

Les ressources annuelles se composent

13.1 Du revenu de ses biens ;

13.2 Des cotisations, droits et redevances de ses membres et partenaires;

13.3 De ressources créées à titre exceptionnel sous réserve de l'autorisation prévue par la loi de Monaco (loi No 1072 du 27 juin 1984) sur les Associations dans les cas.

VI. MODIFICATION AUX STATUTS

ARTICLE 14

14.1

Toute proposition, ayant pour objet une modification aux statuts ou la dissolution de l'Association doit émaner du Conseil ou d'au moins **la majorité des deux tiers** des Membres Effectifs de l'Association.

14.2

Le Conseil doit porter à la connaissance des membres de l'Association, au moins trois mois à l'avance, la date de l'Assemblée Générale qui statuera sur la dite proposition.

ARTICLE 12

In conformity with law n° 1072 dated June the 27th, 1984, the Administrators have to keep **records** of all the discussions and votes by the bodies of the Association and containing all acknowledgements of receipts and administrative authorisations (see Article 7.6). ***The records as registered at the UIM Secretariat*** must be presented upon request by the Administrative or Legal Authorities.

V. ANNUAL INCOME

ARTICLE 13

The annual income is composed of :

13.1 the income on assets and properties ;

13.2 the fees and dues paid by its members and partners ;

13.3 the exceptional income, subject to authorisation provided for by the law of Monaco (Law No 1072 of June 27th, 1984) on associations.

VI. MODIFICATION OF STATUTES

ARTICLE 14

14.1

Any proposal aiming at amending the Statutes or at the dissolution of the Association must be put forward by the Council or by at least ***two third majority*** of the Full Members of the Association.

14.2

The Council must inform the members of the Association with a previous notice of three months at least, about the date of the General Assembly having to make a decision on the aforesaid proposal.

14.3

Aucune décision n'est acquise si elle n'est votée à la majorité des deux tiers des Membres Effectifs présents. Les modifications aux statuts n'ont d'effet qu'après approbation par arrêté gouvernemental.

VII. DISSOLUTION - LIQUIDATION - DEVOLUTION DU PATRIMOINE

ARTICLE 15

La dissolution volontaire peut intervenir :

- a. Lorsque l'Association est devenue sans objet ;
- b. Lorsqu'une décision en ce sens est prise par l'Assemblée Générale.

ARTICLE 16

L'Assemblée Générale appelée à se prononcer sur la dissolution de l'Association est convoquée spécialement à cet effet. Elle doit comprendre au moins la moitié plus un des membres effectifs en exercice. La dissolution ne peut être votée qu'à la majorité des deux tiers des membres présents.

ARTICLE 17

En cas de dissolution, les biens de l'Association peuvent être liquidés soit par l'Assemblée Générale, soit par des liquidateurs nommés par elle à cet effet. L'actif net doit être réparti parmi les Autorités Nationales au prorata de leur taux de cotisation acquitté.

ARTICLE 18

Tous les cas non prévus aux présents statuts relèvent du Conseil d'Administration chargé d'établir un règlement d'ordre intérieur, approuvé et/ou modifié par l'Assemblée Générale de l'association.

14.3

No decision is carried unless voted by a two-third majority of the Full Members present. The amendments to the Statutes can come into force only after they have been approved by a governmental decree.

VII. DISSOLUTION - WINDING-UP DEVOLUTION OF ASSETS AND LIABILITIES

ARTICLE 15

Voluntary dissolution may take place :

- a. Whenever the Association has become objectless ;
- b. Whenever a decision in this direction has been made by the General Assembly.

ARTICLE 16

The General Assembly convened to decide on the dissolution of the Association is convened specifically in this purpose. It must be attended by at least one half plus one of its current members. The dissolution can be voted only by a two-third majority of its members present.

ARTICLE 17

In case of dissolution, the assets of the Association may be liquidated either by the General Assembly or by liquidators appointed by the G.A. in this purpose. The net assets must be distributed among the National Authorities pro rata the amount of fees they paid.

ARTICLE 18

All issues not dealt with in these Statutes are placed under the authority of the Board of Administrators responsible by the General Assembly of the Association.

TABLE DES MATIERES

R.O.I.

CHAPITRE	PAGE
I NOMENCLATURE ET DEFINITIONS	15
II APPLICATION DES REGLEMENTS	17
III ORGANISATION NATIONALE	18
IV ASSEMBLEE GENERALE	20
V CONSEIL	31
VI SECRETARIAT	41
VII DEFINITIONS DES COMMISSIONS	43
VIII COMMISSIONS	49
IX LES ZONES ET VICE-PRESIDENTS CONTINENTAUX ET REGIONAUX	57
X HONORARIAT ET MEMBRES DIVERS	60
XI DISTINCTIONS HONORIFIQUES	63
XII PROCEDURE DISCIPLINAIRE –ARBITRAGE	66
XIII CONSEIL INTERNATIONAL DE CONSULTANTS	67
XIV DROITS DE TELEVISION	68
XV PUBLICITE VIRTUELLE	68

INDEX

BY - LAWS

CHAPTER	PAGE
I NOMENCLATURE AND DEFINITIONS	15
II IMPLEMENTATION OF THE RULES	17
III NATIONAL ORGANISATION	18
V GENERAL ASSEMBLY	20
V COUNCIL	31
VI SECRETARIAT	41
VII DEFINITION OF COMMISSIONS	43
VIII COMMISSIONS	49
IX ZONES AND REGIONAL AND CONTINENTAL VICE PRESIDENTS	57
X HONORARIAT AND OTHER MEMBERSHIPS	60
XI HONORARY DISTINCTIONS	63
XII DISCIPLINARY PROCEDURE - ARBITRATION	66
XIII INTERNATIONAL BOARD OF CONSULTANTS	67
XIV TELEVISION RIGHTS	68
XV VIRTUAL ADVERTISING	68

I. NOMENCLATURE ET DEFINITIONS

1.1 - LANGUE OFFICIELLE ET LANGUE VÉHICULAIRE

1.1.1

Le Français est uniquement la langue officielle des Statuts et du Règlement d'Ordre Intérieur.

1.1.2

La correspondance officielle entre l'U.I.M. et ses membres se fait en Anglais. Cette langue est utilisée lors des réunions. Si l'interprétation s'avère nécessaire, les frais seront supportés par les intéressés sauf pour l'Assemblée Générale, où l'U.I.M. fournit l'interprétation Français-Anglais et Anglais-Français.

1.1.3

En cas de contestation concernant l'interprétation ou s'il y a divergence entre les deux textes, le texte français fera foi en ce qui concerne les statuts et le règlement d'ordre intérieur et le texte anglais fera foi en ce qui concerne tous les règlements sportifs et techniques.

1.1.4

Les publications de l'U.I.M. peuvent être traduites. Il n'est pas perçu de droit de copyright.

1.2 - DÉFINITIONS ET TERMINOLOGIE

1.2.1

Les définitions et abréviations reprises dans ce chapitre seront utilisées dans le présent Règlement d'Ordre Intérieur et dans toutes les publications de l'U.I.M.

- U.I.M. (Union Internationale Motonautique)

I. NOMENCLATURE AND DEFINITIONS

1.1 - OFFICIAL LANGUAGE AND COMMON LANGUAGE

1.1.1

French is the official language of Statutes and By-laws only.

1.1.2

The official correspondence between the U.I.M. and its members is English. This language is used at meetings. If interpreters are necessary, the interested party will bear the cost except for the General Assembly where the U.I.M. provides French-English and English-French interpretation.

1.1.3

In case of disputes regarding interpretation, or if there is any divergence between the two texts, the French text is to prevail regarding the Statutes and the By-Laws and the English text is to prevail regarding all sportive and technical rules.

1.1.4

Translations of the U.I.M. publications are authorised without copyright.

1.2 - DEFINITIONS AND TERMINOLOGY

1.2.1

The definitions and abbreviations under this heading will be used in these by-laws and in all the U.I.M. publications.

- U.I.M. Union Internationale Motonautique

- A.G. (Assemblée Générale des membres représentant l'autorité suprême de l'U.I.M.)
- A.N. (Autorité Nationale: organisation motonautique nationale affiliée à l'U.I.M.).
- Dirigeants (Le Président de l'U.I.M., le Premier et le deuxième Vice-Président, les Vice-Présidents Régionaux et Continentaux, le Trésorier, les Présidents des Commissions Internationales).
- S.G. (la Secrétaire Général).
- Secrétariat (Le Secrétariat Général)
- Vérificateurs aux comptes
- Conseil
- Conseil International des consultants de l'U.I.M.
- COMINSPO (Commission Sportive Internationale de l'U.I.M.)
- **Comité** du Championnat du Monde Formule 1 (Comité International de l'U.I.M. de F1)
- Comité Formules (**Comité** Formules Internationale de l'U.I.M.)
- COMINTECH (Commission Technique Internationale de l'U.I.M.)
- COMINSAFE (Commission Sécurité/Médicale Internationale de l'U.I.M.)
- COMINOFF (Commission Offshore Internationale de l'U.I.M.)
- W.O.C.C. (Comité du Championnat du Monde Offshore)
- PLAISANCE (Commission Internationale de Navigation de Plaisance de l'U.I.M.)
- COMITE AQUABIKE (Comité International de gestion d'Aquabike de l'U.I.M.)
- COMITE DES CELLULES DE SECURITE (Comité International des Cellules de Sécurité de l'U.I.M.)

- G.A. General Assembly of the Members : representing the supreme authority of the U.I.M.
- N.A. National Authority : a national motorboating organisation affiliated to the U.I.M. and representing the majority of its country's motorboaters.
- Officers U.I.M. President, the first Senior- and Vice-President, the Regional and Continental Vice-Presidents, the Treasurer, the Presidents and International Commissions.
- S.G. Secretary General .
- Secretariat The General Secretariat
- Auditors
- COUNCIL U.I.M. International Board
- BOARD OF CONSULTANTS U.I.M. International Board of Consultants
- COMINSPORT U.I.M. International Sports Commission
- F1 WORLD CHAMPIONSHIP **COMMITTEE** U.I.M. International F1 Committee
- FORMULAE COMMITTEE U.I.M. International Formulae **COMMITTEE**
- COMINTECH U.I.M. International Technical Commission
- COMINSAFE U.I.M. International Safety/Medical Commission
- COMINOFF U.I.M. International Offshore Commission
- W.O.C.C. World Offshore Championship Committee
- PLEASURE U.I.M. International Pleasure Navigation Commission
- AQUABIKE COMMITTEE U.I.M. Aquabike International Management Committee
- SAFETY COCKPIT COMMITTEE U.I.M. International Safety Cockpit Committee

1.3 - DRAPEAUX ET INSIGNES DE L'U.I.M.

1.3.1

Le drapeau de l'U.I.M. est rectangulaire. Le battant a 1 1/2 fois la dimension de la hampe.

Sur fond blanc, il y a une croix rouge verticale mesurant 1/6 de la hampe. Au centre, dans un cercle bleu mesurant 1/3 de la hampe et sur fond blanc, il y a une hélice à trois pales, en or.

1.3.2

Le drapeau de l'U.I.M. doit être arboré à toutes les épreuves internationales.

II. APPLICATION DES REGLEMENTS

2.1 - APPLICATION ET MODIFICATIONS

2.1.1

Les règlements généraux, ainsi que le Règlement d'Ordre Intérieur de l'U.I.M. s'appliquent à toutes les Autorités Nationales affiliées, excepté lorsque les dits règlements sont en conflit avec une loi ou une réglementation d'une région géographique donnée.

2.1.2

Tout projet de modification des règlements de l'U.I.M. doit être présenté au Conseil qui le transmettra à l'Assemblée Générale après avoir consulté les Commissions intéressées.

Les propositions doivent être soumises par écrit par une Autorité Nationale, soutenues par un écrit de deux autres Autorités Nationales signé par les Présidents des Autorités Nationales en question ou leurs délégués désignés.

Une Commission U.I.M. a le droit de faire des propositions à l'Assemblée Générale via le Conseil.

1.3 - FLAG AND BADGES OF THE U.I.M.

1.3.1

The flag of the U.I.M. is rectangular with a fly 1 1/2 times its hoist. It has a white background and a vertical Red Cross measuring one sixth of the hoist.

In the centre, in a blue circle with a diameter of one third of the hoist and on white background a three bladed golden propeller.

1.3.2

The flag is to be hoisted at all international meetings.

II. IMPLEMENTATION OF THE RULES

2.1 - IMPLEMENTATION AND CHANGES

2.1.1

The rules and by-laws of the U.I.M. apply to all affiliated N.A.'s except when they are in conflict with a law or regulation of a given geographic area.

2.1.2

Every draft modification of the U.I.M. rules must be presented to the Council who will transmit it to the General Assembly after consultation with the Commissions concerned.

The proposals must be submitted in writing by one National Authority and must be supported in writing by two other National Authorities signed by the Presidents of the supporting N.A. or their designated delegates.

A U.I.M. Commission is entitled to put forward proposals to the General Assembly through the Council.

Les propositions devront être présentées sur le document officiel de l'U.I.M.

Toute règle spécifique a priorité sur une règle générale traitant du même sujet.

III. ORGANISATION NATIONALE

3.1 - AUTORITÉS NATIONALES

3.1.1

Tous les pays du monde peuvent être représentés à l'U.I.M. par une seule Autorité Nationale.

Si l'Autorité Nationale d'un pays ne désire pas régir un secteur sportif particulier de l'U.I.M., une autre organisation du même pays peut alors devenir membre de l'U.I.M. pour le dit secteur sportif exclusivement. Les droits de vote de cette organisation et de ses membres sont alors limités au seul secteur sportif qu'ils représentent.

L'U.I.M. publie chaque année une liste des secteurs sportifs particuliers auxquels s'applique cette exception. Ces secteurs seront déterminés par le Conseil et approuvés par l'Assemblée Générale. L'exception est applicable immédiatement après approbation du secteur sportif particulier par l'Assemblée Générale.

3.1.2

L'Autorité Nationale, au titre de membre effectif de l'U.I.M., est une fédération, union ou association qui peut faire la preuve de sa représentativité de la majorité des clubs motonautiques de son pays, sauf exception acceptée par le Conseil et ratifiée par l'Assemblée Générale.

3.1.2.1

Dans les pays où il n'y a pas de fédération nationale, l'U.I.M. peut reconnaître avec le titre de membre correspondant, un club important

The proposals must be presented on the official U.I.M. form.

Any specific rule has priority over a general rule dealing with a similar matter.

III. NATIONAL ORGANISATION

3.1 - NATIONAL AUTHORITIES

3.1.1

Every country in the world may be represented at the U.I.M. by only one N.A.

If a N.A. in a country does not want to govern an approved special sports sector of the U.I.M., then a different organisation in that country can become a member of the U.I.M. for that special sports sector only. The voting rights of that organisation and its members are limited solely to the special sports sector which they represent.

The U.I.M. will annually publish a list of the special sports sectors to which this exception may be applied. The sectors will be selected by the Council and approved by the General Assembly. The exception may be applied immediately following approval of the special sports sector by the General Assembly.

3.1.2

The N.A. as a full member of the U.I.M. is a federation, union or association having proofed that it represents the majority of the

motorboating clubs in its country, unless an exception is accepted by the Council and ratified by the G.A.

3.1.2.1

In a country where there is no national federation, the U.I.M. can recognise a large club in the country as a Corresponding Member. Such a member does not have the right to vote either at the G.A. or in a mail

de ce pays. Ce membre n'a pas le droit de vote, ni en Assemblée Générale, ni en cas de vote par correspondance (les règlements pour les membres correspondants figurent à l'article 10.2).

3.1.3

Pour être membre effectif de l'U.I.M., l'Autorité Nationale doit:

- satisfaire à l'article 4.1 des Statuts de l'U.I.M.
- déclarer par écrit qu'elle accepte les Statuts, le Règlement d'Ordre Intérieur et les règlements sportifs de l'U.I.M
- s'engager à faire reconnaître dans son pays tout document officiel de l'U.I.M., sauf opposition légale dans le pays considéré.

3.1.4

La demande d'affiliation prévue à l'article 4.1 des Statuts de l'U.I.M. doit être accompagnée du questionnaire officiel de l'U.I.M., dûment complété et signé par le Président de l'association postulante.

3.1.4.1

Toute demande d'affiliation est examinée par le Conseil qui la présentera à l'Assemblée Générale suivante, conformément à l'article 4.1 des Statuts.

3.1.5 L'Autorité Nationale a pour devoir de:

3.1.5.1

Appliquer et faire appliquer les règlements de l'U.I.M. par ses clubs affiliés.

3.1.5.2

Les A.N. peuvent communiquer chaque année à l'U.I.M. la liste de ses clubs affiliés avec leur adresse.

3.1.5.3

Créer des Commissions Nationales chargées de l'assister dans la direction et la surveillance du motonautisme dans son pays.

ballot.(For reference the rules regarding Corresponding Members are stated in the articles of section 10.2)

3.1.3

To be a full member of the U.I.M., the N.A. must:

- comply with article 4.1 of the U.I.M. statutes
- state in writing that it accepts the U.I.M. statutes, by-laws and applicable sports rules.
- commit itself to have all the U.I.M. official documents recognised in its country, unless there is a legal opposition in that country.

3.1.4

The application for membership provided for in article 4.1 of the U.I.M. statutes must be accompanied by the U.I.M. official questionnaire, duly completed and signed by the President of the applying association.

3.1.4.1

Every application for membership is examined by the Council who will present it to the next G.A. in conformity with article 4.1 of the statutes

3.1.5 The National Authority must :

3.1.5.1

Implement the U.I.M. rules and have them implemented by its affiliated clubs.

3.1.5.2

May send each year to the U.I.M. the list of its affiliated clubs and their addresses.

3.1.5.3

Set up national commissions who will help to run and oversee the motorboating in its country.

3.1.5.3.1

Nommer les officiels nécessaires pour le bon fonctionnement de ses Commissions Nationales et l'application des règlements de l'U.I.M., ainsi que des règlements nationaux.

3.1.5.3.2

Communiquer chaque année à l'U.I.M. la composition de ses Commissions Nationales.

3.1.5.4

Présenter chaque année à l'U.I.M. un rapport détaillé de ses activités.

3.1.5.5

Juger tous les différends sportifs qui surgissent sur son territoire selon ses propres régimes législatifs.

3.1.5.6

Veiller dans son pays à ce que les organisateurs de toutes les manifestations motonautiques possèdent effectivement l'aptitude indispensable au bon déroulement de ces manifestations.

3.1.5.7

Toute Autorité Nationale qui, pour une question sportive, intenterait une action judiciaire contre l'U.I.M., sera automatiquement, immédiatement et définitivement suspendue avec déchéance de tous ses droits et prérogatives comme membre de l'U.I.M.

3.1.5.8

Tout club ou personne, soumis(e) à l'autorité d'une Autorité Nationale qui, pour une question sportive, intenterait une action judiciaire contre l'U.I.M., doit être suspendu(e) par son Autorité Nationale.

3.1.5.3.1

Appoint the officials necessary to ensure the smooth running of the national commissions and the enforcement of the U.I.M. and the national rules.

3.1.5.3.2

Send each year to the U.I.M. the composition of its national commissions.

3.1.5.4

Present each year to the U.I.M. a detailed report of its activities.

3.1.5.5

Judge all sports appeals and disputes arising in its country according to its legal regulations.

3.1.5.6

See to it that in its country the organisers of all the motorboating events have sufficient ability to ensure the smooth running of these events.

3.1.5.7

Any National Authority who, with reference to a question of sport, should bring a legal action against the U.I.M., will automatically, immediately and definitively be excluded as a member of the U.I.M. with forfeiture of all rights and privileges.

3.1.5.8

Any club or person affiliated to a National Authority, who would bring any legal action with reference to a question of sport against the U.I.M., will be suspended by its National Authority.

IV. ASSEMBLEE GENERALE

4.1 - COMPOSITION ET POUVOIR

4.1.1

L'Assemblée Générale de l'U.I.M. est constituée suivant l'article 7 des Statuts ; elle possède tous les pouvoirs prévus par les Statuts. Elle doit notamment :

- ratifier le Procès-Verbal de l'Assemblée Générale précédente
- approuver ou rejeter le rapport du/de la Secrétaire Général ;
- approuver ou rejeter le rapport du Trésorier qui contiendra :
 - a) le bilan de l'exercice écoulé
 - b) le projet de budget pour l'exercice suivant
 - c) les montants des droits et redevances pour l'année suivante
- procéder à la nomination de vérificateurs aux comptes ***des expert-comptables officiels provenant d'un cabinet comptable reconnu ;***
- approuver ou rejeter le rapport des vérificateurs aux comptes ;
- ratifier toutes les décisions prises par le Conseil pendant l'exercice écoulé ;
- procéder aux élections statutaires et voter sur les révocations éventuelles;
- décider des admissions, radiations, suspensions et exclusions éventuelles des membres ;
- fixer la date et le lieu de la prochaine Assemblée Générale; décider des modifications aux Statuts ;
- décider des modifications aux Règlements d'Ordre Intérieur ;
- décider des modifications de tout règlement U.I.M. dont l'approbation est donnée par le Conseil U.I.M. et ratifiée ou rejetée par l'Assemblée

IV. GENERAL ASSEMBLY

4.1 - COMPOSITION AND POWER

4.1.1

The U.I.M. General Assembly is set up according to article 7 of the statutes ; it has all the powers provided for in the statutes; among others, it must :

- determine the general policy of the U.I.M. ;
- ratify the minutes of the previous G.A. ;
- approve or reject the Secretary General's report ;
- approve or reject the Treasurer's report which will include :
 - a. the balance sheet of the previous year
 - b. the draft budget for the following year
 - c. the proposed dues and fees for the following year
- ***appoint the official auditors from a recognised accountant office***
- approve or reject the auditors reports ;
- ratify all the Council's previous year's decisions ;
- conduct the statutory elections and vote on possible dismissals ;
- deal with the possible admissions, dismissals, suspensions and exclusion of members ;
- fix the date and place of the next G.A.;
- decide on the modifications of the statutes ;
- decide on the modifications of the by-laws ;
- decide on modifications of any U.I.M. regulation except the WOCC, F1, Formulae and Aquabike Championship regulations where

Générale, à l'exception des règlements **des cellules de sécurité**, des championnats WOCC, F1, Formules et Aquabike

- décider, si nécessaire, de la dissolution de l'Union ;
- ratifier les rapports des Commissions ;
- ratifier les rapports des Vice-Présidents Continentaux et Régionaux ;
- ratifier ou rejeter les autres rapports éventuels ;
- décider des admissions à l'Honorariat.

4.1.2

Les Membres Correspondants peuvent assister à l'Assemblée Générale et aux Commissions et ils ont droit d'intervention, mais pas de droit de vote.

4.2 - VOTES ET PROCURATIONS

L'Assemblée Générale se réunit de plein droit chaque année sous la Présidence du Président de l'U.I.M.

L'Assemblée Générale peut, en outre, être convoquée à la demande du Président ou par 30 % du nombre total d'Autorités Nationales affiliées avec droit de vote (voir article 7.2). Les Autorités Nationales ayant fait la demande de convocation doivent être présentes à l'Assemblée Générale Extraordinaire, sinon l'Assemblée Générale n'est pas valable.

4.2.1

Tout membre effectif (Autorité Nationale) présent en Assemblée Générale a droit à une voix.

4.2.2

L'Autorité Nationale envoie au siège de l'U.I.M. avant l'Assemblée Générale le nom de son mandataire.

approval is decided by the U.I.M. Council and ratified or rejected by General Assembly ;

- decide, if necessary, on the dissolution of the Union ;
- ratify the reports of the Commissions ;
- ratify the reports of the Continental and Regional Vice Presidents ;
- ratify or reject any other reports ;.
- deal with the requests for honorary awards.

4.1.2

The Corresponding Members may attend the General Assembly and the Commissions and they have the right to comment but not to vote.

4.2 - VOTES AND PROXIES

The General Assembly meets each year under the chairmanship of the U.I.M. President.

Furthermore, the General Assembly can be convened at the request of the President or by 30 % of the total number of affiliated National Authorities with right of vote. (ref. statute 7.2). Requesting National Authorities must be present at the Extraordinary General Assembly, otherwise the General Assembly is not valid.

4.2.1

Every full member (N.A.) present in the G.A. is entitled to have one vote.

4.2.2

The N.A. should send to the U.I.M. head office before the G.A., the name of its representative.

4.2.3

Ne peuvent prendre part au vote de l'Assemblée Générale que les Autorités Nationales en règle de cotisation annuelle et de droits de calendrier pour les années précédant l'Assemblée Générale, sauf en cas de nouvelle admission.

4.2.4

Toute décision à l'ordre du jour est prise à la majorité simple des membres effectifs présents au moment du vote, sauf dispositions spéciales.

4.2.4.1

La majorité simple et la majorité des deux tiers sont déterminées en tenant compte du nombre total des voix valablement exprimées. Les bulletins nuls et les abstentions ne sont pas pris en considération. On entend par majorité simple cinquante pour cent des votes valables plus un (ref. 4.3.10). La majorité des deux tiers représente 67% (soixante-sept pour cent) des votes valables.

4.2.4.2

Pour statuer sur tout point non-porté à l'ordre du jour, il faut une résolution prise à la majorité des deux tiers des membres effectifs présents.

Les amendements aux propositions de l'ordre du jour présentés par écrit, ne peuvent être que des modifications mineures concernant la forme et se rattachant directement au sujet traité. Aucun amendement portant sur le fonds ne pourra être admis.

4.2.4.3

Le Règlement d'Ordre Intérieur ou ses modifications sont approuvés à la majorité des deux tiers des membres effectifs présents (voir article 7.5). Les règlements généraux, ainsi que leurs modifications, sont ratifiés à la majorité simple.

4.2.5

Le vote se fait par scrutin secret:

- pour toute élection

4.2.3

Only the N.A. who have paid all their membership and calendar fees for all the years preceding the year of the General Assembly can take part in the voting except in the case of a new admission.

4.2.4

Every decision on the agenda is adopted by the simple majority of the full members present at the time of the vote, except special provisions.

4.2.4.1

Simple majorities and two third majority are determined taking into account the total number of valid “yes” and “no” votes. Abstentions and null-and-void ballot are not taken into consideration.

Simple majority means fifty percent of the valid votes plus one (ref. 4.3.10). Two thirds majority means 67 % of the valid votes.

4.2.4.2

In order to decide upon an item which is not on the agenda, a two-thirds majority of the full members present is necessary.

The amendments to the proposals on the agenda put forward in writing can only be minor modifications regarding the presentation and dealing directly with the item concerned. No amendment as to the content will be admitted.

4.2.4.3

The by-laws or their modifications will be approved by two-thirds majorities of the full members present. (Ref. Statute 7.5). The general rules, as well as their modifications will be ratified by the simple majorities.

4.2.5

Voting by secret ballot will be used for:

- any elections

- pour l'exclusion de membres
- à la demande d'au moins cinq membres effectifs.

4.2.6

Dans les cas non prévus à l'article 4.2.7, le vote se fait à main levée.

4.2.7

Les résolutions prises par l'Assemblée Générale sont d'application à partir du 60ème jour après l'Assemblée Générale ou comme décidé par l'Assemblée Générale.

4.2.8

Les sujets soumis au vote à l'Assemblée Générale ne peuvent être rediscutés pendant une période de 3 ans, sauf indication contraire par l'Assemblée Générale ou le Conseil.

4.3 - ELECTIONS

4.3.1

Chaque Autorité Nationale ne peut présenter qu'un seul candidat à l'élection, par comité ou commission, sauf dans les cas prévus dans les Statuts et le présent Règlement d'Ordre Intérieur (voir articles 5.1.2, 5.7.2.1).

Un délégué ne peut occuper qu'une seule fonction dans un maximum de deux Commissions ou Comités dont les membres sont élus.

4.3.1.1

Les Autorités Nationales ne peuvent proposer que des candidats soumis à leur juridiction et âgés de moins de 65 ans en cas de nouvelle candidature. Pour la ré-élection immédiate il n'existe pas une limite d'âge. Un candidat présenté par une Autorité Nationale, élu dans une commission ou comité pour un mandat déterminé, peut rester en fonction pendant la durée de son mandat, sauf dans les cas suivants :

- Sauf opposition de leur propre Autorité Nationale, les membres des Commissions qui, en cours de mandat, perdent leur poste au sein de

- exclusion of members
- at the request of at least 5 full members.

4.2.6

In the cases that are not provided for in article 4.2.7, the vote is taken by a show of hands.

4.2.7

The resolutions taken by the General Assembly will be implemented on the 60th day from the General Assembly or as otherwise agreed by the General Assembly.

4.2.8

The matters submitted to the vote of the G.A. cannot be discussed again during a three-year period, unless otherwise stated by the G.A. or the Council.

4.3 - ELECTIONS

4.3.1

Each N.A. can present only one candidate to the election, per committee, or commission with the exceptions provided for in the statutes and the present by-laws(For Ref. By-law 5.1.2, 5.7.2.1)

One delegate can only have one function in maximum 2 Commissions or Committees where the members are elected.

4.3.1.1

The N.A. can only propose candidates within its jurisdiction and being less than 65 years of age in case of a new candidature. There is no age limit for immediate re-elections. A candidate presented by a National Authority, elected in a commission or a committee for a given mandate, can remain during the duration of his mandate except in following cases :

- Unless their National Authority voices opposition, the Members of the Commissions who, during their term of office lose their position, in their

leur Autorité Nationale continuent de siéger dans le Commissions jusqu'à l'élection statutaire suivante.

- Dans le cas contraire, si leur propre Autorité Nationale exige leur départ immédiat des Commissions, ils sont tenus de démissionner immédiatement. En ce cas, leur siège reste vacant jusqu'à l'élection statutaire suivante.

4.3.1.2

La candidature accompagnée d'un curriculum vitae de la personne proposée doit être envoyée au secrétariat.

4.3.2

La liste des candidatures sera clôturée 30 jours ***avant la date de la réunion*** de l'Assemblée Générale. La liste définitive des candidats sera communiquée aux Autorités Nationales.

4.3.3

Toutes les Autorités Nationales seront informées le plus rapidement possible d'une vacance quelconque survenue dans la direction de l'U.I.M. et parmi les officiels élus, par suite de décès, de démission, ou de révocation.

4.3.4

Le Conseil devra pourvoir à toute vacance survenue en cours d'exercice pour la durée du mandat du prédécesseur. Cette désignation doit être soumise pour ratification à l'Assemblée Générale suivante.

4.3.4.1

Tout membre élu d'un comité ou commission qui sera absent sans raison valable lors de l'élection ou à la première réunion du groupe dont il est membre, perdra son droit et le poste restera vide jusqu'à la prochaine A.G.

4.3.5

Le vote est obligatoire, qu'il y ait un ou plusieurs candidats.

own National Authority, shall continue to sit on the Commission until the next statutory election.

- If on the contrary, their own National Authority requests their immediate departure from the Commissions, they shall be obliged to resign immediately. In this case, their seat shall remain vacant until next statutory election.

4.3.1.2

The candidature and the curriculum vitae of the person proposed must be sent to the secretariat.

4.3.2

The list of candidatures will be closed 30 days ***prior to the date of the General Assembly meeting itself***. The final list of the candidates will be sent to the National Authorities.

4.3.3

All N.A.'s will be informed as soon as possible of any vacancy among the U.I.M. officers and elected officials , following death or resignation or dismissal.

4.3.4

The Council will have to deal with any vacancy arising during the year for the duration of the predecessor's mandate. This designation must be submitted to the next G.A. for ratification.

4.3.4.1

Any member elected in a committee, or commission who will be absent without a valid reason during the election or during the first meeting of the group of which he is a member will lose his rights and his function will be vacant until the next G.A.

4.3.5

Voting is mandatory, whether for one or several candidates

4.3.6

Le candidat ayant obtenu la majorité simple est élu, sauf disposition spéciale (voir article 7.2.1.1).

4.3.7

Si, au premier tour, aucun candidat n'est élu à la majorité simple, il sera procédé à un deuxième tour de scrutin. Si aucune décision ne ressort de ce deuxième tour, il sera procédé à un troisième tour au terme duquel la personne qui aura reçu le nombre de voix le plus élevé sera élue.

4.3.8

Les bulletins de vote destinés aux élections seront imprimés séparément sur du papier de couleur différente, pour :

- le Président ;
- les Administrateurs ;
- les Présidents et membres des Commissions à élire par l'Assemblée Générale ;
- Les coordinateurs et les membres du Comité Formule 1, Comité Formules, **Comité des cellules de sécurité** et Comité Aquabike seront nommés par le Conseil et la composition sera détaillée dans le Règlement d'Ordre Intérieur.

4.3.8.1

Les bulletins de vote porteront :

- le nom et la nationalité des candidats ;
- un petit carré noir avec un disque blanc au centre ;
- la date de l'Assemblée Générale ;
- trois cases destinées au cachet de l'U.I.M. et aux paraphes des scrutateurs ;
- le système de vote sera le remplissage du petit disque blanc laissé au milieu d'un carré noir de 1 cm de côté.

4.3.6

The candidate having obtained a simple majority is elected, unless otherwise stated

4.3.7

If no candidate is elected by a simple majority in the first ballot, a second ballot is held. If no decision is made in the second ballot, a third one is held with the person receiving the greatest number of votes being declared the winner.

4.3.8

The ballot papers for the elections will be printed separately on papers having different colours for :

- the President ;
- the Administrators ;
- the Presidents and members of the Commissions to be elected by the General Assembly ;
- the Chairmen and the members of the F1 Committee, Formulae Committee, **Safety Cockpit Committee** and Aquabike Committee will be appointed by the Council and the composition will be listed in the By-Laws.

4.3.8.1

The ballot papers will include :

- the name and the nationality of the candidates ;
- a small black square with a blank circle in the middle ;
- the date of the General Assembly ;
- three spaces for the U.I.M. stamp and the scrutinisers' initials
- the ballot system chosen will be the following : following-in the small blank circle in the middle of a black square of 1 cm x 1 cm.

4.3.8.2

Tous les textes explicatifs des bulletins de vote seront en anglais.

4.3.9

Pour assurer le bon déroulement des opérations de vote, l'Assemblée Générale désigne un Comité de Vote composé d'un Président et de deux scrutateurs de pays différents.

4.3.9.1

A l'appel de son nom, chaque votant reçoit des mains du Président du Comité de Vote les différents bulletins nécessaires et signe pour réception la liste établie par Le Secrétaire Général.

4.3.9.2

Le Comité de Vote recueille les bulletins par appel nominal.

4.3.10

Le Comité de Vote procède au dépouillement comme suit :

- il retire les bulletins blancs (non complétés) ;
- il retire les bulletins jugés nuls par les scrutateurs ;
- il détermine le nombre de voix requises pour obtenir la majorité simple (la moitié des votes jugés valables plus une voix) en cas d'ex-aequo.

4.3.11

En cas d'égalité entre deux candidats, on appliquera les règles indiquées à l'article 4.3.7.

4.3.12

Toutes les pièces ayant servi aux élections sont mises sous scellé et conservées par Le Secrétaire Général jusqu'à la ratification du procès-verbal de l'Assemblée Générale.

4.3.8.2

All explanatory texts on the ballot papers will be in English.

4.3.9

To ensure the smooth running of the voting, the General Assembly will appoint a Polling Committee composed of a President and two scrutinisers from different countries.

4.3.9.1

When being called, each voter receives from the President of the Polling Committee the various ballot papers and signs the list drawn up by the Secretary General .

4.3.9.2

The Polling Committee gathers the ballot papers by calling out names.

4.3.10

The Polling Committee counts the votes as follows :

- removing the blank ballot papers (not completed) ;
- removing the ballot papers considered as null and void by the scrutinisers ;
- determining the winning vote count for the simple majority (half the valid votes plus one vote, in case of an even number of valid votes).

4.3.11

In the event of equality of votes for two candidates, the rules in article 4.3.7 will apply.

4.3.12

The Secretary General keeps all the papers used in the elections, sealed until ratification of the minutes of the General Assembly.

4.4 - VOTE PAR CORRESPONDANCE

4.4.1

Le vote par correspondance est prévu à la demande du Conseil pour tous les cas urgents à l'exclusion des modifications aux Statuts et au Règlement d'Ordre Intérieur.

Le Secrétaire Général, sur instructions du Conseil, rédige le texte du vote par correspondance et l'envoie par lettre recommandée aux Autorités Nationales.

4.4.2

Toutes les Autorités Nationales en règle de cotisation ont droit à une voix.

4.4.3

Le résultat de chaque proposition présentée au vote par correspondance est déterminé par le plus grand nombre de voix obtenues.

4.4.4

Les résolutions prises par vote par correspondance des membres effectifs ont le même statut juridique que les résolutions prises en Assemblée Générale.

4.4.5

Les résolutions prises par vote par correspondance sont effectives immédiatement après le dépouillement du scrutin et dès leur notification aux Autorités Nationales.

4.4.6

Le bulletin de vote par correspondance doit porter :

- la date d'envoi
- la date limite pour la réponse
- les questions
- les cases "oui" et "non" pour la réponse

4.4 - MAIL BALLOT

4.4.1

Voting by mail ballot is provided for at the request of the Council for any urgent case other than the modifications of the statutes and of the by-laws.

The Secretary General , at the request of the Council, draws up the text of the mail ballot and sends it by registered letter to the National Authorities.

4.4.2

All the National Authorities having paid their dues are entitled to have one vote.

4.4.3

The outcome of each proposal in the mail ballot is determined by the choice receiving the greatest number of votes.

4.4.4

The resolutions decided by mail ballot of full members have the same legal standings as those balloted in G.A. meetings.

4.4.5

The resolutions taken by mail ballot are effective immediately after the count and upon notification to the N.A.'s.

4.4.6

The mail ballot must include :

- the sending date
- the deadline for the answer
- the questions
- the spaces "yes" and "no" for the answer

La réponse doit être signée par le Président de l'Autorité Nationale ou son représentant légal.

4.4.7

La réponse peut être précédée d'un telefax ou E Mail pour respecter la date limite. Une lettre de confirmation devra parvenir dans les trente jours.

4.5 - PROCÉDURE ET DÉCISIONS

4.5.1

L'Assemblée Générale est présidée par le Président de l'U.I.M. en collaboration avec les membres du Conseil.

4.5.2

Le Secrétaire de l'Assemblée Générale est Le Secrétaire Général de l'U.I.M. ou son remplaçant nommé par le Conseil. Sa présence est obligatoire pendant toute la durée de l'Assemblée Générale.

4.5.3

L'Assemblée Générale annuelle doit avoir lieu sauf cas de force majeure aux dates fixées par l'Assemblée Générale précédente.

La confirmation du lieu et de la date doit être envoyée par le Secrétariat trois mois à l'avance, à toutes les Autorités Nationales, Commissions Internationales et aux personnes qui ont le droit d'y participer.

4.5.4

Le Secrétaire Général doit envoyer deux mois avant la date de l'Assemblée Générale, une convocation, l'ordre du jour de l'Assemblée Générale, à toutes les Autorités Nationales, aux membres du Conseil et aux membres des Commissions.

Les rapports du/de la Secrétaire Général et du Trésorier, comprenant les décisions à ratifier, les projets, ainsi que le bilan et le budget, doivent être envoyés à toutes les Autorités Nationales au moins un mois avant l'Assemblée Générale.

The answer must be signed by the President of the N.A. or his legal representative.

4.4.7

The answer may be preceded by *an e-mail* or a telefax to comply with the deadline. A letter of confirmation must be received within 30 days.

4.5 - PROCEDURE AND DECISIONS

4.5.1

The G.A. is conducted by the U.I.M. President in collaboration with the members of the Council.

4.5.2

The Secretary of the G.A. is the U.I.M. Secretary General or his substitute appointed by the Council.

His presence is mandatory during the whole duration of the General Assembly.

4.5.3

The annual General Assembly must be held on the dates determined by the previous General Assembly, except in case of force majeure.

The confirmation of the venue and date should be sent by the Secretariat three months in advance to all the National Authorities, International Commissions and everyone who has the right to participate.

4.5.4

The Secretary General must send two months in advance of the meeting date of the General Assembly, an invitation to this meeting and the meeting agenda, to all the National Authorities, the Council members and the members of the Commissions.

The reports of the S.G. and the Treasurer, including the decisions to be made, the projects as well as the balance sheet and the budget must be sent to all the National Authorities at least one month before the General Assembly.

4.5.5

Toutes les propositions destinées à être mises à l'ordre du jour de l'Assemblée Générale, des Commissions doivent parvenir par écrit au Secrétariat 60 jours avant l'Assemblée Générale. Dans les cas spéciaux, ce délai peut être raccourci. Ces propositions de changement des règlements doivent être examinées par les Commissions. Elles doivent être présentées en plénière pour explication ou clarification. Le Conseil doit approuver ces propositions. Elles seront distribuées à l'Assemblée Générale par écrit, en temps utile, avant le vote par Le Secrétaire Général.

4.5.5.1

Le Secrétaire Général effectuera le tri des propositions et les transmettra aux différents intéressés 30 jours avant l'Assemblée Générale. Il est indiqué de transmettre à chaque Président de Commission une copie complète des propositions. Exception pour les propositions décrites à l'article

4.5.5.2

Le Conseil ou le Comité Exécutif se réunira avant l'Assemblée Générale pour examiner l'ordre du jour proposé par Le Secrétaire Général et décider des problèmes qui sont de sa compétence parmi les propositions.

4.5.5.3

Une réunion plénière de chaque Commission devra être organisée au moins une fois par an. Si elle ne se réunit qu'une fois par an, elle est tenue d'être plénière. Si elle se réunit deux fois, il convient de stipuler laquelle des réunions est plénière et laquelle est à huis clos.

4.5.5.4

Le Conseil se réunira la veille de l'Assemblée Générale pour prendre toutes les dispositions finales.

4.5.5

All the proposals to be put on the agenda of the G.A., and Commissions should be sent to the Secretariat 60 days before the G.A., in writing. Under special circumstances proposals can be put on the agenda on shorter notice. Such proposals dealing with rule changes must be discussed at the meetings of each commission. It must be presented at the plenary meetings for explanation or clarification. The Council must approve any such proposals. These proposals will be distributed at the General Assembly in writing in good time, before the voting, by the Secretary General .

4.5.5.1

The Secretary General will sort out the proposals and pass them on to the different parties interested 30 days before the G.A. He must send a complete copy of the proposals to the Presidents of the Commissions. Exceptions are made for proposals described in 4.5.5.

4.5.5.2

The Council or the Executive Committee will meet before the G.A. to examine the agenda proposed by the Secretary General and deal with the problems of its competence among the proposals.

4.5.5.3

A plenary meeting of each Commission should be organised at least once a year. If there is only one meeting per year, this one must be plenary. If there are two meetings, it must be mentioned which one is closed and which one is open.

4.5.5.4

The Council will meet the day before the G.A. to make the final arrangements.

4.5.6

Pour tous les documents qui ont une date limite, seule compte la date de réception au secrétariat.

4.5.7

Le procès-verbal de l'Assemblée Générale doit être envoyé par Le Secrétaire Général dans les 30 jours après l'Assemblée Générale et aux personnes qui ont le droit de le recevoir. Le Secrétaire Général doit recevoir dans les 60 jours suivants les observations éventuelles qui seront transmises au Conseil pour examen et décision.

4.5.7.1

Les propositions des Commissions Internationales sont examinées par l'organisme ayant demandé l'avis des Commissions.

V. CONSEIL

5.1 - COMPOSITION ET POUVOIRS

5.1.1

Le Conseil est l'organisme qui gère l'U.I.M.

5.1.2

Les administrateurs élus par l'Assemblée Générale forment le Conseil. Le Conseil est composé d'un Président et de cinq membres minimum dont un au moins doit résider dans le pays où se trouve le siège social. Aucune Autorité Nationale ne peut détenir plus d'un mandat d'administrateur sauf exception prévue dans le présent Règlement d'Ordre Intérieur ou adoptée par l'Assemblée Générale statuant à la majorité de 2/3. Le nombre maximum des membres élu par l'A.G. est limité à 17.

5.1.3

Le Conseil élit en son sein un premier et un deuxième Vice-Président ainsi qu'autant de Vice-Présidents Régionaux et Continentaux qu'il est

4.5.6

For all the documents having a deadline, the date of receipt at the Secretariat will be taken into account.

4.5.7

The Secretary General should send the minutes of the G.A. to all the N.A.'s and persons having the right to receive them, within 30 days of the G.A. The Secretary General should receive within the 60 following days the observations which will be passed on to the Council for examination and decision.

4.5.7.1

The proposals of the International Commissions are examined by the body, which asked for the opinion of the Commissions.

V. COUNCIL**5.1 - COMPOSITION AND POWERS****5.1.1**

The Council is the organisation, which runs the U.I.M.

5.1.2

The administrators elected by the G.A. compose the Council. The Council is composed of a President and a minimum of 5 members, of which one at least must reside in the country of the Registered office ; no N.A. can have more than one mandate of administrator unless exception provided for in these by-laws or adopted by the G.A. voting by a two-thirds majority. The maximum number of members elected by the G.A. is limited to 17.

5.1.3

The Council elects among its members a Senior Vice President and a Vice-President and as many regional and continental Vice Presidents

nécessaire pour la bonne marche de l'Association. Ils ne sont élus que par les Administrateurs élus.

Le Comité Exécutif proposera au Conseil un candidat au poste de Trésorier. Le Trésorier est nommé par le Conseil et n'est pas tenu d'être un administrateur élu. Au cas où il ne serait pas un administrateur élu, il n'aurait aucun droit de vote au Conseil.

Le Comité Exécutif est composé du Président de l'U.I.M., du Premier et du deuxième Vice-Président , **du Consultant Juridique** et du Trésorier Ce Comité s'occupe de la gestion de l'U.I.M. sous le contrôle absolu du Conseil. Les réunions de ce Comité sont ouvertes à tous les administrateurs.

Si des réunions du Comité Exécutif sont requises hors de la semaine de l'Assemblée Générale, les membres du Comité Exécutif seront remboursés sur la base d'un ticket Apex et des frais d'hôtel.

5.1.3.1

Il nomme Le Secrétaire Général qui est tenu(e) d'assister et de participer aux réunions du Conseil, mais qui n'a pas de droit de vote.

5.1.4

Les Présidents Commissions Internationales participent aux réunions du Conseil avec droit de vote. Personne ne peut être Président de plus d'une Commission.

5.1.5

Tous les Administrateurs sont élus pour quatre ans. Ils sont rééligibles. Les mandats d'un quart d'entre eux seront renouvelés chaque année. Si un Administrateur n'est plus disponible, il y aura l'élection d'un nouvel Administrateur pour la période restante.

5.1.6

Tout officiel de l'U.I.M., qu'il soit membre du Conseil ou non, ne peut avoir qu'une seule fonction de direction au sein de l'U.I.M., sauf dérogation accordée par le Conseil et ratifiée par l'Assemblée Générale.

as necessary to ensure the smooth running of the association. These are elected by the elected administrators only. The Executive Committee will propose a candidate to be the Treasurer to the Council. The Treasurer is appointed by the Council and must not necessarily be an elected administrator. In case the appointed Treasurer is not an elected administrator, he will have not right of vote at the Council.

The Executive Committee is composed of the U.I.M. President, the Senior Vice-President, the Vice President, ***the Legal Consultant*** and the Treasurer. This Committee will deal with the U.I.M. administration under the absolute control of the Council. The meetings of this Committee will be open to all administrators.

If Executive Committee meetings are required outside the General Assembly week meetings, the Executive Committee Members will be reimbursed for the Apex Airfare and the hotel accommodation.

5.1.3.1

It appoints a Secretary General who must attend and participate in the meetings of the Council but does not have the right to vote.

5.1.4

The Presidents of the International Commissions attend the Council meetings and have the right to vote. Nobody can be President of more than one Commission.

5.1.5

All administrators are elected for 4 years. They are re-eligible. The mandates of 25 % will be renewed each year. If an administrator is not available anymore, there will be an election of a new administrator for the remaining period.

5.1.6

Each U.I.M. official, whether a member of the Council or not, can have only one executive position in the U.I.M., except special dispensation given by the Council and ratified by the G.A.

5.1.7

Aucun membre du Conseil de l'U.I.M. ne peut représenter une Autorité Nationale à l'Assemblée Générale, sauf dérogation accordée par le Conseil.

5.1.8

Les pouvoirs du Conseil sont ceux qui lui sont conférés par les Statuts:

5.1.8.1

Engager les dépenses dans les limites du budget approuvé par l'Assemblée Générale, mais il ne peut emprunter ou faire un appel de fonds sans l'autorisation préalable de l'Assemblée Générale statuant à la majorité des 2/3.

5.1.8.2

Suivre les actions judiciaires, tant comme demandeur que défendeur.

5.1.8.3

Examiner et approuver les propositions et les avis des Commissions et les présenter à l'Assemblée Générale.

5.1.8.4

Superviser la préparation de l'ordre du jour de l'Assemblée Générale.

5.1.8.5

Combler les vacances survenues en cours d'exercice sous réserve de ratification par l'Assemblée Générale suivante.

5.1.8.6

La médaille d'honneur est décernée par le Conseil ***selon l'article 11.1 du Règlement d'Ordre Intérieur.***

5.1.7

No member of the Council of the U.I.M. may represent a N.A. at the G.A., unless the permission is given by the Council.

5.1.8

The powers of the Council are those granted by the statutes :

5.1.8.1

To incur expenses within the limits of the budget as approved by the G.A. but it cannot borrow funds or make a call for capital without the previous authorisation of the G.A. by a two-thirds majority.

5.1.8.2

To follow any judiciary action either as Plaintiff or Defendant.

5.1.8.3

To examine and approve the propositions and the opinions given by the Commissions and to forward them to the General Assembly.

5.1.8.4

To supervise the preparation of the agenda of the G.A.

5.1.8.5

To fill any vacancy that might arise during the year subject to ratification by the next G.A.

5.1.8.6

The medals of Honour are awarded by the Council ***according to article 11.1 of the by-laws.***

5.2 - PRÉSIDENT

5.2.1

Il est élu par l'Assemblée Générale parmi les candidats proposés par les Autorités Nationales. Il n'entre pas dans le décompte des nationaux (voir article 5.1.2).

5.2.2

Le mandat du Président est décidé comme suit :

- a. Le Président U.I.M. est élu pour une période de quatre années ; il est éligible pour une période supplémentaire de quatre années.
- b. Le Président peut également être élu pour une troisième et dernière période, à condition d'obtenir au moins 70 % des voix.
- c. Si le Président se présentant pour un troisième mandat n'obtient pas 70 % des voix, ***et si aucun autre candidat n'obtient 51 % des voix, une nouvelle élection de tous les candidats aura lieu..***
- d. Le candidat qui obtient minimum 51 % est élu.***
- e. Si aucun candidat n'obtient 51 %, les deux candidats qui ont obtenu le plus de votes se présentent pour un vote final.***

5.2.3

Le Président est collégialement responsable, avec les autres membres du Conseil, de l'exécution de toute activité de l'U.I.M.

Le Président est déchargé de son mandat:

- à la fin de son mandat
- à sa démission
- à son décès
- sur décision de l'Assemblée Générale statuant à la majorité de deux tiers des voix valablement exprimées des membres effectifs présents

5.2 - PRESIDENT

5.2.1

He is elected by the G.A. among the candidates proposed by the N.A.'s.
He is not counted as a national (c.f. art. 5.1.2.).

5.2.2

The President's mandate is decided as follows :

- a. The U.I.M. President is elected for a four-year term ; he is eligible for a further four-year period.
- b. The President can also be elected for a third and last term, should he obtain at least 70 % of the votes cast.
- c. In the event that the President standing for a third term does not obtain 70 % of the votes cast, ***and no other candidate receives minimum 51 %, then a new election of all candidates takes place.***

d. The candidate who obtains minimum 51 % is elected.

e. If no candidate obtains 51 %, then the 2 candidates who have obtained most of the votes are standing for a final vote.

5.2.3

The President is responsible, together with the other members of the Council, for the carrying out of the U.I.M. activities.

The President is relieved from his duties:

- at the end of his mandate
- if he resigns
- if he dies
- if decided by the G.A. with a two thirds majority of the valid votes of the full members present

Sauf dans le premier cas, qui mène aux élections régulières, la vacance est comblée par le Premier Vice-Président jusqu'à l'Assemblée Générale suivante qui élit un nouveau Président pour la période non expirée du mandat du prédécesseur.

5.2.4

Le Président préside les réunions de l'Assemblée Générale et du Conseil.

5.2.5

Le Président est membre ex-officio non votant de toute Commission, Comités ou groupes de travail. Si le Président préside une Commission, un Comité ou un groupe de travail, la voix du Président est prépondérante à l'exception de l'Assemblée Générale.

5.2.6

Le Président ne peut pas garder de fonction dans une Autorité Nationale tant qu'il remplit les fonctions de Président de l'U.I.M.

5.2.7

Dans le but de déterminer la politique à mener, le Président est guidé par le Conseil.

5.2.8

Dans les cas de force majeure, le Président peut prendre les mesures qui s'imposent dans l'intérêt de l'U.I.M. à condition de les soumettre dès que possible à l'approbation du Conseil et à la ratification de l'Assemblée Générale.

5.2.8.1

Le Président peut déléguer une partie de ses pouvoirs au Premier Vice-Président.

5.2.8.2

Le Président ne peut déléguer l'entière responsabilité de la Présidence (voir article 5.2.3).

Except in the first case which leads to regular elections, the vacancy is filled by the Senior Vice President until the next G.A. elects a new President for the remaining period of the predecessor's mandate.

5.2.4

The President presides over the G.A. and the Council meetings.

5.2.5

The President is a non-voting ex-officio member of all the Commissions, Committees or Working Groups.

If the President is chairing a Commission or Committee or Working Group, he has the casting vote with the exception of the G.A.

5.2.6

The President cannot hold any position in a National Authority while being the President of the U.I.M.

5.2.7

In order to determine the policy to follow, the President is guided by the Council.

5.2.8

In the cases of force majeure, the President may take any necessary measures in the interest of the U.I.M. providing he submits them as soon as possible to the Council for approval and to the G.A. for ratification.

5.2.8.1

The President may delegate part of his powers to the Senior Vice President.

5.2.8.2

The President cannot delegate the whole of his responsibility as President (cf.5.2.3).

5.3 - ADMINISTRATEURS

5.3.1 Premier Vice-Président

Il est choisi par le Conseil parmi les Administrateurs élus par l'Assemblée Générale.

La Candidature à cette fonction ne doit pas être présentée par une Autorité Nationale.

5.3.2

La durée du mandat du Premier Vice-Président est de quatre ans.

5.3.3

Le Premier Vice-Président est rééligible.

5.3.4

Le Premier Vice-Président assiste ou remplace le Président dans le cadre de l'article 5.2.

5.3.5

Le Premier Vice-Président est déchargé de ses fonctions :

- à la fin de son mandat
- à sa démission
- à son décès
- sur décision de l'Assemblée Générale statuant à la majorité des deux tiers des membres effectifs présents

Sauf le premier cas qui mène à des élections régulières, la vacance du Premier Vice-Président est pourvue par le Président.

Toutefois, un nouveau candidat doit être désigné par le Conseil pour présentation à l'Assemblée Générale, mais seulement pour la période non expirée du mandat de son prédécesseur.

5.3 - ADMINISTRATORS

5.3.1 Senior Vice President

He is chosen by the Council among the Administrators elected by the G.A.

The candidature for this position must not be presented by a National Authority.

5.3.2

The Senior Vice President has a four-year mandate.

5.3.3

The Senior Vice President is re-eligible.

5.3.4

The Senior Vice President assists or replaces the President according to the article 5.2.

5.3.5

The Senior Vice President is relieved from his duties :

- at the end of his mandate
- if he resigns
- if he dies
- if decided by the G.A. with the two thirds majority of the full members present

Except in the first case which leads to regular elections, the vacancy of the Senior Vice President will be filled by the President.

However, a new candidate should be designated by the Council to be presented to the G.A. but only for the remaining period of the predecessor's mandate.

5.3.6

Toute vacance survenue en cours d'exercice parmi les dirigeants de l'U.I.M., sauf pour les Vice-Présidents, est pourvue par le Premier Vice-Président jusqu'à la réunion suivante du Conseil. Toutefois, un nouveau candidat doit être désigné par le Conseil et soumis à la ratification de

l'Assemblée Générale, mais seulement pour la période non expirée du mandat du prédécesseur.

5.3.7 Le Deuxième Vice-Président

Le Deuxième Vice-Président est choisi de la même façon que le Premier Vice-Président. Le Deuxième Vice-Président couvrira les mêmes fonctions en cas de vacances ou absence du Premier Vice-Président.

5.4 - RESERVE NUMBER

5.5 - LE TRÉSORIER

Le Comité Exécutif proposera au Conseil un candidat au poste de Trésorier. Le Trésorier est nommé par le Conseil et n'est pas tenu d'être un administrateur élu. Au cas où il ne serait pas un administrateur élu, il n'aurait aucun droit de vote au Conseil.

La candidature à cette fonction ne doit pas être soumise par une Autorité Nationale.

Le mandat peut être renouvelé.

Le Trésorier est assisté par **le Comité Exécutif**.

Le Trésorier doit :

- s'occuper de la gestion des fonds de l'U.I.M. et de leur utilisation suivant les directives générales ;
- proposer à l'Assemblée Générale toute mesure utile pour assurer la rentrée de fonds destinés à assurer la gestion de l'U.I.M. ;

5.3.6

Any vacancy arising among the U.I.M. officers, except for the Vice Presidents will be filled by the Senior Vice President until the next meeting of the Council.

Nevertheless a new candidate should be designated by the Council and submitted to the G.A. for ratification. but only for the remaining period of the predecessor's mandate.

5.3.7 The Vice-President

The Vice-President is chosen in the same way as the Senior Vice-President. The Vice-President will have the same functions in case of vacancy or absence of the Senior Vice-President.

5.4 - RESERVE NUMBER

5.5 - THE TREASURER

The Executive Committee will propose a candidate to be the Treasurer to the Council. The Treasurer is appointed by the Council and must not necessarily be an elected administrator. In case the appointed Treasurer is not an elected administrator, he will have no right of vote at the Council.

The candidature for this position must not be submitted by a National Authority.

The mandate can be renewed.

The Treasurer is assisted by *the Executive Committee*.

The Treasurer must :

- deal with the management of the U.I.M. funds and their use according to the guidelines ;
- propose to the G.A. any useful measure to ensure the collection of funds for the management of the U.I.M. ;

- présenter à l'approbation de l'Assemblée Générale le bilan pour l'exercice écoulé ;
- présenter à l'approbation de l'Assemblée Générale un projet de budget pour l'année suivant l'Assemblée Générale.

Le Trésorier peut déléguer une partie de ses tâches quotidiennes au/à la Secrétaire Général, sauf les deux derniers points.

5.6 - AUTRES MEMBRES

5.6.1

Le Conseil a le droit de nommer autant de Conseillers qu'il est nécessaire pour l'aider. Ces Conseillers ne disposent pas du droit de vote.

Des conseillers peuvent être nommés. Leurs nominations doivent être faites pour une période limitée et le motif de leur nomination doit être justifiée.

5.7 - RÉUNIONS / ORDRE DU JOUR ET PROCÈS-VERBAL

5.7.1

Le conseil se réunit au moins avant et après l'Assemblée Générale. Le Conseil se réunit autant de fois qu'il est nécessaire pour la bonne marche de l'U.I.M.

5.7.1.1

La date et le lieu exacte de la prochaine réunion est fixée et acceptée à la fin de la réunion.

5.7.1.2

Si un membre ne peut pas assister à une réunion, il peut s'excuser et présenter par écrit tout commentaire qu'il désire faire au Conseil.

5.7.2

Tous les membres du Conseil présents aux réunions du Conseil ont droit à une voix. Les procurations ne sont pas admises.

- present to the G.A. for approval the balance sheet of the previous year;
- present to the G.A. for approval a draft budget for the year following the G.A.

The Treasurer may delegate part of his daily tasks to the Secretary General except the last two points.

5.6 - OTHER MEMBERS

5.6.1

The Council has the right to appoint as many advisers as necessary to assist it. These advisers do not have the right to vote.

Advisers may be appointed. Their appointments shall be for a limited period and the reason for their appointment must be justified.

5.7 - MEETINGS - AGENDA AND MINUTES

5.7.1

The Council meets at least prior and after the General Assembly. The Council meets as often as necessary to ensure the smooth running of the U.I.M.

5.7.1.1

The accurate date and place of the meeting is determined and accepted at the end of the meeting.

5.7.1.2

If a member cannot attend a meeting, he can apologise and put forward any comment he wishes to make to the Council in writing.

5.7.2

All the Council members present at the Council meetings have the right to have one vote. Proxies are not allowed.

5.7.2.1

En vertu d'un protocole d'accord passé en 1953 avec l'A.P.B.A., celle-ci dispose de deux mandats d'administrateur dont un est réservé au Président de l'A.P.B.A.

5.7.3

Si les circonstances l'exigent, le Président peut demander une consultation par correspondance, fax ou e-mail.

5.7.4

Si, pour un motif grave et exceptionnel, le Président juge utile de réunir le Conseil, la convocation est faite par **email** ou télécopie.

5.7.5

La confirmation de la réunion ordinaire, accompagnée de l'ordre du jour, est expédiée au moins 30 jours avant la date de la réunion.

5.7.6

Les réunions du Conseil sont présidées par le Président. Si le Président est absent, il est remplacé par le Premier ou le Deuxième Vice-Président. Si le Président, le Premier Vice-Président et le Deuxième Vice-Président sont absents, le Conseil désigne un Président de séance pour cette réunion.

5.7.7

Des décisions ne peuvent être prises que sur les points portés à l'ordre du jour. Si tous les membres du Conseil à l'unanimité sont d'accord, des points spécifiques peuvent être discutés et soumis pour approbation.

5.7.8

Toute proposition à mettre à l'ordre du jour doit parvenir au secrétariat 45 jours avant la date de la réunion.

5.7.9

Les réunions du Conseil se font aux frais de ses membres.

5.7.2.1

According to a Memorandum of Understanding signed by the A.P.B.A. in 1953, it was granted two mandates of administrator, one of which must be given to the A.P.B.A. President.

5.7.3

If necessary, the President may ask for a consultation by mail, fax or E-mail.

5.7.4

If, for any exceptional and serious reason, the President decides that the Council should meet, the invitation to attend the meeting will be sent by **Email** or telefax.

5.7.5

The confirmation of the ordinary meeting with the agenda is sent at least 30 days before the date of the meeting.

5.7.6

The President presides over the Council meetings ; if the President is absent, he is replaced by the Senior Vice-President or Vice President. If both the President and the Senior Vice-President and Vice President are absent, the Council appoints a chairman for that meeting.

5.7.7

Decisions can be made only for the items put on the agenda ; if the Council members agree unanimously, special items may be discussed and submitted for approval.

5.7.8

Any proposal to be put on the agenda must reach the secretariat 45 days before the date of the meeting.

5.7.9

The expenses for the Council meetings will be borne by its members.

5.7.10

Les membres qui le désirent, peuvent se faire accompagner par un interprète de leur choix à leurs propres frais. La traduction simultanée Français-Anglais est assurée par l'U.I.M. pour la réunion de l'Assemblée Générale seulement.

5.7.11

Toute correspondance adressée au Conseil ne peut émaner que :

- d'une Autorité Nationale
- d'un membre du Conseil
- d'un Président ou le Secrétaire (avec l'accord du Président) de Commission

5.7.12

Les frais occasionnés par une mission confiée à un membre du Conseil, sont remboursés après approbation.

5.7.13

Les frais occasionnés par toute délégation spéciale demandée par un tiers sont à charge du demandeur et une provision suffisante doit être versée.

5.7.14

Le procès-verbal, rédigé en langue française et anglaise, sera approuvé au début de la réunion suivante.

5.7.15

Le procès-verbal doit être considéré comme un document confidentiel.

A sa demande, chaque Autorité Nationale a le droit de recevoir le procès-verbal. Ce procès-verbal sera cependant uniquement considéré comme information interne et non pour diffusion au public ou à la presse.

5.7.10

The members who wish it can come with an interpreter of their choice at their own expenses. The simultaneous interpretation French-English will be provided by the U.I.M. for the General Assembly meeting only.

5.7.11

The correspondence addressed to the Council can only come from :

- a National Authority
- a member of the Council
- the President or the Secretary (if approved by the President) of a Commission

5.7.12

The expenses incurred during a mission assigned to a Council Member are reimbursed after approval.

5.7.13

Any third party requesting a special delegation must pay for the expenses incurred, and a sufficient deposit should be paid in advance.

5.7.14

The minutes, written in English, must be approved at the beginning of the following meeting.

5.7.15

Minutes should be regarded as confidential documents.

Each member National Authority has the right to receive the minutes upon request. These minutes will however only be considered for internal information and not for further release to the public or media.

5.7.16

Le Secrétaire Général est tenu de rédiger le procès-verbal des réunions du Conseil et après approbation par le Président l'enverra à tous les membres du Conseil.

5.7.17

Les remarques concernant le procès-verbal doivent parvenir au Secrétariat dans les 60 jours de son expédition. Passé ce délai, la rédaction sera considérée comme finale et sera soumise au Conseil suivant pour approbation.

VI. SECRETARIAT**6.1 - LE SECRÉTAIRE GÉNÉRAL****6.1.1**

Le Secrétaire Général est un employé de l'U.I.M. A titre de chef de bureau, il est responsable du fonctionnement du bureau, ainsi que des fonctions administratives de l'U.I.M.

6.1.2

Le Secrétaire Général est nommé à la majorité des votes valablement exprimés et peut être révoqué par le Conseil à la majorité des deux tiers des votes valablement exprimés.

Les abstentions ne sont pas reprises dans les votes valablement exprimés.

6.1.3

Le Secrétaire Général est responsable de la publication et de la diffusion de toutes les décisions prises par l'Assemblée Générale et le Conseil.

6.1.4

Il/elle est responsable de tout ce qui a trait au secrétariat :

5.7.16

The Secretary General shall draw up the minutes of the Council meetings and after having them approved by the President and the Secretary General shall send them to all Council members.

5.7.17

The observations regarding the minutes should be sent to the secretariat within 60 days, taking into account the date the minutes were sent. After this period of time, the minutes will be considered as approved and will be submitted to the following Council for approval.

VI. SECRETARIAT**6.1 - THE SECRETARY GENERAL****6.1.1**

The Secretary General is a salaried employee of the U.I.M. As office manager, the S.G. is responsible for the operations of the office and the U.I.M. administrative functions.

6.1.2

The S.G. is appointed by a majority of votes and can be dismissed by the Council with the two-thirds majority of valid votes.

Abstentions are not counted as valid votes.

6.1.3

The S.G. is responsible for the publication and transmittal of all the decisions made by the Council and the G.A.

6.1.4

The S.G. is responsible for everything relating to the secretariat:

- préparation, avec l'aide des Présidents compétents, et rédaction des ordres du jour des réunions du Comité Exécutif, du Conseil et de l'Assemblée Générale, ainsi que de la préparation matérielle des dites réunions ;
- rédaction du procès-verbal et des documents relatifs à ces réunions;
- publication des règlements généraux et particuliers décidés par le Conseil et l'Assemblée Générale;
- publication après accord, du Conseil, du livret des règlements et du calendrier international ;
- préparation et diffusion des bulletins de l'U.I.M.

6.1.5

Le Secrétaire Général s'occupe des contacts au nom de l'U.I.M. :

- avec les Autorités Nationales ;
- avec les organisations internationales intéressées par le motonautisme et susceptibles d'aider le motonautisme ;
- avec les autorités gouvernementales.

6.1.6

Le Secrétaire Général contrôle les recettes et les dépenses et établit des états trimestriels.

6.1.7

Le Secrétaire Général est membre ex-officio, non votant, de tous les organismes de l'U.I.M.

VII. DEFINITIONS DES COMMISSIONS

7.1 - BUTS ET DEVOIRS

L'Assemblée Générale peut créer, sur proposition des Autorités Nationales ou du Conseil, autant de Commissions et Comités

- preparation, with the cognisant chairmen, and drawing up of the agendas for the meetings of the Executive Committee, the Council and the G.A. as well as the preparation for these meetings;
- drawing up of the minutes & materials of these meetings;
- publication of the general and particular rules decided upon by the Council and the G.A.;
- publication of the rulebook and the international calendar after approval by the Council;
- preparation and circulation of the U.I.M. bulletins.

6.1.5

The S.G. must deal with the contacts on behalf of the U.I.M. :

- with the National Authorities ;
- with the international organisations interested in, motorboating and being able to contribute to motorboating ;
- with government authorities.

6.1.6

The S.G. supervises the receipts and the expenses and draws up quarterly reports.

6.1.7

The Secretary General is a non-voting ex-officio member of all the U.I.M. bodies.

VII. DEFINITION OF THE COMMISSIONS

7.1 - AIMS AND DUTIES

The G.A. may set up, on the proposal of the N.A.'s or the Council, as many Commissions and Committees as necessary to ensure the

nécessaires à la bonne marche de l'Union et à la réalisation de l'objet de ses Statuts.

7.1.2 Les Commissions suivantes sont indispensables :

- Commission Sportive
 - Sous-commission River Marathon
 - Sous-commission Formule Future
 - Sous-commission Jet Sprint
- Comité des Formules
- Comité du Championnat du Monde Formule 1
- Commission Technique
- Commission Sécurité/Médicale
- Commission Offshore
 - W.O.C.C.
- Commission de Navigation de Plaisance
- Comité Aquabike
- Comité des cellules de sécurité

7.1.3

Chaque Commission doit émettre un avis circonstancié sur toute question qui lui est posée.

7.1.4

Toute question à soumettre aux Commissions doit être adressée au Secrétariat qui la transmet aux intéressés.

7.1.5

Le Conseil peut consulter les Commissions sur toutes les questions qui sont de sa compétence. Les Commissions doivent préparer leurs recommandations et suggestions en temps voulu afin de permettre la

smooth running of the Union and to achieve the objectives set by the statutes.

7.1.2 The following commissions are indispensable :

- Sports Commission
 - River Marathon Sub Commission
 - Formula Future Sub Commission
 - Jet Sprint Sub Commission
- Formulae Committee
- Formula 1 World Championship Committee
- Technical Commission
- Safety/Medical Commission
- Offshore Commission
 - Wocc
- Pleasure Navigation Commission
- Aquabike Committee
- Safety Cockpit Committee

7.1.3

Each Commission must express a detailed opinion on any question raised.

7.1.4

Any question for a Commission should be sent to the Secretariat who will pass it on to the party concerned.

7.1.5

The Council may consult the Commissions on all questions it is competent for. The Commissions must prepare their recommendations and suggestions in due time in order to be able to notify the National

notification de tous les points aux Autorités Nationales avant la date limite pour permettre à ces dernières d'étudier les propositions lors de leurs réunions.

7.1.6

Les Commissions ne peuvent entretenir de correspondance avec les Autorités Nationales que via la Secrétaire Général.

7.2 - COMPOSITION

7.2.1

Chaque Autorité Nationale en règle de cotisation de membre et de droits de calendrier pour toutes les années précédant l'Assemblée Générale peut être représentée par un délégué aux réunions plénières des Commissions.

7.2.1.1

Chaque Commission est composée de :

- 1 Président élu pour 4 ans par l'Assemblée Générale. Il est rééligible et n'entre pas dans le décompte des nationaux ;
- 8 collaborateurs élus par l'Assemblée Générale parmi les candidats présentés par les Autorités Nationales. Ils sont rééligibles. Tout candidat président est automatiquement candidat collaborateur sauf opposition de son Autorité Nationale. Les candidats obtenant le nombre de voix le plus élevé et représentant au moins 20% de la totalité des votes valablement exprimés, sont déclarés élus ;
- 1 secrétaire choisi parmi les collaborateurs.

7.2.1.2

Le système de vote est celui de l'Assemblée Générale.

7.2.1.3

Les Présidents des Commissions sont déchargés de leur fonction pour cause de :

Authorities of all the items before the deadline so that these N.A.'s can study the proposals at their meetings.

7.1.6

The Commissions may correspond with the National Authorities only through the Secretary General .

7.2 - COMPOSITION

7.2.1

Each National Authority having paid their membership and calendar fees for the years preceding the year of the General Assembly can be represented by a delegate at the plenary meetings of the Commissions.

7.2.1.1

Each Commission is composed of :

- One President elected for a four-year period by the General Assembly. He is re-eligible and is not counted as a national ;
- 8 collaborators elected by the General Assembly among the candidates presented by the National Authorities. They are re-eligible. The candidate president is automatically a candidate-collaborator unless the National Authority is opposed to it. The candidates receiving the greatest number of votes, and at least 20 % of the total valid votes, are declared elected.
- a secretary chosen among the collaborators.

7.2.1.2

The voting system is the one of the General Assembly.

7.2.1.3

The Presidents of the Commissions are relieved from their duties :

- fin de mandat
- démission
- décès
- A tout moment, sur décision du Conseil statuant à la simple majorité des membres présents et ratifiée par l'Assemblée Générale.

7.2.1.4

Les postes vacants des Présidents des Commissions survenues en cours d'exercice sont remplies par le Conseil jusqu'à l'Assemblée Générale suivante.

7.2.2

Le Secrétaire d'une Commission peut remplacer son Président aux réunions de l'Assemblée Générale et du Conseil avec l'autorisation écrite du Président. Il ne dispose pas du droit de vote.

7.2.3

Les Présidents et Secrétaires des Commissions sont seuls habilités à correspondre avec le Conseil et les autres Commissions. Le Secrétaire Général doit recevoir une copie de toute la correspondance.

7.2.4

Les réunions plénières des Commissions peuvent constituer autant de sous-commissions ou groupes de travail qu'elles le jugent nécessaire pour étudier et résoudre des problèmes spécifiques.

7.2.5

Les Présidents ou les Secrétaires des Commissions font un rapport de leurs activités lors des réunions du Conseil.

Les Présidents ont le vote prépondérant en cas d'égalité.

7.2.6

Les frais de secrétariat des Commissions (timbres, téléfax, téléphone) peuvent être remboursés sur présentation de documents comptables.

- at the end of their mandates
- if they resign
- if they die
- At any time, if decided by the Council with the simple majority of the members present and ratified by the G.A.

7.2.1.4

The vacancies arising for the Presidents of the Commissions are dealt with by the Council till the next G.A.

7.2.2

The Secretary of a Commission may replace his President at the G.A. and the Council meetings with a written authorisation of the President. He does not have the right to vote.

7.2.3

The Presidents and the Secretaries of the Commissions are the only ones entitled to correspond with the Council and with the other Commissions. The Secretary General should receive a copy of all the correspondence.

7.2.4

The plenary meetings of the Commissions may decide to set up as many sub-commissions or working groups as necessary in order to study or to solve specific problems.

7.2.5

The Presidents and the Secretaries of the Commissions put forward a report of their activities at the Council meetings.

The Presidents have a casting vote in case of a tie.

7.2.6

The secretarial expenses of the Commissions (stamps, telephone, and telefax) may be reimbursed on presentation of written proof.

7.3 - RÉUNION ET ORDRE DU JOUR

7.3.1

Les Commissions doivent se réunir en session plénière autant de fois qu'il est nécessaire, en tout cas au moins une fois par an. La session plénière

d'une Commission ou d'un Comité peut être fermée à tout membre aux non-membres.

7.3.1.1

Le lieu et la date des réunions sont fixés par les Présidents des Commissions. En tous les cas, la dernière réunion plénière qui précède l'Assemblée Générale doit être organisée dans la ville choisie pour l'Assemblée Générale.

7.3.2

Les personnes suivantes ont le droit de participer aux réunions plénières :

- le Président et le Secrétaire des Commissions ;
- les membres collaborateurs repris à l'article 7.2.1.1 ;
- un représentant officiel de chaque Autorité Nationale admise à l'Assemblée Générale de l'U.I.M. (7.2.1) ;
- le Président, les Administrateurs de l'U.I.M., le Secrétaire Général et le consultant juridique.

7.3.2.1

Les Présidents des Commissions peuvent inviter aux réunions toute personne qu'ils jugent nécessaire.

7.3.3

L'ordre du jour des réunions plénières est rédigé par le Président de la Commission qui doit tenir compte des propositions prévues aux articles 4.5.5 et 4.5.5.1. Il doit être envoyé au secrétariat 60 jours avant la réunion.

7.3 - MEETING AND AGENDA

7.3.1

The Commissions should meet in plenary meetings as often as necessary but at least once a year. The plenary session of a Commission or a Committee may be closed to non-members at any time.

7.3.1.1

The venue and the date of the meetings are determined by the Presidents of the Commissions.

In any event, the last plenary meeting before the G.A. must be held in the city chosen for the General Assembly.

7.3.2

The following persons have the right to participate in the plenary meetings :

- The President and the Secretary of the Commissions ;
- The collaborators (cf. art. 7.2.1.1) ;
- An official representative of each N.A. admitted in the U.I.M. General Assembly (7.2.1.) ;
- The U.I.M. President, the Administrators, the Secretary General and the Legal Consultant.

7.3.2.1

The Presidents of the Commissions may invite to their meetings anyone they consider necessary.

7.3.3

The agenda of the plenary meetings is drawn up by the President of a Commission who must take into account the proposals provided for in art. 4.5.5 and 4.5.5.1 It must be sent to the Secretariat 60 days before the meeting.

7.3.4

La convocation pour la réunion, accompagnée de l'ordre du jour, sera envoyée 15 jours avant la date de la réunion (voir Assemblée Générale) par Le Secrétaire Général.

7.3.5

Le cas échéant, le Président d'une Commission peut demander une consultation par correspondance en suivant la règle du vote par correspondance pour ce qui est de la forme et des détails.

7.4 PROCÈS-VERBAL DES RÉUNIONS

7.4.1

Le procès-verbal des réunions est rédigé par le Secrétaire de la Commission intéressée. Il doit mentionner les points suivants :

- une liste de toutes les personnes qui ont participé à la réunion et le titre de cette participation ;
- un résumé des points qui sont traités pendant la réunion, les réponses éventuelles aux questions soumises et les propositions de création ou de modification de règlement ;
- la proposition de modifications ou d'ajouts doit être accompagnée d'une courte explication et du texte proposé.

7.4.2

Le procès-verbal des réunions des Commissions doit être envoyé aux Autorités Nationales pour diffusion auprès de leurs délégués présents à la réunion, au Conseil de l'U.I.M. et au/à la Secrétaire Général(e). Toute demande d'information complémentaire émanant des Autorités Nationales peut être adressée aux Présidents des Commissions avec copie pour information au/à la Secrétaire Général(e).

7.4.3

Le procès-verbal ne peut, en aucun manière, être divulgué à des tiers. Cette remarque s'applique spécialement à la presse.

7.3.4

The invitation to attend the meeting as well as the agenda will be sent 15 days before the date of the meeting (cf. G.A.) by the Secretary General.

7.3.5

If necessary, the President of a Commission may ask for a consultation by mail to be carried out according to the rules relating to the mail ballot as to the time limit and the form.

7.4 - MINUTES OF THE MEETINGS**7.4.1**

The minutes of the meetings are drawn up by the Secretary of the Commission concerned. It should include the following items :

- a list of all the persons who participate in the meeting and the reasons for their participation ;
- a summary of the items that were tackled at the meeting, the possible answers given to the questions raised and the proposals to draw up or to modify the rules ;
- the proposal to proceed to modifications or additions must be accompanied by a short note of explanation and the text suggested.

7.4.2

The minutes of the meetings of Commissions must be sent to the National Authorities for distribution to the attending delegates , to the U.I.M. Council, to the Secretary General. Any request for further information from the National Authorities can be sent to the Presidents of the Commissions, with a copy to the Secretary General for information.

7.4.3

The minutes can in no circumstances be disclosed to a third party. This comment applies particularly to the press.

7.4.3.1

Le Secrétaire des Commissions peut rédiger un communiqué de presse à diffuser, à la demande de la Commission, par Le Secrétaire Général de l'U.I.M.

7.4.4

Les procès-verbaux des Commissions n'ont pas force de loi et ne peuvent servir de moyen de pression ou de persuasion en dehors des milieux de l'U.I.M.

7.4.5

Les réunions des Commissions se font aux frais de leurs membres.

VIII. LES COMMISSIONS

8.1 - LA COMMISSION SPORTIVE INTERNATIONALE (COMINSPO)

8.1.1

Les attributions de la COMINSPO sont :

8.1.1.1

Etudier tous les règlements sportifs internationaux en vue de leur application et les modifier le cas échéant. Elle doit soumettre le tout au Conseil.

8.1.1.2

Etablir en étroite collaboration avec le Secrétaire Général le calendrier sportif de toutes les épreuves qui entrent dans sa compétence.

8.1.1.3

Soumettre au Conseil toutes les propositions relatives aux questions sportives.

7.4.3.1

The Secretary of the Commissions may draw up a press release to be circulated, at the request of the Commission, by the U.I.M. Secretary General .

7.4.4

The minutes of the Commissions do not have force of law and cannot be used as means of pressure or persuasion outside the U.I.M.

7.4.5

The expenses of the meetings of the Commissions are borne by their members.

VIII. COMMISSIONS**8.1 - THE INTERNATIONAL SPORTS COMMISSION
(COMINSPOORT)****8.1.1**

The duties of the COMINSPOORT are as follows :

8.1.1.1

To study all the international sports rules in order to implement them and to modify them if necessary. It must submit the whole to the Council.

8.1.1.2

To draw up in close collaboration with the Secretary General the sports calendar of all the events for which it is competent.

8.1.1.3

To submit to the Council all the proposals relating to sports issues.

8.1.1.4

Homologuer les records mondiaux et en adapter périodiquement la réglementation.

8.1.1.5

Etablir les normes et rédiger une liste de commissaires. Nomination des commissaires, coordonné par le Secrétariat.

8.1.1.6

Elle doit en outre surveiller le déroulement des épreuves.

8.2 - LE COMITÉ DES FORMULES**8.2.1**

Le Comité des Formules est composée du Président de l'U.I.M. et les Présidents de la COMINSPORT, COMINTECH et COMINSAFE, un délégué pilotes (voir définition délégué pilotes) et les promoteurs contractés par l'U.I.M.

Toutes les courses motonautiques de Formules et tous les titres de championnats motonautiques sont la seule propriété de l'U.I.M. La gestion de ces courses est déléguée au Conseil.

8.2.2

Le Comité des Formules doit établir les règlements des différentes formules respectives pour les présenter au Conseil qui les soumettra à l'Assemblée Générale pour ratification. Elle doit s'efforcer d'unifier le plus possible les règlements.

8.2.3

Le Comité des Formules doit, en accord avec la COMINSPORT et Le Secrétaire Général, établir les calendriers et les classements.

8.1.1.4

To homologate the world records and to adapt periodically their rules.

8.1.1.5

To establish the requirements and draw up a listing of commissioners.
To appoint the Commissioners, co-ordinated by the General Secretariat.

8.1.1.6

It must also control the course of the events.

8.2 - THE FORMULAE COMMITTEE**8.2.1**

The Formulae Committee is composed of the U.I.M. President, the Presidents of the COMINSport, COMINTECH and COMINSafe, the drivers representative (see definition drivers representative) and the U.I.M. contracted promoters.

All Formulae powerboat races and all powerboat racing championship titles are the sole property of the U.I.M. The management of the competitions is delegated to the Council.

8.2.2

The Formulae Committee should draw up the rules for the various formulae in order to present them to the Council who will submit them to the G.A. for ratification. It must try to standardise as much as possible the rules.

8.2.3

The Formulae Committee in agreement with the COMINSport and the Secretary General should draw up the calendars and the classifications.

8.3 - LA COMMISSION TECHNIQUE INTERNATIONALE (COMINTECH)

8.3.1

Les attributions de la Commission Technique Internationale sont :

8.3.1.1

Etudier tous les règlements techniques proposés au niveau international.

8.3.1.2

Superviser les tests faits sur les bateaux à moteur, les équipements et les matériaux spéciaux, standardiser les bateaux à moteur et l'équipement et procéder aux homologations nécessaires.

8.3.1.3

Etudier et proposer toute mesure technique susceptible d'intéresser les pilotes, d'accroître les avantages qu'ils peuvent retirer des bateaux à moteur et d'améliorer la sécurité.

8.4 - LA COMMISSION SÉCURITÉ/MÉDICALE INTERNATIONALE (COMINSAFE)

La Commission Sécurité/Médicale Internationale est composée d'un Président et de huit membres : quatre de ses huit membres étant spécialisés en "Sécurité/Médical".

Le Président et ses quatre membres sont élus. Les quatre autres membres sont les Présidents des Commissions Sportive, Technique, Offshore et Aquabike.

Les Présidents des commissions peuvent déléguer un spécialiste de leur commission aux réunions.

8.3 - THE INTERNATIONAL TECHNICAL COMMISSION (COMINTECH)

8.3.1

The duties of the International Technical Commission are as follows :

8.3.1.1

to study all the technical rules proposed at the international level.

8.3.1.2

to supervise the tests on motorboats, equipment and special materials, to standardise motorboats and equipment and to proceed to the necessary homologations.

8.3.1.3

to study and propose any technical measure likely to interest the drivers, to increase the advantages they can draw from motorboats and to improve the safety.

8.4 - THE INTERNATIONAL SAFETY/MEDICAL COMMISSION (COMINSAFE)

The International Safety/Medical Commission is composed of one President and 8 members : four of the eight members being "Safety/Medical" oriented.

The President and these four members are elected. The remaining four members are Presidents of Sports, Technical, Offshore and Aquabike commissions.

These Chairmen can delegate to the meetings a specialist from their commission.

8.5 - LA COMMISSION OFFSHORE INTERNATIONALE (COMINOFF)

8.5.1

La COMINOFF est élue par les représentants des nations où se pratique l'offshore (organisations et pilotes). Ils sont élus au cours de ***la réunion de l'Assemblée Générale mais seules les nations offshore peuvent voter.***

8.5.1.1

La COMINOFF élira un Président, un Vice-Président et un Secrétaire, ainsi que d'autres officiels, si cela s'avère nécessaire, en tenant compte de la répartition géographique.

Un membre du Conseil sera nommé comme délégué permanent du Conseil à la Commission Offshore.

8.5.2

Les attributions de la COMINOFF sont :

8.5.2.1

Etudier, rédiger et faire appliquer les règlements offshore (séparés des règlements des autres activités motonautiques).

8.5.2.2

Etudier et formuler les règlements des Championnats Continentaux et Mondiaux Offshore.

8.5.2.3

La COMINOFF pourra soumettre à l'avis consultatif :

- de la COMINTECH tout problème technique relatif à l'Offshore ;
- de la COMINSAFE tout problème médical ou de sécurité relatif à l'Offshore.

8.5 - THE INTERNATIONAL OFFSHORE COMMISSION (COMINOFF)

8.5.1

The COMINOFF is elected by the representatives of nations participating in offshore (organisations and drivers). They are elected at the ***General Assembly meeting but only Offshore Nations are voting.***

8.5.1.1

The COMINOFF will elect a President, a Vice President and a Secretary and other officials if necessary taking into account the geographic distribution.

A member of the Council will be appointed as a Council permanent delegate to the Offshore Commission.

8.5.2

The duties of the COMINOFF are as follows :

8.5.2.1

To study, draw up and enforce the offshore rules (separated from the rules for the other motorboating activities).

8.5.2.2

To study and draw up the rules for the Offshore World and Continental Championships.

8.5.2.3

The COMINOFF can ask the advisory opinion :

- of the COMINTECH on any technical issue related to offshore ;
- of the COMINSAFE on any medical or safety issue related to offshore.

8.5.2.4

Etablir en collaboration avec Le Secrétaire Général, le calendrier et les classements des Championnats Offshore.

8.5.2.5

Examiner les demandes d'homologation de records.

8.5.3

Les nouveaux règlements **proposés** par la COMINOFF et **approuvés par** l'Assemblée Générale **entrent** en vigueur **le 1^{er} Janvier de l'année** suivant l'Assemblée Générale, sauf exception :

- décidée par l'Assemblée Générale elle-même ;
- motivée par des raisons de sécurité des pilotes ;

8.5.3.1

Les réunions de la COMINOFF devront se dérouler conformément aux Statuts et Règlement d'Ordre Intérieur. La langue officielle de l'U.I.M. est le français. L'anglais est la langue de travail de l'U.I.M. et la langue officielle pour tous les autres règlements. Le Français est la langue officielle uniquement pour les Statuts et le Règlement d'Ordre Intérieur.

8.5.3.2

La COMINOFF peut créer en son sein un groupe de travail technique pour fournir un avis sur toute question technique offshore.

8.5.3.3

La COMINOFF peut créer une sous-commission offshore III chargée des matières de la classe III.

8.6 - LA COMMISSION DE NAVIGATION DE PLAISANCE

Les fonctions de la Commission de la Navigation de Plaisance sont les suivantes :

8.5.2.4

To draw up in collaboration with the Secretary General the calendar and classifications of the offshore Championships.

8.5.2.5

To examine the requests for record homologation.

8.5.3

The new rules ***proposed*** by COMINOFF and ***approved by*** the General Assembly come into force ***on January 1st of the year*** that follows the General Assembly with certain exceptions :

- if decided by the General Assembly itself ;
- if motivated by reasons that involve the drivers' safety ;

8.5.3.1

The COMINOFF meetings will be held in conformity with the Statutes and the by-laws. The official language of the U.I.M. is French. English is the working language of the U.I.M. and the official language for all other rulebooks. French is the official language of Statutes and By-laws only.

8.5.3.2

The COMINOFF may set up a technical working group that will give its opinion on any offshore technical matter.

8.5.3.3

The COMINOFF may set up an Offshore III sub-commission to deal with the matters of Class III.

8.6 - THE INTERNATIONAL PLEASURE NAVIGATION COMMISSION

The duties of the Pleasure Navigation Commission are as follows

- Etudier et appliquer les règlements de la Navigation de Plaisance
- Etablir le calendrier de la plaisance en collaboration avec le/la Secrétaire Général(e)
- Nommer les commissaires pour ses épreuves en collaboration avec le/la Secrétaire Général(e)
- En collaboration avec le/la Secrétaire Général(e), contacter les organisations internationales et gouvernementales intéressées à promouvoir les épreuves de la Navigation de Plaisance
- Par l'intermédiaire du Conseil, soumettre ses propositions à l'Assemblée Générale (Endurance, Régularité, bateaux historiques, à énergie électrique et solaire, radio- commande, records mondiaux de longue distance, etc.)
- Prendre en compte et mesurer l'impact du motonautisme sur l'environnement et faire des propositions visant à réduire les effets préjudiciables à l'environnement.

Les sous-commissions des bateaux historiques et de la radio-commande sont placées sous l'égide de la Commission de la Navigation de Plaisance.

8.7 - LE COMITÉ DU CHAMPIONNAT DU MONDE FORMULE 1

8.7.1

Dans le but d'assurer du professionnalisme au Championnat du Monde Formule 1, l'U.I.M. constitue un Comité Directeur composé comme suit:

- To study and implement the Pleasure Navigation rules.
- To draw up in collaboration with the U.I.M. Secretary General the Pleasure Navigation calendar.
- To appoint the Commissioners for the Pleasure Navigation events in co-operation with the U.I.M. Secretary General .
- To contact, in coordination with the S.G. ,international organisations and Governmental associations interested in promoting Pleasure Navigation activities.
- To submit to the General Assembly through the Council all proposals relating to Pleasure Navigation (Endurance, Regularity, Electric, Solar and Historical Boats, Radio Controlled, Long distance World Records, etc).
- To consider and evaluate the impact of powerboating on the environment and to recommend measures to reduce any harmful effects.

The Historical Boat Sub-Commission and the Radio Control Sub-Commission report to the Pleasure Navigation Commission.

8.7 - FORMULA 1 WORLD CHAMPIONSHIP COMMITTEE

8.7.1

For the purpose of ensuring professionalism of the Formula 1 World Championship, the U.I.M. institutes a Management Committee composed as follows :

COMITE DIRECTEUR			
Président		Vote prépondérant	
Promoteur		1 vote	
Consultant technique		1 vote	
Représentant des organisateurs F1 / Autorités Nationales	<ul style="list-style-type: none">• Représentant des pilotes• Représentant des directeurs d'équipe• Représentant des constructeurs de bateaux	Comité des représentants de fabricant	Commissaires F1 : <ul style="list-style-type: none">• Commissaire permanent• Commissaire technique
	CONSULTATIF			1 VOTE	1 VOTE

8.7.2

Les membres du Comité ne peuvent être représentés par une autre personne et les procurations ne sont pas acceptées.

8.7.3

Le Président du Comité doit être un officiel U.I.M. et est nommé (selon l'article 4.3.8).

8.7.4

L'ensemble des commissaires F1 sera nommé par le Président du Comité, conformément au contrat du promoteur.

8.7.5

Toutes les décisions du Comité sont prises par simple majorité. Le Président a le vote prépondérant en case d'égalité.

8.7.6

Ce Comité fonctionnera sous le titre "Formules".

DAILY MANAGEMENT COMMITTEE Chairman Casting vote Promoter 1 vote Technical consultant 1 vote			
Appointed representatives of F1 organisers/ Nat. Authorities	<ul style="list-style-type: none"> • Drivers Representative • Team Managers Representative • Boatbuilders Representative 	Committee of engine manufacturers representatives	F1 Commissioners : <ul style="list-style-type: none"> • Permanent Commissioner • Technical Commissioner
ADVISORY	1 VOTE	1 VOTE	1 VOTE

8.7.2

Members of the Committee can not be represented by somebody else and proxies are not allowed.

8.7.3

The Committee Chairman must be a U.I.M. officer and is appointed according to art. 4.3.8.

8.7.4

The pool of F1 Commissioners will be appointed by the Committee Chairman respecting the contract with the promoter.

8.7.5

All decisions by the Committee are by simple majority. The Chairman has the casting vote in case of a tie.

8.7.6

This Committee will operate under the "Formulae" status.

8.8 - LE COMITÉ INTERNATIONAL AQUABIKE

Les fonctions du Comité International Aquabike sont les suivantes :

8.8.1

Etudier tous les règlements sportifs internationaux afin de les mettre en application et de les modifier le cas échéant. L'ensemble doit être soumis au Conseil.

8.8.2

L'organisation des courses Aquabike et du Championnat est régie par l'U.I.M. et appartient à cette dernière, qui en a délégué la gestion au Conseil. Le Conseil est en droit de modifier le règlement le cas échéant. Le Comité International Aquabike est responsable de la gestion quotidienne. Les règlements sportifs, techniques et de sécurité sont fournis par l'U.I.M. Cela signifie que les propositions de règlements seront adoptées par le Conseil et ratifiées par l'Assemblée Générale.

8.8.3

Le Comité International Aquabike est composé d'un Président et de huit membres :

- Président** = **Commissaire permanent sportif de course**
- 8 membres** = **Promoteur avec droit de vote**
- = **Commissaire technique avec droit de vote**
- = **Deux commissaires sécurité qui se partagent un vote**
- = **4 représentants de chaque continent/AN organisateur**

Les autorités nationales peuvent présenter des candidatures pour occuper les postes des 4 représentants des continents/AN organisateurs..

Ceux-ci sont nommés selon l'article 4.3.8. Les réunions sont ouvertes aux constructeurs de machines sans droit de vote.

8.8. - AQUABIKE INTERNATIONAL MANAGEMENT COMMITTEE

The duties of the Aquabike International Management Committee are as follows :

8.8.1.

To study all the international sports rules in order to implement them and to modify them if necessary. It must submit the whole to the Council.

8.8.2.

The organisation of the Aquabike races and championship is owned and ruled by the U.I.M. which has granted the management of the competition to the Council. The Council may update the rules whenever necessary. The Aquabike International Management Committee is responsible for the daily management. The sports, technical and safety rules are provided by the U.I.M. It means that rule proposals will be adopted by the Council, to be ratified by the General Assembly.

8.8.3.

The Aquabike International Management Committee is composed of one President and 8 members :

- Chairman** = *permanent race and sports commissioner*
- 8 members** = *promoter with right of vote*
- = *technical commissioner with right of vote*
- = *two safety commissioners sharing one vote*
- = *4 representatives of each organising continent/NA*

The National Authorities can enter candidatures to fill in the positions of the 4 representatives of organising Continent/NA's

These are appointed as per art. 4.3.8. The meeting is open to all engine manufacturers but they have no right of vote.

8.8.4

Les décisions du Comité sont prises à la majorité simple. En cas d'égalité, le Président a la voix prépondérante.

8.9 - LE COMITE DES CELLULES DE SECURITE

Le Comité des Cellules de Sécurité pour courses sur circuit est composé des membres suivants, avec droit de vote :

- *Président qui doit être nommé par le Conseil*
- *Président de la commission sportive*
- *Expert en composites et conception*
- *Médecin sportif*
- *Représentant des constructeurs de bateaux*
- *Représentant de l'équipe de sauvetage*

Le Président est en droit d'inviter **d'autres** experts sans droit de vote.

Les modifications des règlements seront ratifiés par le Conseil et l'Assemblée Générale.

IX. LES ZONES ET LES VICE-PRESIDENTS CONTINENTAUX ET REGIONAUX

9.1 - LES ZONES

9.1.1

Dans le but de faciliter les rapports internationaux entre les Autorités Nationales et l'U.I.M., il a été décidé de partager le monde en zones.

9.1.2

Les zones sont établies comme suit :

8.8.4.

The decisions by the Committee are by simple majority. The Chairman has the casting vote in case of a tie.

8.9. - SAFETY COCKPIT COMMITTEE

The creating of the Safety Cockpit Committee for circuit racing consists of following members with right of vote :

- ***Chairman to be appointed by Council***
- ***President Cominsport***
- ***Expert on composites and design***
- ***Sports Doctor***
- ***Boatbuilders representative***
- ***Rescue team representative***

Other experts can be invited by the Chairman but have no right of vote.

Rule changes will be ratified by Council and General Assembly.

IX. ZONES AND REGIONAL AND CONTINENTAL VICE PRESIDENTS

9.1 - ZONES**9.1.1**

In order to further the international relations between the National Authorities and the U.I.M., it has been decided to divide the world into zones.

9.1.2

These zones are as follows :

Europe de l'Est : C.I.S., Pologne, Hongrie, Bulgarie, Roumanie, Slovaquie.

Scandinavie : Finlande, Norvège, Suède, Danemark, Islande.

Europe Occidentale : Allemagne, Autriche, Suisse, Belgique, Hollande, Luxembourg, Grande Bretagne, Ile of Man, Irlande, France.

Bassin Méditerranéen : Espagne, Portugal, Monaco, Italie, Yougoslavie, Albanie, Grèce, Turquie, Malte, Chypre, Tunisie, Algérie, Maroc.

Afrique Centrale: tous les pays du Continent Africain excepté les pays méditerranéens et l'Afrique du Sud.

Afrique Australe : Afrique du Sud, Zambie, Rhodésie, Madagascar, Mozambique, Angola, Botswana, Afrique du Sud-Ouest, Malawi, Ile Maurice et Swaziland.

Amérique du Nord : Canada et les Etats-Unis d'Amérique

Amérique Central : Mexique et tous les pays jusqu'à Venezuela, Colombie.

Amérique du Sud: tous les pays du Continent Américain excepté ceux mentionnés dans la zone d'Amérique du Nord et Central.

Asie Centrale : tous les pays du Continent Asiatique excepté ceux mentionnés dans la zone d'Extrême Orient et du Moyen Orient.

Moyen Orient : Syrie, Liban, Egypte, Iran, Iraq, Libye et tous les Emirats.

Extrême Orient : Japon, Corée du Nord, Corée du Sud, Chine et Philippines, Thaïlande, Malaisie, Singapour et Indochine.

Océanie: Australie, Nouvelle Zélande, Iles Indonésiennes et Iles d'Océanie.

9.1.3

Une Autorité Nationale peut demander au Conseil d'être assignée à une zone différente de celle indiquée à l'article 9.1.2. L'Autorité Nationale devra justifier sa demande. Le Conseil a tout pouvoir de décision.

Eastern Europe : C.I.S., Poland, Czech, Slovakia, Hungary, Bulgaria and Rumania.

Scandinavia : Finland, Norway, Sweden, Denmark and Iceland

Western Europe : Germany, Austria, Switzerland, Belgium, the Netherlands, Luxembourg, Great Britain, Isle of Man, Ireland, France.

Mediterranean Countries : Spain, Portugal , Monaco, Italy, Yugoslavia, Albania, Greece, Turkey, Malta, Cyprus, Tunisia, Algeria and Morocco.

Central Africa : all the countries of the African continent except the Mediterranean countries and South Africa.

Southern Africa : South Africa, Zambia, Rhodesia, Madagascar, Mozambique, Angola, Botswana, South Western Africa, Malawi, Mauritius and Swaziland.

North America : Canada and the United States of America

Central America : Mexico and all countries up to the Southern border of Panama.

South America : all the countries of the American continent except those mentioned in the North and Central America zones.

Central Asia : all the countries of the Asian Continent except those mentioned in the Far East and Middle East Zone.

Middle East : Syria, Lebanon, Egypt, Iran, Iraq, Libya and all Emirates.

Far East : Japan, North Korea, South Korea, China and Philippines, Thailand, Malaysia, Singapore and Indochina.

Oceania : Australia, New Zealand, Indonesian Islands and Ocean islands.

9.1.3

A National Authority may ask the Council to be allocated to a zone different from the one mentioned in article 9.1.2. The National Authority should justify its request. The Council has the right to decide.

9.2 - LES VICE-PRÉSIDENTS CONTINENTAUX OU RÉGIONAUX.

9.2.1

Dans le but de faciliter les rapports entre les Autorités Nationales d'une même zone, l'U.I.M. élit des Vice-Présidents Continentaux ou Régionaux pour chaque zone.

9.2.2

Les Vice-Présidents Continentaux ou Régionaux sont choisis parmi les administrateurs. Ils doivent résider dans leur région. La durée du mandat des Vice-Présidents Régionaux et Continentaux est de quatre ans. Le mandat peut être renouvelé. Les postes vacants survenus en cours d'exercice sont comblés, si nécessaire, par le Conseil.

9.2.3

Les Vice-Présidents Continentaux ou Régionaux ont pour tâche de :

9.2.3.1

Rester en contact constant avec le Conseil, Le Secrétaire Général et les Autorités Nationales de leur zone.

9.2.3.2

Contrôler si les Autorités Nationales répondent aux devoirs qui leur sont imposés par les Statuts, les Règlements de l'U.I.M. et les décisions de l'Assemblée Générale et du Conseil.

9.2.3.3

Favoriser les rapports entre les Autorités Nationales de leur zone.

9.2.3.4

Aider les Autorités Nationales de leur zone dans les rapports avec les organismes centraux de l'U.I.M.

9.2.3.5

S'occuper de toute question soulevée par le Conseil et l'informer.

9.2 - CONTINENTAL AND REGIONAL VICE-PRESIDENTS

9.2.1

In order to further the relations between the National Authorities belonging to the same zone, the U.I.M. elects Continental and Regional Vice Presidents, one for each zone.

9.2.2

The Regional and Continental Vice Presidents are chosen among the administrators. They must reside in their region. The Regional and Continental Vice Presidents have a four-year mandate. The mandate can be renewed. Any vacancies arising during the mandate are dealt with, if necessary by the Council.

9.2.3

The duties of the Continental and Regional Vice Presidents are as follows

9.2.3.1

To remain in constant contact with the Council, the Secretary General and the National Authorities belonging to their zone.

9.2.3.2

To check whether the National Authorities carry out the tasks they were assigned by the Statutes, the U.I.M. rules and the decisions made by the Council and the G.A.

9.2.3.3

To further the relations between the National Authorities of their zone.

9.2.3.4

To help the National Authorities of their zone in their contacts with the U.I.M. central organisation.

9.2.3.5

To deal with all the questions raised by the Council and inform it.

9.2.4

Les Vice-Présidents Continentaux ou Régionaux doivent envoyer au/à la Secrétaire Général un rapport sur l'activité de leur zone 30 jours avant chaque réunion de l'Assemblée Générale et au Conseil.

9.2.5

Les Vice-Présidents Continentaux ou Régionaux sont déchargés de leur mission :

- à la fin de leur mandat (4 ans)
- par leur démission; par leur décès
- par décision du Conseil statuant à la majorité des 2/3 des administrateurs présents.

X. HONORARIAT ET MEMBRES DIVERS

10.1 - TITRE DE MEMBRE D'HONNEUR

10.1.1

L'Assemblée Générale sur proposition du Conseil peut décerner le titre de Membre d'Honneur de sa fonction à un dirigeant sortant et non réélu.

10.2 - MEMBRES CORRESPONDANTS

10.2.1

Le Conseil peut nommer des Membres Correspondants dans les pays où il n'existe ni de fédération de clubs, ni d'Autorité Nationale, mais où des clubs isolés font cependant preuve d'activité.

10.2.2

Le Membre Correspondant est nommé aux conditions suivantes:

9.2.4

The Continental and Regional Vice Presidents must send the Secretary General a report on the activities of their zone 30 days before each G.A. and Council meeting.

9.2.5

The Continental and Regional Vice Presidents are relieved from their duties :

- at the end of their mandate (four years)
- if they resign, if they die
- if decided by the Council by the two-thirds majority of the administrators present.

X. HONORARIAT AND OTHER MEMBERSHIPS

10.1 - HONORARY MEMBERSHIPS

10.1.1

On the proposal of the Council, the G.A. may provide an honorary membership of their previous function to an outgoing and non re-elected officer.

10.2 - CORRESPONDING MEMBERS

10.2.1

The Council may appoint Corresponding Members in the countries where there is neither a federation of clubs nor a National Authority but where isolated clubs are however active.

10.2.2

The Corresponding Member is appointed under the following conditions:

10.2.2.1

Il ne peut y avoir qu'un seul Membre Correspondant par pays.

10.2.2.2

La demande d'affiliation doit être accompagnée du questionnaire officiel de l'U.I.M. dûment complété.

10.2.2.3

Le club ou groupement, ainsi reconnu, a le droit d'organiser des compétitions motonautiques, de participer aux compétitions organisées par les membres affiliés dans d'autres pays, d'établir des records et de participer à toutes les activités organisées sous le nom de l'U.I.M. et de ses Autorités Nationales.

10.2.2.4

Le Membre Correspondant reçoit les circulaires et bulletins de l'U.I.M.

10.2.2.5

La cotisation annuelle pour le Membre Correspondant est fixée par l'Assemblée Générale.

10.2.2.6

Le Membre Correspondant s'engage à fournir à l'U.I.M. la liste de tous les clubs existant dans son pays et à s'efforcer de créer une Fédération Nationale.

10.2.3

L'U.I.M. se réserve le droit d'établir des contacts avec d'autres clubs dans le but de réaliser ce qui est décrit à l'article 10.2.2.7 de ce présent Règlement d'Ordre Intérieur.

10.2.4

Une année après la nomination d'un Membre Correspondant, le Conseil peut :

10.2.2.1

There can only be one Corresponding Member per country.

10.2.2.2

The request for affiliation should be accompanied by the U.I.M. official questionnaire duly completed.

10.2.2.3

The club or group, once recognised, has the right to organise motorboating races, to participate in races organised by the affiliated members in other countries, to establish records and to take part in all the activities organised under the name of the U.I.M. and its National Authorities.

10.2.2.4

The Corresponding Member will receive the U.I.M. correspondence and bulletins.

10.2.2.5

The annual fee for the Corresponding member is determined by the G.A.

10.2.2.6

The Corresponding Member promises to provide the U.I.M. with the list of all the clubs existing in its country and must endeavour to set up a national federation.

10.2.3

The U.I.M. reserves the right to contact other clubs in order to achieve what has been described in article 10.2.2.7 of these rules.

10.2.4

One year after the appointment of a Corresponding member, the Council :

- le transformer en Autorité Nationale si une Fédération Nationale a été créée ;
- prendre les mesures qu'il juge utiles si les autres clubs du pays s'opposent à la création d'une Fédération Nationale ;
- nommer, Autorité Nationale, le Membre Correspondant si les autres clubs ne s'intéressent pas suffisamment à la question ;
- sur demande du club intéressé, continuer à le considérer comme Membre Correspondant sans droit de vote ni en Assemblée Générale, ni dans les Départements et Commissions Internationales.

10.2.5

L'Assemblée Générale et/ou le Conseil peuvent, à tout moment, retirer au Club le titre de Membre Correspondant.

10.3 - MEMBRES ASSOCIÉS

10.3.1

L'U.I.M. reconnaît des membres personnels parmi les personnes s'intéressant au sport motonautique. Ce titre est décerné uniquement aux personnes physiques, à l'exclusion de groupements, fédérations, etc.

10.3.2

Le titre de Membre Associé décerné aux personnes physiques indiqué à l'article précédent, ne confère aucun droit et n'entraîne aucune responsabilité dans l'organisation de l'U.I.M.

10.3.3

La cotisation annuelle du Membre Associé est déterminée par l'Assemblée Générale.

10.3.4

Tous les membres du Conseil, les Présidents et les membres des Commissions Internationales peuvent être Membres Associés de l'U.I.M.

- may appoint it as a National Authority if a National Federation has been set up ;
- may take the necessary measures if the other clubs in the country are opposed to the setting up of a National Federation ;
- may appoint the Corresponding Member as a National Authority if the other clubs are not sufficiently interested in the matter ;
- may, if requested by the club, continue to consider it as a Corresponding Member without the right to vote at the G.A. or at the meetings of the Departments and International Commissions.

10.2.5

The General Assembly and/or the Council may, at any time, withdraw the title of Corresponding Member.

10.3 - ASSOCIATE MEMBERS

10.3.1

The U.I.M. recognises personal members among persons interested in motorboating. This title is awarded only to individuals to the exclusion of groups, federations, etc.

10.3.2

The title of Associate Member awarded to individuals as indicated in the previous article does not confer any right or entail any responsibility in the U.I.M. organisation.

10.3.3

The annual fee of the Associate Member is determined by the G.A.

10.3.4

All the members of the Council, the Presidents and the members of the International Commissions can be U.I.M. Associate Members.

10.3.5

Le Membre Associé a le droit de porter un badge spécial de l'U.I.M. Il reçoit le bulletin mensuel.

10.4 - MEMBRES ADHÉRENTS

10.4.1

Le Conseil U.I.M. peut accorder le titre de membre adhérent à une organisation sportive, une société commerciale, ou à toute autre association intéressée qui ne peut pas devenir membre associé.

Un délégué représentant le membre adhérent peut être invité, sans droit de vote, à assister à une commission U.I.M. si l'accord est donné par le Conseil et la Commission concernée.

10.4.2

Le titre de Membre Adhérent ne confère aucun droit et n'entraîne aucune responsabilité dans l'organisation de l'U.I.M.

10.4.3

La cotisation minimum du Membre Adhérent est fixée annuellement par l'Assemblée Générale.

XI. DISTINCTIONS HONORIFIQUES

11.1 - MÉDAILLES D'HONNEUR

L'U.I.M. peut décerner chaque année une médaille d'honneur par continent pour :

- le sport (sur proposition de la COMINSPORT) I
- le travail (sur proposition du Conseil)
- la navigation de plaisance (sur proposition de la Commission de Navigation de Plaisance)

10.3.5

The Associate Member has the right to wear a special U.I.M. badge. He gets the monthly bulletin.

10.4 - ADHERENT MEMBERS**10.4.1**

The U.I.M. Council may award the title of Adherent Member to a sports organisation, a Commercial Company, or to any other interested association which may not become an Associate Member.

A delegate representing an Adherent Member may be invited, without voting rights, to attend a U.I.M. Commission if agreed by the Council and the relevant Commission.

10.4.2

The title of Adherent member does not confer any right or entail any responsibility in the U.I.M. organisation.

10.4.3

The minimum fee for the Adherent Member is determined annually by the G.A.

XI. HONORARY DISTINCTIONS**11.1 - MEDALS OF HONOUR**

The U.I.M. may award each year one medal of honour per continent for:

- the sport (on the proposal of the Cominsport)
- the work (on the proposal of the Council)
- the pleasure navigation (on the proposal of the Pleasure Navigation Commission)

- L'offshore (sur proposition de la COMINOFF)
- La sécurité et la médicale (Cominsafe)

Ces médailles peuvent être décernées à titre posthume.

11.1.2

La médaille d'honneur pour le sport est décernée à un sportif particulièrement méritant qui a accompli pendant l'année ou les années précédentes une ou plusieurs performances remarquables.

11.1.3

La médaille d'honneur pour le travail est décernée à toute personne ayant fait pendant l'année ou les années précédentes des efforts particuliers pour :

- promouvoir l'U.I.M.
- promouvoir le motonautisme international en général
- avoir contribué au développement du motonautisme international en tant que dirigeant de Club, organisateur, etc.

11.1.4

La médaille d'honneur pour la navigation de plaisance est décernée à toute personne ayant fait pendant l'année ou les années précédentes des efforts particuliers pour :

- améliorer les rapports de l'U.I.M. avec les organisations internationales concernées par la navigation de plaisance et le tourisme ;
- étudier et résoudre au sein de l'U.I.M. les problèmes concernant la navigation de plaisance et le tourisme motonautique ;
- développer une activité internationale exceptionnelle de plaisancier par des croisières, le Pavillon d'Or, etc. ;
- promouvoir la navigation de plaisance et le tourisme motonautique au sein d'une Autorité Nationale (organisation de sections de navigation de plaisance, assistance aux plaisanciers, défense des plaisanciers, etc.) ;

- the offshore (on the proposal of the Cominoff)
- the safety and medical (Cominsafe)

These medals may be awarded posthumously.

11.1.2

The medal of honour for the sport is awarded to a particularly deserving sportsman who has carried out one or several outstanding performances during that year or during the previous years.

11.1.3

The medal of honour for the work is awarded to any person who during that year or the previous years has made special efforts to :

- promote the U.I.M.
- promote the international motorboating as a whole
- contribute to the development of the international motorboating, as club official, organiser, etc.

11.1.4

The medal of honour for pleasure navigation is awarded to any person who during that year or the previous years has made special efforts to :

- further the relations of the U.I.M. with the international organisations concerned with Pleasure Navigation and Tourism ;
- study and solve within the U.I.M. the problems concerning the Pleasure Navigation and motorboating tourism ;
- develop an exceptional international activity with cruises, Pavillon d'Or, etc. ;
- promote the Pleasure Navigation and the motorboating Tourism within a National Authority (organisation of pleasure navigation sections, assistance to the yachtsmen, defence of the yachtsmen, etc.) ;

- montrer, même au-dehors de l'U.I.M. et des Autorités Nationales, une activité particulièrement méritoire en ce qui concerne la navigation de plaisance et le tourisme motonautique.

11.1.5

La médaille d'honneur pour l'Offshore est décernée sur proposition de la COMINOFF à un sportif ou une personne qui a accompli dans le domaine de l'offshore les mêmes performances que celles reprises aux articles 11.1.2 et 11.1.3 pour le circuit.

11.1.6

Les candidatures pour les médailles d'honneur doivent être envoyées chaque année 180 jours avant l'Assemblée Générale au Secrétariat qui les transmettra :

- au Conseil pour la médaille du travail
- à la Commission Sportive pour la médaille du sport
- à la Commission de Navigation de Plaisance pour la médaille de la Navigation de Plaisance
- A la Cominsafe pour la médaille de la médicale et de la sécurité

11.1.7

Les candidatures peuvent être proposées par les Autorités Nationales pour leurs ressortissants par le Conseil, par les Départements et par les Commissions Internationales pour toute personne.

11.1.8

Les candidatures doivent être accompagnées du curriculum vitae du candidat.

11.1.9

Pendant leur mandat, les membres du Conseil ne peuvent être proposés comme candidats pour une médaille d'honneur sauf accord du Conseil.

- show, even outside the U.I.M. and the National Authorities a particularly outstanding activity concerning the Pleasure Navigation and Tourism.

11.1.5

The medal of honour for the offshore is awarded on the proposal of the Offshore to a sportsman or a person who has carried out in the offshore field the same performances as those covered in art. 11.1.2. and 11.1.3. for the circuit.

11.1.6

The candidatures for the medals of honour should be sent each year 180 days before the General Assembly to the Secretariat who will pass them on to :

- the Council for the medal of work
- the Sports Commission for the medal of sport
- the Pleasure Navigation Commission for the medal of pleasure
- the Cominsafe for the medal of medical and safety

11.1.7

The candidatures may be proposed by the National Authorities for the persons within their jurisdiction, by the Council, by the Departments and the International Commissions for any person.

11.1.8

The candidatures must be accompanied by the curriculum vitae of the candidate.

11.1.9

During their mandates, the members of the Council cannot be proposed for a medal of honour unless agreed upon by the Council.

11.1.10

Le Conseil pour la médaille du travail, l'Assemblée Plénière de la Commission de Navigation de Plaisance pour la médaille de Navigation de Plaisance et l'Assemblée Plénière de la Commission Sportive pour la médaille du sport, proposent les médailles d'honneur sur la base des candidatures présentées et les soumettent pour approbation à l'Assemblée Générale.

11.1.10.1

La proclamation officielle a lieu au cours de l'Assemblée Générale.

11.1.11

Le cumul des médailles est possible, mais non souhaitable.

11.1.12

La médaille d'honneur est en bronze doré d'un diamètre de 7,5 cm, suivant le modèle créé à cet effet.

Elle porte l'indication de l'année, du nom du lauréat et de la discipline pour laquelle la médaille lui a été attribuée.

XII. PROCEDURE DISCIPLINAIRE - ARBITRAGE

12.1 - SANCTIONS**12.1.1**

L'U.I.M. peut imposer des sanctions par voie disciplinaire :

- à toute A.N. ou Organisation reconnue par l'U.I.M.
- à tout officiel de l'U.I.M.

11.1.10

The Council for the medal of work, the plenary meeting of the Pleasure Navigation Commission for the medal of Pleasure Navigation and the plenary meeting of the Sports Commission for the medal of sport propose the medals of honour on the basis of the candidatures and submit them to the G.A. for approval.

11.1.10.1

The official announcement is made during the G.A.

11.1.11

It is possible but not advisable for one person to receive more than one medal.

11.1.12

The medal of honour is made of gilded bronze with a diameter of 7.5 cm according to the model.

It will indicate the year, the name of the recipient and the discipline for which the person has received the medal.

XII. DISCIPLINARY PROCEDURE - ARBITRATION

12.1 - SANCTIONS**12.1.1**

The U.I.M. may impose sanctions through disciplinary proceedings :

- To all N.A. or organisations recognised by the U.I.M.
- To all U.I.M. officials.

12.1.2.

Les sanctions doivent se référer à des infractions contre les Statuts et les règlements de l'U.I.M.

12.1.3

Les sanctions peuvent être imposées à la demande d'une A.N. ou d'une Commission Internationale.

Les sanctions disciplinaires peuvent seulement être imposées qu'à travers un expédient disciplinaire qui est instruit avec l'audience de l'intéressée et qui doit avoir l'occasion de se défendre des accusations portées contre lui.

12.1.4.1

Les infractions graves aux Statuts et aux Règlements de l'U.I.M. seront sanctionnées avec la suspension ou l'exclusion temporaire des droits ou des fonctions.

12.1.4.2

Les infractions très graves seront sanctionnées avec la suspension définitive des fonctions ou expulsion.

12.1.5

Les décisions seront prises par le Conseil statuant à la majorité simple de ses membres effectifs présents, à proposition de l'instructeur désigné par le Conseil.

12.1.6.1

Un appel pourra être interjeté contre la décision du Conseil dans un délai de 90 jours à partir de la date de la notification de la sanction.

12.1.6.2

L'appel sera décidé par l'Assemblée Générale statuant à la majorité des deux tiers des membres effectifs présents ou représentés et au scrutin secret.

12.1.2

The sanctions should be related to infractions to the U.I.M. Statutes and rules.

12.1.3

The sanctions may be imposed on the request of a National Authority or of an International Commission.

The disciplinary sanctions can only be imposed through disciplinary expedient, instructed with the hearing of the interested party who should first be given the opportunity to defence against the charges.

12.1.4.1

Serious infractions to the U.I.M. Statutes and rules will be sanctioned with the suspension or temporary exclusion of the rights or the functions.

12.1.4.2

Very serious infractions will be sanctioned with the definitive suspension of the function or with expulsion.

12.1.5

The decisions will be taken by the Council by a simple majority of the effective members present, upon proposal of the appointed instructor by the Council.

12.1.6.1

An appeal might be lodged against the Council decision within a delay of 90 days after the notification of the sanction.

12.1.6.2

The appeal will be decided by the G.A. by the two-thirds majority of the full members present and by secret ballot.

12.1.7.1

Une Autorité Nationale peut imposer des sanctions en accord avec ces propres normes et sa législation nationale, à toutes les personnes ou organisations étant sous sa juridiction.

12.1.7.2

Elle doit informer l'U.I.M. immédiatement de ses décisions.

12.1.8

l'U.I.M. et les Autorités Nationales ont le droit de publier la décision prise.

XIII. CONSEIL INTERNATIONAL U.I.M. DE CONSULTANTS

Composition : Maximum de 12 membres & 1 Président

Accès : Nomination par le Conseil

Pouvoir : Consultatif au Conseil ou aux Commissions

Réunion : A la demande de son Président ou du Conseil

Membres : Représentants des gouvernements, des industries etc.
Dirigeants antérieurs de l'U.I.M.

XIV. DROITS DE TELEVISION

Les droits de télévision concernant les épreuves motonautiques sont la propriété de l'U.I.M.

En ce qui concerne les séries internationales des promoteurs sous contrat de l'U.I.M., l'U.I.M. attribuera un pourcentage des recettes engendrées par le pays organisateur à l'Autorité Nationale de ce dernier.

12.1.7.1

The N.A. might impose sanctions according to its' proper standards and its national legislation to all persons or organisations under its jurisdiction.

12.1.7.2

The N.A. should communicate its decision immediately to the U.I.M.

12.1.8

The U.I.M. and the N.A. are entitled to publish the decision taken.

XIII. U.I.M. INTERNATIONAL BOARD OF CONSULTANTS

Composition : Maximum 12 members & 1 President

Access : Appointed by the Council

Power : Consultative to the Council or Commissions

Meeting : Upon request of its President or the Council

Meeting costs are born by the members

Members : Representatives from governments, industries etc.
Former officers of the U.I.M.

XIV. TV RIGHTS

Television rights in respect of International Powerboat racing are the property of the U.I.M.

In respect of International Series contracted to U.I.M. promoters, the U.I.M. will apportion a percentage of the revenue generated in the relevant territory to that National Authority.

Pour toute les autres épreuves internationales, l'U.I.M. accordera les droits de télévision et les revenus de ces droits au pays organisateur, automatiquement et sans rétribution (ceci sera reconsidéré par le Conseil tous les trois ans).

XV. PUBLICITE VIRTUELLE

Etant donné la difficulté d'afficher des publicités lors d'épreuves motonautiques, l'U.I.M. recommande l'utilisation de la publicité virtuelle qui offre la possibilité d'insérer des placards publicitaires supplémentaires n'existant pas réellement sur les circuits.

Les conditions suivantes doivent être prises en considération :

- Au début et à la fin de chaque émission contenant de la publicité virtuelle, les spectateurs seront informés du fait que l'émission contient de la publicité virtuelle.
- La publicité virtuelle pour des produits ou des services prohibés dans le pays organisateur de l'épreuve motonautique est interdite.
- La publicité pour le tabac ou concernant des sujets moralement inacceptables est interdite.
- La publicité virtuelle aux épreuves internationales motonautiques de l'U.I.M. ne peut être mise en place que par un fournisseur technique agréé par l'U.I.M.
- La publicité sur les bateaux, moteurs, uniformes de participants ou d'officiels ne peut être appliquée qu'avec le consentement de la personne concernée.
- L'U.I.M. est responsable de tout manquement à ces règles, qu'un tel manquement soit le fait de l'Autorité Nationale, d'un de ses clubs affiliés ayant organisé l'épreuve ou d'un tiers quelconque ayant obtenu directement ou indirectement le droit d'organiser l'épreuve motonautique en question.

In respect to all other events, the U.I.M. shall automatically licence the relevant National Authority without fee (review every 3 years by Council) to such television rights and proceeds of such rights.

XV. VIRTUAL ADVERTISING

Considering the particular difficult environment for placing advertising support where powerboat events are organised, U.I.M. recommends the use of virtual advertising which offers the possibility of inserting additional advertising placements which are non existing on the actual race course.

Following conditions must be considered :

- At the beginning and the end of each broadcast containing virtual advertising, the viewers must be informed that the broadcast contains virtual advertising.
- Virtual advertising for products or services of which advertising is banned in the country where the powerboat event takes place, is not authorized.
- Advertising of Tobacco products or morally objectionable materials is not authorized.
- Virtual advertising on U.I.M. international powerboat events can only be produced by the U.I.M. approved technical supplier.
- Virtual advertising on boats, engines, participants or officials' uniforms cannot be made without their own consent.
- U.I.M. will be held responsible for failure to observe these regulations, the National Authorities and their affiliated clubs organising the event whether such failure shall be caused by these parties or by third parties to whom the rights to the powerboat event in question has been directly or indirectly granted.

A

ADMINISTRATEURS	5.3
APPLICATION DES REGLEMENTS	II
AUTORITES NATIONALES	3.1

C

COMINOFF	8.5
COMINSAFE	8.4
COMINSPORT	8.1
COMINTECH	8.3
COMITE DES FORMULES	8.2
COMITE DU CHAMPIONNAT DU MONDE FORMULE 1	8.7
COMITE INTERNATIONAL AQUABIKE	8.8
COMITE CELLULE DE SECURITE	8.9
COMMISSION DE NAVIGATION DE PLAISANCE	8.6
COMMISSIONS (DEFINITION)	VII
COMPOSITION (COMMISSIONS)	7.2
COMPOSITION ET POUVOIR (ASSEMBLEE GENERALE)	4.1
COMPOSITION ET POUVOIR (CONSEIL)	5.1
CONSEIL	V
CONSULTANTS (CONSEIL INTERNATIONAL DE)	XIII

A

ADHERENT MEMBERS	10.4
SECRETARY GENERAL	6.1
ADMINISTRATORS	5.3
AGENDA AND MINUTES (COUNCIL MEETINGS)	5.7
ASSOCIATE MEMBERS	10.3
AQUABIKE COMMITTEE	8.8

B

BADGES	1.3
--------	-----

C

COMINOFF	8.5
COMINSAFE	8.4
COMINSPORE	8.1
COMINTECH	8.3
COMMISSIONS (DEFINITION)	VII
COMPOSITION (COMMISSIONS)	7.2
COMPOSITION AND POWER (G.A.)	4.1
COMPOSITION AND POWERS (COUNCIL)	5.1
CONSULTANTS (INTERNATIONAL BOARD OF)	XIII

D

DEFINITIONS ET TERMINOLOGIE	1.2
DRAPEAUX	1.3
DROITS DE TELEVISION	XIV

E

ELECTIONS (ASSEMBLEE GENERALE)	4.3
--------------------------------	-----

I

INSIGNES	1.3
----------	-----

L

LANGUE OFFICIELLE	
-------------------	--

M

MEDAILLES D'HONNEUR	11.1
MEMBRES ADHERENTS	10.4
MEMBRES ASSOCIES	10.3
MEMBRES CORRESPONDANTS	10.2

CORRESPONDING MEMBERS	10.2
-----------------------	------

COUNCIL	5.1
---------	-----

D

DEFINITIONS AND TERMINOLOGY	1.2
-----------------------------	-----

E

ELECTIONS (GENERAL ASSEMBLY)	4.3
------------------------------	-----

F

FLAGS	1.3
-------	-----

FORMULA1 WORLD CHAMPIONSHIP COMMITTEE	8.7
---------------------------------------	-----

FORMULAE COMMITTEE	8.2
--------------------	-----

H

HONORARY MEMBERSHIP	10.1
---------------------	------

I

IMPLEMENTATION OF THE RULES	II
-----------------------------	----

INTERNATIONAL BOARD OF CONSULTANTS	XIII
------------------------------------	------

O

ORDRE DU JOUR ET PROCES-VERBAL (REUNION DU CONSEIL)	5.7
--	-----

P

PRESIDENT	5.2
PROCEDURE ET DECISIONS (ASSEMBLEE GENERALE)	4.5
PUBLICITE VIRTUELLE	XV

S

SANCTIONS	12.1
SECRETAIRE GÉNÉRAL	6.1
SECRETARIAT	VI

T

TITRE DE MEMBRE D'HONNEUR	10.1
TRESORIER	5.5

V

VICE-PRESIDENTS REGIONAUX/CONTINENTAUX	9.2
VOTE PAR CORRESPONDANCE	4.4

M

MAIL BALLOT (GENERAL ASSEMBLY)	4.4
MEDALS OF HONOUR	11.1
MEETINGS (G.A.)	5.7
MINUTES OF MEETINGS (COMMISSIONS)	7.4

N

NATIONAL AUTHORITIES	3.1
----------------------	-----

O

OFFICIAL LANGUAGE	1.1
-------------------	-----

P

PLEASURE NAVIGATION COMMISSION	8.6
PRESIDENT	5.2
PROCEDURE AND DECISIONS (GENERAL ASSEMBLY)	4.5

R

REGIONAL/CONTINENTAL VICE-PRESIDENTS	9.2
--------------------------------------	-----

VOTES ET PROCURATIONS (ASSEMBLEE GENERALE)	4.2
--	-----

Z

ZONES	9.1
-------	-----

S

SAFETY COCKPIT COMMITTEE	8.9
SANCTIONS	12.1
SECRETARY GENERAL	VI

T

TREASURER	5.5
TV RIGHTS	XIV

V

VIRTUAL ADVERTISING	XV
VOTES AND PROXIES (GENERAL ASSEMBLY)	4.2

Z

ZONES	9.1
-------	-----

NOTES

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Achevé d'imprimer
sur les presses de
MULTIPRINT - MONACO
en mars 2005



**UNION INTERNATIONALE
MOTONAUTIQUE**

**CIRCUIT
RULES**

2005

2004 CIRCUIT WORLD CHAMPIONS

Formula 1



GILLMAN Scott - USA

Formula 500



TROMBETTA Tiziano - Italy

OSY 400



LUCAS Oliver - Austria

S 850



BRUNILA Joachim - Finland

Formula 3



COENRADI Johan - Ned

O 700



BACCHI Mauro - Italy

O 250



POLITI Michelangelo - Italy

S 550



GROSS Uģis - Latvia

Formula 2000



ANDERSSON Jonas - Sweden

O 350



SYNORACKI Henryk - Poland

O 125



SHI HAIWEN - China

F 1000



DEL PIN Roberto - Italy

2004 CIRCUIT WORLD CHAMPIONS

SST 45



FOX Jay - USA

SST 120



VOISIN Vince - USA

PR 550



Driver 1
LADIE Mario - Italy



Driver 2
DAL BEN Ilaria - Italy

2004 CIRCUIT ENDURANCE WORLD CHAMPIONS



CHRETIEN Bruno
France



LEMOINE Sébastien
France



CLASS 1
SOUCHAL Philippe
France



KOEHL Matthieu
France



DESGUISE Cédric - France



CLASS 2
DESGUISE Jean-Vital - France



CHARLOT P. - France



BOYARD Christophe - France



CLASS 3
PETIOT Thierry - France



SAVIN Xavier - France

2004 JET BOAT WORLD CHAMPIONS



Class A

THOMPSON Doug - Canada



Class B

CRAWFORD Karey - Canada



Unlimited

CROMIE Mark - NZL



Ex

ROSS Neil - NZL



Overall

WRIGHT Leo / HARDEN Tim - USA



Sport Jet

BOICE Greg - USA

2004 JET SPRINT WORLD CHAMPIONS



Group A

BRIANT Rex - New Zealand



Superboat ex aequo

POHIO Rodney - New Zealand



Superboat ex aequo

BRIANT Peter - New Zealand

2004 RIVER MARATHON WORLD CHAMPIONS



OB 3000

BAHENA Calixto
Mexico

2004 FORMULA FUTURE WORLD CHAMPIONS



Class 1
AZRI Muhamed -
Malaysia



Class 2
IMMLER Kevin
Germany



Class 3
AMINUR Hafiz
Malaysia



Class 4
BOTELHO Diogo
Portugal



Class 5
THIRIART Alain jr
Portugal

2004 ENDURANCE PNEUMATICS



CLASS 1
FEDOTOV Konstantin -
Russia



SAFONOV Andrey
Russia



DGURENKO Dimitri -
Russia



GODES Valery -
Russia



CLASS 2
PYATYGIN Andrey
Russia



MUSHKIN Pavel
Russia



SMIRNOV Yury
Russia



KREPNIN Sergey.
Russia



CLASS 3
KONDRAATENKO Vadim -
Russia



ANDREEV Boris
Russia



BORZENKO Evgeny -
Russia



BARANOV Yury
Russia



GRECHKO Sergey -
Russia



PERVYH Sergey
Russia



PROCOPCHIK Dmitry -
Russia



ULYANOV Eduard
Russia

CLASS 4



BERNITSYN Andrey -
Russia



NIKITIN Sergey
Russia



VANDYSHEV Dmitry -
Russia



WARTINGER Bob
USA

CLASS 5

2004 CIRCUIT EUROPEAN CHAMPIONS



Formula 4
FREMONDIERE
Mickael - France



Formula 125
JUNG Marian -
Slovakia



Formula 250
POLITI Michelangelo
- Italy



Formula1000
CARELLA Alex - Italy



Formula R 1000
GERBERT Reinhard-
Germany



O 700
BACCHI Mauro - Italy



O 500
TROMBETTA Tiziano
- Italy



O 350
BOBBIONI Marco -
Italy



OSY 400
LUCAS Oliver -
Austria



S 550
RYBARCZYK
Lechoslaw - Poland



T 550
GOSY Gergeley -
Hungary



T 400
PAASONEN Marko -
Finland



PR 550 - Driver 1
MARTINEZ Pere -
Spain



PR 550 - Driver 2
MARTIN VILCHES
Nicolas - Spain



JT 250
CIOLEK Katarzyna -
Poland

2004 ENDURANCE PNEUMATICS



PYLAeva Nadejda - Russia



PYLAEV Petr - Russia



EREMEEV Vadim - Russia

CLASS 1

**CLASS 2**

KONSTANTINOV
Aleksandr - Russia

KUZNETSOV Vitaly -
Russia

MUHIN Artem -Russia

SVETLOV Ilya - Russia

**CLASS 3**

OVCHINIKOV Aleksandr - Russia

KONDIN Igor - Russia

NOSKOV Aleksey - Estonia

**CLASS 4**

FEDORKO Sergey
Russia

STAROKOZEV Valery -
Russia

SINITSYN Aleksey -
Russia

BYKOV Mihail - Russia

**CLASS 5**

BERNITSYN Andrey -
Russia

KLYUSHNIKOV Anton -
Russia

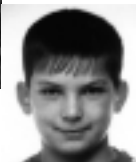
OVCHINIKOV Andrey -
Russia

OSIPOV Veniamin Russia

2004 FORMULA FUTURE EUROPEAN CHAMPIONS



Class 1
SCHULTZ Philippe -
Germany



Class 2
MRKVICKA Ondrej -
Czech Rep



Class 3
BRUGEL Florian -
Germany



Class 4
KOLB Dimitri -
Russia



Class 5
PYLAEV Ivan -
Russia

2004 RIVER MARATHON JET BOAT EUROPEAN CHAMPIONS



CLASS U
ABURTO Rurico
(Mex)



ABURTO Rurico
(Mex)



CLASS B
HUMPHREY Gordon
(Canada)



JACKSON James
(Canada)



CLASS SP 2500
GARCIA TORRES Virgilio
(Mex)



TENORIO Victor
(Mex)



CLASS FX
CARBAJAL GUERRERO
Fernando (Mex)



CARBAJAL CASTILLO
Fernando (Mex)

GROUP 100

U.I.M. ADMINISTRATION

- 101 CALENDAR**
 - 101.01 - INSCRIPTION FEE
 - 101.02 - OTHER MEETINGS
 - 101.03 - DEADLINE DATES
 - 101.04 - LATE REQUEST, CHANGE OF DATE AND CANCELLATION
- 102 COMMISSIONERS**
 - 102.01 - GENERAL
 - 102.02 - FUNCTIONS
 - 102.03 - AFTER THE RACE
 - 102.04 - ACCOMMODATION AND TRAVEL
- 103 RECOGNISED RACES**
- 104 NON APPROVED RACES**
- 105 TITLED INTERNATIONAL EVENTS**
- 106 INTERNATIONAL CHAMPIONSHIP**
 - 106.01 - GENERAL RULES
 - 106.02 - WATER REGISTRATION
- 107 WORLD CHAMPIONSHIP**
- 108 CONTINENTAL AND WORLD CHAMPIONSHIP**
 - 108.01 - NUMBER OF CHAMPIONSHIPS
 - 108.02 - EUROPEAN CHAMPIONSHIP SERIES
 - 108.03 - ENTRIES
 - 108.04 - HEATS AND LENGTH
 - 108.05 - REPAIRS AND MEASURING
 - 108.06 - RESULTS
- 109 ENDURANCE RACES**
 - 109.01 - DEFINITIONS
- 110 ENDURANCE CHAMPIONSHIP**
 - 110.01 - DEFINITIONS
- 111 HANDICAP**

112 RULES FOR DRAG RACES**112.01 - DEFINITIONS****120 RACING LICENCES****120.01 - COMPULSORY LICENCE****120.02 - REGISTRATION****120.03 - VALIDITY OF LICENCES****120.04 - COST OF LICENCE****120.05 - EXAMINATION OF LICENCES****120.06 - CANCELLATION OF LICENCE****120.07 - PSEUDONYM****120.08 - SUPER-LICENCE ENDORSEMENT FOR WORLD FORMULA SERIES
RACING**

GROUP 200

- 200 ORGANISATION OF THE RACES**
 - 200.01 - GENERAL
 - 200.02 - INTERPRETATION OF THE RULES
- 201 ORGANISING BODIES**
 - 201.01 - RACE COMMITTEE
 - 201.02 - INTERNATIONAL OFFICERS LISTS
 - 201.03 - THE OFFICER OF THE DAY (OOD)
 - 201.04 - THE DEPUTY OFFICER OF THE DAY
 - 201.04.01 - THE MEDICAL OFFICER
 - 201.05 - THE RACE SECRETARY
 - 201.06 - DUTIES OF THE RACE OFFICIALS
 - 201.07 - THE TECHNICAL OFFICERS
 - 201.08 - THE BUOY AND COURSE OFFICERS
 - 201.09 - THE TIMEKEEPERS AND STARTER
- 202 ADVANCE-PROGRAMME**
 - 202.01 - FORWARDING DATE
- 203 ENTRY**
 - 203.01 - RATIFICATION
 - 203.02 - ADMISSION
 - 203.03 - ENTRY FREE
 - 203.04 - MINIMUM AGE
 - 203.041 - MINIMUM AGE INTERNATIONAL
 - 203.05 - NATIONALITY
 - 203.051 - PUBLICITY ON THE BOATS
- 204 RACE INSTRUCTIONS**
- 205 SAFETY RULES**
 - 205.01 - ANNUAL MEDICAL EXAMINATION
 - 205.02 - MEDICAL EXAMINATION AT EVENT
 - 205.02.01 - MEDICAL EXAMINATION FOR SUPERLICENCE
 - 205.02.02 - DOPING TEST CONSUMPTION OF ALCOHOL
 - 205.02.03 - ANTI-DOPING REGULATIONS
 - 205.03 - MEDICAL EVALUATION SHEET
 - 205.04 - GENERAL OBJECTIVE EXAMINATION
 - 205.05 - UNDERWATER TRAINING
 - 205.06 - LIFE JACKET

205.07 - PROTECTING HELMET
205.08 - SAFETY BOATS
205.08.01 - TOWING BOATS
205.08.02 - FIRST AID STATION
205.09 - INSURANCE
205.10 - TECHNICAL PROTECTION
205.11 - CLOTHING PROTECTION
205.12 - LIFTING OF RACEBOATS

206 **DISTINCTIVE MARKS**
206.01 - NATIONALITY
206.02 - NUMBERS
206.03 - LOSS OF RACE NUMBER

GROUP 300

- 300 RULES FOR CIRCUIT RACE**
 - 300.01 - DEFINITIONS**
- 301 TIMING**
- 302 CIRCUIT**
 - 302.02 - RACE CONTROL**
- 303 MARKS OF THE COURSE**
 - 303.01 - DEFINITIONS**
- 304 SIGNALS FROM THE SHORE**
 - 304.01 - CALL SIGNALS**
 - 304.02 - FLAG SIGNALS**
 - 304.03 - STARTING IN GROUPS**
- 305 STARTING SIGNALS**
- 306 FLYING START WITH CLOCK, DISCS AND GUN**
 - 306.01 - PLACE OF THE CLOCKS**
 - 306.02 - DISCS**
 - 306.03 - GUN**
 - 306.04 - STARTING LINE**
 - 306.05 - SAFETY ZONE**
 - 306.06 - BEFORE THE START**
 - 306.07 - THE START**
 - 306.08 - APPROACHES TO THE STARTING LINE**
 - 306.09 - BOAT STARTING BEFORE TIME**
- 307 DEAD ENGINE START FROM A QUAY,
A JETTY OR A BEACH**
- 308 RACE CANCELLED**
- 309 RACE POSTPONED**
- 310 RACE SHORTENED**
- 311 RACE STOPPED**
 - 311.01 - GENERALITY**
 - 311.02 - RACING IN HEATS**
 - 311.03 - SINGLE HEAT RACING**

- 312 RULES OF THE ROAD**
 - 312.09 - OVERLAP**
 - 312.10 - OVERTAKING**
 - 312.11 - PASSING MARKS**
 - 312.12 - ALTERING COURSE**
- 313 ROUNDING AND FOULING MARKS**
- 314 FOULING COMPETING BOATS**
- 315 MEANS OF PROPULSION**
- 317 FINISHING A RACE**
 - 317.01 - THE FINISH**
- 318 CLASSIFICATION**
- 319 DEAD HEAT**
- 320 POSTING OF THE RESULTS**
- 321 INSPECTION OF HULLS; ENGINES MOTORS**
- 322 THE PRIZES**
- 323 AFTER THE MEETING (REPORTS)**

GROUP 400

- 401 DEFINITIONS**
- 402 JUDICIAL BODIES**
 - 402.01 - THE INTERNATIONAL JURY
 - 402.02 - INTERESTED PARTIES
 - 402.03 - THE U.I.M. APPEALS BOARD
- 403 PROTESTS PROCEDURES**
 - 403.01 - VALIDITY OF PROTESTS
 - 403.02 - PROTEST FEE
 - 403.03 - RIGHT OF PROTEST
 - 403.04 - TIME TO LODGING A PROTEST
 - 403.05 - JUDGEMENT
 - 403.06 - NOTIFICATION OF DECISION
 - 403.07 - AVAILABILITY OF DOCUMENTS
 - 403.08 - RETURN OF FEES
- 404 HEARING OF THE PROTEST**
 - 404.01 - RIGHT TO A HEARING
 - 404.02 - WITNESSES
 - 404.03 - THE HEARING PROCEDURE
- 405 THE APPEALS PROCEDURES**
 - 405.01 - APPEAL BOARD
 - 405.02 - INTERESTED PARTIES
 - 405.03 - RIGHT OF APPEAL
 - 405.04 - NOTICE OF INTENTION TO APPEAL
 - 405.05 - TIME LIMIT
 - 405.06 - LODGING OF AN APPEAL
 - 405.07 - NOTIFICATION OF THE PARTIES OF THE APPEAL
 - 405.08 - TIME LIMIT FOR DECISIONS ON APPEALS
 - 405.09 - APPEAL BOARD DECISIONS
 - 405.10 - COSTS
- 406 PENALTIES**
 - 406.01 - GENERAL
 - 406.02 - REPRIMAND
 - 406.03 - YELLOW CARD
 - 406.04 - DISQUALIFICATION
 - 406.05 - TEMPORARY SUSPENSION

406.06 - PROLONGED SUSPENSION

406.07 - EXCLUSION

407 PRIORITY OF THESE RULES

408 POWERBOAT PROTEST FORM

409 ACCIDENT REVIEW BOARD

GROUP 500

500	SERIES AND CLASSES
	500.01 - DEFINITIONS
	500.02 - INTERNATIONAL SERIES AND CLASSES
	500.03 - RECOGNISED SERIES AND CLASSES
	500.04 - ENTRY LEVEL RACING CLASSES
	500.10 - RECOMMENDED RULES FOR FORMULA FUTURE
501	MEASUREMENT CERTIFICATE
502	INSPECTIONS
503	EQUIPMENT - PEOPLE ABOARD - MOTORS
504	NOISE REDUCTION
505	HOMOLOGATION
508	FUEL
509	REINFORCED COCKPITS
510	RACING INBOARDS
515	RACING INBOARD - HISTORIC
520	RACING OUTBOARDS
525	OSY - 400
530	SPORTS INBOARDS
540	SPORTS OUTBOARDS
550	MONOHULL OUTBOARDS
560	INFLATABLE BOATS
565	RIGID INFLATABLE BOATS
570	BOATS WITH DIESEL MOTORS
580	TURBO-JETS AND ROCKET PROPELLED BOATS

590 BOATS WITH AERIAL PROPELLER

595 ELECTRIC POWERED BOATS

500 INTERNATIONAL SERIES AND CLASSES

SERIES	CLASSES	HULL	MACHINERY	FUEL
R.510	511	512	513	514
HR.515	516	517	518	519
O.520	521	522	523	524
OSY.525	...	527	528	529
E.530	531	532	533	534
S.540	541	542	543	544
T.550	551	552	553	554
P.560	561	562	563	564
PH.565	566	567	568	569
D.570	571	572	573	574
TR.580	581	582	583	584
A.590	591	592	593	594
EL.595	596	597	598	...

GROUP 600

- 600 RECORDS AND PERFORMANCES**
 - 600.01 - GENERAL**
- 601 CLASSES ELIGIBLE FOR RECORDS**
 - 601.01 - QUALIFICATION**
- 602 DEFINITION OF UNITS**
 - 602.01 - TIME, DISTANCE AND SPEEDS**
 - 602.02 - STATUTE MILE, NAUTICAL MILE AND KILOMETRE**
- 604 THE COURSE**
- 605 CALCULATION OF SPEED**
- 606 MARGIN REQUIRED**
- 607 MOTORS**
- 608 REQUEST TO ORGANISE**
- 609 OFFICIALS**
- 610 SPEED RECORDS**
- 611 DISTANCE RECORDS**
- 612 HOUR RECORDS**
- 613 WORLD COMPETITION RECORDS**
- 614 PERFORMANCE CERTIFICATES**
- 615 HOMOLOGATION OF RECORDS**
- 616 PROTESTS**

GROUP 700

- 701 NOISE LEVEL**
- 702 EMISSIONS**
- 703 RE-FUELLING**
- 704 ENVIRONMENTAL**

GROUP 800

- 800 CUPS AND TROPHIES**
800.01 - INTERNATIONAL CUPS
- 801 ALFRED BUYSSE CUP**
- 802 JOHN WARD TROPHY**
- 803 BOUCQUEY TROPHY**
- 804 DIETER KONIG MEMORIAL**
- 805 JOHN HILL TROPHY**

GROUP 900

900 RIVER MARATHON RULES

900.01 - GENERAL

900.02 - APPLICABLE RULES

900.03 - RACE COURSE

900.04 - CREW

900.05 - EQUIPMENT

900.06 - TECHNICAL SAFETY REGULATIONS

900.07 - INSPECTION PROCEDURES

900.08 - GENERAL RACING RULES

900.09 - FLAG SIGNALS

900.10 - STARTING

900.11 - RACE IN PROGRESS RULES

900.12 - FINISHING RULES

900.13 - SUPPORT BOATS

900.14 - RACE OFFICIALS

900.15 - INTERNATIONAL RACING CLASSES

901 JET SPRINT RULES

1. GENERAL RULES

2. BOAT AND SAFETY EQUIPMENT

3. ROLL CAGE SPECIFICATIONS

4. INTERNATIONAL JET SPRINT CLASSES

5. TESTING PROCEDURES

6. SPORTSMANSHIP AND COMPETITOR CODE OF CONDUCT

7. EVENT RULES

902 ENDURANCE CHAMPIONSHIPS

101 - SPORTS CALENDAR

101.01 - INSCRIPTION FEE

Registration of international races in the annual calendar of the U.I.M. is compulsory and subject to the payment of a fee, the amount of which is fixed annually by the General Assembly.

If two titles are organised at one event, the second calendar fee will be invoiced at 50 %.

If three titles are organised at one event, the second calendar fee will be invoiced at 50 % and the third one at 25 %.

Payment of these fees must be made within three months after the fixing of the international calendar

Permission to organise these meetings is only given by the U.I.M. upon payment of these fees.

No permission is to be granted to National Authorities who are in arrears of payment.

All requests to organise an international championship must be sent to the U.I.M. Secretariat before the time set by the deadline in the rules.

The organisation of an event cannot be granted if the title, the classes to race, the locality and the date of the event are not given.

The titled meetings have priority over all other meetings before the closing of the registrations for the sports calendar. However, when the sports calendar is finalised, all the motorboating meetings are on an equal footing.

In order to promote new series and classes, new hulls, motors or engines, new ideas regarding propulsion and new concepts of racing, the Council may, provided the request to organise be accompanied by an amount equal to double the fee of an ordinary meeting, approve the organisation of international contests for such boats, motors or engines, type of propulsion and races of a kind not yet known to the U.I.M.

provided such events do not clash with other U.I.M. titled meetings at the sole discretion of U.I.M.

101.02 - OTHER MEETINGS

It is forbidden to organise national championships for series and classes which are in the programme of a titled meeting taking place the same day.

The calendar of national or local events is not to be submitted to the U.I.M. The interested National Authorities take responsibility for their own calendar.

No U.I.M. national authority in the same continent shall have a race on its calendar of the same class, as a U.I.M. World or Continental Championship on the same date, based upon dates sanctioned at the U.I.M. General Assembly.

101.03 - DEADLINE DATES

1 March : Forwarding of requests for international titled events

1 April : Allotting of titled events to NA for the following year

10 September : Forwarding of dates and venues of titled events by the NAs.

Forwarding of request for ordinary international events, complete with dates and venues.

October calendar

Meeting : Drawing-up of the international calendar.

15 November : Publishing of the international calendar.

101.04 - LATE REQUEST, CHANGE OF DATE AND CANCELLATION

Titled events

Late request: a late request will be considered only if the title is still vacant or if the title or the event has been cancelled by the NA to whom

it was granted or by the U.I.M. If the title is granted, the organiser will pay the normal inscription fee.

Late forwarding of date or venue : the inscription fee will be raised by 50%

Change of date : the inscription fee will be raised by 25 %. The U.I.M. give its assent to the new date. The request for a change of date must be received by the U.I.M. secretariat not less than 90 days before the calendar date. The new date will be approved and announced not less than 30 days after the date on which the request was received.

At less than 90 days before the calendar date, the date cannot be changed. The event will be cancelled or run as an ordinary event, but no part of the calendar fee will be refunded.

Cancellation : if the title or the event is cancelled 90 days or more before the calendar date, 50 % of the inscription fee will be refunded. In a later cancellation there will be no refunding. A cancelled title will be treated like a title not yet requested.

If the titled event is cancelled less than 30 days before the calendar date, the National Authority may not be allowed to organise a titled event the following year.

Ordinary events

Late request : the inscription fee will be raised by 25%.

Change of date : the inscription fee will be raised by 25%.

Cancellation: If the event is cancelled 30 days or more before the calendar date, 50% of the inscription fee will be refunded. In later cancellations there will be no refunding.

102 - U.I.M. COMMISSIONER

102.01 - GENERAL

The presence of a U.I.M. Commissioner is required at any international titled meeting. For F1, two Commissioners are required.

The function of Commissioner must be fulfilled most carefully.

As soon as U.I.M. gets the Commissioner's agreement, the Secretary General of the U.I.M. will send a letter to the National Authority informing them that the Commissioner has been designated for the events referred in the letter.

No commissioner will be appointed to an event organised by his National Authority.

The U.I.M. Commissioner sent to the competitions must come from a Nation near to the organising one and not from different continents.

National Authorities are requested to send yearly to the U.I.M. Secretariat a list of candidate Commissioners with the following data :

- Name, address, telephone, telefax and/or e-mail
- languages spoken
- languages read and understood
- at what times the candidate is at disposal
- knowledge in sports and technical matters.

102.02 - FUNCTIONS

The U.I.M. Commissioner

- assists the local organiser and more particularly the Officer of the Day;
- is a voting member of the international jury;
- approves last minute amendments to the advance-programme;
- ensure that the course is in accordance with the water registration;
- takes part in important decision-making during the race with the Officer of the Day, the Deputy OOD and the 2nd Commissioner, if any;
- takes part in urgent decision-making with the Officer of the Day and the organising promoter, in matters related to the overall organisation.

The U.I.M. Commissioner is entitled to take action with the race organising committee if safety conditions are not met. In some cases,

he will even be entitled to request that the event be suspended if safety conditions are not improved.

The legality of the fuel is solely under the judgement of the U.I.M. Commissioner, based on all tests available to him.

GUIDELINES for U.I.M. Commissioners

- The commissioner must always be aware that he is the sole representative of U.I.M.

On the day prior to the first race/practise day, a meeting shall be held between the U.I.M. Commissioner and the race organisers/officials, and the promoter (if any) to review all aspects of the race organisation. Time for the meeting will be at 18.00, unless otherwise agreed.

The commissioner should also ask for

- * programme
- * list of jury members and National Authority delegates
- * list of entries with names and nationalities.

- He must meet the OOD and Jury President.
- He must assist the OOD to resolve all race problems.
- He must be present at any driver's meeting.
- He must check and approve the race course to comply with U.I.M. and the advance regulations.
- During the meeting he must keep close contact with OOD and be satisfied that everything is being done correctly according to U.I.M. rules.
- He must ensure a Jury meeting is held promptly and any penalty is imposed according to U.I.M. rules.
- In case of an accident, he must keep all the information on the progress or otherwise of the injured persons and he must send completed U.I.M. accident report form to U.I.M. as soon as possible.
- The Commissioner is a full member of the Jury.

- After completion of the races, he must receive a copy of all classifications and documents to be transferred to U.I.M.
- He should be present at the Technical Inspection of the winning boat.
- He must survey the weighing of the boats (if applicable).
- He must be present at the prize giving.

102.03 - AFTER THE RACE

After the event, he sends his report to the U.I.M. Secretariat by using the official form, within ten days after the race. The official results of the event must be enclosed with the report.

The reports from Commissioners are to be considered as confidential matter and cannot be used for aims other than U.I.M.'s, not be communicated to the press or otherwise disclosed for any purpose whatsoever.

Should any Commissioner fail to obtain all necessary data and results for transmission to U.I.M., he will note this fact in his report and will urge the organisers to send all required documents by themselves to the Secretariat of U.I.M., as soon as possible.

A copy the Commissioner's report will be sent, as soon as possible, by the U.I.M. office to the President of Cominsport, all members of Cominsport and to the NA of the organiser.

102.04 - ACCOMMODATION AND TRAVEL EXPENSES

The National Authority must remind the organisers that the Commissioner must be accommodated in a good hotel for the whole duration of the event and as long as required by the accomplishment of his mission. The costs of such accommodation are borne by the organisers.

Travel expenses must be compensated by the organisers to the Commissioner from his place of residence to the race venue, calculated upon the basis of a return air ticket, economy class.

If the organisers do not offer daily meals and local transportation to the Commissioner, they must pay him/her up to 30 € per day.

No other costs are to be reimbursed to the Commissioner unless expressly agreed between the organisers and the Commissioner.

Reimbursement shall be done by the organisers directly to the Commissioner prior to departure. If the organisers fail to do so, the Commissioner shall invoice the U.I.M. The U.I.M. then sends the organisers' National Authority an equivalent invoice, adding a 10% surcharge. The invoice must be settled to the U.I.M. within 30 days.

103 - RACES RECOGNISED BY THE U.I.M.

The U.I.M. recognises the following events:

- Titled international events
- Ordinary international events
- International cruises
- National events
- Private or club events

104 - NON APPROVED RACES

Races not organised according to U.I.M. rules are not to be recognised and all officials and drivers who take any part in such races are suspended or excluded if offence recurs.

The same applies to motorboating events not approved by National Authorities.

Any driver having competed in a race not approved by the National Authority of that country or forbidden by the National Authority may be suspended for a period to be fixed by the National Authority.

In the case of an international meeting being organised by a non-affiliated club which has not asked permission to organise such a meeting, the National Authority must :

- notify the organising club that the drivers can be suspended.
- notify the drivers that they will be suspended in their own country if they take part in international races not approved by the National Authority; the latter may request U.I.M. to extend this suspension to other countries.

A national race, or a race by "invitation" in which a foreign driver is invited to take part must be entered on the U.I.M. Calendar otherwise it becomes an unauthorised event. Invited drivers entering such unauthorised races, may lose their international licences. Remember that drivers having international licences, are only allowed to enter a meeting outside their country provided their National Authority gives them permission to do so.

105 - TITLED INTERNATIONAL EVENTS

There are five categories of titled event:

- World Championship events
- World Cup events
- Continental Championship events
- Continental Cup events
- Grand Prix events

There can be one World Cup and Continental Cup event per class and per year, provided that a World or Continental Championship is not organised for the class in question.

There can be only one Grand Prix event per country and per year, except when a World Championship series race takes place in that country

Additional classification, title and awards for low emission entrants within all classes S, T, P, PR and F4 titled events. Same classes must race together to qualify for these UIM titles. A minimum number of starters must be 3. The intent is to award UIM titles to only low emission engine competitors from January **2008**.

No meeting is allowed a title unless authorised to do so by the U.I.M. who draw up the annual list in the official calendar.

No organiser can run more than 2 titled events (e.g. WC or CC) at any one meeting.

As from 2006 onwards, to alleviate the congestion of the UIM calendar in the smaller “O” classes, as most are now a “series”, it is proposed to encourage organisers to hold two or three UIM titled races at one event. Financial proposals encourage this without making it mandatory to hold at least two titled races at one event. The UIM Sports Commission will look favourable when allocating UIM titled events.

U.I.M. aims to protect titled meetings, the Council may refuse to sanction any meeting which takes place at the same time. Such refusal is only notified at the specific request of the National Authority lodging a protest.

106 - INTERNATIONAL CHAMPIONSHIPS, WORLD AND CONTINENTAL CUPS

106.01- GENERAL RULES

World Championships are open to all U.I.M. classes as well as all A.P.B.A. classes. There can be only one World Championship per class, per year. The requests to organise these championship must be sent in due time. The Sports Commission will allocate the Championships.

No exception is to be allowed to the proceeding rule.

U.I.M. has created Formula World Championships for the F1, Boat GP, F500 and Formula European Championships for F4, and F250, F1000, Historic Formula R 1000, F125 **(and F350 as from 2006 onwards).All to be run as a series.**

The U.I.M. delivers following requests made by the champions to their National Authorities and transmitted by that National Authority to the U.I.M. Secretariat a “champion’s certificate”.

The cost of such certificate is fixed annually by the General Assembly and is to be paid by the National Authority to the U.I.M. when the request is made.

A list of champions is issued annually

The U.I.M. rules and those of the National Authority are applicable for any point not foreseen by these rules.

A non-European may be proclaimed European Champion provided he has had a European licence for more than two years. The same applies for other continents.

A non-European driver can only compete in any UIM European titled event if :

- a) It is a series***
- b) He/she competes on equal terms for any start/prize monies and/or event trophies***
- c) He will not accumulate points for the Series***
- d) His results will be discounted when calculating points to go forward to the final series point count.***

Immediately after the Championship, the name of the Champion must be transmitted by fax and/or e-mail to the U.I.M. secretariat.

The entry for the drivers is free of charge.

106.02 - WATER REGISTRATION

Permission to organise an International Meeting is granted ONLY if the race is run on a course approved by the National Authority and registered by the U.I.M.

As from 2006 onwards, for UIM World and Continental championships, these events will only be granted permission on a venue within a group of UIM venue listings.

All the National Authorities MUST approve such courses and send a list (updated every four years) to the U.I.M. Every list must include ALL the following criteria in English:

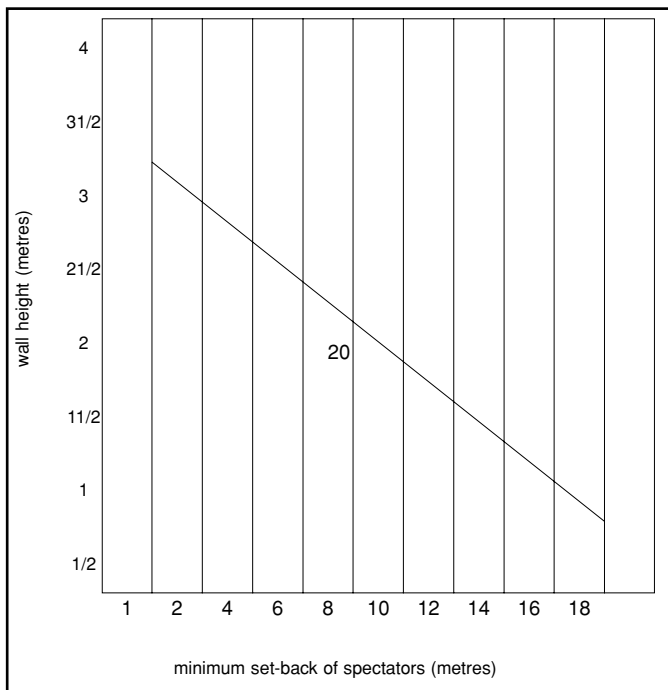
A PLAN to scale (***1:500 ... 1:2 000***) with graphic ***scale*** representation ***and geographical north***, showing accurately:

- a) The minimum depth of the race course
- b) Position of ALL race buoys
- c) Position of finish line
- d) Position of jetty/beach (if applicable) or start line
- e) Direction of the race course
- f) Other obstructions duly marked
- g) Any other buoys not applicable to the race
- h) Position of control
- i) Entry to water... crane/slipway
- j) Area for pits
- k) Spectator barriers etc. (see drawing)
- l) Position of ambulance and CLEAR access
- m) An ALTERNATIVE race course (if possible) only for use under "Force Majeure" conditions.
- n) Altitude of venues.
- o) Coloured photographs of the surrounding area (banks, sea walls and others)
- p) Tides chart of the relevant race days***

q) Categories to race**r) Number of boats of each class/category to race**

List of all possible Classes allowed to race. List MAXIMUM number of boats allowed to race in each class. .

For circuit races, bridges in the circuit should be avoided. If unavoidable, minimum opening clearances must be 50 m width for hydros, 80 m width for other classes and 15 m vertical clearance.



107 - WORLD CHAMPIONSHIP

World Championships, allocated by the U.I.M. Sports Commission are raced under the same rules as the Continental Championships.

To avoid any confusion about the value of the title, there will be no class World Championship when such Championship is organised under the Formula system.

108 - CONTINENTAL AND WORLD CHAMPIONSHIPS

108.01 - NUMBER OF CHAMPIONSHIPS

There can be one championship per year, per class, per continent (except where there is a series) but only in classes approved by the Sports Commission at least 13 months before the applicable year.

The right to organise Continental and World Championships is annually rotated in alphabetical order of the Initials of the English names of the affiliated countries applying.

If a nation declines to organise a championship the next in rotation is taken into consideration.

If a country applies for the organisation of a championship for a certain year but fails to organise it, this country is considered as having held the championship as far as rotation is concerned.

108.02 - EUROPEAN CHAMPIONSHIP SERIES

European Championships will be a series in the following classes :

- O 125 Formula 125 series
- O 250 Formula 250 series
- Formula R 1000 Formula R 1000 series

The Championship consists of up to 4 races organised in different countries. For F250, there should be a minimum of three weeks between races. No more than two events in any one National Authority.

Each leg of the Championships should be run in accordance to U.I.M. 108 rule.

Classification : In order to obtain a general classification of the European Championship, the first driver classified will be given points as follows: 20/17/15 ... (see F500 - rule 3)

If there any dispute of how the above rule, guidance should be taken from:

- 1) the F500 rules
- 2) the U.I.M. ordinary rules

U.I.M. will provide medals for 1-2-3 in the series at the final race.

Foreign drivers : Those drivers who successfully start in either a qualifying heat & race must receive the following money:

Formula R1000 Euros 100.

Formula 250 Euros 160.

Formula 125 NIL.

Formula 350 Euros 160 (as from 2006 onwards)

108.03 - ENTRIES

Each country may designate drivers according to its own rules.

Entries are to be sent by the National Authority to the organiser, no later than 7 days, before the beginning of the championship.

108.04 - HEATS AND LENGTH

A championship can only be held during an International meeting. The race must include four heats. Every heat must have a length of :

6 to 8 km for the class OSY 400, JT 250, T250, O/125.

12 to 17 km for racing outboards 175 cc to 700 cc
 for in boards up to 1000cc
 for all classes in S.P.T. up to 550 cc

20 to 30 km for racing outboards 850 cc to ∞

20 to 25 km for all other classes

With interval of at least one hour. Between finish of one heat and the start of the following one.

108.05 - REPAIRS AND MEASURING

Repairs are permitted. The substitution of parts (except crankcase) is permitted but must be done under supervision of a technical officer. In those classes where the engine(s) or motor(s) are restricted, such substitution is only to be effected with original parts. Should it be necessary to effect the repairs outside the controlled area, this must be done under supervision of a technical officer.

In multi-heat races a complete engine change is possible: then if it is before time trials no penalty. If after time trials, start low end of jetty. If after first heat or more, all points gained are removed. Start next heat with zero points.

After the race, the first classified boat and engine must be measured by the Technical Officer. A sufficient number of boats must stay in the "parc fermé" until the winner is established.

108.06 - RESULTS

A driver's three best heats are considered for the results.

When through force majeure (bad weather etc.) the heats are reduced in number or length the final positions are a valid race result.

All classes must race separately even if they belong to the same series.

The races for the Championships are not to be combined with races for other cups or trophies bearing another name.

The organising club will present a gold – silver – bronze medal to the top three drivers, unless UIM distributes the medals (to World Championships and to Formula European Championships).

Other prizes may be awarded.

A driver who has entered for a Continental Championship cannot be compelled to enter other races programmed during the meeting.

As soon as the results have been verified, the National Authority forwards these results to the U.I.M. Secretariat.

The results must be printed or typed. The results must include position, name of driver, country, time/laps and points. Additional information such as sponsor, boat may be mentioned.

If a low emission certified engine is raced, the engine manufacturer, complete with engine model number must be mentioned in the full results and on the commissioner's report.

109 - ENDURANCE RACES

109.01 - DEFINITIONS

An endurance trial is raced for at least one hour.

All U.I.M. recognised classes and those admitted by the organising committee are allowed to start together. A separate classification is made for each class, general classification not being allowed.

A general classification of "index of performance" may be included.

The classification is done according to the greatest distance covered by each competitor in his class between the start and the finishing time.

Competitors, still on the course after the finishing time, must finish their lap in order to benefit from the fraction of the distance covered over and above the finishing time.

The classification of the competitors who do not cross the finishing line is made by taking the distance covered by them on their last full circuit.

It is recommended that, for ease of timing, the length of the circuit be calculated in relation to the number of competitors (40 meters per entrant).

The above rules notwithstanding, certain experimental and unclassified boats may take part in endurance trials, but they must be scrutinised by technical officers before being allowed to race. They are not to be considered for prizes or classified.

110 - ENDURANCE CHAMPIONSHIP

110.01 - DEFINITIONS

To be taken into consideration for these championships, a trial must be raced for 3 hours total, raced in 1 or 2 heats of equal duration. In this case, an interval of one hour minimum and a maximum of 24 hours must be allowed between the starts of the heats.

If the trial lasts longer than 3 hours, more than, one driver must be used per boat, each member of the team gets the number of points provided for by the classification of the team. Each driver of the team has to be appointed before the start and no later change will be authorised.

The number of drivers for each team authorised to take part in a trial will be fixed by the Sports Commission in the calendar.

For this championship, 6 trials maximum and 4 trials minimum held in various countries will have to be organised if these figures are not reached, the title will not be granted.

The general classification of a trial will give the drivers the points set out in article 318.01 (1st/400, 2nd/300, etc...). To be classified, a boat must have covered more than 50% of the distance of the winner.

Granting of the title will be made taking into account the best results of each driver, according to the following table :

6 or 5 organised trials : 4 results

4 organised trials : 3 results

In case of a dead-heat for the first place, the best additional classification obtained from the series will be taken into consideration.

Requests for entry are to be sent in conformity with the rules (article 101.03).

Entry fees are the same as those requested for circuit championships.

The advance-programme of endurance championships must show clearly the starting system which will be used.

The boats belonging to class racing outboard unlimited are not allowed to start in races for endurance championship races with class racing outboards 2000 cc.

111 - HANDICAP

Handicapping is only allowed for private or local meetings and for invitation races. Handicaps must be published by the race committee and communicated to all the drivers before the start.

112 - RULES FOR DRAG RACES

112.01 - DEFINITIONS

The word "DRAG RACING", defines a particular kind of racing which originated in the United States and is derived from the slang of American mechanics.

First practised in motor racing, drag racing has now been adopted by motorboat racing enthusiasts.

World records of the different U.I.M. series and classes as well as those of the American Power Boat Association, drag records included, can be homologated by the U.I.M.

The racing regulations of the U.I.M. apply for any point not mentioned in the rules issued annually in the American Power Boat Association Year Book (part. 1 Racing Rules) Year Book, volume 1, Racing Rules).

120 - RACING LICENCES

120.01 - COMPULSORY LICENCE

No one can take part in a U.I.M. race, establish or improve on a U.I.M. record in international class boats, without being in possession of an international licence from his National Authority.

In order to obtain an International licence or Super Licence, following disclaimer must be signed by the licence holder :

"The undersigned... Licensed pilot (licence n), participating to U.I.M. sanctioned events, yields the media rights of any coverage by the promoter taken during the event, to the U.I.M. for further use by press, radio or television. The right however remains the property of the undersigned"

120.02 - REGISTRATION

The National Authority establishes each year a list of people to whom licences have been delivered and this is sent to the U.I.M. Secretariat.

The list must include: the name and surname, the address, the class(es) and whether National or International Licence.

These lists may be sent to other National Authorities.

Licence numbers are given annually by the National Authority to the owners and drivers entered on the list.

Any person wishing to obtain a licence must apply to the National Authority through his club.

The licence is granted by the National Authority and is marked "International" as decided by the National Authority. Licence holders must have a U.I.M. rule book. ***For international endurance races,***

titled or not titled, only one example of the UIM rules will be required, one per boat and not per each pilot.

Any National Authority has the right to deliver licences to :

- its nationals
- nationals of another country affiliated to the U.I.M. only with the agreement of the National Authority of that nation.

With the approval of the U.I.M. a National Authority may also deliver licences to persons belonging to a country which is not yet affiliated to the Union.

Drivers are only permitted to hold licences from one National Authority.

A National Authority may refuse to give a licence. Notification of this is sent to all affiliated clubs and to the U.I.M.

120.03 - VALIDITY OF LICENCES

Licences are valid from the 1st of January until the 31 st of December each year, unless local circumstances require other dates, but validity should always be shown on the licence itself, and must be for one year.

A licence is valid in any country affiliated to the Union and entitles the holder to enter or drive in his boat in all competitions organised in any affiliated country, provided he complies with what is stated in the U.I.M. general racing rules.

However, for any event held under special rules, the licence holder must abide by the rules in question.

120.04 - COST OF LICENCE

The cost of the yearly licence is to be fixed by the National Authority.

120.05 - EXAMINATION OF LICENCES

At any meeting, the holder must produce his licence at the request of any official of the meeting.

120.06 - CANCELLATION OF LICENCE

Anybody who takes part in an unauthorised meeting loses his licence.

However, if the unauthorised meeting is held in waters of a country other than the one which has delivered the licence, both National Authorities must agree as to the duration of the suspension. The U.I.M. decides finally in case of dispute.

120.07 - PSEUDONYM

The use of a pseudonym must be requested to the National Authority and the National Authority marks the pseudonym on the licence.

A licence holder, as long as he is listed under a pseudonym, may not take part in any sports event under any other name.

Changing a pseudonym is subject to the same formalities as the adoption of it. Anyone who has chosen a pseudonym may not return to his actual name until the National Authority has agreed and has delivered a new licence.

The driver may only race under his name or the pseudonym written on the licence.

120.08 - SUPER-LICENCE ENDORSEMENT FOR WORLD FORMULA SERIES RACING

120.08.01 The superlicence is required for Formula 1, and Boat GP.

120.08.02 Before being granted a superlicence, drivers must have taken part in 10 circuit races for class S catamarans equipped with an engine of a capacity higher than 750 cc, or in three races for class O boat with an engine of a capacity 500 cc or higher. Updates rules see blue pages.

120.08.03 The application for a superlicence must be lodged to the U.I.M. 4 weeks before the date of the meetings in which the driver plans to participate. The super licence fee of 120 €

will be doubled if application is after the four week deadline.

Enclosed with the application, there must be :

- the driver's sports résumé, with the results obtained at the qualifying events (120.08.02);
- an immersion certificate (205.05)
- two passport size photos
- a U.I.M. medical certificate providing all the data required by U.I.M. rules;
- the amount of fees fixed by the General Assembly.

A current rule book will be given to the licence holder together with the super licence.

The National Authorities are responsible for the information given. The superlicence application will be examined by the Secretariat of U.I.M., with the assistance of the F1/Formulae Committee. They will be free to decide upon possible waivers.

1. Any new driver must be vetted during practise by the O.O.D., the U.I.M. Commissioner and the drivers representative.
2. A new super-licence is provisional for the first 3 races, and can be withdrawn at any of the first 3 races by a committee composed by the O.O.D., the U.I.M. Commissioner and the drivers representative. The withdrawal cannot be protested or appealed.

Suspension for any circumstances may be made by the Formulae (1) committee.

When a superlicence is withdrawn, a re-application can only take place for the next season.

120.08.04 The superlicence is valid for one year (as from the 1st of January or the date of delivery to the 31st of December of the same year). Each year, the National Authorities concerned will have to send to U.I.M., before the end of December, the list of drivers asking to renew their licence. As to new licences, see 120.08.03.

- 120.08.05** In F250, F500, F1000 and F4, there is no superlicence but National Authorities will have to send to U.I.M. before the end of February the list of drivers racing in these categories and formulas, with their addresses.
- 120.08.06** The Secretariat of U.I.M. will hold all the information referred to in 120.08.05 as soon as it is received.

200 - RACE ORGANISATION

200.01 - GENERAL

The following bodies have the right to organise any motor-boating event: National Authorities; Affiliated Clubs; Corresponding members of the U.I.M. contracted promoters (with the agreement of the National Authority).

All events have to be sanctioned by the National Authority.

Affiliation to the National Authority implies their acceptance of the national rules and those of the U.I.M.

A club, member or driver who is suspended, is not allowed to take part in any sporting event in his country or in any other country neither as a driver nor as an official, as long as the suspension lasts.

A member or a driver who takes part in any race in a foreign country affiliated to the U.I.M. is ipso facto under the jurisdiction of the National Authority of that country.

A member or a driver belonging to a country which is not affiliated, is to be considered as a national of the country where he takes part in a race.

All clubs, race organisers, officials and drivers are obliged to know the general rules of the U.I.M. as well as the rules related to races or series (local, national or international). Two copies of the present U.I.M. rulebook and up to date amendments must be available at every race.

All races organised by the National Authorities or their clubs are subject to the U.I.M. racing rules. All rules and programmes must mention this.

The general racing rules and the rules related to records are applicable to all the international series.

200.02 - INTERPRETATION OF THE RULES

In case of conflicting rules, those rules concerning series and classes are to prevail over the general rules.

In all cases not foreseen by the International rules or seemingly inaccurately defined, the National Authority, a race committee, race jury or an appeal board, is to judge bearing in mind the intention of the disputed rule.

201 - ORGANISING BODIES

201.01 - RACE COMMITTEE

All races are under control of the organising club who is to nominate :

- An Organising Committee which organises the general and administrative aspects of the meeting;
- A Race Committee which organises and supervises the actual racing and decides whether competitors are eligible or not.

The Race Committee is composed as follows: the Officer of the Day, the Deputy Officer of the day, the Medical Officer, the Race Secretary, the Technical Officers.

The decisions of the Race Committee and the Jury must be based upon the U.I.M. rules, but as no rules can be devised capable of covering every case, the race committee should discourage all attempts to win by means other than fair racing, superior speed and skill.

The organising committee appoints a Committee that verifies all the documents required i.e. Measurement Certificates, Driving Licence, etc. This Committee also verifies whether or not the boats and motors conform to the rules.

No officer, nor member of the Jury, nor member of a National or International Committee or Commission, is allowed to intervene in the judgement of a problem in which he is involved himself or is an interested party.

The following are the Race Officials :

1. The Officer of the Day;

2. The Deputy Officer of the day;
3. The Medical Officer;
4. The Secretary of the Race Committee;
5. The Technical Officers;
6. The Course and Buoy Officers;
7. The Timekeepers;
8. The Lap Scorers;

201.02 - INTERNATIONAL OFFICERS LISTS

- Lists of International Officers of the days and of International Jury Chairman are deposited in the Secretariat of the U.I.M.
- Every year, each National Authority has to transmit the Secretariat its new list of O.O.D. and I.J.C. or the confirmation of the previous year's one not later than September 30th.
- Each National Authority may propose for enrolment in the list of O.O.D. and I.J.C. only those race Officers that have been regularly qualified by said National Authority.
- Requirements for the enrolment of a candidate :
 - a) a personal record (containing personal data, address, telephone, fax, numbers and/or e-mail)
 - b) knowledge of French or English
 - c) not less than a five year experience in the position applied for
- Only people enrolled in the U.I.M. lists are allowed to be appointed to the charges mentioned above and therefore to be appointed by their National Authority to perform such duties in international races valid for U.I.M. titles.

201.03 - THE OFFICER OF THE DAY (OOD)

The Officer of the Day must supervise the organisation of the races on behalf of the Organising Club. He must maintain order on the course, ascertain that all services work properly, ensure that all correspond to the rules and observe the Racing Rules according to the Programme, gather written reports of the Officers and Timekeepers and any other documents that will enable the results to be compiled.

201.04 - THE DEPUTY OFFICER OF THE DAY

The Deputy Officer of the day co-ordinates (together with the Officer of the day) the safety services on shore (fire) as well as on the water (boats, people on board, material). When necessary he stands in for the Officer of the day.

201.04.01 - THE MEDICAL OFFICER

The Medical Officer is responsible for the medical organisation in the racing area, on shore as well as on the water.

In agreement with the Officer of the day and his deputy, he controls the order of the means of emergency such as life-boats, ambulances, mobile reanimation unit, an eventual airborne ambulance and the first aid station.

In case of intervention on the water, he will take command of the rescue operation following stopping of the race by the Officer of the day.

201.05 - THE RACE SECRETARY

The Race Secretary is responsible on behalf of the O.O.D. for the practical side of the meeting and organisation and must satisfy himself that the various officials know their respective duties and that they have all the necessary documents and equipment. He must collect all documents.

201.06 - DUTIES OF THE RACE OFFICIALS

The Race Officials shall perform only the duties with which they are entrusted.

201.07 - THE TECHNICAL OFFICERS

The Technical Officers are to make sure that all drivers have their Measurement Certificates and shall verify that the hull and engine conform to the certificate and rules, and make sure that the drivers carry the homologation sheets of the engines or hulls used.

They check and control safety devices for boats and drivers as provided for in the rules.

Special attention must be paid to the automatic engine shut-off and that the racing number conform with the rules.

They verify the synchronisation of the clock and the camera if used.

After the race, they must do all necessary checks, all infringement must be communicated in writing to the O.O.D.

201.08 - THE BUOY AND COURSE OFFICERS

They must ensure that the drivers conform to the racing rules. Any irregularities must immediately be reported to the O.O.D. by radio, and in writing following the event, the O.O.D. will enforce the rules when needed. Buoy and course officials are allowed to be in boats. If they cannot be in boats inside the circuit, there will be two of them : one at the entrance and one at the exit of each turn.

201.09 - THE TIMEKEEPERS AND STARTER

The Timekeepers will :

- calculate on completion the elapsed time of each drivers;
- establish the finishing order after having made all necessary calculations and establish the average speeds;

- for international races there must be at least two timekeepers. Their results must only be handed to the Officer of the Day;
- the starter signals the start when ordered to do so by the Officer of the Day.

202 - ADVANCE-PROGRAMME

202.01 - FORWARDING DATE

45 days before any international race the organising committee must forward at least one advance-programme to the U.I.M. Secretariat, to the National Authorities and to the invited Clubs and drivers. The advance-program will also be forwarded to the commissioners for recommendations. Any such recommendation should be accomplished at least two weeks prior to the event.

Should the 45 days delay not be complied with, the calendar fee will be double.

The above advance-programme must be written in the language of the organising country and in English and must contain the following information :

- 202.020** the name and the address of the organiser;
- 202.021** the series and classes of boats catered for;
- 202.022** the race or races which will be run;
- 202.023** the venue and the date of the race(s);
- 202.024** the course of each race and plan drawn to scale showing all details as per rule 106.02. Start method, flying start or dead engine start, and maximum numbers of boats on the course.

The spectator areas must be shown and minimum distances between spectator areas and the course marked on the plan. Natural or man-made spectator protection such as fences, docks, etc. shall be noted.

The circuit plan and spectator areas shall be positioned to provide for spectators and drivers safety. Spectators and crews shall not be in the water during the running of a race.

- 202.025** - the races are held under the U.I.M. rules;
- 202.026** - the special conditions which in accordance with the U.I.M. rules may be added or modified;
- 202.027** - Time limit and place to which entries must be sent, and whether written, by fax or by hand;
- 202.028** - The amount of the entrance fee, except for Continental or World Championships for which there is no fee;
- 202.029** - All information regarding the prizes for each race;
- 202.030** - An entry form must be included in the advance programme. Where and when racing instructions can be obtained;
- 202.031** - Transport facilities and concessions;
- 202.032** - Terms and conditions of a mandatory insurance for personal and material damage to third parties, competitors included;
 - The minimum limit of coverages required;
 - The quality of risks to be covered by the insurance;
 - Whether insurances from other National Authorities are authorised and if they are, in which language they must be drawn up;
 - The amount of insurance fees to be paid on the spot (90 € maximum).
 - If the charge to the drivers for the insurance exceeds 90 euros, the UIM will fine the organising National Authority with twice the amount exceeding 90 euros per driver that they have had to pay the extra insurance.

- UIM will then return only the extra insurance fee to each drivers National Authority.

- 202.033** Minimum age of competitors;
- 202.034** Times for practice per class and the interval between the heats;
- 202.035** Full instructions regarding noise regulations;
- 202.036** Date and times for scrutineering;
- 202.037** Should no publicity be allowed during a race, this is to be mentioned in the advance-programme;
- 202.038** The time limits for late starters and finishers;
- 202.039** No modifications are to be made to any particular rule after the advance-programme is posted except if decided by the Race Committee and for cases of force majeure only. The modifications must be communicated, by written means, to the officials and participants, at least 1 hour before the first start.
- 202.040** Place and time of the drivers meeting.
- 202.041** The protest fee.
- 202.042** Organisers must announce if they will supply fuel at the meeting, at what price, and to what manufacturing specification and octane rating.
- 202.043** ***There should not be any financial penalty for not respecting any UIM rules other than what is provided in the UIM rulebook.***

203 - ENTRIES

203.01 - RATIFICATION

The National Authorities must ratify in writing any entry for drivers wishing to race abroad.

Direct correspondence between the organising committee and foreign drivers is permitted but no entry is to be accepted unless approved by the driver's National Authority.

The Organising Committee will keep blank entry forms at the disposal of the drivers.

An entry is not valid unless it reaches the organising committee within the time prescribed in the advance programme.

203.02 - ADMISSION

No organiser, organising an international race has the right to refuse the entry of a foreign driver duly commissioned by his own National Authority provided said driver conforms to U.I.M. rules.

Drivers need only to supply such particulars as are applicable to the class of their boat.

However, if a driver wishes to enter a boat with a reinforced cockpit (rule 509) in a class where a reinforced cockpit is not mandatory he must mention this in his entry. Acceptance of the entry then binds the organiser to provide the proper rescue facilities as per rule 205.08.

No entry shall be considered accepted until the boat has passed scrutineering and the documents are checked.

Only one boat per driver is allowed to be entered for any championship meeting. Spare hulls and engines are allowed but may only be used as foreseen in the rules.

203.03 - ENTRY FEE

Each organising committee fixes the amount of entry fee for the races. Entry is valid only after payment of the entry fee.

The National Authorities have the right to establish a rule whereby higher fees are imposed on late entries.

203.04 - MINIMUM AGE

Each National Authority has the right to fix a minimum age for the drivers allowed to race on its waters. Such age restriction is to be shown in the advance programme.

203.041 - MINIMUM AGE INTERNATIONAL

The minimum age for all drivers racing international races is 16 years. For racing in the classes S-P-T up to 400cc the minimum age is 14 years.

Drivers allowed to race in the class JT250 must be 10 years of age (by the race date) and may complete the race season in which they reach 16 years of age.

Minimum age of drivers allowed to race in class Formula Future is listed under 500.10 pt 1.1.

203.05 - NATIONALITY

The nationality of only the driver must be taken into consideration in the classification.

203.051 - PUBLICITY ON THE BOATS

Advertisements may be freely applied on the entire surface of the boat, but there must be a free distance of space of no less than 0,15 meter around the boat number.

No crew can be compelled to carry any advertising whatsoever on their person, clothes or upon the boat that is being raced and his-their refusal to do so cannot in any way be an obstacle to their participation in any race.

Penalties may be applied to any driver who, in the course of any sporting event, has made any advertisement contrary to the truth or distorting the facts.

Political publicity or immoral advertisements are not allowed.

Should no publicity be admitted for an event, this has to be announced in the advance programme.

Any National Authority has the right to fix its own rules regarding publicity for its own boats and courses.

204 - RACE INSTRUCTIONS

204.01

The Organising Committee must clearly state the following points in their race instructions :

204.011 Places and dates of the race;

204.012 Layout of course drawn to scale :

- Length of the course;
- Direction of the course;
- Starting line and its marks;
- Finishing line and its marks;
- Full details of buoys;
- Full details of obstacles.
- Prohibited area.

204.013 Starting time of each race;

 Signals for each race;

- 204.014** Full particulars of prizes for each race;
- 204.015** Time limit and place for handing over the protests;
- 204.016** Time and place of the distribution of prizes;
- 204.017** Composition of the Race Committee, of the Jury and of the Technical Commission;
- 204.018** List of boats entered for each race and if possible, names and clubs of the drivers;
- 204.019** Place and time at which written instructions will be issued. Where and when the official time is to be communicated and place and time of driver's meeting.
- 204.020** Only drivers whose entries have been accepted by the organising committee have their names printed in the programme;

204.02 - ORGANISING COMMITTEE MEETING

On the day prior to the first race/practise day, a meeting shall be held between the U.I.M. Commissioner and the race organisers/officials, and the promoter (if any), to review all aspects of the race organisation. Time for the meeting will be at 18.00, unless not otherwise agreed.

204.03 - DRIVERS MEETING

A driver's meeting must be held before any training.

It is mandatory for all drivers to attend all driver's meetings.

If these meetings are not held in English, a translation into English must be made by an interpreter.

It is mandatory to inform the Drivers of all environmental matters at the Drivers meeting (See Group 700).

205 - SAFETY RULES

205.01 - ANNUAL MEDICAL EXAMINATION

Each National Authority must submit all the drivers under its jurisdiction to an annual medical examination.

Each National Authority must adopt a form to be used for the medical examination.

Each National Authority must submit to a special medical examination any handicapped driver, any driver who has had a serious accident or any driver who, during two or three races, has shown odd behaviour when driving his boat : this check is compulsory before participation in any international race and must be certified by a commission set up by the National Authority of the interested driver; the commission will be composed of a well known specialist, a technician and a delegate of the National Authority; the Commission may give or refuse authorisation as to the eligibility of the driver.

Each National Authority which organises an international race must abide by the regulations approved by the Medical Commission regarding the medical organisation of international races.

205.02 - MEDICAL EXAMINATION AT EVENT

The medical officer who is to be present at any motorboating event has the right to submit any participating driver to psycho-physical examination and this at any time during the event, an alcohol or drug test may be performed.

The result of such examination is immediately communicated to the Race Committee who basing themselves on the report of the medical officer, may suspend the driver from the competition.

Following any accident, the driver must undergo a physical examination and be passed fit by the event medical staff before being permitted to continue in that racing event.

Following any accident, damaged boats must be completely rechecked and approved by the official scrutineer and the U.I.M. Commissioner, before being permitted to rejoin the event.

205.02.01 - MEDICAL EXAMINATION FOR SUPERLICENCE

In order to qualify for a superlicence or for drivers in classes where the U.I.M. world speed record exceeds 210 kmh, 131.25 mph (at 1st January of the relevant year), additional items in the medical examination are mandatory.

205.02.02 - DOPING TEST CONSUMPTION OF ALCOHOL

- Competitors shall not be under the influence of alcohol while competing and practising
- Testing when carried out shall be by using an alcometer
- A competitor shall, if requested by an authorised official, submit to an alcohol test. Written notification of selection for testing will be handed to a competitor by an authorised official. Refusal or failure to do so may be taken as if a positive test had been ordered and dealt with accordingly.
 - a. For international races, any competitor found to have more than **BAK 0,15 0/00 (0,15 g/L blood // AAK 0,075 mg/L air) 1 hour before and during testing or race** shall be immediately suspended and disqualified from the whole event. A format report shall be forwarded to the NA of the competitor.

If in an organising country, the legal requirements of the above numbers are lower, then these numbers have to be announced in the advance regulations and prevail.

205.02.03 - ANTI-DOPING REGULATIONS

***Anti-doping regulations based upon the world anti-doping code
As per separate publication***

205.03 - MEDICAL EVALUATION SHEET

Established at _____ Date _____

Name _____ Surname _____

Born in _____ Date _____

Address _____

Status _____ Profession _____

Date of first involvement in the sport _____

Other sports practised _____

PRESENT STATE OF HEALTH AND FORMER DISEASES

Disease _____

With special mention of _____

o Cardiopathy o Coronary o Trouble Diabetes

o Epilepsy o High blood pressure

Operations undergone previously _____

Dates of these operations _____

Accidents (when, where, under which circumstances) _____

Alcohol _____ Tobacco _____

Allergies _____

Medicines usually/frequently taken _____

Signature of the interested person _____

205.04 - GENERAL OBJECTIVE EXAMINATION

Height _____ Weight _____

Congenital or acquired deformities _____

Respiratory system _____

Spirometry _____

FVC _____ FEV1 _____

Cardiovascular system _____

Blood pressure _____

Heart frequency at rest _____ after stress _____

Electrocardiogram at rest and after stress (**see 205.04.1**) _____

Sight-testing a) Faculty of vision _____ R _____ L _____

with correction _____ R _____ L _____

b) Vision of colours

Hearing testing _____ Rh _____

Urine : Albumine _____ Sugar _____

Nervous system _____

EEG _____ Date _____

Others _____

Signature of the doctor _____

Name in capital letters _____

Degree _____

Address _____

Tel/Fax _____

Aptitude assessment _____

205.04.1 - ADDITIONAL ITEMS FOR SUPER-LICENCE

1 electro cardiogram and stress effort each year

Clinical medical stress test: Electrocardiogram performed under stress in a sitting "position" or on a tread mill starting at 50 watts and increasing by 25 watts every two minutes (or equipment test). The driver must be capable of reaching 2.5 watts (males) – 2.2 watts (females) per kilogram of body weight.

1 exhaustive ophtalmological check-up, tested and certified by a professional ophthalmologist to be renewed every five years and after skull trauma

205.05 - IMMERSION TEST

Competitors taking part in a race and using a reinforced cockpit as defined in rule 509, must have a certificate of underwater training. This is given by a National Authority and includes an immersion test in a simulated reinforced cockpit. All tests must include demonstrating the drivers ability to use the air supply successfully under water in simulated accident conditions. The immersion test is only valid for 14 months.

A model for the certificate and instructions of how to perform an immersion test are available at the U.I.M. Secretariat.

205.06 - LIFE JACKET

Each person aboard a race boat must wear an efficient life jacket during the race and practice conforming with the following rules :

The efficiency of a life jacket is the sole responsibility of the wearer who must be assured that it conforms with the U.I.M. rules and carries indelible confirmation of this;

Race organisers are required to repeat this important clause in race instructions and in the advance-programme;

This compulsory regulation is for all international races and recommended for national ones. In any case, life jackets must be in conformity with following rule;

Buoyancy : a life jacket must have a minimum of 7,5 kg solid buoyancy for a person weighing up to 60 kg and 9 kg solid buoyancy for those over 60 kg (closed cells foam only);

The disposition of the solid buoyancy must be such as to ensure that an unconscious person shall float face up in the water;

All adjustable straps must be at least 40 mm wide and have a minimum breaking strain of 1000 kg. The crutch straps must be attached at the front at the position of iliac crest and at the back in the sacral region. A recovering attachment must be placed either on the shoulders, or as a chest strap and made with strap of the same quality as indicated above;

Manufactured from fire resistant material and coloured orange;

The back must include a protective plate;

The compulsory collar must have rounded edges and extend above the bottom of the helmet. It must not exceed 180 degrees;

A racing life jacket manufacturer must undertake to comply with the rules and print indelibly on the life jacket the following : “conforms with the regulation for a person weighing a maximum of 60 kg” or “for a person weighing over 60 kg”, in respect of the buoyancy.

205.07 - PROTECTING HELMET

Any person aboard any boat taking part in races must wear an adequate helmet **which at least the upper 50 % (Area) must be** of fluorescent orange, red, yellow or international orange color. These helmet colors must be bright enough to be clearly visible in the water.

The wearer is entirely responsible for the efficiency of his helmet. The organisers are requested to repeat this important rule in their particular rules, as well as in their advance-programmes, race instructions and programmes.

Each National Authority may request that their nationals wear a helmet of a type laid down in their own national rules.

205.08 - SAFETY BOATS

- 1) The crew of a safety boat should be as follows : one driver, two divers, one doctor or a paramedic who has to be on board at least one boat
- 2) The safety-boat must be in contact by radio with the Officer of the day or the person designated by him
- 3) The safety-boat crew will lift the victims of an accident from the water keeping them in a horizontal position as much as possible, using a rigid stretcher
- 4) It is recommended that one person capable of providing basic life support administration be on board the boat
- 5) When competitors use safety harnesses, the divers must know how to deal with these systems.

205.08.01 - TOWING BOATS

There must be at least four boats for towing damaged boats on circuits up to 2 000 metres.

Each of these boats must cover 1/4 of the circuit, the space to be controlled being in any case limited to a length not more than 500 linear metres. The towing boat crew will consist of two people. The towing boat must contain the following equipment :

one signal flag, one fire extinguisher of a high capacity homologated for fuel fire, ropes and a boat hook.

Towing boats can only come into action once the race has been completed or stopped.

205.08.02 - FIRST AID STATION

A first aid station with qualified medical must be located within the racing area.

At least one ambulance and one mobile resuscitation unit with a qualified doctor must be located within the racing area.

205.09 - INSURANCE

All drivers must be properly insured, either by themselves, with the policy to be produced before the start, or by an insurance covered by the National Authority, the fee being paid by the driver.

If insurance can or must be bought at the race meeting the total cost must be stated in the advance-programme. The cost to the driver must not exceed 90 €.

Do not underestimate your responsibility in accidents occurring during official trials before the verification of the boats and the various documents required. Only verification will make it possible for you to be sure that the driver is insured properly.

205.10 - TECHNICAL PROTECTION

It is forbidden to start the motor with the propeller rotating in the air, except when the boat is launched immediately.

After launching of a boat whose cylinder capacity is more than 750cc, it is forbidden to start the motor or the engine with the afterpart of the boat lifted up and the screw rotating. If it is necessary in order to start a boat of less than 750cc to lift it up, a contrivance must be set up to protect the person(s) who must launch the boat.

This device must leave a free space between the propeller and the above mentioned people.

205.11 - CLOTHING PROTECTION

It is mandatory that cut resistant clothing “with equivalent properties to” Kevlar 29 fabric be worn by participants in all boats without reinforced cockpits. All limbs of drivers in both reinforced and non-reinforced cockpits must be covered. ***A one-piece overall suit must be worn in reinforced cockpits.***

205.12 - LIFTING OF RACEBOATS

Boats intended to be craned in and out of the water, must have slings and hooks certified to lift minimum the double of the minimum boat weight.

The certification must be located on the slings and the hooks.

All hooks used for lifting raceboats must be a closed hook type.

Lifting eyes on the raceboat must be bolted through the construction of the boat, with minimum 8mm bolts and washers of at least 10 sq.cm pr. lifting points.

No boat ***shall*** be lifted over people. ***Lifting a boat with a person on board other than for weighing will disqualify that boat and it's driver.***

No boat can be lifted higher than 0,5 m from the boat's trailer or the ground/water with any person on board, while weighing.

205.13 - TESTING

Testing outside the race circuit and before or after the authorised hours is prohibited during the event.

205.14 - ACCIDENT REVIEW BOARD

At the request of the UIM Commissioner at an event and/or the initiation by the Safety Commission upon receipt of an accident report, an accident review will be held. The Safety and Medical Commission shall oversee and conduct the review. This review may consist of interviews

with pertinent and appropriate parties, examination of the relevant racing equipment, review of relevant reports, etc. in order to determine as much useful information as possible. A report of the findings will be filed with UIM and may be used as the basis for rule changes.

206 - NATIONALITY AND RACE NUMBERS

206.01 - NATIONALITY

The national flag, painted in a rectangle 25 cm x 40 cm on the deck, the hood, or the vertical part of the sides : this flag must be visible on both sides of the hull. The national flag should show the nationality of the drivers license.

206.02 - NUMBERS

A race number is allocated to every driver by his National Authority.

Should two drivers of an international competition have the same number:

- 1) the driver of the inviting nation will change his number following the instructions of the Technical officer.
- 2) If none of the drivers is from the organising nation, the driver who entered last will change his number following the instructions of the Technical Officer.

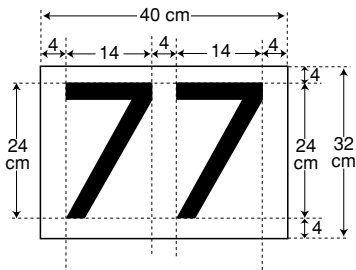
The race number may be painted or fixed :

- 1) on a surface corresponding to the following measures and characteristics : the surface measures 32 x 40 cm and the numbers must be painted in black on a white surface on both sides of the hull in digits of 24 x 4 cm.

The space between the rectangular with the digits must be 4 cm.

The white surface must surround the digits with 4 cm.

All numbers must be plain and upright (no italics). No number shall begin with zero, e.g. for 2 digit numbers (sketch as shown below) :



In case of a 3 digit numbers, the length of the surface must be modified consequently.

- 2) the race numbers may also be painted or glued to the slightly curved surfaces and on one vertical surfaces provided that the whole number is within 30 degrees from the vertical position.

It is recommended to use two digits only.

National Flag and the race number must remain visible during the whole race.

206.03 - LOSS OF RACE NUMBER

The absence of the race number or part of that race number leads to the disqualification of that boat.

206.03 - UIM STICKERS

UIM Stickers must be attached to both sides of all boats participating in UIM titled events.

RULES FOR CIRCUIT RACES

300 - RULES FOR CIRCUIT RACES

300.01 - DEFINITIONS

French	English	Italian	German
Course	Race	Gara	Rennen
Manche	Heat	Prove	Lauf
Tour	Lap	Giro	Runde
Circuit	Circuit	Circuito	Rundestrecke
Parcours	Dist. covered	Percorso	Rennstrecke
Base	Base	Base	Messtrecke

Circuit

A circuit is a closed course defined by buoys where races are to be held.

Circuits must be anti-clockwise.

Distance covered :

Distance covered is the total distance to be covered per heat or race.

Base :

A base is a course expressly measured for record attempts.

Marks of the Course :

Any objects specially indicated as such in the race instructions, are to be considered as marks of the course.

Obstacle :

Any non specified object is considered as being an obstacle to free navigation.

Race :

A race is a competition in speed between boats held in one or more heats.

Event :

An event is one or several races held at the same venue and within one, or more days.

Laps:

When calculating percentages of races in any circuit rules: ONLY full completed laps (Rounded up) can be counted.

(eg; 75% when full race is 7 laps = 5.20. Rounded up = 6 laps)

301 - TIMING

In all international races and national championships the timing is carried out by officially appointed timekeepers using certified timing devices graduated to one tenth of a second. The devices must have a split second system one part of which must be capable of being stopped and restarted or any electronic of the same or better accuracy.

302 - CIRCUIT**302.1**

All circuits must be measured on site by official surveyors or measured on an official chart of not less than 1/25 000.

At U.I.M. titled races the length of circuits must be for :

OSY.400 – S.T.P.

Classes up to 550 cc	Lap of 1000 metres to 1500 metres with maximum of any one straight 600 metres.
----------------------	--

All other classes	Minimum of 1500 metres, with maximum of 850 metres for any one straight.
-------------------	--

The alignment of the start pontoons, or the start line should be such that the pole position has the maximum advantage towards the first turn buoy, compared with the last positions on the starting grid.

If the length of the course cannot be certified, the marks having drifted or for any other reason, the speed is not to be declared in the results (or used in the event of a “dead-heat”)

The organisers must ensure that the position of the buoys does not present any danger to the drivers.

It is recommended that the pits be sited where possible away from the race course on the outside.

302.2 - RACE CONTROL

- a) Race control should be situated so that the circuit can be easily seen and controlled;
- b) The race should be observed by three persons :
 - the O.O.D.; or his assistant;
 - one or two Commissioners or one or two members of the Jury;
- c) The O.O.D.; is the general observer of the race, being in radio or telephone contact with the assistant, the Deputy Officer Of the Day and the rescue team;
- d) The other two supervisors share the circuit in observation;
- e) The race can only be stopped by order of the O.O.D.;

303 - MARKS OF THE COURSE

303.01 - DEFINITIONS

The Marks of the Course are any objects (boat, buoy...) that are specifically designated as such in the racing instructions. The objects that are not designated specifically as such are to be considered as obstacles.

Course turn buoys must be of a single colour of either orange or yellow 70 x 70 cm (28` x 28`) except the mark on the starting/finishing line buoy

which carries a black and white chequered buoy or a white and black striped buoy.

The safety zone (150 m minimum) is defined by at least one buoy which carries a white and red chequered identification or a white and red striped identification.

The buoys are to be made of rubber or similar material.

An Official is posted near the turning marks. He must observe the turning of the marks and report to the Race Committee.

Should any mark be removed from its proper position either by accident or otherwise, the Officer of the day shall, if possible, have it replaced. Should it be impossible to replace the mark in time for the boats to round it, the Officer of the day shall decide whether the race is to be restarted or not.

Protection buoys may be placed on the circuit.

304 - SIGNALS FROM THE SHORE

304.01 - CALL SIGNALS

When a race, or heat has been started a call signal shall be put up to inform the drivers in the pit area which class is the next to start.

A signal consists of a 35 x 90 cm white board, with the class designation (O500, S850 ...) in black.

The signal should be put in a place where it is easily read from the pit area. If two or more classes start together the call signals for these are to be put up together.

If the time to the next start is so long, that it is not necessary to put the boats into the water immediately, the signal can be supplemented by a sign of similar size and colours showing the estimated starting time.

304.02 - FLAG SIGNALS

The flags used for signalling are the same for all series and must be used in all countries.

The flags used for signalling have the following meaning :

Red : to stop the race and mark serious danger on the circuit;

Yellow : danger on the circuit;

Minimum size of red/yellow flags : 1000 mm x 600 mm

Black : The driver to whom the flag is shown must immediately return to the pits where he will receive instructions from a race official. The flag must be accompanied by the race number of the boat to be stopped.

Chequered, white and black : finish;

Chequered, white and red : course mark showing the safety zone;

Green : This is the signal that all boats are ready to go to start procedure.

304.03 - STARTING IN GROUPS

If the number of drivers entered is too large for all of them to start together, the Race committee has to split the group of drivers into several groups.

The Race committee will have each group race a selection race followed by a second chance heat. ***It is recommended that time trials are run to evaluate boats into the two groups instead of random selection.***

Drivers of one same nation must be evenly distributed into separate groups.

Example: based on a circuit that can accommodate 20 boats, with 30 entries.

2 groups of boats ;

After the selection heats :

the first 8 in each group are selected = 16 boats

A second chance heat grouping the 14 boats that have not been selected:

the first 4 in this heat are selected = 4 boats

Participants in the final heats : 20 boats

The number of laps in the qualifying heats will be reduced and this number, which will be fixed by the organiser, must be mentioned in the advance-programme and recalled at the drivers briefing.

A minimum of one-hour interval is required between the last qualifying heat and the first final heat.

The championship will then take place in four heats, the three best heats for each driver being taken into consideration in the final classification.

If necessary, after all the qualifying races, the organisers reserve the right to make up the number of boats in the final races to the maximum permitted from the repechage heat, when any qualified driver indicates that they cannot participate in the final heats. This cannot be done after the first heat.

305 - THE START

305.01

Starts are :

- flying start with clock (14 boats maximum)
- dead engine start.

305.02

After the starting signal has been given, no boats are allowed on the course other than competitors and official service craft.

305.03

Any boat starting late shall not cross the starting line later than 5 minutes after the starting signal has been given in circuit races (heat races).

Any boat starting late is not allowed to start after the first boat which finishes the race has crossed the finishing line.

Any boat starting late is not allowed to start after the delay provided for in the advance-programme in endurance or long distance races.

306 - FLYING START WITH CLOCK, DISCS AND GUN

The clock must have a dial of no less than 2 meters (6ft. 3/4 ins.) and marked out as follows :



This clock is to be driven electrically or mechanically. It has only one hand moving continuously and indicating the seconds.

The official start is given by the clock. The discs and the gun are supplementary to the clock.

The clock must be run for the majority of the training periods.

306.01 - PLACE OF THE CLOCK

The clock must be placed 3 or 4 metres before the starting line and less than 5 metres above the water level.

306.02 - DISCS

A set of five discs, each having a minimum diameter of 40 cm (15 3/4 ins.) pivots in a white frame. One side of the disc is black and the other white. The discs are numbered 5, 4, 3, 2, 1 on the white side. The figures must be 25 cm high at least and 5 cm thick. These discs are worked by hand. It is recommended, to use discs as large as possible.

Discs must be located in a place which is clearly visible to all the drivers. Sometimes, it is necessary to have two sets of discs (one close to starting line and the other easily visible from the boat park).

306.03 - GUN

Any audible signal loud enough to be heard above the sound of the motors. The gun may be replaced by a rocket pistol.

No other signal than those described above are to be used.

306.04 - STARTING LINE

The starting and finishing line is marked by two poles erected on the shore and surmounted by a triangular mark with one top upwards and one top downwards.

Both poles are erected on the shore at different heights placed in line one behind the other adjacent to the jury.

The distance between the starting line and the first turning buoy must be at least 300 metres (984 feet).

The distance between the shore and the inside safety buoys before the starting line must be at least 50 metres(164 feet).

306.05 - SAFETY ZONE

It is compulsory to determine a zone of 150 metres minimum in length marked by one or two buoys with a red and white chequered or striped identification at one end, the other end is the starting line marked by one or by two buoys with a black and white chequered or striped identification.

306.06 - BEFORE THE START

During the five minutes preceding the start, boats must not enter in this safety zone, except for the start.

When the warning signal is given 5 minutes before the start, no boats are allowed to be on the water except racing boats and official boats.

The rules of the road (Rule 312) are also applied in that zone. Any infringement will lead to the disqualification for that particular heat or race.

The organisers using this starting system must use a still or movie-camera or a video tape recorder which must be synchronised with the clock.

306.07 - THE START

The start will be given as follows :

- a) 5 minutes before the start, the class letter is hoisted and a gun fired. The five discs are shown with the numbered side towards the course. The hand of the clock is placed on the zero position (12 o'clock).
- b) 4 minutes before the start, the disc number 5 is turned black.
- c) 3 minutes before the start, the disc number 4 is turned black
- d) 2 minutes before the start, the disc number 3 is turned black
- e) 1 minute before the start, the disc number 2 is turned black, the clock is started

- f) At starting time, the disc number 1 is turned black. The clock is stopped and a gun is fired.

The clock, discs, letters and gun and camera / video must be synchronised.

306.08 - APPROACHES TO THE STARTING LINE

Any boat approaching the starting line other than at a right angle within a zone of 150 metres minimum (495 feet) marked by at least one buoy with a red and white chequered or striped identification (which must be left to port), will be disqualified. Boats can not enter in this zone during the five minutes preceding the start, boats must not navigate in the reverse direction of the course and rules of the road apply.

A boat which has not left after the starting signal, can not pass in reverse direction between the starting line.

A mandatory training period will be held to allow drivers an opportunity to familiarise themselves with the safety zone and approach to the starting line prior to actual competition.

306.09 - BOAT STARTING BEFORE TIME

Boats starting before time are disqualified for that race, heat or series.

Boats which cross the starting line in either direction during the five minutes before the start are disqualified and get zero points in that heat.

If the numbers of boats which started before time are all known the race continues and is valid but the boats which started before time are disqualified for that race (or heat).

If the Race Committee cannot note all the numbers of the boat which started before time, it will restart the race. The leading boat across the starting line that starts before time will be disqualified and not allowed to participate in the restart of that race.

307 - DEAD ENGINE START FROM A QUAY, A JETTY OR A BEACH

307.01

All start pontoons of the relevant classes must be inclined sufficiently so as to not have the start line perpendicular to the line of travel of the start. By derogation, the Cominsport will allow perpendicular orientation during 2005 provided the commissioner's report clearly explains the need.

Boats should be drawn up stern first, engine dead, to a jetty, floating stage or beach.

The jetty, floating stage or beach must be long enough for all the boats starting plus a length equal to 0.5 metres for each boat. Otherwise the number of boats starting must be reduced.

There shall be no buoys in front of any start pontoon except in case of extreme need where there must be only one buoy. The drivers taking part in the start with a buoy may be split into two groups, taking into consideration all safety aspects.

Positions on the jetty are determined by individual qualifying times (if required) or positions in qualifying heats.

When qualifying heats are run the jetty positions for the final heats are allocated by the times from the two heats. The qualifying boats from the "repechage" (second chance) heat are positioned at the end of the jetty.

307.02

The distance between the start line and the first turn buoy must be in a straight line and of at least 300 meters.

At dead engine start drivers must keep their lane from the start toward and through completion of the first turn. From then on it is the ordinary rules of the road, as mentioned in § 312, that will apply. Breaking this rule will result in disqualification.

It is prohibited to stand up during start and while racing.

At all dead engine starts at international titled races, the start may be videotaped (or similar). This is to be done from a position as high as possible and/or from a place in extension of the start lane.

307.03

The start will be given as follows :

Two minutes before the red light is switched on a gun is fired or a flag signal is given. Alternatively the time can be lengthened to 3 minutes if notified at driver's meeting.

One minute before red light a "1" minute signal is given.

30 seconds before red light a "30" signal is given.

Engines must not be run after this signal.

At the end of the 30 seconds the red light is switch on.

After 5-12 seconds the red lights are switched off, this is the signal to start.

307.04

Those drivers running their engines during the last 30 seconds before the start are disqualified.

Those drivers starting their engines or leaving the pontoon whilst the red light is on lose one lap.

Those drivers rotating their engines (with ignition switched off) prior to the red lights going off lose one lap.

After an announced time the red light will be shown again.

Late starters : A driver who has failed to start before the red light is shown again, must wait until the whole field has passed the start jetty and the pontoon marshal indicates that he may join the race in the proper way.

The O.O.D. may decide to cancel the start by showing the red flag and recommence the starting procedure.

Any boat starting unsuccessfully must be returned to shore. Any crew member swimming, or wading, out to a boat will cause his entry to be disqualified.

308 - RACE CANCELLED

The Race Committee has the power to cancel any race should unfavourable weather or other serious circumstances render such action necessary.

309 - RACE POSTPONED

309.01

The Race Committee has the right to postpone a race, in which case the letter "R" and the class letter are hoisted. Races this postponed are subject to written notice on the notice board.

The Officer of the day may postpone the start, even after the first signals have been given, but only in cases of force majeure or if an error in the starting signals has been made.

309.02

In both cases, signalling is to be recommenced. When the need for postponement has passed, the Race Committee signals the start according to the instructions contained in the programme.

New entries are not to be accepted for postponed races.

Only drivers who duly entered and were present when the race was postponed are admitted.

309.03

A race or heat must in no case be postponed more than 24 hours. If it is impossible to race within this time, the other races or heats held will be used as a basis for classification.

310 - RACE SHORTENED

A shortened race is a race over a course which has been shortened by the Race Committee before the start.

If a race is shortened due to force majeure, it is at the discretion of the OOD (and the U.I.M. Commissioner if present) to alter the time table, and if necessary the course layout accordingly.

If the race is declared to be on rough water, the number of laps should ideally be maintained, but may be reduced if seen fit by the OOD. If time permits, the circuit layout can be changed, as agreed by the OOD, U.I.M. commissioner and the driver's representative.

If there is a change to the race course after time trials, there may be extra fee practise, if time permits.

The decision with instructions about the shortened course must be handed in writing to the drivers and the timekeepers.

311 - RACE STOPPED AND RE-STARTS

311.01 - GENERALLY

1. A stopped race is a race interrupted by the OOD after the start.
2. The stopping of a race is the sole decision of the OOD and officials specifically mandated by the OOD. A signal to stop a race will be the display of a red flag and/or a red flare or red smoke. ***It is recommended that the start lights are turned toward the general direction of racing after the start and switched on, in a red flag situation as an additional warning.***

3. A race is also stopped when anybody is in the water on the course or is considered to be injured, and/or continuation of the race constitutes a danger to those involved.
4. Drivers whose actions result in a stoppage shall not be scored or restart. If the sanctioned driver protests, he shall be permitted to restart, the result being subject to the decision of the jury. So that the racing is not delayed, and protest about the inclusion of a driver after stoppage may, at first, be verbal. A normal written protest must be submitted after the end of the heat or race.
5. Any penalty incurred in any heat is carried forward to any restart of that heat or race.
6. Disqualified drivers shall not restart (unless under protest see 4).
7. Jetty positions are the race positions at the end of the lap prior to the stoppage.
8. Drivers who do not succeed in reaching the jetty with the power of their own engines shall not restart. This rule applies only for classes with electric start.
9. Repairs may only be carried out on boats that have returned to jetty (Rule 8). Repaired boats may rejoin the race as directed by the jetty marshal. Further definition of the repair areas will be given at drivers briefing.
10. In the event of a restart any repaired boat must be ready to take up its correct position on the jetty as directed by the jetty marshal. If the boat is not ready it must start from the bottom end of the jetty. If the race has already restarted the driver can only start under the directions of the jetty marshal.
11. All boats that are ready and eligible to race may start in a restarted race.

311.02 - RACING IN HEATS

1. When the leading boat has completed less than 70 % of the required laps the race shall be restarted.

2. There shall be only one restart.
3. Laps gained in previous starts are discarded.
4. The restart shall be for the original number of laps and re-fuelling is permitted. Place for re-fuelling shall be indicated by the organiser.
5. Only the drivers scored at the lap before the stoppage shall be eligible to restart.
6. When a race is stopped after the leading boat has completed at least 70 % of the required laps there is no restart and full points are awarded.
7. Points for a restarted race which is also stopped are :
 - up to 35 % of the required laps : 50 % POINTS
 - over 35 % of the required laps : FULL POINTS

Up to 35% means: 'After lead boat has completed at least one full lap and can all be identified by timing control. Failure means zero points.

Minimum one full lap and provided timing control has been able to identify.

311.03 - SINGLE HEAT RACING

1. Eligible drivers may rejoin restarts even if not in previous starts.
2. The race shall be restarted when after having deducted one lap for each stoppage the remaining laps are at least 20 % of the original total.
3. Eligible drivers must return directly to the start, no outside assistance, no refuelling.
4. The number of laps to be scored are those at the end of the lap prior to stoppages.
5. Laps scored from the original start and all restarts shall be aggregated for final positions.

312 - RULES OF THE ROAD

312.01

When two boats are approaching one another so as to involve risk of collision, one of them must keep out of the way of the other as follows :

312.02

When two boats meet head on, each must alter the course to the right side.

312.03

When two boats are crossing, the one which has the other on her right side must give way;

312.04

Where by any of these rules one of the two boats is to give way, the other must keep her course and speed.

312.05

Every boat, which is directed by these rules to give way to another boat must, if the circumstances of the case admit, avoid crossing ahead of the other.

312.06

Every boat which is directed by these rules to give way to another boat must on approaching her, if necessary, slacken her speed, or stop or reverse.

312.08

In obeying these rules, due regard must be made to all dangers of navigation and collision, and to any special circumstances which may render a departure from the above rules necessary in order to avoid immediate danger.

312.09 - OVERLAP

An overlap may only be established when two boats are on the same course or approximately on the same course and the overtaking boat has no longer a free choice on which side he shall pass.

312.10 - OVERTAKING

Any boat overtaking any other must give way to the overtaken boat.

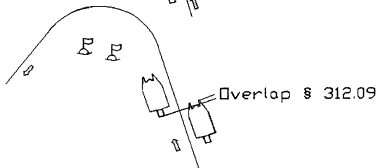
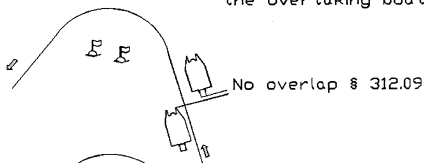
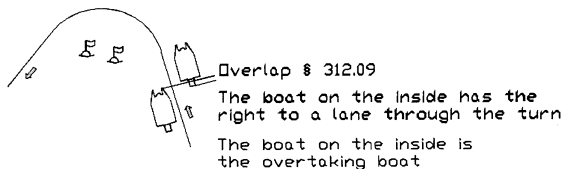
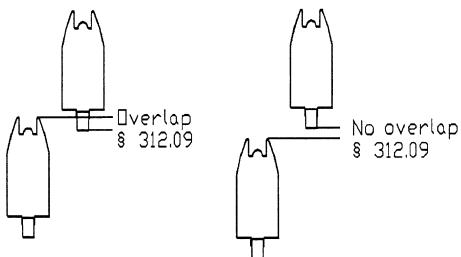
An overtaking boat shall keep clear of the boat which is being overtaken.

312.11 - PASSING MARKS

When trying to overtake a boat on the inside, the pilot must foresee the direction of the boat he is overtaking and take all the necessary precautions including stopping the boat in order to avoid a collision.

When passing a mark and an overlap has been established, both boats must maintain their lanes, i.e. they must maintain their distance from the mark until the mark has been rounded.

Overlapping while overtaking does not give priority to the boat overtaking on the inside.



312.12 - ALTERING COURSE

When one of the boats is obliged to keep clear, the other shall not alter her course, to avoid risk of fouling.

A boat must not bear out of her course so as to hinder another in passing to the right.

313 - ROUNDING AND FOULING MARKS

313.01

All marks must be on the left side of the boat unless otherwise stipulated in the advance programme and repeated during the drivers meeting.

On circuits with both left and right hand turns, orange turn buoys must be for left hand turns and yellow buoys must be for right hand turns.

313.02

A boat which rounds a mark in the wrong way loses a lap. For safety reasons, a boat is not allowed to round the mark again after rounding it the wrong way. A boat which rounds a mark again is disqualified.

A boat is considered as having rounded a mark in the wrong way when the buoy official considers that it has passed the mark on the wrong side after starting to negotiate the turn.

313.03

Every turn on the race course should have two turn buoys.

In the following text first and second buoys refers to the sequence in which they are damaged or destroyed by a boat, not to their position in the turn and the order in which they are to be passed.

In this case, if a driver :

- touches the first turn buoy in a turn : no penalty
- damages or destroys the first buoy : one lap penalty and a fine that will be decided by the Organising Committee and announced in the advance regulations

- touches the second buoy : no penalty
- damages, destroys or dislodges the second buoy : disqualification and a fine that will be decided by the Organising Committee and announced in the advance regulations.
- In the case of a single buoy, penalties will consider that buoy as the second buoy

The penalty lap will be deducted at the end of the heat.

Recommendation : the two buoys should not be tied up together.

313.04

A driver who is forced on to a mark by another driver may lodge a protest against that driver.

314 - FOULING COMPETING BOATS

If a boat, in consequence of her neglect of any of these rules fouls another boat, or compels others to foul each other, she is disqualified.

315 - MEANS OF PROPULSION

All methods of propulsion other than motor and paddle are prohibited.

The action of the wind and current will not entail the disqualification on the competitor.

317 - FINISHING A RACE

317.01 - THE FINISH

The finish is shown to a driver by waving a black and white chequered flag.

317.02

A driver who finishes a race or heat must withdraw from the course without hindering the boats that are still in the race.

Drivers have to fulfil at least one extra lap after crossing the finishing line, unless the organiser specifies something else.

A driver who has finished that race and cuts across the course will be disqualified in that race or heat and get zero points.

A boat is timed as completing a race when her stem crosses the finishing line. After finishing the race, the driver must continue to observe any special regulations prescribed by the Race Committee as to keeping clear of the finishing line and the course.

317.03

The Race Committee has the right to stipulate the maximum duration of each race.

Races for the international classes are limited to 4 consecutive days.

For races on the sea, an interval of consecutive 24 hrs might be conceded, if the water conditions justify it.

318 - SCORING SYSTEM

318.01

The boat placed first will be the one having covered the scheduled distance in the shortest time.

All boats will be classified by taking into account the number of complete laps that they have covered and for those, which have completed the same number of laps, the order in which they cross the finish line.

Unless otherwise specified, the following scoring system is to be adopted in all races consisting of two or more heats :

1	-	400 pts	11	-	22 pts
2	-	300 pts	12	-	17 pts
3	-	225 pts	13	-	13 pts
4	-	169 pts	14	-	9 pts
5	-	127 pts	15	-	7 pts
6	-	95 pts	16	-	5 pts
7	-	71 pts	17	-	4 pts
8	-	53 pts	18	-	3 pts
9	-	40 pts	19	-	2 pts
10	-	30 pts	20	-	1 pts

318.02

To be awarded points a driver shall have completed at least 75 % of the race winner's distance and cross the finishing line after the leading boat has taken the chequered flag.

Only full completed and recorded laps are taken into account when calculating % (Rounded up)

[e.g. Heat of 7 laps. $75\% = 5.25$ laps. So round up to 6 laps]

For drivers with the same total of laps, their finishing order shall be determined by their positions at the finish of the race.

For single heat races

Boats that complete 90%, or more of the race distance, but do not cross the finishing line after the winner has crossed the finishing line, will be classified after those boats that have done the same number of laps and also cross the finishing line after the winner.

For a race stopped, the finishing order is that existing at the end of the lap prior to the stoppage.

Organisers may limit the race to a given time after the winning boat has finished, this must be mentioned in the advance programme.

318.03

The official results must be posted on the official notice board within an hour of the end of the races.

318.04

A general classification of different classes racing together is not permitted.

318.05

National classes may be raced at international meetings.

318.06

If boats of different series or classes start together every boat must be classified within its own class.

When the programme for a race meeting does not provide races for all classes, boats not provided for may race in the next higher class in the programme, subject to their acceptance by the Race Committee.

318.07

It is forbidden to take part in a race for a lower class than that to which a boat belongs except with the written consent of all competitors.

The Race Committee may hold a race, as a separate class, for one-design hulls equipped with identical motors.

318.08

If post-race inspection reveals that a boat or motor is illegal, that entry shall be disqualified for each heat in which it raced, and the order of finish shall be corrected for each heat. Final overall race positions shall be obtained by adding the points from the heats after the order of finish has been corrected for each heat.

319 - DEAD-HEAT

319.01

Two boats are in a “dead heat” when :

- They cross the line together
- They obtain the same points in a points race

319.02

In case of a dead heat, the boat having the higher average speed, in any heat, will be declared the winner.

Only full completed heats can be used for this purpose.

If the course has been changed considerably between heats, the average speed cannot be used to declare a winner. Then, the total of the smallest time difference in percent to the winning time in each point giving heat is the winner. The decision about different course has to be done and the drivers informed before the last heat.

319.03

If the above has been applied, and there is still a “dead heat”, the boat having a higher average speed on any lap in any heat, will be declared the winner.

320 - POSTING OF THE RESULTS

The results of each race must be posted at the pits in a clearly visible position, specified during the driver's meeting.

321 - INSPECTION OF HULLS AND ENGINES

321.01

Upon completion of a race the engines must remain sealed to prevent any alterations being made.

Refusing to present the engine or the boat entails disqualification.

321.02

It is compulsory for a driver to make arrangements (tools and special tools and mechanics) so that his motor can be disassembled if the Technical Officers at a post race inspection want to check any part of the motor.

Both driver or/and mechanics shall work as efficiently as possible so that inspection duration can be kept to a minimum.

Engines and boats will be placed pending inspection in a well defined place, which remains under the care of the “ boat park ” official. This is called the “ parc fermé. ”

321.03

After the pre-race technical inspection, the boats must not leave the boat park except for practising or racing.

321.04

If any repairs can only be effected outside the boat park, they must be carried out in the presence of a Technical Officer.

321.05

Officers allowed to attend the inspection :

- The Officer of the Day

- The delegate of National Authority of the driver who's boat/engine is being scrutineered.
- The measurers named by the organising committee
- The driver and a mechanic if necessary
- The U.I.M. Commissioner(s)

The presence of other persons is only allowed if authorised by the Officer of the day.

322 - PRIZES

322.01

Prize giving cannot take place until at least 1 hour after posting of provisional results and NOT BEFORE the technical post race inspection is finished and a protest time against its results has expired.

The Race Jury, in the meantime, must not be dismissed.

It is recommended that immediately after the final race, the provisional first three drivers are paraded in front of the spectators, and introduced to the public and to the media on the official podium.

322.02

A National Authority may ask to reduce the number of prizes according to the number of competitors. However, the value of the prizes must be maintained, as provided for in the advance-programme. This possibility must be explained in the advance-programme.

322.03

In S, T and P series Titled events, the final heat must be completed at least 2 hours before the scheduled time of prize giving to ensure the technical scrutineer has enough time to find the correct winners.

323 - REPORTS

323.01

At the end of the meeting, the Secretary of the Race Committee must send to the Sports Commission of his National Authority a report including :

- 323.011** Classification and distribution of prizes :
- 323.012** Report of the timekeepers;
- 323.013** List of drivers;
- 323.014** Penalties :
- 323.015** Documents on any records achieved in conformity with the U.I.M rules;
- 323.016** List of protests and decisions relating to these protests;
- 323.017** All necessary documents in order to evaluate the meeting or the race;
- 323.018** These documents must be sent to the Sports Commission of the National Authority within 8 days of the end of the meeting or the race;
- 323.019** After each titled meeting, the organisers must send the results to the Secretariat of the U.I.M.
- 323.020** If the results are taken into consideration for the granting of a trophy or a prize, the points must be sent to the U.I.M. by fax and/or e-mail within 48 hours;
- 323.021** 3 days after a competition, the organising club must send the final result to the National Authorities of all the participants and to the Secretariat of the U.I.M.
- 323.022** Details of technical disqualifications of drivers to be reported to their National Authorities per rule 502.07.

400 - JURISDICTION

401.01 - DEFINITIONS

Jurisdiction applies to :

The U.I.M. Offshore, Circuit, Aquabike Rules.

402 - JUDICIAL BODIES

402.01 - THE INTERNATIONAL JURY

The Jury hears any protest raised by a driver.

1. Composition

The Jury must be composed of at least three members, the Chairman included. The Organisers of the event appoint the Chairman. Each competing country, including the hosting country, may appoint one member delegate who can represent only one country. These appointments must be made in writing, authorised and signed by a National Authority Official and delivered to the event Secretary.

If there are not enough NA appointed members to form a Jury of three persons, the Organisers must appoint additional members, but only enough to reach three persons.

The Jury composition will be announced at the first drivers meeting. The jury must be present at the first drivers meeting.

Race Officials can not be Jury members.

The U.I.M. Commissioner appointed to the event is a full member of the Jury.

In an event comprising races of several classes there may be a different Jury for each class.

2. Authority and Duties

The Jury sits as long as the event lasts, and hears all protests relating to that event.

The jury must decide as soon as possible on all protests presented to it during the event.

402.02 - INTERESTED PARTIES

No member of a Jury who has taken part as a competitor or Official in the race concerning which a judgement is to be given, or knows himself to have directly or indirectly an interest in one of the parties involved, may judge the protest.

However, a member of an International Jury shall not be disqualified even if one of the parties is from his NA.

402.03 - THE U.I.M. APPEALS BOARD

The U.I.M. will decide on Appeals arising from International events according to the provisions laid out in Rule 405.1.

In each case, the U.I.M. Executive Committee, or in case of an appeal lodged against an Executive Committee's decision, the appointed neutral Council member will nominate three members, one to be Chairman after consultation of the relevant commission. The members must not be from the National Authorities of the parties to the case.

One member will act as Clerk to the Board.

403 - PROTEST PROCEDURES

403.01 - VALIDITY OF PROTESTS

Only a driver may protest against facts pertaining only to the races in which he takes part.

Joint protests signed by several drivers will not be considered.

All protests must be in writing - type written or hand written in printed characters in English.

It must state the reason for the protest and be accompanied by any relevant documents that shall provide evidence within the given time and any protest fee as stipulated by the Race Organiser.

All protests must be signed by the protester.

The protest must be handed to the Race Secretariat who must, in the presence of the protester, record the time of receipt.

Any costs involved in verification are to be borne by the losing party.

A written protest can be withdrawn by the protester ; he then loses the protest fee.

403.02 - PROTEST FEE

The amount of the Protest fee must be decided by the National Authorities and it must be published in the advance programme. The maximum amount for a Protest fee will be decided by the U.I.M. General Assembly. It will be 80 €, or an equivalent sum in the national currency.

The Jury must determine whether the protest presented to it, has been lodged according to the rules. If found to be invalid, the reasons must be given to the protester.

If a protest is upheld by the Jury, the fee must be returned to the protester.

403.03 - RIGHT OF PROTEST

A protest can be lodged against the posted provisional results or against any decision made by the Race Committee or against one or several competitors.

A protest can only be lodged on a subject which directly concerns the protester himself. A protest which complies with these rules can not be refused by the Jury.

403.04 - TIME OF LODGING A PROTEST/ PROTEST PERIOD

A protest regarding the qualification of a boat, a motor, or a driver must be made before the drivers briefing. Starting in a race is considered as acceptance of the conditions and the eligibility of the other competitors. The only exception to the above is when the protester can prove the facts were not given to him before the drivers briefing, then a protest can be lodged up to one hour after the posting of the results.

Any other protest must be lodged within one hour of the first results being posted with the following exceptions:

A protest against new results can be lodged within one hour of these new results being posted, but only concerning the decision, which lead to the changing of the results.

A protest against any other decision, posted after the posting of the first results can be lodged within one hour after the posting of this decision.

403.05 - JUDGEMENT

Decisions by the Jury shall be reached by simple majority of votes. In the case of equal votes, the Chairman's vote will be the casting vote.

Any judgement pronounced by the Jury stays in effect until altered by the Appeal Board.

403.06 - NOTIFICATION OF DECISION

All decisions by any jury must be notified to the concerned parties in writing, including their right of appeal.

403.07 - AVAILABILITY OF DOCUMENTS

All documents relevant to any decision by the Jury must be filed with the organising N.A. and held available for a higher authority until the time limit for an appeal has expired.

404 - HEARING OF THE PROTEST

404.01 - RIGHT TO A HEARING

Any person being party to a protest, or being charged for an offence against the rules shall have the right to be heard before the Jury in order to defend himself.

The Jury must give notice to all parties of where and when the hearing will take place.

If any party duly notified, fails to appear without giving an acceptable reason, judgement can be rendered by default.

404.02 - WITNESSES

Each party may bring witnesses to the hearing.

Any costs incurred by the appearance of witnesses shall be borne by the respective parties unless decided otherwise by the Jury.

404.03 - THE HEARING PROCEDURE

The following procedure must be followed in all hearings unless otherwise stated elsewhere in these rules :

- Written minutes must be taken.
- All parties to the case are entitled to be present and to hear and question all evidence at the hearing up to the time the Jury makes its decision. The protest, shall be read out to the parties.
- The parties shall be invited to state their cases. The parties may call witnesses. Each witness, after having given his account of the case, may be questioned by all parties and by the members of the Jury.
- Any member of the Jury who is familiar with the case may give his evidence
- The parties shall be entitled to question each other and any member of the Jury who may have given evidence.

- The witnesses shall withdraw and the parties shall be invited to make a final statement of their cases.
- The Jury may recall any party, previous witness or new witness and call on other evidence.
- The parties must be present during the whole of the recall and must be given the opportunity to question any new evidence after which they may re-make their final statements.

405 - THE APPEALS PROCEDURE

405.01 - APPEAL BOARD

The members of the Appeal Board may hold other sports functions and consist of a Chairman and two members.

The Appeal Board sits whenever the Chairman calls a meeting.

405.02 - INTERESTED PARTIES

No member of an Appeal Board who has taken part as a competitor or Official in the race concerning which a judgement is to be given, or knows himself to have directly or indirectly an interest in one of the parties involved, may judge the appeal.

405.03 - RIGHT OF APPEAL

A decision by a Jury may be appealed by the parties involved when they consider that an injustice has been made against them and/or fresh evidence has been made available that may alter the decision of the Jury.

A driver can also appeal against :

- a penalty, imposed by the U.I.M. Executive Committee, or imposed by the OOD for the reason of post race technical scrutinizing

- new results, but only concerning the decisions, which lead to the changing of the results,

without the prior lodging of a protest, but only if this was not possible, because the race jury has had dissolved itself before or within the protest period after the finish of the event.

405.04 - NOTICE OF INTENTION TO APPEAL

The intention to appeal must be notified in writing to the Secretariat of the U.I.M. by the interested party within four days of the day following the Jury decision or in the case of a decision against which a driver can appeal without the prior lodging of a protest within four days of the day following the notification of this decision to the NA of the driver or if earlier to the driver himself.

405.05 - TIME LIMIT

All appeal documents and fees must be received within ten days from the date when the notice to appeal was received by the U.I.M. Secretariat.

405.06 - LODGING OF AN APPEAL

The appeal must be sent by telefax, letter or E-mail and it must be signed by the appellant. The reasons for the appeal must be stated.

Address, telephone, e-mail and/or fax should be clearly mentioned on the appeal, so that the U.I.M. can send any correspondence directly to the driver with copy to relevant N.A.

405.07 - NOTIFICATION OF THE PARTIES OF THE APPEAL

The U.I.M. Secretariat must within two days from receiving the appeal, notify by telefax or letter the other parties that an appeal has been lodged and that they may send a rejoinder.

Eventual rejoinders must be received within ten days from receiving this notification.

405.08 - TIME LIMIT FOR DECISIONS ON APPEALS

All appeals must be determined no later than forty-five days after the date when the appeal documents and fees were received by the U.I.M. Secretariat.

405.09 - APPEAL BOARD DECISIONS

The Appeal Board takes its own decision based on every information available. The decision of the jury may be upheld, changed or not upheld. The decision of the Appeal Board is final.

405.10 - COSTS

The appeal fee is 1525 €. In addition the driver has to pay 2290 € to the U.I.M. for possible costs of the appeal committee (meetings, telephone or fax costs). The appeal fee for a single championship/invitation race (not a Formula race, or a WOC race or similar) is 760 €, in addition the driver has to pay 760 €, to the U.I.M. for possible costs of the appeal committee.

The driver who appeals has to pay for the meeting costs anyway if the appeal is upheld or not. If the costs of the appeal committee is less than 2290 € then the difference will be paid back to the driver.

The appeal fee of 1525 € will be refunded if the appeal is upheld. If the original decision is changed by the appeal board (405.09), it can decide to refund a percentage of the appeal fee if appropriate.

The driver may agree to forego an Appeal Committee meeting and for the appeal to be handled through telephone, E-mail and fax, if possible, in order to minimise the cost for the appeal.

406 - PENALTIES

406.01 - GENERAL

Any proven breach of applicable Rules may be penalised.

If the rules do not determine a special penalty for the relevant breach, the penalty to be given has to be proportional to the seriousness of the breach. The proportionality is at the discretion of the decision maker.

The U.I.M. Executive Committee or the relevant National Authority shall only impose penalties, when they deem the relevant infringement to be so serious, that it has to be penalized although the deadlines for penalizing or lodging a protest have already expired. After the expiry of the aforementioned deadlines, a penalty for an ordinary breach of the rules shall in general not be imposed.

The Race Jury or the Appeal Board following a protest or an appeal can (only) impose penalties, which can be imposed by the OOD.

406.02 - DEADLINES FOR PENALIZING

The OOD and the U.I.M. Commissioner can only impose Penalties until the moment, the first results of the race are posted.

The National Authority and/or the U.I.M. Executive Committee can only impose penalties within 3 month from the day, the infringement occurred.

These deadlines are not valid for penalties imposed by the OOD for the reason of post race technical scrutinizing. In this case, the penalty has to be imposed as soon as possible after the receipt of the scrutinizing results.

406.03 - UNACCEPTABLE BEHAVIOUR

In order to protect the interests of the sporting community, the following actions may also be penalised by the OOD, by the U.I.M. Executive Committee or by the NA of the Licence Holder.

- any deliberate act taken to gain unfair advantage.

- any false act made or statement given with the intention of suppressing facts required for the proper conduct of the race.
- any attempt to bribe or the taking of a bribe.
- any abusive or unsportsmanlike behaviour.

406.04 - REPRIMAND

A Reprimand is a notice of disapproval of an unacceptable action. It must be recorded by the Race Secretariat. A reprimand must be witnessed. A reprimand automatically constitutes warning that if the offence recurs, a heavier penalty will be given. A Reprimand is valid for 12 month.

A reprimand can be given by the OOD, or the National Authority of the Licence Holder.

406.05 - YELLOW CARD

A yellow **or red** card may be given by the OOD or U.I.M. Commissioner.

A yellow **or red** card will be confirmed on **UIM** form which will be posted with the results **and if it is confirmed it must be included in the commissioners report. The recipient must be notified privately before a red card is posted.**

The driver has one hour from the posting of the penalty to protest (rule 403). If the protest is not upheld he can make an appeal (rule 405).

All yellow/red cards count towards the drivers CV.

Yellow/**red** cards can be given for **bad behaviour on the water including :**

- up to two yellow cards at a time for dangerous driving ;
- **one** red card for extreme **blatant** dangerous driving.

A driver with a red card is immediately prevented from taking any further part in the event or further heats if a multi-heat race.

A driver **with** a red card or three yellow cards will **also** lose the right to take part in the next titled UIM race in his classes//formulae. Then all cards are removed.

All **yellow** cards will remain valid for the four events in which he competes following the date on which the last **yellow** card was delivered.

Yellow/**red** cards must be notified to the U.I.M. and **the drivers'** National Authority within three days.

The U.I.M. will notify all National Authorities **when** a driver **has one red card or** three yellow cards and is **therefore** suspended from international competition.

406.06 - DISQUALIFICATION

Disqualification deletes a competitor from the results of the heat or race where the offence occurred.

Disqualification is done by the OOD.

406.07 - TEMPORARY SUSPENSION

Temporary suspension suspends a competitor, a competitor together with crew, a crew member, or an Official from all or part of an event. Temporary Suspension may be given for serious indiscipline.

Temporary Suspension will be imposed by the OOD.

406.08 - PROLONGED SUSPENSION

Prolonged suspension can be imposed on a competitor, an Official or an organisation for deliberate fraud, repeated or very serious indiscipline or very serious misconduct.

Prolonged Suspension can be imposed only by the NA of the licence holder or organisation, or by the U.I.M. Executive Committee.

A Prolonged Suspension is always international. The NA must immediately inform the U.I.M. of a Prolonged Suspension and the U.I.M. must inform all other NA's.

406.09 - EXCLUSION

Exclusion means a permanent loss of all rights to take part in any activities falling under the U.I.M. and its affiliated organisations. A person or an organisation who has committed a moral or sporting offence of extreme gravity is liable to be excluded.

A sentence of exclusion can be pronounced only by the NA of the licence holder or organisation, or by the U.I.M. Executive Committee.

Exclusion is always International. The NA must immediately inform the U.I.M. when it has imposed a sentence of Exclusion and the U.I.M. must inform all other NA's.

In the case of suspension, prolonged suspension or exclusion, all National Authorities and therefore their affiliated clubs bind themselves to respect the decision and apply it in their own country.

407 - PRIORITY OF THESE RULES

The course of the U.I.M. has to be exhausted, before the dispute can be brought before civil court.

408 - POWERBOAT PROTEST FORM

PART A

is for you, the driver, to complete and hand in within the specified time limit.

PART B

Is for you, the driver, to complete and give to the Protest Jury Chairman at the beginning of the Protest Meeting.

DRIVERS PROTEST FORM PART A

SECTION 1

NAME OF EVENT: _____ DATE OF RACE OR HEAT: ____

HEAT NUMBER: _____ CLASS: _____

SECTION 2

YOUR NAME (PROTESTOR) _____

YOUR BOAT NUMBER: _____ BOAT NAME: _____

SECTION 3

I (the Protestor) am protesting against: (tick ✓ as appropriate)

- ☐ To seek redress from the Race Organising Committee for actions or omissions.
- ☐ Another driver, name: Boat No:
- ☐ The Results as posted

SECTION 4

Which rule has been infringed _____

Time of incident: _____

On which lap was the incident _____

Where was the incident _____

SECTION 5

Your signature _____

Time of signature _____

SECTION 6

You must see this section completed by the official receiving this Protest:

Name of official: _____

Time Protest received: _____

Protest fee received: _____

PART B

SECTION 7

Either, explain incident with another driver; in writing and by drawing;

Or, explain your reason for Protesting against the Race Committee:

500 - INTERNATIONAL SERIES AND CLASSES

500.01 - DEFINITIONS

The general racing rules and the rules governing records are applicable to all international series.

By boat we understand any vessel used in powerboating (hydroplane, monohull, catamaran etc....).

500.02 - INTERNATIONAL SERIES AND CLASSES

The following example serves to clearly define the words "series" and "class":

The series of Racing Outboards is divided into eleven capacity classes being : 175cc, 250cc, 350cc, 500cc, 700cc, 850cc, 1000cc, 1500cc, 2000cc, 3000cc and ∞ cc.

The request to accept any new international series or class has to be presented by at least three National Authorities.

All series and classes for which the rules have been drawn up by U.I.M. are international.

All national bodies are bound to observe these rules without any alteration whatsoever. Only U.I.M. has the right to change the rules.

500.03 - RECOGNISED SERIES AND CLASSES

All national series and classes for which the rules have been properly established by a National Authority are recognised by U.I.M.

500.04 - ENTRY LEVEL RACING CLASSES

U.I.M. proposes to establish, in each National Authority, Junior classes (as from 12 years of age) in conformity with the national legislation.

U.I.M. will publish the list (unrestrictive) of classes already in operation (i.e. APBA classes, OSY 400, etc.).

U.I.M. reserves the right to propose other classes and to draw up, whenever required in the future, international rules permitting competition between countries.

500.10 - RULES - FORMULA FUTURE

1. PARTICIPANTS AND EQUIPMENT

1.1 Participants

Classes (5 age groups)

Class 1	Girls and boys	8 - 9 years	(until 31 of December)
Class 2	Girls and boys	10-11 years	(until 31 of December)
Class 3	Girls and boys	12-13 years	(until 31 of December)
Class 4	Girls and boys	14-15 years	(until 31 of December)
Class 5	Girls and boys	16-18 years	(until 31 of December)

In order to participate in the competition girls and boys must be able to swim and have a relevant waiver of responsibility signed by their parents or tutors.

Teams set up by the educational institutions, sport clubs and other organisations can take part in the competitions. Each team includes two competitors (pilots) from every age group.

Each team must have an official adult representative and a trainer (coach).

A team nominates a captain.

The drivers must wear sports clothes and low sole shoes with no heels.

The international jury composing of at least 3 members representing different competitive parties should be set up.

In case the participating Nations are more than 3 (three), the Jury's members should be enlarged in their number in order to have the representation of each nation; on the other hand the 3 (three) members, as mentioned above, are to be chosen with public drawing among the

Official Delegates. The composition of the Jury must be exposed before the driver's registration.

1.2 Boats and engines

Boats, engines, propellers and other equipment indispensable to run a race are provided for by the organisers. The above boats, engines, propellers and other equipment should be of usual commercial production.

Drivers in classes 1-3 (8 to 13 years old) use RIB type boats, powered by an outboard engine with a minimum power of 5 h.p. (3.7 KW) and a maximum power of 8 h.p. (6 KW), with manual engine steering (tiller arm), if possible with automatic reverse and must be equipped with 2 (two) separate kill switches.

Drivers in classes 4-5 (14 to 18 years old) use RIB type boats, powered by an outboard engine with a minimum power of 10 h.p. (7.3 KW) and a maximum power of 15 h.p. (11.2 KW), with steering wheel, speed lever, seats for the drivers and must be equipped with 1 (one) kill switch.

Boats certified for the races must have the following dimensions (sizes) :

- Length max. length – up to 3,6 m
- Width 1,3 – 1,7 m

When registering, the weighting of the driver is to be conducted. The balancing of the driver's weights by the additional weight is to be done within each class.

Statistical average weight :

Class 1 girls and boys	8-9 years	32 kg
Class 2 girls and boys	10-11 years	40 kg
Class 3 girls and boys	12-13 years	52 kg
Class 4 girls and boys	14-15 years	64 kg
Class 5 girls and boys	16-18 years	71 kg

For participant with lower weight, ballast must be used, securely attached to the boat, to be located near the driving position. In the relevant classes, the co-drivers must be the same. Alternatively they may be different but carrying ballast located near their position, to have the same weight.

At the end of the drivers weighing procedure, the list of them for each Nation must be exposed with indication of the race number, name and surname of the pilot, sex, date of birth (day/month/year), the weight verified and the possible ballast. For classes 1-3 also the name of the co-driver must be indicated.

1.3 Personal numbers for the drivers

When passing a registration procedure each participant receives a personal number and a registration form. It is obligatory for the driver to wear a personal number at the time of the race. This personal number presents a white-colour square of 25 cm to 25 cm and has black figures of 20 cm high.

1.4 Buoys

Inflatable buoys are to be used for **all** events with **a minimum of 70 cm** high, marked in order to easily visualize eventual rotations caused by touches **showing above water level**.

All gates must have a width of 2,50 m between buoys.

Red colour buoys show gates and turns.

Yellow colour buoys show the final gates.

Blue colour buoys for the final turn for 5 class.

White colour buoys for start-finish.

1.5 Flags

All flags should have a dimension of 80 to 80 cm.

Green : Start (departure)

Yellow : Danger on the circuit

Red : Stoppage of the race

Chequered : Finish

2. RACE SCHEDULE

2.1 Advance programme

All participants and official representatives of the teams must familiarise themselves with the Rules. Any modifications to the Rules should be submitted to teams representatives not later than 45 days before any race.

It is mandatory that a drivers meeting be held at the jury office after arrival and registration of the participants.

2.2 Racing format

Race includes 3 stages :

- | | |
|---------|----------------------------------|
| 1 stage | Free practice |
| 2 stage | Manoeuvring |
| 3 stage | Parallel slalom with final races |

2nd and 3rd stage will be treated as separate championships. Scoring the results will be awarded according to U.I.M. rule 318.

At the end of 3rd stage, points of 2nd and 3rd stage will be added.

An overall title will be awarded to the three first participants of each class who have the higher added score of the 2nd and 3rd stages.

2.3 Description of stages

2.3.1 - Free practice

The manoeuvring race course is laid for the practice on the water. The driver chooses himself – either to practice in speed manoeuvring or in parallel slalom. Timing and duration of the trial is identified by the organiser.

2.3.2 - Manoeuvring

A participant must show his skill in binding knots (see addendum).

All participants run a manoeuvring according to the drawings attached to each class.

Each participant has 2 attempts of which the best is taken into account.

2.3.3 Points system

U.I.M. points are given to the drivers according to the following scoring system :

1 – 400 pts	8 – 53 pts	15 – 7 pts
2 – 300 pts	9 – 40 pts	16 – 5 pts
3 – 225 pts	10 – 30 pts	17 – 4 pts
4 – 169 pts	11 – 22 pts	18 – 3 pts
5 – 127 pts	12 – 17 pts	19 – 2 pts
6 – 95 pts	13 – 13 pts	20 – 1 pt
7 – 71 pts	14 – 9 pts	

2.3.3.1 - Parallel slalom

The slalom course is laid out as shown in the attached drawings.

The speed race is to take place on the parallel slalom race course.

Each driver has 2 attempts. After the first attempt the drivers change with boats and race courses.

Drivers, having 1-4 result in each class, participate in the semi finals and finals.

The classification is defined according to time results and a number of penalty seconds. The U.I.M. points are given to the **combined** time results with penalties according to the U.I.M. rule 2.3.3.

2.3.3.2 - Semi finals and finals of the Parallel Slalom

Pairs setting for the semi-finals :

First pair-drivers of the 1 and 4 places

Second pair-drivers of the 2 and 3 places

The results of the semi-finals will determine ***the two finalists and the third and fourth places*** (play off system).

2.3.3.3 - Restart to the competition

If the engine stops during the race, the driver should start it again by himself (in 1-3 classes the co-driver's assistance is allowed) and finish the race. After the race is finished the driver has the right to submit a protest. The commissioner decides whether to allow a restart or not.

3. SAFETY MEASURES

Each boat is equipped with two separate kill switches (classes 4-5 boats only one kill switch).

In classes 1-3 a co-driver ***is compulsory and*** must accompany a pilot.

Each driver and a co-driver must wear a helmet and a life jacket provided for by the organisers.

Organisers cover all the drivers with an insurance against accidents.

4. PRIZE GIVING / RESULT

Prize giving is arranged for by the organisers.

The prizes are given separately according to the results of the 2nd, 3rd ***stages***, individual competition (total sum of the 2nd and 3rd ***stages***) and team competition.

Individual competition

U.I.M. points for manoeuvring and Parallel slalom are calculated and summed up and a total number of points form a ranking list.

The three best drivers are awarded by prizes.

Team competition

The final result of the team competition is defined by a number of the U.I.M.-points ***of the three best results*** from each team in manoeuvring

and parallel slalom ***in a total of 2 400 points***. The results must show time of posting and are signed by the OOD and the chief timekeeper.

Protocols

Protocols should be posted after being submitted for approval by all judges and should be signed by the main referee and the secretary of the race.

Protocols must be organized separately in every stage in columns showing clearly :

Manoeuvring : knots test penalties ; 1st attempt times ; penalties ; 1st attempt times with penalties ; 2nd attempt times ; penalties ; 2nd attempt times with penalties ; best times including knots test penalties ;

Manoeuvring overall : UIM points / ranking

Parallel slalom preliminary heat / semi finals / final : 1st attempt times ; penalties ; 1st attempt times with penalties ; 2nd attempt times ; penalties ; 2nd attempt times with penalties ; attempts added times ; UIM points / ranking

Parallel slalom overall : UIM points / ranking

Individual competition / General : Manoeuvring overall UIM points ; Parallel slalom overall UIM points ; UIM summed up points / ranking

Team competition : Manoeuvring three best results / UIM points (identification of the relevant drivers and classes) ; Parallel slalom three best results / UIM points (identification of the relevant drivers and classes) ; UIM summed up points / ranking.

5. ENTRIES

An enrolment application must be addressed to the organisers 4 weeks before the date of the event.

6. DESCRIPTION OF THE RACE

All participants must complete the prescribed disciplines according to the drawings and guidelines attached to each class.

Drivers run against the clock.

Classes 1 - 3 sit in the boat. At classes 4 - 5 participants must sit on the jockey seat and may hold the side inflatable areas of the boat to increase stability and safety.

Sitting on the side of the inflatable boat or bending is forbidden and leads to the disqualification.

6.1 Departure

Departure is implemented without an additional touching of the starting pontoon.

Additional touch leads to a 5 seconds penalty.

6.2 Desembarquement

After passing the finishing line a boat must land smoothly and the driver should bind a boat to a mooring.

6.3 Man-over-board

This manoeuvre is implemented in classes 4 – 5, by turning the boat with the port-side to the mark, stopping by reversing or setting the engine on neutral. Manoeuvring the boat backwards is not allowed..

The driver rise up a safety belt (or similar) over the stick on the mark by both hand and lay it down without throwing the belt or dropping it down and then continue the course. Failure to do it from the first time will lead to the next attempt but the driver may move only forward.

6.4 Binding knots

A skill should be demonstrated in binding the following knots :

- Reef knot
- Bowline

- Half-hitch
- Clove-hitch

7. RESPONSIBILITIES

Participation in the events is carried out at somebody's own risk. Non-sportive behaviour of the team members will lead to disqualification of the driver for the relevant heat. Non-sportive behaviour of the accompanying persons is not welcomed.

8. CAPTAIN OF A TEAM

Each team must nominate a captain (among the drivers) whose responsibilities are to provide management and guidance as well as to look after proper behaviour of his young team mates.

9. DEFINING A WINNER

Defining a winner of "Manoeuvring".

Defining a winner of "Parallel Slalom"

Points received in 2nd and 3rd **stages** are added. The overall winner / Champion is the one with the highest number of **UIM** points (classification).

Dead-heat situations will be decided according to UIM Rule 319.02.

10. PROTESTS

Protests are accepted according to the U.I.M. rule 403. Protests should only be submitted by the team representative.

11. PENALTIES

- Touching of the turning buoy 10 seconds
- A second **and third** attempt on the race course (**each**) 10 seconds

- A fourth attempt	Disqualified
- Passing the gate not with the full length	20 seconds
- Touching the buoy while reversing	10 seconds
- Not properly rising the safety belt over the stick and head	20 seconds
- Second attempt to rise the safety belt	10 seconds
- Unproperly done knot (each)	5 seconds
- Bumping the landing pontoon	10 seconds
- Wrong binding of the boat	10 seconds
- Participants standing on their feet during the manoeuvring and parallel slalom race	10 seconds (class 4-5 only)
- Additional touch of the starting pontoon	5 seconds
- Sitting on the side of the inflated boat	Disqualified (attempt or total)
- Error doing the circuit	Disqualified (attempt or total)
- Incorrect man-over-board manoeuvre (each part)	20 seconds

12. LANGUAGE

All the preparatory work and correspondence between the U.I.M., organisers and participating countries will be carried out in compliance with p.1.1.2 of the U.I.M. Statutes and By-laws. All the technical and public announcements should be done both in English and the language of the host country.

Note : as to the other technical matters with regards to description of the race, advance programme, timing, entries, race circuits, determination of the winner and time counting etc. should be as per U.I.M. general rules.

501 - MEASUREMENT CERTIFICATE

A boat is not allowed to take part in an international race without a measurement certificate, issued by a National Authority, made up in the native language and in French or English according to the official U.I.M.

model. ***The certificate is valid for one year. It can be renewed without re-measuring, provided the rules for the relevant class have not been changed.***

When measuring a boat with a reinforced cockpit, all specifications in rule 509 must be checked.

501.01

Each National Authority stipulates the cost of measurement and the duration of the validity of the certificate.

The request to obtain a measurement certificate must be accompanied by the homologation file of the motor, if such is requested for the devices being used.

501.02

Any owner who has made alterations affecting the measurements in the certificate must notify his National Authority at once. Non observance of this rule invalidates the certificates. The National Authority re measures the boat at the owner's expense. Any boat that has been altered but not re measured is not allowed to race.

501.03

Any international measurement certificate must be as described hereafter, and must be adopted and used by all National Authorities and the U.I.M.

501.04

The serial number of the motors may be omitted in the measurement certificate.

UIM Union Internationale Motonautique		NA National Authority	
MEASUREMENT CERTIFICATE			
NUMBER	CLASS	SERIE	
<div>Owner</div> <div>Club</div> <div>Boat Name</div> <div> SAMPLE </div>			
<div>Measurer</div> <div>National Authority</div>			
Date		Valid till	

HULL

BUILDER

YEAR

DESIGNER

MATERIAL

LENGTH

BEAM

WEIGHT

COCKPIT

LENGTH

BEAM

DEPTH

BRAND

TYPE

YEAR

N°

HOMOLOGATION N°

SPECIFICATIONS

CYCLE

N°

CYLINDER

N°

BORE

mm

STROKE

mm

CYLINDER VOLUME

c

m³

501.05

No blank certificate may be issued by National Authorities. Documents must be completely filled in with all particulars pertaining to the series for which the boat is measured. The certificate must also be signed and stamped by the National Authority.

501.06

Drivers need only supply and measurers need only check such particulars as are applicable to the class of boat.

501.07

Anyone involved in the design, building (manufacturing or marketing) of a hull or motor in any other way has an interest in a certain boat is not to measure the boat .

501.08

All measurements should be taken at least twice. Measurements should remain within the limits provided for on the homologation sheet.

501.09

The owner of a vessel pays all fees and expenses for measuring his boat and/or motor(s).

The measurement certificate is only to be handed over when all dues have been settled by the owner to the National Authority.

501.10

When a re measurement is made at the request of the National Authority, the expenses and fees for such re measurement are paid by the National Authority if the first measurement is upheld.

501.11

A log book is mandatory for all boats equipped with reinforced cockpit, to be filled out by the race scrutineer indicating the race location, date, and if any damage occurred to the cockpit or boat. It will be part of the measurement certificate.

502 - INSPECTIONS

502.01

Hulls, motors, fuel, accessories and equipment, subject to restrictions regarding dimensions or other characteristics, must be submitted for verification.

All race boats must be inspected before going in the water, and as a help to the inspectors they may use the recommended minimum check list

PRE RACE INSPECTION

At every race meeting, the boats must be inspected before going in the water due to safety reasons.

Minimum check list for Pre-race inspection of race boats

BOAT NUMBER	U.I.M. art.
Life jacket	205.06/509.04
Crash helmet	205.07
Lifting eyes and slings	205.12
National flag	206.01
Boat number on boat deck and under cockpit	206.02/509.19
Mooring eye	503.01
Paddle (not required in boats with reinforced cockpits)	503.01
Flotation	503.01/509.02
Throttle & ignition cut off	503.03
Steering drum and steering cables	503.05

Rigging of fuel systems	503.05
Rigging of electrical systems	503.05
ID plate on cockpit	509.01
Seat belts and belt buckle (check for wear)	509.03
Driver fitting in cockpit	509.05
Water deflector	509.07
Energy absorbing padding in cockpit	509.08
Sharp edges in cockpit	509.09
Removable steering wheel	509.10
Rear view mirrors	509.11
Motor shut off switch outside of cockpit	509.12
Air vents	509.14
Water inlet holes in back of boat	509.15
Bottom of cockpit coloured orange	509.19
Oil absorbing carpet	703
Main power switch (electrical)	Formula Rules
U.I.M. Sticker for all titled events	Formula Rules
Oil	508.07

Drivers must be able to show :

- a valid license
- a valid measurement certificate for the relevant class
- a complete homologation sheet (if mentioned in the measuring certificate)
- all pages should have a U.I.M. or N.A. original stamp
- a copy of the boatbuilder's U.I.M. registration for reinforced cockpits
- a valid immersion test certificate (§ 205.05) for boats with reinforced cockpits
- a current U.I.M. rulebook
- for drivers with a physical handicap, a doctors written approval that the driver may race (mentioning any special conditions)

502.02 - MOTOR INSPECTION

Verification of motors in restricted series is carried out according to the facts contained in the homologation file.

At world record runs, world championships and continental championships, inspection of the winning motor is compulsory.

Inspections may also be done whenever found necessary at all international and national events.

The following procedure is mandatory for these inspections:

1. Check that the motor type code and visual appearance conforms with the motor homologation sheet.
2. The measurements stated in the homologation sheet must be checked on the motor. If the results are within the specified tolerances the motor should be accepted as legal. Only adjustments to meet dimensions shown in the homologation sheet are allowed.
3. When ports in cylinders are adjusted to the dimensions specified in the homologation sheet, material may only be removed in the specified opening to a depth of 10 mm, to match the shape of the original adjacent connecting passage (channel) outside the adjusted port opening. This also applies to other openings in the motor for which the dimensions are specified in the homologation sheet.
4. Parts not specified with measurements in the homologation sheet shall only be checked by visual comparison with standard parts. As the actual manufacturing tolerances are not published, small differences between the inspected part and the reference part must be accepted.
5. Cylinder dimensions and compression volume are measured when the motor is cold. Allowance is made for the carbon and oil present in the combustion chambers. Checking is to be done with oil. A large scale graduated cylinder or burette with adequate orifice is to be used.
6. Inspection procedure for the low emission engines will also be clarified in the homologation sheet.

502.03

The driver must report with his boat at the inspection and must comply with the rules of the organising race committee.

502.04

Technical inspectors have the right, once races are over, to carry out all the checks they consider necessary, even when inspection has taken place before practice, and they have the right to inspect as they think fit.

The post-race inspection should be about speed and power enhancing matters.

502.05

Drivers are at all times responsible for the condition of their boat (hull, motors, accessories, equipment, etc.).

502.06

Errors, if any, on the part of the manufacturer, builder, mechanic or even the previous owner do not justify non conformity with the rules.

502.07

A driver who does not comply with the decisions or instructions given by the Technical officers, or who infringes any rules which it is the duty of the technical officers to check is not allowed to start in the race, or should he have raced shall be disqualified and reported to his National Authority, for possible further penalties.

Breach of details related to the technical rules, will render a disqualification to the whole race or heat. Exception is lost engine cowlings, exhaust pipes, broken parts ; then only disqualification in the relevant heat.

Foreign drivers are to be reported to their own N.A. through the N.A. of the organising country.

Each N.A. must then inform U.I.M. of further penalties, or reasons why no further penalty has been given by his N.A.

A driver who refuses his boat and/or motor to be inspected after a race shall be penalised as severely as for a serious infringement of the technical rules.

If this occurs at a international titled event, a mandatory penalty will be that the driver will loose his right to participate in international events for 12 months.

502.08

It is prohibited to change the crankcase, the hull or its sponsons after qualifying races (if any) and after the start of the first race/heat.

These items can be changed during or after the timed trials, in this case the times achieved with the items used before the change is void.

The replacement engine will require sealing and the exchanged boat or sponsons, will be considered as a new entry, requiring full scrutineering. If no timed practise opportunity remains, the boat will start from the end of the grid.

Any outside assistance on the race course, for repairs or any other reason will result in disqualification.

If any repairs can only be effected outside the boat park, they must be carried out in the presence of a Technical Officer.

503 - EQUIPMENT - PEOPLE ABOARD MOTORS

503.01

Every boat must be fitted at the bow with a mooring eye which must be strong enough to support the boat if it sinks and to make towing possible.

One or two oars or adequate paddles, adequately secured and stowed, so that they are immediately available, are to be on board. Boats with reinforced cockpits do not need to have oars or paddles.

Every boat must have adequate buoyancy to ensure that it floats in case of an accident. This buoyancy shall be equivalent to a volume of 0.055 m³ of foam per 100 kg. of boat weight. If a reinforced cockpit is fitted, at least one half of this volume must be permanently attached to the cockpit structure. Any foam is to have a maximum density of 30 kg/m³.

The flotation provided by the airbag is not to be included in the calculation for required flotation.

503.02 - WINDSCREEN

Boats must be uncluttered and the windscreen if fitted must be easily removable to present no obstacle to the driver getting free, and to not impede lifesaving and salvage operations.

The edges of the coamings and the windscreen must not be sharp and must be lined with rubber or other similar material to avoid injuries in case of accident.

503.03 - AUTOMATIC THROTTLE SHUT-OFF

All boats must be fitted with an efficient automatic throttle shut-off device as well as an automatic device in the ignition circuit which will stop the motor if the driver is thrown out.

Boats with reinforced cockpits do not need to be fitted with this kind of ignition shut-off device.

The device must be clearly visible, as must be the link to the driver. The link must be short enough to be efficient.

The system must be working at all times during a race, practice or trials.

A driver who does not conform is disqualified in a race or is made to pay a fine in case of practice or trial runs.

Lanyard connection to the life vest is required to shut off the fuel on boats with electric fuel pumps.

503.04

During a race, the boat must have onboard all the equipment required by the Race Committee in the detailed instructions contained in the advance programme.

503.05

All attachments, navigation lights, hand holds, steering gear, pulleys, fairleads, cleats and anchor eye, etc. must be bolted to the hull.

503.06

In all series, there must be only one driver per boat.

The Race Committee may rule otherwise in the advance program for long distance races.

503.07 - PISTON MOTORS

Reciprocating piston motors are to be fitted on boats of the international series.

The total cylinder capacity of the motor or motors forms the basis for eligibility in the corresponding classes.

The motors may be of the two stroke types. They may have carburettor(s) or fuel injection.

503.08 - ROTARY MOTORS

Rotary motors with trochoidal rotors are admitted on the basis of an equivalent cylinder capacity.

The rotary motor capacity is twice the volume determined by the difference between the greatest volume and the least volume in the working chamber.

503.09 - SUPERCHARGING

When the motor includes a device used for supercharging it, the nominal cylinder capacity is to be multiplied by 1.4. and the boat will pass into the class corresponding to the calculated volume resulting from this operation. The supercharger must be a standard part of the motor. A dynamic air inlet for ducting the atmospheric air toward the motor's air intake is not considered as a supercharging device.

503.10 - FINS

Fins are not allowed to be retractable underway.

503.11 - ELECTRICAL ISOLATION

All boats, with batteries, must have an isolation switch as close to the positive terminal of the battery(ies) as practicable (negative terminal if positive earth system).

The opening handle of the switch must be easily accessible on the deck or other surface of the boat on the port side between 1 to 3 metres forward of the stern.

This switch must be marked with a red flash + and the "On/Off" positions shown.

504 - NOISE REDUCTIONS

504.01

The use of an efficient device to attenuate the noise is compulsory.

A race organiser shall have at hand a sound level meter as described below as well as personnel trained in its use.

It is recommended that associated National Authorities obtain such meters and lend them to race organisers when needed.

504.02

The noise level must not exceed levels listed below, except for A- and TR- serie boats.

504.03

The noise level is measured by means of a precision sound level meter which conforms to the specifications laid down by I.E.C. and A.N.S.I., using the "A" weighting and set to slow response. The registering microphone is placed at a distance of 25 m 90 to the course of the boat with no interfering objects within 25 m. The boat must pass the microphone at top speed in two directions.

The measurements must be carried out while the boat approaches the microphone and while moving away from the microphone. The measuring must start when the boat is minimum 100 m before the closest point on the course to the microphone and continue until the noise level has dropped considerably from the measured value.

Neither of the measurements is to exceed 91 dB(A) with a 2 dB(A) tolerance for motors up to 750 cc, and 91 dB(A) with a 4 dB(A) tolerance for motors above 750 cc.

Only for motors up to 750 cc, without water pump

If the above method is not practicable, reference measurements may be made with the boat on dry land, the motor running under no load at a mean piston speed of 13 m/sec. The microphone must be placed 0.5 m from the exhaust orifice and at a height of at least 20 cm above the ground. The maximum noise level, using a meter as above is to be 101 dB(A) with a 2 dB(A) tolerance. This level is experimentally determined and may be changed.

505 - HOMOLOGATION

505.01

The homologation of new motors will be evaluated at the COMINTECH meeting which is held to review the General Assembly proposals. Requests for homologation should be received by the U.I.M. secretariat by October 15 for eligibility in the next year.

Motors will be inspected to ensure that they confirm to the homologation file at a time and place to be agreed between the COMINTECH homologation group and the manufacturer, but must be before February 28 for the homologation to be valid in that year.

The manufacturers of outboard motors must certify that the noise level of their motors' are within the limits specified in the rule 504.03 when the motor is used on an appropriate race boat.

The homologation group has the right to demand an inspection on the motor manufacturers noise measuring test, to check if the motor in question meets the demanded noise restrictions required for the approval of the homologation.

The required number of motors (specified in the machinery section of the series rules), is to be verified by the manufacturer and the National Authority of the country of manufacture. A written certificate, witnessed by a practising lawyer, of the firm intention to manufacture the specified numbers is required from the manufacturing company, endorsed by the National Authority. When the homologation specification is agreed, the manufacturing schedule to meet homologation number requirements must be presented to the homologation group and adopted.

505.02

The homologation becomes valid when all the correct completed documentation has been received at the U.I.M. office.

This must be before April 1 for the homologation to be valid for that year.

An existing homologation remains valid for ten years from the latter of April 1 of the first year of validity or from the year of any running

production change. If a motor continues in common use after this date, to the same specification as filed in the homologation sheet, then provided that all spare parts are readily available, an extension of validity for 3 years or 5 years is granted. If it continues to meet these criteria for a further period, a further extension of validity may be applied for and granted on a yearly basis. The application for extension of validity must be applied for using the same dates as in § 505.01.

The fee for 3 years extension of validity is 30% of the normal homologation fee, for 5 years extension of validity is 50 % of the normal homologation and for 1 year 10%.

The fee for a running production change will be 50% of the normal homologation fee.

505.03

Validation by U.I.M. is subject to the payment of a fee, the amount of which is fixed every year by the General Assembly.

505.04 - HOMOLOGATION SHEETS

Homologation sheets must be on U.I.M. forms; they must be complete and, when necessary, accompanied by explanatory notes, when interpretation might lead to misunderstanding.

505.05

Homologation sheets are obtainable against payment of a fee, the amount of which is decided upon annually, at the General Assembly. The copyright thus bought allows the National Authority to reproduce the file with as many copies as necessary for its drivers and technical services.

Should the National Authority so wish, it may fix the fee to be paid, for copies provided.

505.06

If the driver is unable to get a copy of a foreign homologation file from his own National Authority, the U.I.M. may supply the document against payment of a fee. Such document is individual and is not to be reproduced without authority.

505.07

An homologation sheet is not always required for boats with diesel motors, aerial propellers, racing outboards and racing inboards.

505.08

A homologation sheet is required for each motor and/or propulsion unit. This sheet is valid only for the model described: any model deriving from it must be subject to homologation separately.

A copy of the homologation sheet must accompany any motor, boat or accessories used in the classes in which homologation has been provided for.

505.09 - REFUSAL OF HOMOLOGATION

A National Authority has the right to refuse to put forward an homologation if the latter is considered as being contrary to the interests of powerboating. The refusal may be appealed against before the Council of U.I.M. after examination by the Technical Commission.

505.10

The Technical Commission shall refuse any sheet put forward by manufacturers via the National Authorities concerned that has not been properly filled in.

506 - HOMOLOGATION PROCEDURE

506.01

Inspection of motors is to be carried out by the Homologation Group. The Homologation Group consists of a chairman and two other members appointed by Comintech. Two members of Comnoff to control class 1 & 2 requirements may be coopted as required.

For any inspection, a minimum of one Homologation Group member plus either one other member or a member from the Technical Commission of the National Authority of the country of manufacture is required.

506.02

The expenses of inspection are to be borne by the motor manufacturing company requesting the homologation via its National Authority. These expenses must be refunded to the inspectors before they leave the country of inspection, against receipt of an expense report and supporting proof of expenditure.

For Homologation Group members, travel expenses from their town of residence to the place of inspection and back, hotel accommodation and meals, and transfer cost between home, airport and inspection place are to be paid. For an National Authority. Comintech member, the National Authority will reimburse the expenses. The National Authority is authorised to charge the manufacturer a reasonable administration fee to cover its secretarial expenses.

506.03

A request for homologation outside the deadline dates set in § 505.02 will be considered on payment of one-and-a-half times normal fee, plus inspection expenses. To be valid in that year, all documentation must be complete and received at the U.I.M. office by the deadline in § 505.02

507 - RUNNING PRODUCTION CHANGES

507.01

"Running Production Changes" may be requested at any time. The Homologation Group decides whether any requested change is acceptable as a Running Production Change, or requires the model to be completely rehomologated. Modifications to specifications dictated by manufacturing or structural requirements are admitted, but not those primarily aimed at improving the performance.(as a guide, any changes or accumulation of changes likely to increase the power by more than 5% or the speed performance corresponding to more than 5% power increase above that given by the original homologation specifications may be grounds for requiring rehomologation.)

An approved Running Production Change is to be announced to the National Authorities by the U.I.M. and is legal for racing 30 days after this.

508 - FUEL

508.01

- 1) To maximise the equality of competition ***and to simplify the checking of conformity***, organisers ***must arrange the supply of the same fuel for all competitors*** at all international titled events, except for classes O/125 to O/700 where the fuel is alcohol based (508.05). ***It does not of necessity have to be free of charge.*** It is ***also*** recommended for ordinary international events.

Organisers must announce in the advance programme.

The cost of fuel that they will be arranging, the octane number, the brand name of the supplier, and the logistic method of making it available, e.g. distribution tanker in the pits or organised shuttle service to a local fuel station operated by the local organiser. Drivers must provide suitable containers for such use, marked in accordance with appropriate international hazardous standards, and clearly marked with the boat race number.

Drivers must use ***the arranged*** fuel ***as*** provided and must not bring their own fuel to the event. Contravention of this rule will be penalised. ***Checks may be carried-out on arrival in the pits and during scrutineering.***

508.02 - SPORTS ENGINE FUEL (S. T. P. E. OSY400 F1000 BOAT GP F4 AND O CLASSES – O850 AND ABOVE)

- 1) Fuel will be unleaded automotive gasoline only for use in non-racing automobiles equipped with catalytic converters.
- 2) Fuel may also be “green fuel” intended for sport engines supplied to the organiser by an international fuel company.
- 3) Fuel will have a ***minimum*** octane rating of 95 RON, ***but to maximise engine reliability, organisers must provide the highest octane rating available in their locality.***
- 4) All fuels supplied must be in conformity with the minimum requirements of the engine manufacturers

508.03 - RACING ENGINE FUEL (F1 R CLASSES)

- 1) Fuel produced by an international company for motor sports use. Octane rating to be minimum 100RON maximum 104RON. Unleaded fuel is preferred.
- 2) Aviation fuel AVGAS 100LL, may be used. (No longer allowed in EEC countries).
- 3) All fuels supplied must be accompanied by a data sheet.
- 4) All fuels supplied must be in conformity with the minimum requirements of the engine manufacturers

508.04 - DIESEL ENGINE FUEL

- 1) The fuel must be a commercial grade for automotive or marine use, as obtained from normal roadside or marine service stations.

508.05 - ALCOHOL BASED FUEL

- 1) The fuel must be Methanol (CH_3OH) or Ethanol ($\text{C}_2\text{H}_5\text{OH}$)
- 2) The fuel may contain water (H_2O).

508.06 - ADDITIVES

- 1) Only lubricating oil for two stroke engines may be added to fuel. The use of any other additives is prohibited to either the fuel [carburant] or the air intake [comburant]

508.07 - LUBRICATING OIL

- 1) Only two stroke engines that are not equipped with mechanical oil injection systems will be permitted to use fuel with lubricating oil pre-mixed in it.
- 2) Competitors must use oil from the list of regularly available and approved oils that is posted on the U.I.M. web site together with there readings.
- 3) Additions may be made to the U.I.M. list by N/As submitting a unopened sealed sample for examination and registration.
- 4) Drivers must nominate, at scrutineering, which specific oil that they are using.
- 5) During the **2005** season, additions to the approved list may be made by the U.I.M. commissioner at a race site and the U.I.M. informed via the commissioners report

508.08 - RANDOM TESTING OF COMPETITORS

- 1) Fuel samples taken at random from competitors during the event will be tested at the race venue, and may also be chemically analysed afterwards at a laboratory.
- 2) The entire installed fuel system may be dismantled, removed from the boat and inspected.
- 3) Gasoline supplied by an organiser will be tested at the start of an event to establish its Digitron value. The result of this test will be posted by the organiser at the time of issuing the fuel.
- 4) In the event that the organiser does not supply fuel and the competitors bring their own, each competitor's fuel will be checked. This will be part of the pre race scrutineering.
- 5) At an event where the competitor's fuel is used, it will be the competitor's responsibility to ensure that at least one litre of neat gasoline is always available for sealing as a sample.
- 6) The presence of illegal fuel at any time will result in the driver being immediately disqualified up to that point.

508.09 - TESTING PROCEDURES

- 1) Gasoline fuel or fuel / oil mixture may be checked at any time using U.I.M. approved equipment and procedures. Meters and equipment will be calibrated each time in accordance with the U.I.M. fuel testing guidelines obtainable from the U.I.M. Secretariat.
- 2) Non-gasoline fuels will be tested in accordance with U.I.M. current procedures.

508.10 - DISQUALIFICATIONS, PROTESTS AND APPEALS

- 1) Any competitor's fuel when tested, and judged as illegal, from timed trials, heats or races will be disqualified up to that time.

- 2) The legality of the fuel is to be judged only by the U.I.M. commissioner, or if he is not present at the event, by the technical scrutineer, based on all tests and information available to him at the time.
- 3) Any further infringement of these fuel rules will lead to immediate Temporary Suspension (art. 406.07) from all of the event, where the infringement occurred.
- 4) Repeated violation of these rules the U.I.M. may sentence a stronger penalty.
- 5) Any disqualification must be followed by the sealing-up of two samples of the competitors fuel ,in an approved container of at least 1 litre if possible. No sample to be opened or disposed of unless the U.I.M. inform accordingly.
- 6) If the competitor protests or appeals his disqualification, he will have the right to have the samples checked using Gas Chromatography, or similar tests to establish what components are present in the fuel sample. All costs incurred in this analysis will be paid by the competitor irrespective of the result.
- 7) If one competitor protests another competitor's fuel, there must be a factual basis for the protest, samples will be taken and tested. The protest must be accompanied by a fee of 250 euro which may be used to defray the cost of the analysis
- 8) The result of the analysis which will confirm the presence of illegal additives or not, will be taken as final and cannot be appealed.

508.11

Refuelling

- 1) Refuelling is allowed only at places announced by the event organiser.

508.12

Starting aids

- 1) No chemical starter aids are permitted on the start pontoon except for alcohol based fuels.

509 - REINFORCED COCKPITS

These rules provide only a minimum standard for reinforced cockpits. It is important that constructors continually develop cockpits beyond these minimum's and produce cockpits that exceed these standards.

The approved Newton standard for a cockpit is to be found from the cockpit registration number. A list of registration numbers and the corresponding Newton standards is available from the UIM Secretariat and the UIM website.

All cockpits must meet the following requirements :

- For outboards classes from 1501-2000 cc and upwards, cockpits must have passed the 1000 Newton test. Such 1000 Newton cockpits may race until January 1, 2006. Cockpits that have not passed the 1000 Newton test, must not race internationally in the above listed classes.
- For all other reinforced cockpit classes, any registered cockpit may be raced until January 1, 2006.
- Cockpits builders who have cockpits that have passed the 1000 Newton test and who certifies that their cockpit built prior to 1999, is built by the same method will be valid for racing until January 1, 2006. All such certificates must be approved by the U.I.M. Cockpit Committee. Sample of certificate can be obtained from U.I.M.

FOR OUTBOARD CLASSES UP TO 1500 cc AND ALL OTHER COCKPIT BOATS

Any cockpit of 1000 Newton must **from 2006** be updated to 2000 Newton by the addition of enough composite material, to have 2000

Newton cockpit, $1000 + 1000 = 2000$. It is recommended to upgrade cockpits to 3000 Newton.

FOR OUTBOARD CLASSES FROM 1500 cc AND ABOVE

Any cockpit of 1000 Newton **from 2006** must be upgraded to 3000 Newton by the addition of enough composite material, to have 3000 Newton cockpit, $1000 + 2000 = 3000$.

UPGRADING

This upgrading on cockpit must at least be done on both vertical sides above deck, over the helmet area, over the leg area, the backplate, and the entire bottom. An approved cockpit builder who has passed the at least 2000 Newton panel test must do the upgrade.

Upon any upgrade completion, **his** cockpit registration number must be moulded or ground into the upgraded cockpit.

Cockpits must have two cockpit registration numbers, one for the original cockpit and one for the updated reinforcement. These two numbers must be written in the measurement certificate.

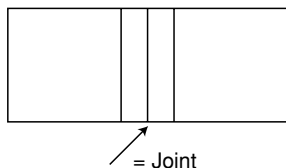
Cockpits built in 2002 and onward must have passed the 2000 Newton test.

Cockpits built in 2005 and onward, for outboard classes from 1501 – 2000 cc and upwards, must have passed the 3000 Newton test.

U.I.M. COCKPIT TEST STANDARD

Sample Construction Requirements

1. Sample quantity must be 3, one of which must include a sample of the joint/bonding of the cockpit halves, transverse to the length of the sample.



2. Trimmed sample size must be 100mm \pm 1.0mm wide x 800mm \pm 5mm long, with the width being parallel
3. For fibre orientation the 800mm length being parallel with the bottom surface of the intended homologated cockpit
4. Sample must be laminated on a flat surface using the same manufacturing process, materials and fibre orientation's as the intended homologated cockpit construction
5. The sample must have a uniform thickness with no core crushing along any edges
6. The sample must have one moulded face and the other face being unmoulded, the moulded face will be taken as being the external surface of the cockpit during testing
7. The sample must be representative of the thinnest lay-up of the safety cell
8. The sample and cockpit must be manufactured using balanced or unbiased materials

Sample Test Method

1. The sample will be supported across its full width perpendicular to the 800mm edges by two parallel 25mm Steel bars at a distance of 500mm apart. The load will be applied equally through two 25mm Steel bars, each a distance of 167mm parallel from each support
2. The moulded face of the sample will have the load applied and the unmoulded face will support the sample

3. The load will be applied at 0.4mm/sec and the deflection will be measured at the two 25mm Steel bars applying the load within 2 minutes.

Sample Test Requirements

1. The sample when loaded with a force of **3000N** must have no more than a maximum deflection of 25mm without the sample failing
2. The sample weight in grm/m² will be calculated, skin thickness and sample thickness will be measured to enable inspection and comparison of damaged homologated cockpits
3. Further non-destructive test analysis methods maybe used to compare test samples with homologated cockpits during the life of each cockpit

Sample Manufacturing Information Requirements

1. Ply laminating sequence (stating which ply is the moulded face)
2. Ply materials
3. Ply weave styles
4. Ply material weight in grm/m² (dry weight ie. without resin)
5. Ply orientation (where 0° is parallel with the 800mm edges)
6. Core material and density in lbs/ft³ or kg/m³
7. Manufacturing method (stating vacuum, pressure and temperature)
8. A 100mmx100mm sample of all materials used (resin samples not required)

Samples as per sample construction requirements must be sent to the following address :

Siegfried Boettle
1283 Blackburn Avenue
60031 Gurnee, Illinois USA
Tel. + 1 847 6235845 Fax + 1 847 6235826
E-mail : ziggy@boettle.org

509.01

Any boat having a restraint system (driver harness) must have a reinforced cockpit complying with § 509, even if a reinforced cockpit is not mandatory in its class.

Matters relating to cockpits/deformable structures shall be dealt with by the cockpit working group.

Experts may be co-opted when required.

The manufacturer of any reinforced cockpit will submit his name, or company name if applicable, and completed U.I.M. reinforced cockpit questionnaire to his National Authority. and thence to U.I.M., before that cockpit will be allowed to compete. The manufacturer's name, type designation, date of manufacture and registration number must be permanently moulded into a visible place on or in the reinforced cockpit.

As from 2006, if this is not moulded in, a scrutineer shall grind the UIM registration number, and the manufacturer serial number into the cockpit/midsection of the boat.

The National Authority. of the manufacturer of the reinforced cockpit will receive an acknowledgement from the U.I.M. office.

In the case of series manufactured cockpits, the letter will refer to the cockpit manufacturer and type.

509.02 - PICKLE-FORKS

Cockpit structure must include permanently attached flotation capable of floating the cockpit, driver, motor and any attached structure.

All race boats of catamaran type, designed and equipped with pickle-forks, must have deformable and energy absorbing pickle-forks according to the following:

“The most forward end of the picklefork must be deformable and must be designed to crush.

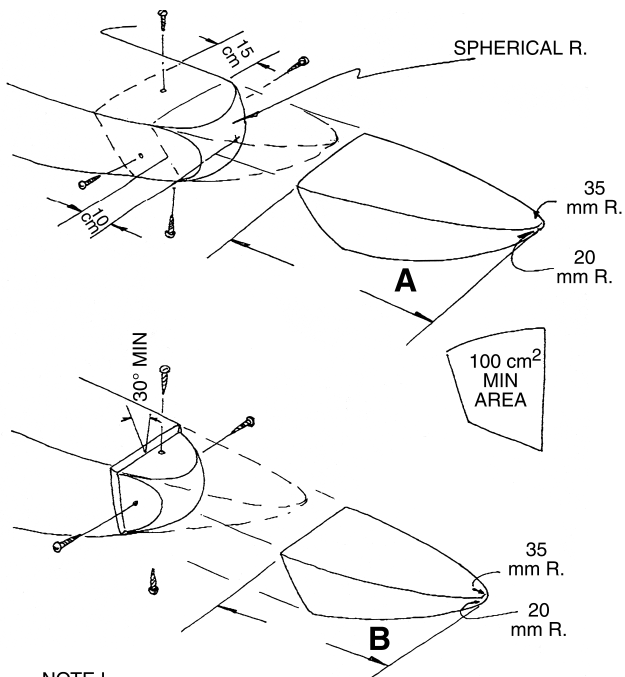
“The deformation must start at a maximum of 140 kg.

“The main body of the sponson must end in a minimum 100 sq. cm (16 sq. in) forward bulkhead made of minimum 8 mm (5/16 in) thick plywood or equivalent material. This bulkhead must be an integral part of the main structure of the sponson and must have a rounded area of at least 10 cm length ”

“The pickle fork must :

- be detachable and have nothing inside
- have a front end of minimum horizontal radius of 35 mm
- have of front end minimum radius of 20 mm in all other directions

“According to the following drawings”



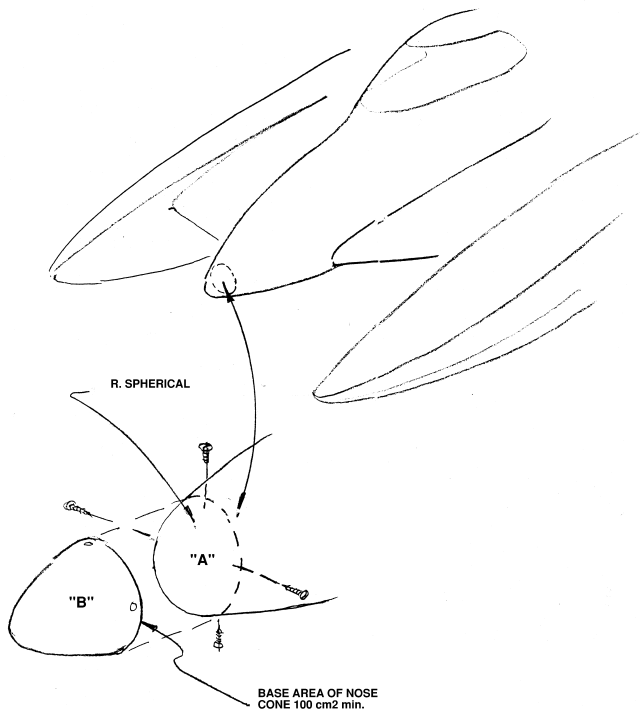
PICKLEFORK MUST DEFORM AT 140 kg MAXIMUM

FOR ALL FORMULA BOATS
EXCEPT F-500 DIMENSION
A=50cm B=35cm
THESE ARE MINIMUM
DIMENSIONS.

FOR ALL OTHER PICKLEFORK BOATS
A=40cm B=25cm
THESE ARE MINIMUM
DIMENSIONS.

The nose of the cockpit of boats built after January 1, 2001 must have the same minimum dimensions as the front of the sponsons before the collapsible picklefork.

If a collapsible front is used on the cockpit, it must have the same minimum dimensions as the picklefork.



- A. The front of cockpit must have the minimum radius as the front of the main body of the sponson, before the crushable picklefork.
- B. A deformable nose may be added to the cockpit but it must be detachable, have nothing inside, have the same minimum dimensions as the detachable picklefork and deformation must start at a maximum of 140 kg.

509.03

Any competitor driving in an approved reinforced cockpit design boat will be in a restraint system consisting of a minimum 5 strap/4 point mounting design.

Not closing the belt in cockpit classes will result in a suspension for the 2 following races of the Series and for titled Championships run in one round, in a suspension at the next Championship.

Strap locations will be minimum two each hip, crotch, two-shoulder, all hooked together in a central release system. This release mechanism must remain released/open once it is activated and until it is reset. Belting shall be a minimum of 5 cm (2 in) width.

The driver harness attachments bolts in reinforced cockpits must consist of a minimum of 8 mm bolts, 1.25 thread, grade A4 and 3 mm stainless steel plate of 100 sq.cm per attachment point. There must be a spacer and a washer on the attaching bolt. The spacer must be glued into the cockpit.

It is mandatory for boats that the shoulder harness straps should be mounted to a metal channel spanning the backboard securely fastened to the sides of the centre section of the boat as shown in 509.18.

Boats manufactured starting from January 1st, 1997 will have to be provided with reinforced cockpit having the following minimal measures:

It is taken into account the need to leave around the body the volume necessary to absorb some slight deformation without remaining trapped into the cockpit :

- The length between the back rest and the base of the feet must be cm. 126 or more
- ***The height between the base and the roof of the cockpit for the race boat furnished with closed cockpit and seat shall be such that the minimum head clearance of 509.05 is maintained. The driver shall not be reclined more than 45 ° from the vertical.***
- The height between the base of the cockpit and the roof for the race boat furnished with enclosed cockpit must be cm. 98 or more.
- The width at the level feet must be cm. 28 for a cockpit with only two pedals or no pedals. If three pedals are required the minimal width must be cm. 36
- At knee level the width must not be less than cm. 40
- At hip level the width must be cm. 48 or more
- The vertical internal dimension at foot level must be cm. 33 or more
- The vertical internal height at knee level must be, in order to obtain an easily extraction of the pilot in case of accident, at least cm. 45
- The length of the opening in the cockpit ***for boats built after 1st January 2006*** must not be inferior to cm. 65, ***and, a minimum width at some point, of 46 cm*** in order not to represent an obstacle to the extraction of the pilot
- The width at shoulder level, not less than cm. 56 from the lower point of the cockpit, must be cm. 48

If a homologated “integrated canopy structure” is used then the canopy must be fitted, closed and latched at all times while engaged in practice or racing.

509.04

A jacket with ballistic covering of orange colour having approximately 4.5 kg (10 lbs) flotation and equipped with epaulette type extraction loops on the shoulders and properly fitted to prevent being accidentally pulled over the head is mandatory.

509.05

Minimum 5 cm (2 in) clearance above driver's head (with 10 cm (4 in) recommended), covering at least 50 % of the top of the helmet and at least 50 % of the side of the helmet with the driver's head in the furthest aft position, as an integral part of the cockpit construction.

For new boats built after January 1st 2006, the cockpit should afford a minimum 5 cm (2in) clearance above the drivers head (with 10 cm – 4 in – recommended) and cover 50 %, +- 5 % of the top of the helmet and 50 % +- 5 % of the side of the helmet with the driver's head in the furthest aft position, as an integral part of the cockpit construction.

509.06

Minimum 12 mm (1/2 in) 4 kg (9 lbs) Ethofoam or other energy absorbing foam support completely around the inside of helmet area to prevent injury from helmet hitting cockpit. ***Acceptable free space for helmet is approximately 5 cm.***

509.07

The top of the front of the cockpit will be a maximum of 10 cm (inch) below eye level, and so constructed as to deflect water away from the driver.

509.08

Energy absorbing foam padding is to be strategically placed inside the cockpit –limiting side-to-side head movement and areas where the knees, hands, etc., might impact.

In addition the Hans Device is highly recommended for F1 and is recommended for F2000, S2000, S3000 and F3.

Seats are mandatory for F1 cockpits and highly recommended for all other cockpit classes.

OBJECTIVE OF SEAT

Work with the restraint belts connecting driver's body securely to the cockpit, thus creating minimal movement between the cockpit and body.

Spreading of load over the body, reducing the chance of local loading and injury.

SEAT

Construction to be stiff and strong enough so that attachment to cockpit is secure and movement between any part of the seat and cockpit is minimal (less than 10 mm) in crash loading.

Closely fitted to the rear and side of each driver's shoulders, ribcage, pelvis and thighs (to at least midway from hips to knees).

Minimal padding that is sufficient for driver comfort while racing (typically 10 mm).

RECOMMENDED

Flat panels beside the legs, to reduce possible injury.

No exposed hardware, inside cockpit, which would come in contact with the driver, in a crash.

509.09

No sharp edges or protrusions anywhere inside or around the entryway of cockpit.

509.10

A removable steering wheel is mandatory.

The steering wheel must be easily removable without tools.

It is recommended that a centre pull ring type release (using only one hand) be used with detachable steering wheels.

509.11

Left and right rear-view mirrors are mandatory.

509.12 - MOTOR SHUT OFF SWITCH

All boats with reinforced cockpits will have an ignition shut- off switch for the motor, located on the left (port) outside of the cockpit. It will be clearly marked "MOTOR SHUT OFF SWITCH". Kill switch lanyard connections are not required on boats with reinforced cockpits.

Lanyard connection to the life vest is required to shut off the fuel on boats with electric fuel pumps.

509.13 - VISIBILITY IN COCKPITS

For boats built after 1st January 2006, the driver must have, when strapped in his cockpit, at least a free view of 180 degrees.

509.14

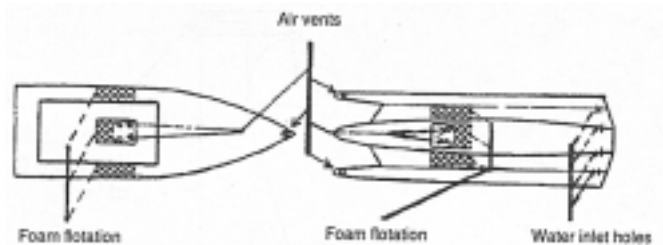
Air vents must be provided in the sponsons directly behind the pickle forks ahead of driver's seat of all catamarans : 2 x 20 cm² (3in²). These air vents can be behind the driving positions of V nosed boats and in the bow.

Airtight compartments in the aft end of racing boats are prohibited.

Catamaran boats fitted with airbag systems require air vents to be fitted either behind the pickle forks but ahead of the drivers seat or between the sponson and the cockpit forward of the driver to all catamarans.

The size to be 2 x 20 cm² (2 x 3 in²) min.

RECOMMENDED FOAM FLOTATION (see § 503.01) AND AIR VENTS LOCATION



509.15

Water inlet holes on catamarans as shown in the sketch must be in the rear of the centre section fore and aft sides.

These holes must be at least 40 sq. cm (6 sq. in) in an area each side and communicate with air space inside the sponson. Any communicating holes in intervening bulkheads must be the same size. Air/water must be able to pass freely from these holes to air vents in the upper front of the sponson/boat.

509.16

All windscreens will be shatterproof material. All reinforced cockpit canopies shall have an external handle or hand hold opening to facilitate the opening of the canopy.

The following requirements are specified for reinforced cockpit canopies ***(For F1, mandatory for new construction after January 1, 2005. For S3000, Boat GP (S2000), mandatory for new construction after January 1, 2006 :***

1. Canopy windscreen/window material and shapes shall be a minimum of 3/8" (8mm) polycarbonate material of high optical grade with scratch resistant outer coating. Such canopy shapes, forms,

and materials are available, for example, from Formtech Plastics Inc., USA +1 800 860 7572.

2. All canopy support inner and outer frames, "U" and "H" channels, shall be constructed of a minimum of 2000 grams of "S" glass and/or Kevlar and bonded utilizing epoxy resin.
3. If the polycarbonate canopy frames are not glued, the frames shall be affixed thru-bolted utilizing bobbins/sleeves made of nylon, delrin, aluminium, or other material so as to not directly cause clamping forces to the polycarbonate material by thru-bolts. Bolt spacing shall be no greater than every 5 cm and shall incorporate the use of 6mm diameter (minimum) stainless steel bolts. All bolts shall utilize stainless steel nylock or acorn nuts with washers. No bolt threads or sharp edges shall endanger the driver in the interior of the cockpit.
4. All canopy hatches, lids, and caps shall be constructed of a minimum of 12 mm foam or balsa core material of 3 kg density laminated with 2000 grams of "S" glass and/or Kevlar bonded with epoxy resin.
5. It is recommended that All polycarbonate material should be able to withstand the loads consistent with a minimum of a 350 knot, 4 pound bird strike test as employed by Texstar Inc., USA, manufacturers of the F-16 canopy. Such tests may be specified by UIM at a future point in time.
6. All outer edges of canopies and hatches shall be protected from water intrusion leading to possible failure by the use of water deflectors, frames, or channels.
7. All canopy or hatch openings which allow access to the cockpit or driver shall include provision for a slot and pry bar pad to be utilized by rescue in the event of jamming or failure.
8. All canopy structures, hardware hinges, mechanisms, construction techniques and installation methods are subject to approval by the cockpit commission.

- 9. For the purpose of the development of an integrated canopy structure, the UIM cockpit Committee can determine that rules 509.05, 509.06 and 509.07 do not apply under the condition that the manufacturer certifies in regard of the canopy :**

The sufficient strength of the construction and at a minimum equal strength and integrity to existing cockpit/canopy systems.

That the canopy passed all tests foreseen in this rulebook.

The driver of such cockpit must declare in writing, that he is aware to drive such experimental cockpit under this rule. Such construction may race for a maximum period of 2 years without further homologation in the rulebook.

This permission can be withdrawn by the UIM cockpit Committee for safety reasons.

All hinges attached to a reinforced cockpit canopy must have pins easily pulled out, and utilise pull rings or knobs, accessible both externally (for diver) and by the driver below the closed canopy.

Mandatory for F1, S3000, Boat GP and S2000.

The following minimum specification is required for canopy hinges:

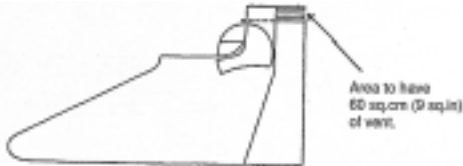
- a. The canopy safety hinge shall be manufactured of stainless steel.
- b. The minimum length of the hinge shall be 10 cm overall.
- c. The minimum width of the hinge shall be 3 cm.
- d. The minimum thickness at the mounting surface shall be 3 mm with 3 fasteners at the end.
- e. The pivot point shall be a fixed 6mm diameter axle pin and shall not be a quick release point of the hinge.
- f. A secondary quick release pin of the push/pull type with a minimum 6 mm diameter shall be incorporated in such a manner as to enable the hinge to be easily released by rescue personnel.
- g. The secondary push/pull shall include a drive out pin feature, as well as a stainless steel pull ring.

- h. The design of the hinge shall prevent the binding of the quick release pin in the event of an incident requiring its removal.

509.17

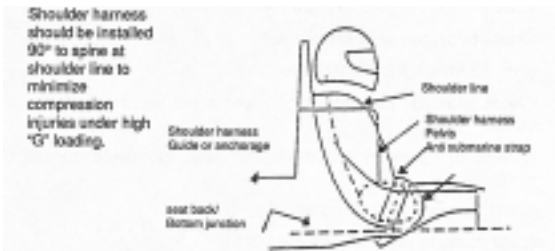
It is recommended that in a reinforced cockpit there is an opening of 60 sq. cm (9 sq. in) above the helmet so as to reduce the hydraulic effect on the driver in case of an accident.

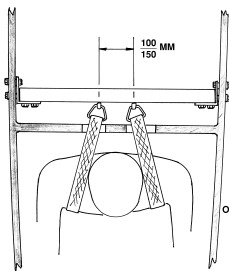
For old and new cockpits the reinforced cockpit backboard behind the driver shall be at a minimum the same strength as the sides of the reinforced cockpit. The lay up schedule and bonding mechanism shall be defined in the reinforced cockpit registration questionnaire.



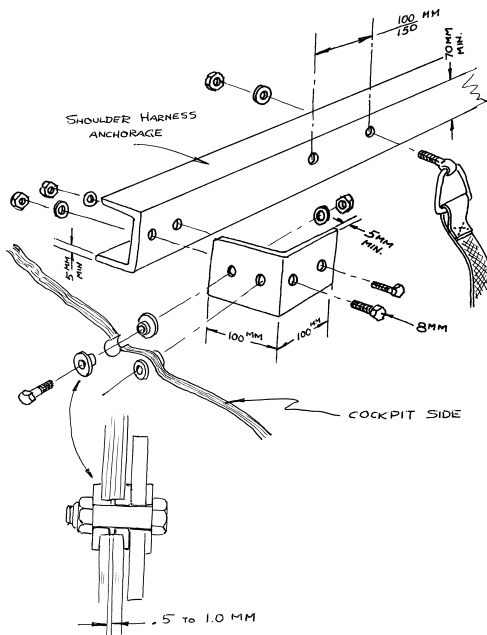
509.18

These are the mandatory requirements for the placing of seat belts and shoulder harness to be used in a reinforced cockpit: ***Any substitution system must be approved by Safety Cockpit Committee. Retrofit strengthening required as from 2005 onwards.***





BAR MUST BE INSTALLED
DIRECTLY AGAINST
THE BACKBOARD
OR AGAINST A FULL LENGTH
SPACER BETWEEN
THE BACKBOARD
AND THE BAR.



509.19

To assist rescue crews, cockpit boats must have the bottom of that part of the cockpit containing the driver or all materials covering the bottom of the cockpit painted international orange and labelled in the centre with 30 cm (12 in) letters for the boat number, and 10 cm (**4 inch**) letters specifying other critical information such as : breathing apparatus (**BrA**), head restrain (**HsD**), etc. ***With breathing apparatus means only breathing equipment that automatically serve an unconscious driver with breathing air. Not the “spare air” type that must be handled by the submerged driver.***

GUIDELINES FOR INSPECTION OF REINFORCED COCKPITS.

- Check manufacturers ID plate and registration number.
- Check buoyancy behind bulkhead.
(Permanently means not removable)
- Check seatbelts for wear, attachment points and release systems.
(Check for corrosion, lack of maintenance, lubricated release system etc.)
- Check removable steering wheel systems.
(Lack of maintenance, lubrication etc.)
- Have the driver to put on all safety equipment and get into the cockpit. Fasten all seatbelts, put steering wheel on, cover drivers visor and check to see that the driver can release him/herself.
- Check that the driver fits inside the cockpit as described in §509.05.
- Check water deflector as in §509.07 on cockpit top front.
- Check for sharp edges and energy absorbing foam inside cockpit.
- Check left and right rear view mirrors.
- Check air vents. As in §509.14
- Check water inlet holes in back of boat centre section
- Check bottom of cockpit for orange colour and boat number.
- Check function and position of MOTOR SHUT OFF SWITCH.

These guidelines are only as a help to the scrutineers of reinforced cockpits.

509.20 - AIR SUPPLY

The driver must have air available as either "Spare air" or air which is supplied continuously to the driver.

The air bottle is to be fully charged prior to the time trials and racing. This air supply bottle may be carried on the driver or securely mounted in the boat. It is the responsibility of the driver to have a fully charged bottle prior to the time trials or race. The regulator shall work in any position.

510 - RACING INBOARDS (R)

511 - CLASSES

R 1000	cylinder capacity of the motor up to 1000 cc incl.
R 1500	cylinder capacity of the motor from 1001 to 1500 cc incl.
R 2000	cylinder capacity of the motor from 1501 to 2000 cc incl.
R 2500	cylinder capacity of the motor from 2001 to 2500 cc incl.
R 5000	cylinder capacity of the motor from 2501 to 5000 cc incl.
R 7000	cylinder capacity of the motor from 5001 to 7000 cc incl.
R ∞	cylinder capacity of the motor from 7001 to infinity

512 - HULL

512.01

The hull design is free.

512.02

Any device to produce a sudden braking effect, causing excessive water spray reducing visibility, is forbidden.

512.03 - MINIMUM DIMENSIONS

Class	Weight	Length	Reinforced cockpit
R 1000	300 kg	4.20 m	mandatory
R 1500	350 kg	4.50 m	mandatory
R 2000	390 kg	4.80 m	mandatory
R 2500	420 kg	5.10 m	mandatory
R 5000	600 kg	5.70 m	mandatory
R 7000	700 kg	6.10 m	mandatory
R ∞	800 kg	7.00 m	mandatory

- 1) The minimum weights mentioned above are the weight of a complete rig weighed directly after the race without driver, or residual water but including the residual fuel.
- 2) Measurements are taken while the boat is ashore.

The length must be measured as the overall length of the hull between perpendiculars at the foremost and aft most rigid part of the hull.

Any extending parts, rubbing starches, fenders, stabilising and trim tabs and rudder are not to be included.

513 - MACHINERY

513.01

The motor is free.

The motor must be fixed inside the hull.

The power transmission train must pass through the structure of the hull.

All modifications are allowed.

513.02

Only one propulsion device is allowed and the total effort must be applied to the water.

513.03

When the propulsion is of the water jet type, the jet may be ejected in the air.

513.04

An electric starter and battery must be on board.

513.05

The transmission is free.

514 - FUEL

See 508.01, 508.03, 508.06, 508.07, 508.08, 508.09, 508.10, 508.11.

515 - RACING INBOARD - HISTORIC (H.R.) - FORMULA R 1000

516 - CLASSES

Formula R 1000 cylinder capacity of the motor up to 1000cc incl. (for old Wartburg engines max capacity is 1020 cc).

Minimum weight hull only 200 kg

Minimum weight with driver 280 kg

HR 2000 Cylinder capacity from 1001 cc. to 2000 cc. included.

Cockpits are required for the HR 2000 only.

517 - HULL

517.01

The hull must be of a hydro type (the sponson should not exceed 60% of the total length of the hull).

Pickle forks: as per 509.02.

Either a reinforced cockpit or side protection as per rule 522.02, even if the driver is in a sitting position. ***If in a sitting position the reinforcing must extend up to approximately the driver's shoulder height and forward to approximately the driver's feet.***

517.02

Any device to produce a sudden braking effect, causing excessive water spray reducing visibility, is forbidden.

518 - MACHINERY

518.01

Only naturally aspirated reciprocating carburetted boat, motorcycle or motor car engines older than 10 years, are permitted.

It will be the responsibility of the driver to prove the eligibility of the engine.

The drive line between engine and propeller to be basically straight (no Z drives). A gearbox may be attached at the motor however it is forbidden to change the gears while racing.

518.02

For HR 2000 cc. only automobile motors built before 1987 are allowed.

519 - FUEL

See 508.01, 508.03, 508.06, 508.07, 508.08, 508.09, 508.10, 508.11.

520 - RACING OUTBOARDS (O)

521 - CLASSES

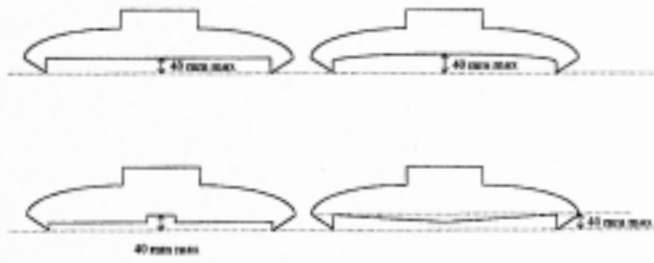
- O 125 cylinder capacity up to 128 cc
- O 175 cylinder capacity of the motor to 175 cc incl.
- O 250 cylinder capacity of the motor from 176 up to 250 cc incl.
- O 350 cylinder capacity of the motor from 251 up to 350 cc incl.
- O 500 cylinder capacity of the motor from 351 up to 500 cc incl.
- O 700 cylinder capacity of the motor from 501 up to 700 cc incl.
- O 850 cylinder capacity of the motor from 701 up to 850 cc incl.
- O 1000 cylinder capacity of the motor from 851 up to 1000 cc incl.
- O 1500 cylinder capacity of the motor from 1001 up to 1500 cc incl.
- O 2000 cylinder capacity of the motor from 1501 up to 2000 cc incl.
- O 3000 cylinder capacity of the motor from 2001 up to 3000 cc incl.
- O ∞ cylinder capacity of the motor from 3001 cc to infinity.

522 - HULL

522.01

For O 125, O 175, O 250 and O 350 the hull must be only of Hydroplane type with a maximum sponson length of 60 % of the total length of the hull.

Any longitudinal air entrapment structures on either side of the hull behind the sponsons must be less than 40 mm deep at the transom. They must not extend beyond a straight line drawn from the rear lower point of the transom and the point 40 mm below the bottom of the transom (measured from a straight line across the boat at the transom as shown on the sketches).



For the classes O 500 to O ∞ , the hull design is free.

522.02

All hydroplane hulls in classes O 250, O 350, OSY 400 must have reinforced side protection as shown in the sketches. All dimensions are minimum unless stated. Measurements taken from inside of hull from solid floor on which the driver lays or kneels (no cushions). Reduction to the front side panel is permitted for vision or to have vision panel installed.

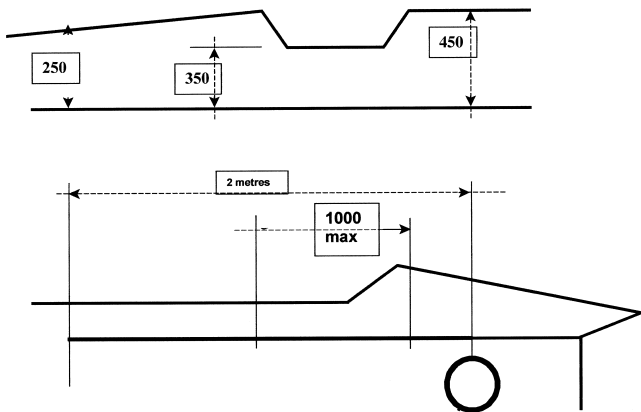
On kneelers the amount of cut out to keep stabilisation is as per drawings and no reduction at front.

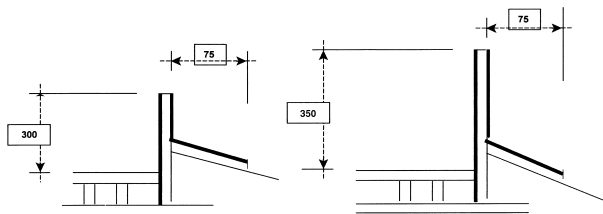
Reinforcing must be two layers of 315 grm Aramid Fibre (Kevlar) or comparable material.

This should be bonded to clean wood free of varnish paint etc.

As from 2006, all hydroplane hulls with pickle forks must have sponson leading edges of a rounded surface of at least 50 mm diameter in one direction. If extensions of the pickle forks are used, it must have deformable pickle forks for a minimum length to give an exposed rounded surface when removed of at least 50 mm diameter in one direction.

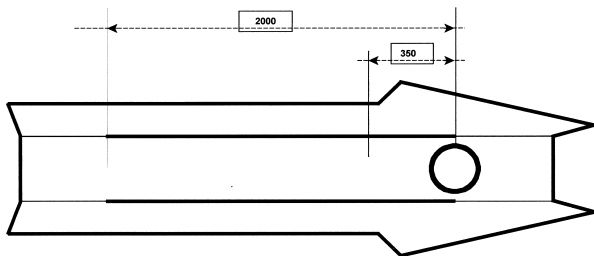
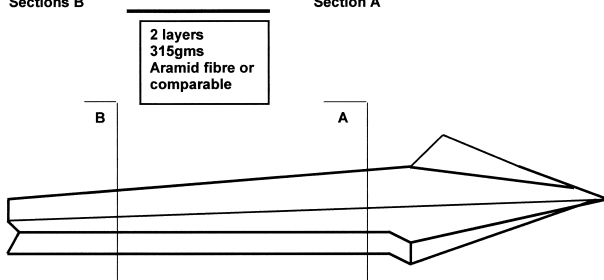
KNEELERS





Sections B

Section A



522.03

Any device to produce a sudden braking effect, causing excessive water spray, reducing visibility, is prohibited.

522.04 - MINIMUM DIMENSIONS

Class	Minimum Weight Hull only	Minimum Weight with driver	Reinforced cockpit
O 250	120 kg	190 kg	
O 350	130 kg	210 kg	mandatory *
O 500	180 kg	260 kg	mandatory
O 700	180 kg	260 kg	mandatory

Class	Weight	Length	Reinforced cockpit
O 850	250 kg	3.90 m	mandatory
O 1000	300 kg	4.20 m	mandatory
O 1500	350 kg	4.50 m	mandatory
O 2000	390 kg	4.80 m	mandatory
O 3000	450 kg	5.10 m	mandatory
O ∞	520 kg	5.30 m	mandatory

* Only for boats with a sitting driver (steering wheel over body, legs or feet)

- 1) The minimum weights mentioned here above are the weight of a complete rig weighed directly after the race without driver, residual water but with residual fuel.
- 2) Measurements are taken while the boat is ashore.

The length must be measured as the overall length of the hull between perpendiculars at the foremost and aftmost rigid part of the hull.

Any extending parts, rubbing starches, fenders, stabilising and trim tabs and rudder are not to be included.

523 - MACHINERY

523.01

The motor is free except Class O 125 which is restricted to single cylinder only.

An outboard motor is a mechanical propulsion assembly which can be removed from the boat as a single unit, complete with its transmission, and which does not transmit the power through the hull at any point. Any mechanism intended to modify the angle of attachment and/or the height of the motor is authorised.

The motor thus taken off and placed ashore must be capable of being started, fed by its fuel tank.

The mounting supports fixed to the boat, the control levers, the tachometer with its connections, the battery with its conductors and the fuel tank and fuel lines do not form part of the power unit.

523.02

Only one propulsion device is allowed and the total effort must be applied to the water.

523.03

When the propulsion is of the water jet type, the jet may be ejected in the air.

523.04

The transmission is free.

523.05

All un encased outboard motors with capacitor discharged ignition shall have a flywheel guard.

524 - FUEL

See 508.05, 508.06.

525 - CLASS OSY 400**527 - HULL****527.01**

The hull must be of a hydro type (the sponson must not exceed 60% of the total length of the hull).

Minimum weight hull only 100 kg

Minimum weight with driver 170 kg

527.03

A reserve of visible and sufficient flotation must be provided to keep the hull floating in case of accident. (see § 503.01)

527.05

Any device to produce a sudden braking effect, causing excessive water spray, reducing visibility, is prohibited.

528 - MACHINERY

Any machinery aimed at modifying the motor attachment angle and/or the height of the motor while the motor is running is forbidden.

528.01

The motor must be a Yamato model 102, 202 or 302 series duly homologated by U.I.M.

528.10 - SOLE MODIFICATIONS ALLOWED

- 1) It is permissible to use any ignition wire, spark plugs, propeller nut.
- 2) It is permissible to drill water drain holes in the ignition castings (coil plates and crankcase end cap) and these holes may be tapped and plugged.
- 3) It is permissible to use any fuel line or fittings providing threads in tank and carburettor are not changed. A fuel filter may also be used.
- 4) It is permissible to enlarge the gas tank by cutting it apart at the seam and adding at least 2" band (2" will give one third of a gallon more) or may build a complete tank, but it must be mounted the same as the original. The tank mount plate may not be modified.
- 5) A fuel pipe may be used for marathon racing but the hole in the crankcase must be plugged when racing same motor in closed course.
- 6) Bolt threads may be repaired by tapping oversize or use of helicoils.
- 7) Broken parts may be repaired by welding or use of plastic compound, if all contours remain the same.

- 8) Any water flow restrictions are permitted.
- 9) No polishing or removal of metal on any internal passages or carburettor, intake manifold, or tuned exhaust system.
- 10) No polishing or balancing of reciprocating or rotating parts.
- 11) Any type of thrust bracket adjustment is allowed providing no alterations are made to the motor and may not be adjustable while boat is moving.
- 12) The tilt lock lever may be removed and the spacer on the tilt bracket bolt.
- 13) The holes in steering bar may be drilled larger for attaching steering cable.
- 14) Any device may be used to hold spark lever in advance position.
- 15) Spray shield may be modified to use throttle cable, and for choking carburettor.

Where a hole is in the side it must not exceed 30 mm in diameter or equivalent area.

- 16) The propeller shaft may be turned down to fit any propeller.

The propeller is free

- 17) The use of non-Yamato parts are forbidden.
- 18) Measurements not quoted in the homologation sheet may not be altered. If a detail cannot be verified with the aid of the pictures in the homologation sheet, the part in question can be compared to corresponding stock part. In such a comparison it is only possible approximately to use the comparable measurements as the manufacturing tolerances are not available.
- 19) The driver is responsible for the condition of the engine as raced. Errors on the part of one manufacturer, mechanic or previous owner is no excuse.
- 20) Shorting switch is mandatory (i.e. jack plug).

- 21) Oversize pistons allowed to first bore only (max. bore size 2.618 - 66.50 mm).
- 22) Any type, size or make of nut, bolt, screw, washer may be used except for butterfly screw which must be standard.
- 23) Painting - it is permissible to paint any part of engine and accessories any colour.
- 24) Any holes can be drilled to safely attach engine to the hull.
- 25) A part from the above provisions, it is absolutely forbidden to make any alterations to the engine manufacturers standard components either by removal or addition of material. In particular this includes components being subjected to any form of machining, grinding, hand tooling, reworking, reshaping, fettling, polishing etc. unless it is specifically permitted.
- 26) Plug caps may be changed.

529 - FUEL

See 508.01, 508.02, 508.06, 508.07, 508.08, 508.09, 508.10, 508.11.

530 - SPORT INBOARDS (E)

531 - CLASSES

E 1000	cylinder capacity of the motor to 1000 cc incl.
E 1500	cylinder capacity of the motor from 1001 to 1500 cc incl.
E 2000	cylinder capacity of the motor from 1501 to 2000 cc incl.
E 2500	cylinder capacity of the motor from 2001 to 2500 cc incl.
E 5000	cylinder capacity of the motor from 2501 to 5000 cc incl.
E 7000	cylinder capacity of the motor from 5001 to 7000 cc incl.
E ∞	cylinder capacity of the motor from 7001 to infinity

532 - HULL

532.01

The hull design is free.

532.02

Any appendage intended to lift the boat by an aerodynamic effect is prohibited. Hydrofoils are not allowed.

532.03

Aboard inboard craft, the motors must be housed in a covered compartment or situated under the deck but adequately and efficiently ventilated.

Transmission links and all moving parts liable to cause injury or accident must be efficiently guarded.

532.04

Any device to produce a sudden braking effect, causing excessive water spray, reducing visibility, is prohibited.

532.12 - MINIMUM DIMENSIONS

Class	Weight	Length	Reinforced cockpit
E 1000	300 kg	4.20 m	mandatory
E 1500	350 kg	4.50 m	mandatory
E 2000	390 kg	4.80 m	mandatory
E 2500	420 kg	5.10 m	mandatory
E 5000	600 kg	5.70 m	mandatory
E 7000	700 kg	6.10 m	mandatory
E ∞	800 kg	7.00 m	mandatory

1. The minimum weights mentioned here above are the weight of a complete rig weighed directly after the race without driver, or residual water but with residual fuel.
2. For all series mentioned above :

Measurements are taken while the boat is ashore.

The length must be measured as the overall length of the hull between perpendiculars at the foremost and aftmost rigid part of the hull.

Any extending parts, rubbing starches, fenders, stabilising and trim tabs and rudder are not to be included.

533 - MACHINERY

533.01

The motor must be fixed inside the hull.

The power transmission train must pass through the structure of the hull.

533.02

Marine motors homologated by U.I.M. as having been built in series, that is with all identical and interchangeable parts and fitted ready for running, with a minimum number of 30 motors produced.

533.03

The marine inboard motor is a mechanical assembly producing power, specially designed and manufactured for the propulsion of boats and which is catalogued and sold by an industrial firm.

533.04

The motor must be ready to function, and must include the following items:

- all the accessories required for the functioning and regulation of the cooling system,
- an oil cooler,
- the motor installation mounts,
- the starting device and a generator for recharging the battery.

533.05

The gear box and the exhaust manifold do not form a part of the motor.

533.06

Only one propulsion device is allowed and the total effort must be applied to the water.

533.07

Only automobile motors homologated by the Federation Internationale de Sport Automobile as laid out in the general list of FISA homologated cars in group N + A are allowed.

Group N: Production cars

Group A: Touring cars

533.08

No other automobile motors homologated by the F.I.A. are allowed for powerboating.

533.09

The alterations allowed by F.I.S.A are not allowed in motor boating. Only the alterations listed in Article hereunder are allowed.

533.10 - SOLE MODIFICATIONS ALLOWED

1. The replacement of the dynamo by an alternator is allowed provided that the securing points of the replacing accessory and its driving system remain unchanged.
2. The fitting of a transistorised ignition system is allowed, provided that no mechanical part of the motor is altered.
3. For U.I.M. or F.I.S.A homologated motors reboring is allowed provided the total cylinder capacity does not exceed the limits that have been provided for the class in which the motor has been homologated.
4. The air filter can be taken out or altered.
5. Dynamic air intakes can be fitted to the carburettors.
6. The mechanically operated fuel pump may be exchanged with an electrically operated fuel pump, without obligation to keep the original location.
7. An oil filter and/or a radiator may be added when not provided by the manufacturer. The original(s) may be changed.
8. The carburettor(s) fitted by the manufacturer may be replaced by one or more carburettors or by a fuel injection system provided they can be fitted without altering the cylinder head.
9. The modification or replacement of the intake manifold is allowed.
10. The valve springs may be replaced by others from any origin, but without modification of the number fitted by the manufacturer and provided they can be fitted without any modification to the original brackets.
11. Pistons are free.

12. The camshaft(s) is (are) free.
13. Increasing the compression ratio by polishing the cylinder head (or using a thin gasket or the suppression of the latter) is allowed.
14. Any tuning operation such as polishing the original parts but not replacement of same, except for pistons, the springs and the cam shafts as specified above, are allowed.
15. In other words, provided it is always possible to establish indisputably the origin of a mass produced part, this component may be rectified, balanced, adjusted, its form reduced or changed by brightening, excluding any addition of material, any mechanical extension or any treatment modifying the characteristics of the molecular structure of the metal surface.

533.11 - ELECTRIC STARTER

An electric starter and its battery are required on board, as well as a device for recharging the battery.

533.12 - TRANSMISSION

Reverse gear is compulsory.

The transmission is free.

Going astern must be obtained by reversing the rotation of the transmissions which supply the propulsive effort. In waterjet propelled boats, the astern movement is obtained by diversion of the waterjet.

The control handle for reverse gear, ready for use, must be within hand reach of the driver. Astern manoeuvring of the boat must be possible by the action of reverse gear solely.

533.13

Only one propulsion device is allowed and the total effort must be applied to the water.

534 - FUEL

See 508.01, 508.02, 508.06, 508.07, 508.08, 508.09, 508.10, 508.11.

540 - SPORT OUTBOARDS (S)

541 - CLASSES

S	175	cylinder capacity of the motor to 175 cc incl.
S	250	cylinder capacity of the motor from 176 to 250 cc incl.
S	350	cylinder capacity of the motor from 251 to 350 cc incl.
S	550	cylinder capacity of the motor from 351 to 550 cc incl.
S	750	cylinder capacity of the motor from 551 to 750 cc incl.
S	850	cylinder capacity of the motor from 751 to 870 cc incl.
S	1000	cylinder capacity of the motor from 871 to 1000 cc incl.
S	1500	cylinder capacity of the motor from 1001 to 1500 cc incl.
S	2000	cylinder capacity of the motor from 1501 to 2000 cc incl.
S	3000	cylinder capacity of the motor from 2001 to 3000 cc incl.
S	∞	cylinder capacity of the motor from 3001 to infinity

541.01 - LOW EMISSION ENGINES

In order to compete with parity in S.2000 class, low emission engines complying with 2006 EPA regulations will be allowed 30 % greater cylinder capacity (2600 cc). In addition, the effect of any air compressor used in support of and as part of the fuel injection system, shall be ignored in determining class displacement.

In order to compete with parity in S.3000 class, low emission engines complying with 2006 EPA regulations will be allowed 30 % greater cylinder capacity (3900 cc). In addition, the effect of any air compressor

used in support of and as part of the fuel injection system, shall be ignored in determining class displacement.

541.02 - LOW EMISSION ENGINES

In order to compete experimentally in S.850 class, low emission engines complying to 2006 EPA regulations will be allowed 20 % greater cylinder capacity (1020 cc). In addition, the effect of any air compressor used in support of and as part of the fuel injection system, shall be ignored in determining class displacement.

542 - HULL

542.01

Only catamarans are permitted from class S 550 onwards.

542.02

Any appendage intended to lift the boat by aerodynamic effect is prohibited. Hydrofoils are not allowed.

542.03

In S2000, **S3000**, **S infinity and endurance 2 + 3**, the homologated delayed operation buoyancy airbag is mandatory for all boats with sponsons constructed of composites (foam construction sandwich).

542.05

Any device to produce a sudden braking effect, causing excessive water spray, reducing visibility, is prohibited.

542.12 - MINIMUM DIMENSIONS

Class	Weight	Length	Reinforced cockpit
S 550	260 kg	3.70 m	_____
S 750	280 kg	3.90 m	_____
S 850	330 kg	3.90 m	mandatory
S 1000	380 kg	4.20 m	mandatory
S 1500	430 kg	4.50 m	mandatory
S 2000	470 kg	4.80 m	mandatory
S 3000	530 kg	5.10 m	mandatory
S ∞	600 kg	5.30 m	mandatory

1. The minimum weights mentioned here above are the weight of a complete rig weighed directly after the race including driver, personal safety equipment, residual fuel but without residual water.
2. Measurements are taken while the boat is ashore.

The length must be measured as the overall length of the hull between perpendiculars at the foremost and aftmost rigid part of the hull.

Any extending parts, rubbing starches, fenders, stabilising and trim tabs and rudder are not to be included.

543 - MACHINERY

543.01

An outboard motor is a mechanical propulsion assembly which can be removed from the boat as a single unit, complete with its transmission, and which does not transmit the power through the hull at any point. Any mechanism intended to modify the angle of attachment and/or the height of the motor is authorised.

The motor thus taken off and placed ashore must be capable of being started, fed by its fuel tank.

The mounting supports fixed to the boat, the control levers, the tachometer with its connections, the battery with its conductors and the fuel tank and fuel lines do not form part of the power unit. Only single motor is permitted.

543.02

To be homologated as a sports motor, an outboard motor must be sold and advertised by an industrial firm as being manufactured in standard production series (that is to say with all parts interchangeable and with identical dimensions and materials with the equivalent part in another motor of the same model.) for the propulsion of boats.

To have homologation approved the numbers verified as in 505.01 must be:

up to 1000cc 1000 complete units

1001cc - 2000cc

- 500 units, 25 of which may be fitted with midsection, brackets and gearcases as in § 543.07.

2001cc and upwards

- 250 units, 25 of which may be fitted with midsection, brackets and gearcases as in § 543.07.

543.03

A sports outboard motor must be able to run at a reduced speed.

543.04

When the motor is in the water, the cooling must be effected by the homologated water circulation pump.

543.05

An efficient gear changing system giving forward, neutral and astern movement is compulsory for motors up to and including 1000cc capacity. The control handle for reverse gear, ready for use, must be within easy hand-reach of the driver when he is in the normal driving position. Manoeuvring of the boat astern must be possible by selecting reverse gear.

The casing of the underwater unit (gearcase) is free. All internal parts referred to on the homologation file must be maintained. Should the homologation provide for it, the exhaust will be a propeller exhaust.

543.06

All lower unit exhaust openings must remain standard within the homologated size.

543.07

For motors 1001cc capacity and above, forward, neutral and reverse gear are not compulsory. Such fixed gear versions are not necessarily intended for sale to or be used by general non-racing public. However, they must be available as an integral part of the complete outboard motor and homologated as such. The mounting brackets, midsection including exhaust pipes and gearcase, together with any parts required to fit these, must retain the dimensions specified on the homologation form.

543.08

An electric or manual starter must assure a quick and easy start without external aids. It must be used as it was supplied by the manufacturer, no adjustment or alteration being allowed.

543.10 - SOLE MODIFICATIONS ALLOWED

1. All studs, screws, nuts, bolts and their washers are free as well as the method of locking them.
2. The original propeller may be replaced by another. Gearcases with a propeller exhaust must use propellers equipped with an exhaust tube of a diameter at least that of the machined recess into which the standard propeller boss fits at the rear of the gearcase. The length of this tube must be at least $1/2 (Q - P)$, where Q and P are the dimensions shown on the sketch of the underwater unit on the homologation file;
3. The trim tab may be altered or removed to accommodate a propeller;
4. The cooling must be provided by the water pump;
5. Thermostats and pressure valves of the cooling system may be removed;
6. The steering mechanism may be altered. If the original steering bar is removed or new ones are installed, any openings created must be sealed to prevent the motor from pulling in additional air;
7. The swivel bracket may be altered for the purpose of installing a power trim and/or a power lift and the dampers (shock absorbers) may be altered or removed;
8. The rubber mounts of the motor may be altered, removed or replaced;
9. Revolution counters, water temperature gauges, water pressure gauges and similar instruments, can be installed;
10. Revolution limiters may be removed;
11. The gear interlock device on the starter may be removed;
12. Spark plugs are free.
13. Original carburettor jets may be replaced for another size;

14. Springs may be added to the throttle lever of the carburetors;
15. Re boring is allowed, but only piston assemblies supplied by the manufacturer of the motor may be used within the cc limit of the class;
16. A part which is dimensioned in the homologation sheet may be machined for the purpose of reaching that specific measure;
17. A part may be machined to attain the weight quoted in the homologation sheet without altering other criteria given for the part in question. It is not allowed to change the flywheel but balancing machining is allowed if the dimensions and weights provided for on the homologation sheet are respected;
18. Measurements not quoted in the homologation sheet shall only be checked by visual comparison with standard parts. As the actual manufacturing tolerances are not published, small differences between the inspected part and the reference part must be accepted.
19. The fuel connector in the lower cover may be removed and the fuel hose from the fuel tank connected directly to the fuel pump. If this is done the opening left after the connector and around the fuel hose must be sealed to prevent extra air entering inside of cover.
20. Electric fuel pumps may be added provided the fuel still runs through the original fuel system and no parts are removed or blocked off.
21. No parts may be added to the motor unless specified in this rule.
22. It is allowed to extend the starter cord of a rewind starter so that the starting handle can be reached from the cockpit

543.11 - MODIFICATIONS NOT PERMITTED ON LOW EMISSION ENGINES

1. Blueprinting of the powerhead is not permitted, equivalent to F2000/SST 120 classes.

2. Alteration or removal of any powerhead components is not permitted.
3. Electrical components and control system performance must remain standard

The following rules are NOT applicable to low emission engine:

543.10 Section: 5, 10, 13, 16, 17 and 20

502.02 Section: 3 and 2, last sentence not valid (Only adjustments to meet dimensions in the homologation sheet are allowed)

544 - FUEL

See 508.01, 508.02, 508.06, 508.07, 508.08, 508.09, 508.10, 508.11.

550 - MONOHULL OUTBOARDS (T)

General: The purpose is to limit this class to simple monohulls easily built by an amateur or readily available commercially, each fitted with one single homologated "stock" motor, available on standard sale.

551 - CLASSES

JT250 cylinder capacity of the motor to 265 cc incl.

T 250 cylinder capacity of the motor to **265** cc incl.

T 400 cylinder capacity of the motor from 251 cc to 400,5 cc incl.

T 550 cylinder capacity of the motor from 401 cc to 550 cc incl.

T 750 cylinder capacity of the motor from 551 cc to 750 cc incl.

T 850 cylinder capacity of the motor from 751 cc to 870 cc incl.

551.01 - LOW EMISSION ENGINES

In order to compete with parity in T classes, low emission engines complying with 2006 EPA regulations will be allowed 30 % greater cylinder capacity.

552.01 - HULL

Only monohull form is permitted. The boat shall not present in any of its lines any configuration which could contribute to aerodynamic lift.

Transverse steps, tunnels, hydrofoils or devices which tend to add to the air pressure under the hull, are prohibited, except that protruding strips substantially parallel to the fore and aft line of the keel are permitted providing that in any channels, etc. so produced the horizontal measurements, of such openings with the boat on an even keel, do not add up to a total of more than 15 cm in any transverse section. Any divergence of such strips from a line parallel to the keel must show a minimum radius of 30 cm. If stopped short of the transom, strips must be tapered off to zero over a minimum length, viewed at 90 to the line of the keel and parallel to the surface of the hull in that area, of 15 cm. A single fixed vertical fin on the underwater body is allowed for directional stability. The maximum length of the fin is 250 mm.

552.05

Any device to produce a sudden braking effect, causing excessive water spray, reducing visibility, is prohibited.

552.12 - MINIMUM DIMENSIONS

Class	Weight	Length	Width	Boat depth
JT250	160 kg	3.10 m	1.25 m	0.35 m
T 250	180 kg	3.10 m	1.25 m	0.35 m
T 400	240 kg	3.50 m	1.30 m	0.35 m
T 550	265 kg	3.75 m	1.30 m	0.40 m
T 750	270 kg	4.00 m	1.35 m	0.40 m

T 850 boat only	250 kg	4.25 m	1.40 m	0.40 m
T850 boat+driver	330 kg			

The maximum allowed ballast weight is 10 % of the specified minimum weight for each individual class.

1. The minimum weights mentioned here above are the weight of a complete rig weighed directly after the race including driver, personal safety equipment, residual fuel but without residual water
2. Measurements are taken while the boat is ashore.

The length must be measured as the overall length of the hull between perpendiculars at the foremost and aftmost rigid part of the hull.

Any extending parts, rubbing starches, fenders, stabilising and trim tabs and rudder are not to be included.

The width of the boat is measured at the widest part of the hull.

553 - MACHINERY

553.01

An outboard motor is a mechanical propulsion assembly which can be removed from the boat in a single unit, complete with its transmission and which does not transmit its power through the hull at any point.

The motor thus removed and placed ashore must be capable of being started, fed by its fuel tank.

The mounting supports fixed to the boat, the control levers, the tachometer with its connections, the battery with its conductors and the fuel tank and fuel lines do not form part of the motor.

The attachment angle and the height of the motor must remain fixed while the boat is under way. But it is allowed to change the attachment angle between a heat and another.

The use of powertrim is allowed for monohull boats provided with safety reinforced cockpit, per rule 509.

For all T classes, the centre of the propeller shaft (measured at the centre point of the rear end of the propeller shaft) shall not be above the bottom of the boat measured at the lowest point of the bottom. No design of the hull, by fins, keels or convex shaped bottom to obtain a higher engine position is allowed. For boats with the power trim this height will be measured with the propeller shaft set parallel with the bottom of the boat.

This dimension will be checked in the “as raced” condition.

This dimension is affected by the attachment angle. In JT.250, T 250, T.400 and T. 550, it is not allowed to change the attachment angle by other means than pulling out and resetting tilt pin or by adjusting a thrust block on the outside of the transom for which adjustment tools must be required. It is only allowed to change the attachment angle when the boat is on land. After any change of the attachment angle the propeller shaft depth must be rechecked.

553.02

To be homologated as stock motor, an outboard motor must be sold and advertised by an industrial firm as being manufactured in standard production series (that is to say with all parts interchangeable and with identical dimensions, weights and materials) for the propulsion of boats.

To be eligible for homologation, a minimum of 1000 units must have been built and assembled, certified by the manufacturer to the National Authority of the country of origin.

553.03

The motor must be able to function at reduced speed.

553.04

When the motor is in the water, the cooling must be effected by a water circulation pump.

The cooling water must circulate through the water pump and be fed only through the standard water intake, neither the position nor the shape of which may be modified.

553.05

An efficient gear changing system giving forward, neutral and astern movement is compulsory.

The control handle for the reverse gear, ready for use, must be within easy hand reach of the driver, when he is in the normal driving position. Manoeuvring of the boat astern, must be possible by selecting reverse gear.

553.06

An electric or manual starter must assure a quick and easy start without external aids. It must be used as it was supplied by the manufacturer, no adjustment or alteration being allowed.

553.10 - SOLE MODIFICATIONS ALLOWED

Only the following modifications are allowed:

1. All studs, screws, nuts, bolts and their washers are free as well as the method of locking them.
2. The original propeller may be replaced by another, but in classes T.750 and T.850 3 blades maximum. Gearcases with a propeller exhaust must use propellers equipped with an exhaust tube of a diameter at least that of the machined recess into which the standard propeller boss fits at the rear of the gearcase. The length of this tube must be at least $1/2 (Q - P)$, where Q and P are the

dimensions shown on the sketch of the underwater unit on the homologation file.

3. The trim tab may be altered or removed to accommodate a propeller, except in classes T.750 and T.850.
4. The cooling system must be provided by the water pump.
5. Thermostats and pressure valves of the cooling system may be removed.
6. The steering mechanism may be altered. If the original steering bar is removed or new ones are installed, any openings created must be sealed to prevent the motor from pulling in additional air.
7. The dampers (shock absorbers) may be altered or removed.
8. The rubber mounts of the motor may be altered, removed or replaced.
9. Revolution counters, water temperature gauges, water pressure gauges and similar instruments can be installed.
10. Revolution limiters may be removed.
11. The gear interlock device on the starter may be removed.
12. Spark plugs are free.
13. Original carburettor jets may be replaced for another size.
14. Springs may be added to the throttle lever of the carburettors.
15. Re-boring is allowed, but only piston assemblies supplied by the manufacturer of the motor may be used within the cc-limit of the class.
16. A part which is dimensioned in the homologation sheet may be machined for the purpose of reaching that specific measure;
17. A part may be machined to attain the weight quoted in the homologation sheet without altering other criteria given for the part in question. It is not allowed to change the flywheel but balancing

machining is allowed if the dimensions and weights provided for on the homologation sheet are respected;

18. Measurements not quoted in the homologation sheet shall only be checked by visual comparison with standard parts. As the actual manufacturing tolerances are not published, small differences between the inspected part and the reference part must be accepted.
19. The fuel connector in the lower cover may be removed and the fuel hose from the fuel tank connected directly to the fuel pump. If this is done the opening left after the connector and around the fuel hose must be sealed to prevent extra air entering inside of cover.
20. Electric fuel pumps may be added provided the fuel still runs through the original fuel system and no parts are removed or blocked off.
21. No parts may be added to the motor unless specified in this rule.
22. It is allowed to extend the starter cord of a rewind starter so that the starting handle can be reached from the cockpit

554 - FUEL

See 508.01, 508.02, 508.06, 508.07, 508.08, 508.09, 508.10, 508.11.

555 - T1 EXPERIMENTAL CLASS

This experimental class is run in North-Central and South America. The rules are based on the S.2000 rules with the exception that only one boat brand ("Lee / Leepro manufacturer") is accepted.

560 - INFLATABLE BOATS (P)

561 - CLASSES

P 550 cylinder capacity of the motor to 550 cc incl.

P 750 cylinder capacity of the motor from 551 cc to 750 cc incl.

562 - HULL

The boats must conform with a model of the official catalogue of the manufacturer. Those designed and commercialised for the specific purpose of competitions must be excluded.

562.01

To reach a minimum weight, ballast is allowed. It must be permanent rigid and fixed to the inside of the hull. It should not exceed 10 percent of the weight of the hull without ballast.

562.02

Boats that take part in competitions must be listed in an official catalogue of the manufacturer, published before October 30 in the previous year.

562.03

Boats can be inflated only with atmospheric air and the number of independent compartments must be such that the boat can float even if one half of the compartments is out of service.

562.04

Boats with an inflated keel are allowed; those with a rigid underwater body made of glass resin or any other type of rigid underwater body are not allowed.

562.05

Any device to produce a sudden braking effect, causing excessive water spray, reducing visibility, is prohibited.

562.06

Only features provided for by the manufacturer are allowed under the water line.

562.07

The boat must be deflectable, collapsible or foldable so that no dismantled part has a length greater than one third of the full length of the inflated boat, excepting the floor boards.

562.08

No aerodynamic appendage is allowed. The shape of the hull is free. The keel, inflatable or rigid must extend along the whole floor length of the boat.

562.09

Steering by means of a steering wheel or tiller arm is allowed, provided the driver sits on the floor when operating a tiller arm.

562.10 - SOLE MODIFICATIONS ALLOWED

- type, number and position of seats;
- gear change and throttle control;

562.12 - MINIMUM DIMENSIONS

Class	Weight	Length	Width
P 550	65 kg	3.70 m	1.60 m
P 750	75 kg	4.20 m	1.70 m

1. The minimum weights mentioned above are the weight of a complete rig weighed directly after the race without driver, or residual water but including the residual fuel.
2. Measurements are taken while the boat is ashore.

The length must be measured as the overall length of the hull between perpendiculars at the foremost and aftmost rigid part of the hull.

Any extending parts, rubbing starches, fenders, stabilising and trim tabs and rudder are not to be included.

The width of the boat is measured at the widest part of the hull.

563 - MACHINERY

563.01

An outboard motor is a mechanical propulsion assembly which can be removed from the boat in a single unit, complete with its transmission and which does not transmit its power through the hull at any point. The motor thus removed and placed ashore must be capable of being started, fed by its fuel tank.

The mounting supports fixed to the boat, the control levers, the tachometer with its connections, the battery with its conductors and the fuel tank and fuel lines do not form part of the motor.

The attachment angle and the height of the motor must remain fixed while the boat is under way.

For P 550 the centre of the propeller shaft (measured at the centre point of the rear end of the propeller shaft) shall not be above the bottom

measured at the lowest point of the transom. This dimension will be checked in the “as raced” condition.

563.02

To be homologated as sports motor, an outboard motor must be sold and advertised by and industrial firm as being manufactured in standard production series (that is to say with all identical dimensions, weights and materials) for the propulsion of tourist boats.

To be eligible for homologation, a minimum, of 1000 units must have been built and assembled, certified by the manufacturer to the National Authority of the country of origin.

Only one propulsion motor is allowed and all of the propulsive effort shall be exerted through the water.

563.03

The motor must be able to function at reduced speed.

563.04

When the motor is in the water, the cooling must be effected by a water circulation pump.

563.05

An efficient gear changing system giving forward, neutral and astern movement is compulsory.

The control handle for the gear changing system, ready for use, must be within hand reach of the driver.

563.06

An electric or manual starter must assure a quick and easy start without external aids. It must be used as it was supplied by the manufacturer, no adjustment or alteration being allowed.

563.10 - SOLE MODIFICATIONS ALLOWED

Only the following modifications are allowed:

1. All studs, screws, nuts, bolts and their washers are free as well as the method of locking them.
2. The original propeller may be replaced by another. Gearcases with a propeller exhaust must use propellers equipped with an exhaust tube of a diameter at least that of the machined recess into which the standard propeller boss fits at the rear of the gearcase. The length of this tube must be at least $1/2 (Q - P)$, where Q and P are the dimensions shown on the sketch of the underwater unit on the homologation file.
3. The trim tab may be altered or removed to accommodate a propeller.
4. The cooling system must be provided by the water pump.
5. Thermostats and pressure valves of the cooling system may be removed.
6. The steering mechanism may be altered. If the original steering bar is removed or new ones are installed, any openings created must be sealed to prevent the motor from pulling air in additional air.
7. The dampers (shock absorbers) may be altered or removed.
8. The rubber mounts of the motor may be altered, removed or replaced.
9. Revolution counters, water temperature gauges, water pressure gauges and similar instruments can be installed.
10. Revolution limiters may be removed.

11. The gear interlock device on the starter may be removed.
12. Spark plugs are free.
13. Original carburettor jets may be replaced for another size.
14. Springs may be added to the throttle lever of the carburettors.
15. Re-boring is allowed, but only pistons supplied by the manufacturer of the motor may be used within the cc-limit of the class.
16. A part which is dimensioned in the homologation sheet may be machined for the purpose of reaching that specific measure;
17. A part may be machined to attain the weight quoted in the homologation sheet without altering other criteria given for the part in question. It is not allowed to change the flywheel but balancing machining is allowed if the dimensions and weights provided for on the homologation sheet are respected;
18. Measurements not quoted in the homologation sheet shall only be checked by visual comparison with standard parts. As the actual manufacturing tolerances are not published, small differences between the inspected part and the reference part must be accepted.
19. No parts may be added to the motor unless specified in this rule.

564 - FUEL

See 508.01, 508.02, 508.06, 508.07, 508.08, 508.09, 508.10, 508.11.

565 - RIGID INFLATABLE BOATS (PR) (PF)

566 - CLASSES

PR 550 cylinder capacity of the motor to 550 cc incl.

PR 700 cylinder capacity of the motor from 725 cc incl.

PR 750 cylinder capacity of the motor from 551 cc to 770 cc incl.

PR 850 cylinder capacity of the motor from 771 cc to 870 cc incl.

PR 1000 cylinder capacity of the motor from 871 cc to 1060 cc incl.

PR 1500 cylinder capacity of the motor from 1061 cc to 1550 cc incl.

PR 2000 cylinder capacity of the motor from 1551 cc to 2050 cc incl.

PR 3000 cylinder capacity of the motor from 2051 c to 3150 cc incl.

A free category (e.g. catamaran) is established in the following classes:

PF 550 cylinder capacity of the motor to 550 cc inclusive

PF 750 cylinder capacity of the motor from 551 cc to 750 cc incl.

PF 850 cylinder capacity of the motor from 751 cc to 870 cc incl.

567 - HULL (PR CLASSES ONLY)

567.01

The bottom of the hull may be convex, but not be concave.

Hull consists of rigid centre section and pneumatic tubes.

The rigid centre section, keel, transom etc. must be made of fibreglass or plastic.

Pneumatic tubes must be inflated with atmospheric air and made with fabrics with plastometers and elastometers in two or more independent compartments.

567.02

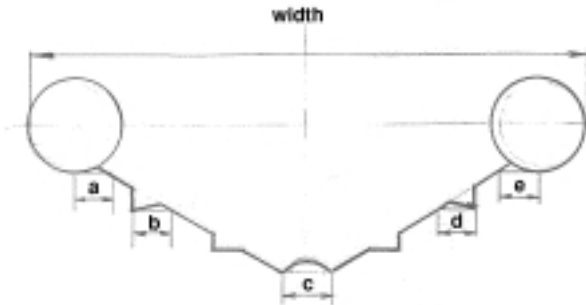
To reach the minimum weight, fixed ballast is allowed, and must be permanent rigid and fixed to the inside of the hull.

It should not exceed 10 percent of the weight of the hull without ballast.

567.03

Only monohull form is permitted. The boat shall not present in any of its lines any configuration which could contribute to aerodynamic lift.

Transverse steps, tunnels, hydrofoils or devices which add air pressure under the hull, are prohibited, except that protruding strips or redans parallel to the fore and aft line of the keel are permitted providing that in any channels, etc. so produced the horizontal measurements, of such openings with the boat on an even keel, do not add up be a total of more than 10% of the width of the hull in any transverse section **for PR 850 and below and 20 % for PR 1000 and up.** This includes the tunnels of the joint between the tubes and the rigid hull **for PR 850 and below.**



For PR 550, 700, 750, 850 : $a+b+c+d+e < 10\%$ of the width
For PR 1000, 1500, 2000, 3000 : $b+c+d < 20\%$ of the width

567.04

Steering by a steering wheel is mandatory, the control handle for reverse gear must be within hand reach of the driver.

Kind, number and position of seats are free.

Restraint system (driver harness) is prohibited.

The use of any device or part of the boat construction, which is blocking the driver from freely getting out of his sitting position, without removing the above-mentioned device or part, is prohibited.

567.05

Any device to produce a sudden braking effect, causing excessive water spray, reducing visibility, is prohibited.

567.12 - MINIMUM DIMENSIONS

Class	Weight	Length	WidthH	Min. Tube Diameter
PR,PF550	200 kg	3.50 m	1.50 m	0.25 m
PR700	280 kg	3.70 m	1.60 m	0.25 m
PR,PF750	280 kg	4.00 m	1.70 m	0.30 m
PR,PF850	300 kg	4.10 m	1.80 m	0.35 m
PR1000	470 kg	4.20 m	1.80 m	0.35 m
PR1500	520 kg	5.00 m	1.80 m	0.35 m
PR2000	600 kg	5.30 m	1.80 m	0.40 m
PR3000	680 kg	5.60 m	2.00 m	0.40 m

1. The minimum weights mentioned here above are the weight of a complete rig weighed directly after the race without driver, residual water but including the residual fuel.
2. Measurements are taken while the boat is ashore.

The length must be measured as the overall length of the between perpendiculars at the foremost part of the boat including the pneumatic tubes to the utmost part of the boat.

Any extending parts, rubbing starches, fenders, stabilising and trim tabs and rudder are not to be included.

The width of the boat is measured at the widest part of the boat.

- 3. *The tube diameter is measured amidships of the boat. The diameter in the bow should be at least 0.25 m irrespective of the class.***

568 - MACHINERY

568.01

Only one propulsion motor is allowed and all of the propulsive effort shall be exerted through the water.

An outboard is a mechanical propulsion assembly which can be removed from the boat in a single unit, complete with its transmission and which does not transmit its power through the hull at any point.

The motor thus removed and placed ashore must be capable of being started and run, fed by its fuel tank.

The mounting supports fixed to the boat, the control levers, the tachometer with its connections, the battery with its conductors and the fuel tank and fuel lines do not form part of the motor unit.

The attachment angle and the height of the motor must remain fixed while the boat is under way.

For PR,PF 550 the centre of the propeller shaft (measured at the centre point of the rear end of the propeller shaft) shall not be above the bottom of the boat measured at the lowest point of the transom. This dimension will be checked in the "as raced" condition.

568.02

The outboard motor must be able to function at reduced speed.

568.03

When the motor is in the water, the cooling water must be fed by the water circulation pump.

568.04

A gear changing system giving forward, neutral and astern movement is compulsory.

The control handle for reverse gear must be within hand reach of driver.

The casing of the underwater unit (gear case) is free. All internal parts referred to on the homologation file must be maintained. Should the homologation provide for it, the exhaust will be a propeller exhaust.

568.05

An electric or manual starter to assure a quick and easy start without complications must be fitted.

It must be used as it was supplied by the manufacturer.

568.10 - SOLE MODIFICATIONS ALLOWED

Only the following modifications are allowed:

1. All nuts, screws, washers and bolts may be replaced.
2. The original propeller may be replaced by another. Gearcases with a propeller exhaust must use propellers equipped with an exhaust tube of a diameter at least that of the machined recess into which the standard propeller boss fits at the rear of the gearcase. The length of this tube must be at least $1/2 (Q - P)$, where Q and P are the dimensions shown on the sketch of the underwater unit on the homologation file.

3. The trim tab may be altered or removed to accommodate a propeller.
4. The cooling system must be provided by the water pump.
5. Thermostats and pressure valves of the cooling system may be removed.
6. The steering mechanism may be altered. If the original steering bar is removed or new ones are installed, any openings created must be sealed to prevent the motor from pulling in additional air.
7. The dampers (shock absorbers) may be altered or removed.
8. The rubber mounts of the motor may be altered, removed or replaced.
9. Revolution counters, water temperature gauges, water pressure gauges and similar instruments, can be installed.
10. Revolution limiters may be removed.
11. The gear interlock device on the starter may be removed.
12. Spark plugs are free.
13. Original carburettor jets may be replaced for another size.
14. Springs may be added to the throttle lever of the carburettors.
15. Re-boring is allowed, but only pistons supplied by the manufacturer of the motor may be used within the cc-limit of the class.
16. A part which is dimensioned in the homologation sheet may be machined for the purpose of reaching that specific measure;
17. A part may be machined to attain the weight quoted in the homologation sheet without altering other criteria given for the part in question. It is not allowed to change the flywheel but balancing machining is allowed if the dimensions and weights provided for on the homologation sheet are respected;
18. Measurements not quoted in the homologation sheet shall only be checked by visual comparison with standard parts. As the actual

manufacturing tolerances are not published, small differences between the inspected part and the reference part must be accepted.

19. No parts may be added to the motor unless specified in this rule.
20. The fuel connector in the lower cover may be removed and the fuel hose from the fuel tank connected directly to the fuel pump. If this is done the opening left after the connector and around the fuel hose must be sealed to prevent extra air entering inside the cover.
21. Electric fuel pumps may be added provided the fuel still runs through the original fuel system and no parts are removed or blocked off.
22. It is allowed to extend the starter cord of a rewind starter so that the starting handle can be reached from the cockpit/drivers position.

569 - FUEL

See 508.01, 508.02, 508.06, 508.07, 508.08, 508.09, 508.10, 508.11.

570 - BOATS WITH DIESEL MOTORS (D)

571 - CLASSES

571.01

As far as speed competitions are concerned, U.I.M. makes no distinction between boats with gasoline or Diesel motors.

571.02

This class is not subject to any restriction as to the form of the hull, the cylinder capacity or weight, except in the "offshore" class boat q.v.

571.03

For world speed, distance and duration records, the U.I.M. recognises a single class of boats with one or more Diesel motors.

572 - HULL**572.01**

The hull design is free.

573 - MACHINERY**573.01**

Reverse gear is not compulsory.

573.02

Any system of supercharging is allowed without penalisation.

573.03

In normal running order: a diesel motor is defined as one in which the ignition of the fuel air mixture is caused only by the heat of the compression.

573.04

The fuel used is injected into the combustion or pre combustion chamber, and not introduced into the inlet manifold together with the air for combustion.

573.05

To start the motor, either before a race or after its stopping at a refuelling station, conditions contained in Articles 573.03 to 573.04 are not obligatory.

573.06

Starting the motor must be done only under the direct supervision of the Technical Officer in order to have the conditions stipulated in rules 573.04 enforced at the start and during the whole event.

574 - FUEL

See 508.04.

580 - TURBO JETS AND ROCKET PROPELLED BOATS (TR)

581 - CLASSES

581.01

The two series are classified as follows:

250 kg 350 kg 500 kg 900 kg 1200 kg; ***unlimited (infinity) kg ; and prototypes.***

581.02

These boats are not entitled to race, though they can attempt world speed records.

581.03

Turbo-jet and rocket propelled boats are accepted as international series.

582 - HULL**582.01**

The hull design is free.

582.02

Weighing of the boats in running order must be made possible when a record attempt is made, by means of a balance duly certified by the appropriate Weights and Measures Authority.

582.03

The boats are to be weighed without driver, residual water, but with residual fuel.

582.04

Any device to produce a sudden braking effect, causing excessive water spray, reducing visibility, is prohibited.

582.05

The measurer must certify that the boat as weighed is in running order, only the addition of the driver, water, oil or fuel needed to operate is allowed.

582.06

A UIM registered reinforced cockpit is required. Following rules are recommended : 509.02 (HANS device and seat) ; 509.16 (canopies) ; 509.20 (air supply).

583 - MACHINERY**583.01**

A turbo-jet is a rotating motor which converts directly into mechanical work, the energy produced from an expanding fluid.

583.02

A rocket is an motor which burns two fuels other than air. These two fuels are combined so as to obtain a mass of gas which is ejected at high speed.

583.03

Propulsion is according to the physical law which states that action and reaction are equal and opposite.

584 - FUEL**584.01**

The fuel is free. (To be ratified by 2005 G.A.).

When the digatron system is used, the recommended instructions for fuel testing can be obtained at the UIM Secretariat.

The fuel tests should be made available to the competitors at least one hour before competition.

590 - BOATS WITH AERIAL PROPELLER (A)

591 - CLASSES

591.01

The boats in this series are divided into classes as follows:

250 kg - 350 kg - 500 kg - 900 kg - 1200 kg and ? kg.

592 - HULL

592.01

The hull design is free

592.02

Weighing the boats in running order must be possible at any meeting or record course, by means of a balance duly certified by the appropriate Weights and Measures Authority.

592.03

The boats are to be weighed without driver, residual water, but with residual fuel.

592.04

Any device to produce a sudden braking effect, causing excessive water spray, reducing visibility, is prohibited.

592.05

The measurer must certify that the boat as weighed is in running order.

593 - MACHINERY**593.01**

The machinery is free.

594 - FUEL

See 508.05.

595 - ELECTRIC POWERED BOATS (E)**596 - CLASSES****596.01**

Battery powered 48 volt runabout

Battery powered 72 volt runabout

Battery powered 144 volt runabout

Battery powered 48 volt hydroplane

Battery powered 72 volt hydroplane

Battery powered 144 volt hydroplane

Solar powered

Hybrid battery/solar powered

596.02

Electrically propelled boats are accepted as an international series.

597 - HULL

597.01

Electric powered Runabout.

Hulls are defined as a displacement type, having no steps, no breaks in the longitudinal or transverse continuity of the immersed surface other than the keel, rubbing strips or lapstrakes of no more than 18.75 mm (75 in) and parallel to the centreline of the boat.

Hulls are not permitted to depend on external air pressure or design which creates a tunnel effect to aid planing.

Hulls are not required to conform to any maximum or minimum dimension restrictions.

Hulls shall not have a sponson/pod protruding from the side of the boat which interrupts the line of the side non-trip or bottom of the boat.

597.02

Electric hydroplane :

Hulls are unrestricted. Any boat having a multiple planing area or which cannot qualify as a runabout is considered a hydroplane.

597.03

Hybrid solar/battery powered marathon boats :

The hull is free.

598 - MACHINERY

598.01

The whole of the propelling power shall be electric.

Excluding noted class restrictions for motors, drive train design is unrestricted.

598.02 - ELECTRIC RUNABOUT AND HYDROPLANE

The whole of the electrical energy shall be battery stored and carried on board and when the boat is being timed or raced over the measured distance, no additional electric power shall be generated.

Power source shall be multiples of commercially available lead/acid batteries, nominally rated at 12 volts.

Motors are restricted to brush type direct current only.

598.03 - BATTERY POWERED MARATHON BOATS

The whole of the electric energy shall be battery stored and carried on board and when the boat is being timed or raced over the measured distance, no additional power shall be generated.

Power source shall be multiples of commercially available lead/acid batteries, nominally rated at 12 volts.

Total battery weight shall not exceed 56.699 kg (125 pounds). All batteries shall be carried on board during a marathon event.

598.04 - SOLAR POWERED

The whole of the energy required shall be from ambient solar power excluding wind. Solar collectors designed as rigid or flexible sails are not allowed.

598.05 - HYBRID SOLAR/BATTERY POWERED

The whole of the propulsion energy shall be battery stored and carried on board.

Solar power may charge the batteries while the boat is being timed or raced over the measured distance.

599 - ELECTRIC BOAT SAFETY RULES

599.01 - ELECTRIC BOATS

Exposed terminals shall be taped over or other wise insulated to prevent accidental shorts.

Batteries must be securely fastened to the boat in such manner that if the boat were to capsize, the batteries would remain with the hull.

Boats must incorporate enough flotation to ensure that, in the event of accident, the boat will float enough to be salvaged.

All electric boats in competition must be equipped with a physical disconnect. The purpose of the disconnect is to stop the motor, disable the electric system and limit power to safe levels in the event of an accident.

In any boat operating at voltages above 72 volts, such disconnect must be incorporated into the power system in a manner which splits battery power into packages of 72 volts or less.

The disconnect must consist of a plug and tether combination. The plug fits an in-line receptacle completing the electrical circuit and allowing it to be energised. The tether must be two metres or less in length and made of or attached to the plug by material not subject to heat damage such as melting or becoming brittle.

All safety tethers must be securely attached to the driver at all times whilst the boat and the driver are under way.

At any time an electric boat is launched, recovered, disabled on the course or is under tow, the disconnect plug shall be removed from the in-line receptacle.

A caution decal must be affixed in the vicinity of the disconnect receptacle, and to the bottom or non-trip at the same approximate station line. Its purpose is to warn rescuers against handling the boat at that point and possibly completing the electrical circuit.

All exposed drives and drive trains must be adequately covered or shielded with appropriate guards or covers.

Commutator shielding is recommended in all electric boats.

Protective clothing must be worn by all drivers participating in electric events.

599.02 - ELECTRIC BOAT RACING RULES

Starts must be dead motor starts. Drivers must draw for lane positions.

Classes and hulls shall be in conformance with class rules.

The competing boats may not be towed to a planing attitude. Any such assistance will result in the disqualification of the competing boat.

Boats may only be towed to an on-course starting point.

Once at the starting point all tenders and assisting craft must be removed to positions away from the racing line.

Electric boat drivers who's boats are not on plane by the time the lead boat reaches a predetermined point must move off course and stop. The shutdown point will be determined by the drivers prior to the drivers meeting. Failure of a driver complying with this rule will result in a one race suspension.

Recharging or replacement of batteries between the initial and return runs through the course is not allowed.

600 - RECORDS AND PERFORMANCES

600.01 - GENERAL

All record trials and their control are submitted to the following rules.

The record belongs personally to the driver who has established or beaten it. The record is entered under the name and nationality of the driver.

Any race against the clock, and called kilometre or mile trials or any similar name is prohibited if not conducted in accordance with these rules.

Combining speed records with distance records is not allowed.

No direct or indirect aid is to be provided to the driver who attempts a record.

600.02

World speed records, hour records, distance records and competition records are all records open to all boats and aquabikes of international classes adopted by U.I.M.

600.03

National records recognised by U.I.M. are the records established by boats of national classes duly recognised by the national authority (the rules of which have been approved by U.I.M.). The Certificate differs from those issued for World Records.

600.04

The World's Unrestricted Water Speed Record is awarded to the fastest boat in the world piloted by a driver, irrespective of the class of boat. This record is established in conformity with U.I.M. rules, ***including a UIM registered reinforced cockpit. Following rules are recommended : 509.05 (Hans Device and seat) ; 509.16 (canopies) ; 509.20 (air supply).***

601 - CLASSES ELIGIBLE FOR RECORDS

601.01

World records may be attempted with the following craft :

- All U.I.M. series and classes
- American Powerboat Association classes
- Prototypes

601.02

A boat can establish a record in its own class only.

601.03 - FROZEN RECORDS

When a class is modified so as to reduce the performance of the class, for example by :

- reducing the cylinder capacity,
- restricting the tuning permitted,
- restricting fuel permitted,
- introducing a minimum weight,
- increasing the minimum weight,
- or any other means,

the records established before the modification comes into force may be frozen on the determination of the Sports Commission and a new series of records will be started.

All proposal for new rules, rule changes and U.I.M. General Assembly minutes shall state :

- whether or not records will be frozen,
- the classes to be frozen
- the date(s) when the classes are to be frozen.

601.04 - QUALIFICATION

For offshore records a boat must have completed the course, taken the chequered flag and finished first, second or third, in a U.I.M. World or U.I.M. Continental Championship race in the class to be attempted and after the date when the class was last frozen.

602 - DEFINITION OF UNITS

602.01 - TIME, DISTANCE AND SPEEDS

The hour is the twenty-fourth part of a terrestrial day. The symbol is letter "h".

The minute of time is the sixtieth part of one hour. The symbol is letters "mn";

The second of time is the sixtieth part of one minute. The symbol is the letter "s".

nautical mile	=	nm
statute mile	=	st.m
kilometre	=	km
nautical mile per hour	=	knot
statute mile per hour	=	mph
kilometre per hour	=	kmh

602.02 - STATUTE MILE, NAUTICAL MILE AND KILOMETRE

The statute mile is an Anglo-Saxon measure.

The nautical mile is used at sea.

The knot is not a unit of length; it expresses the speed of one nautical mile per hour.

One statute mile (st.m) = 5280 feet = 0,8690 nm = 1,609.3 km

One nautical mile (nm) = 6080 feet = 1,852 km = 1,1508 st.m

One kilometre = 3280.844 feet = 1000 metres

604 - BASES

604.01

Records may only be established on bases recognised by National Authorities and notified to the U.I.M. in accordance with these rules.

604.02

The base is measured and certified by an official surveyor holding a certificate of competence from a recognised institute, in the presence of a delegate of the National Authority.

The base must be measured by triangulation or electronic distance measuring equipment (EDM), which must have a first class certificate delivered by an Observatory or similar authority. This certificate must not be older than two years.

The official surveyor provides a certificate in duplicate in which the strict adherence to the above requirements is certified.

The certificate is signed by the surveyor and countersigned by the delegate of the National Authority. One of the copies is for retention by the National Authority for its special national records files and the other is sent to U.I.M. for its special world record files.

The base for a record must be a fixed base, as laid out for a national authority or must be measured for a special occasion in waters designated by the driver. It must be defined by fixed marks ashore.

604.03

No distinction is made between records established on the sea, rivers or lakes. Records must not be attempted during the period between half an hour before and half an hour after a change of direction in flow of water over the course (change of tidal flow).

604.04

Any expenses incurred in the laying of a temporary course are defrayed by the entrant attempting the record.

604.05

For speed records there shall be only one attempt on the course at anyone time.

An attempt must not start until the director of the trial is satisfied that effective rescue boats are on the course.

605 - CALCULATION OF SPEED

605.01

The National Authority calculates the speed from the data provided by timekeepers. The speed is to be shown in kilometres per hour (kmh) and statute miles per hour (mph), even when the attempt takes place on a course of one nautical mile;

The duration of each run is obtained by taking the arithmetical mean of the times recorded by two sets of timing devices the mean time being given to one tenth of a second.

Should one of the timing devices stop or one of the signals fail and only one duration be registered, this duration will be accepted.

605.02

The duration so obtained serve to calculate the speed of each run. Only two decimal places are to be used.

605.03

The speed of the trial is the arithmetical mean of the speeds, two decimal places being used.

605.04

For speed records established on a distance of one nautical mile (1nm= 1.852 km = 1.1508 st.m) the following constants are to be used :

Where t is the time in seconds.

to obtain the speed in mph : $4141,9 \div t$

to obtain the speed in kmh : $6667,1 \div t$

606 - MARGIN REQUIRED

A record will be valid only if its speed at least equals the speed of the previous record multiplied by 1.003.

607 - MOTORS**607.01**

All accessories and/or elements required by the racing rules for each class of boat attempting to establish or improve a record must be on board during said attempt.

607.02

A boat which holds two certificates for two different classes is entitled to establish world records in those two classes, but shall make two distinct trials, one for each class.

607.03

Immediately after the record trial, the motor(s) or engine(s) must be sealed with a distinctive seal in the presence of the officials who have witnessed the trial.

607.04

Within 48 hours of the trial, the two measurers must check that the hulls, motors and equipment are in conformity with the specifications of their class, and this inspection must be carried out in the presence of the Officer of the Day or an Official specially appointed by the National Authority.

607.05

When attempting to break a U.I.M. class record, the noise level of the motor shall not exceed the authorised level (rule. 504).

607.06

It is mandatory to perform a noise measurement during record runs. The results of the measurements must be included in the signed measurers' report, sent to U.I.M.

608 - REQUEST TO ORGANISE

608.01

All applications for records trials must be made in writing to the National Authority at least one week before the trial and must be accompanied by the fee stipulated by the National Authority.

The National Authority applies to the U.I.M. for homologation of records.

608.02

Only records established under the direct control of a National Authority affiliated to U.I.M. can be recognised.

608.03

Each National Authority decides on the application for records to be established on its territory. The National Authority may refuse to consider a record application but must then explain the reasons in writing.

The National Authority indicates the validity of the record permit.

608.04

It is the duty of the driver to accept responsibility for any safety measures deemed necessary for this attempt.

The officials will not accept any responsibility, they will only record the results and ensure that the rules are complied with.

609 - OFFICIALS

609.01

The National Authority shall appoint an Official Observer and other duly qualified persons to hold the official functions.

An official so nominated shall not hold any other office than that to which he was nominated. An official shall not himself establish a record.

609.02

The official timekeepers and officials may receive payment for their services at rates fixed by the National Authority.

609.03

Time keeping may only be carried out by timekeepers and officials who have been recognised and authorised by the appropriate National Authority.

The stop-watches or any other apparatus employed must have a first-class certificate delivered by an observatory or similar authority. This certificate must not be older than two years.

Each timekeeper hands to the National Authority a signed report of the record attempt, countersigned by the official observer.

609.04

Timing from aboard anchored boats or aboard competing boats is not allowed.

610 - SPEED RECORDS

610.01

There is only one world record per class. This record can be established over the nautical mile, land mile or the kilometre, for aquabikes the distance may be 500 metres.

610.02

The course is covered once in each direction consecutively.

The course is to be marked at either end by two posts in solid material, put up on shore with approved distinguishing marks affixed to them. The two lines uniting the two pairs of posts are parallel to each other and at right angles to the line of the measured course. Steering marks may be placed wherever needed.

90 1 mile 90

1 kilometre

1 statute mile

610.03

In wide rivers and lakes, the width of the course may be marked off at each end by two buoys, moored at a distance of one hundred metres from each other. The boats must run between these buoys.

610.04

Timing is to be one tenth of a second up to 200 kmh. Above 200 kmh timing is to be by photoelectrical or similar apparatus to one hundredth of a second.

There must be a timekeeper and a timekeeper observer at each position.

Individual timers may be used at each position or a single timer electrically activated from each position.

Where two timers are used at a position the time is the average of the two timers.

The times of day of each attempt must be recorded.

The time elapsing between the two runs must not exceed 20 minutes except for jet attempts when the permitted interval is one hour.

610.05

A driver may re-start an attempt because of breakdown or other reason and may stop the engine or boat between runs.

If the attempt has not been completed within four hours of a starting time fixed by the National Authority, the officer in charge may stop the trial.

611 - DISTANCE RECORDS

611.01

Distance records are established on a total course of twelve nautical miles for the outboard series and twenty-four nautical miles for other series, and six nautical miles for aquabikes.

611.02

In distance records, the base is covered up and down successively in even numbers and the turns are not included in the distance.

The course for distance records may be :

- a course established for speed records;
- a base of three nautical miles maximum, in straight lines with buoys placed at the corners if necessary, no corner to be less than 135 degrees.

The buoys placed at the corners must always be rounded.

611.03

Any base must be defined by two posts set in concrete at either end.

611.04

Timing is to be to one tenth of a second.

The timing of distance records is done by a timekeeper with two timing devices at the starting line. He is assisted by two officials, one with him and one at the other end.

611.05

Stopping the motor or the boat during a distance record attempt is prohibited.

612 - HOUR RECORDS

612.01

The following international and national records are recognised by U.I.M. : 1, 2, 3, 4, 6, 9, 12 and 24 hours duration records for all the series recognised by U.I.M.

The homologation of a record of lesser duration than that applied for may be granted, but only within such limits as provided for by the above rule.

612.02

The base must not exceed six nautical mile from station to station, that is to say twelve nautical miles up and down. For aquabikes only, the distance may be 3 nautical miles from station to station that is to say 6 nautical miles up and down.

612.03

The base is defined by two posts set in concrete at either end.

612.04

The boat may be driven by more than one driver, but their names must be filed in the record application.

612.05

Repairs and adjustments which can be carried out on board may be effected on the course.

The same motor or engine and hull must be used throughout the trial.

On the other hand, repairs that cannot be effected on board and refuelling and changing of drivers must be effected at the control station and are to be supervised by an official.

612.06

Any time during which a boat stops during an attempt is included in the elapsed time for the attempt.

Turns are included in the course and are consequently not excluded from the timing.

612.07

Timing is carried out by one or several timekeepers with two timing devices stationed at the start. The timekeeper is assisted by two officials, one stationed with him and one stationed at the other end.

612.08

A driver can claim several different hour records during one trial by selecting the best series of consecutive laps.

612.09

The boat must cross the finishing line at the end of the lap (the last lap) during which the record time has elapsed.

The average speed of the last lap is calculated and the distance which corresponds to the time still to be run is added to the distance covered at the end of the penultimate lap.

The result is shown as the distance covered in the record time and the average speed for that distance.

612.10

Example for a two hour record on a base of six nautical miles (nm), twelve nautical miles per lap:

12 nautical miles (nm) \times 1.1508 = 13.81 statute miles (st.m)

boat completes 8 laps in 1h 54mn 40s = 110.48 st.m

ninth lap completed in 13mn 50s = 830 s

time balance 5 mn 20s = 320 s

distance balance = 13,81 st.m : 830x320 = 5.32 st.m

Total distance in 2 h = 115.80 st.m

Average speed = $115.80 \div 2$ = 57.90 mph

613 - WORLD COMPETITION RECORDS

613.01

The U.I.M. recognises world records of 3, 5, 10 and 15 statute miles in races for all U.I.M. circuit classes.

No other records may be established during a race.

Unlimited hydroplanes only are entitled to statute mile records established on :

- a single lap course (two laps for APBA Gold Cup races) while qualifying or racing,
- total heat average speed while racing.

614 - PERFORMANCE CERTIFICATES

614.01

U.I.M. may issue Performance Certificates for the best heats made during races for the principal international trophies.

614.02

Application for these is to be made by the National Authority of the country where the race took place and the accuracy of the results, timing course and class is to be certified by them.

614.03

U.I.M. only recognise the results which beat previous performances.

614.04

The amount of the fee for Performance Certificate is decided upon annually by the General Assembly.

614.05

U.I.M. may issue a performance Certificate for speed trials made by boats not included in U.I.M. classes. These trials must, however, be made in accordance with U.I.M. rules.

U.I.M. award Performance certificates for boats of the Production boats Series having established or broken speed, duration, distance or competition records according to the Record Rules.

614.06

For these performances, it is not permissible to remove any of the boat's accessories, or items of comfort, provided by the builder and these are compulsory for racing.

U.I.M. may award Performance certificates for any of the offshore classes, international and national, established at sea on properly measured runs.

615 - HOMOLOGATION OF RECORDS

615.01

Only records homologated by U.I.M. are considered as World Records.

Only records established under the direct control of a National Authority affiliated to U.I.M. can be recognised.

615.02

If a record has been established on the territory of two National Authorities, it will be entered as a record by the National Authority on whose territory the start was made.

As soon as a record trial has taken place, the National Authority communicate the results to the Secretary General of U.I.M. by fax and confirm it by letter.

The information has no official character and is only transmitted to such National Authorities as apply for it, in order to avoid useless trials.

615.04

The timekeepers and measurers must complete the official U.I.M. record application form which must be signed as correct by OOD and be sent to the National Authority within three days of the trial, seven days for APBA controlled classes.

The National Authority transmits the application for homologation, together with other necessary papers, to the U.I.M. no later than four weeks after the trial, sixteen weeks for APBA controlled classes.

615.05

For a request for homologation to be considered by the U.I.M., the National Authority must send :

- a plan of the course if not already in possession of the U.I.M.,
- the completed official U.I.M. application form which must include any National Authority calculations,
- a copy of the boat's measurement certificate,
- a photograph of the boat under way with the colours, inscriptions, etc. noted on the reverse ; also to be noted on the reverse, the driver's name, class and speed achieved,
- a list of the full names of the officials with, at least, the names of the Timekeepers, Measurers, Rescue Officer, OOD and Observer,
- for offshore boats, a copy of the results in which the boat qualified, as per U.I.M. rule 601.04,
- for attempts on tidal waters, the calculation stating the times of the changes of direction of water along the course (changes in tidal flow) and the source of the information,
- a cheque for the amount due

615.06

The recognition fee is fixed annually by the General Assembly.

615.07

Pending the official confirmation by U.I.M., neither the driver, builder nor any other shall publish or cause to be published, circulate or cause to be circulated, the results of the trial, until the delegate of the National Authority has given the actual results in writing, after checking the calculations in the presence of timekeepers.

Even when such authorisation has been received, results may only be published with the addition of the words "Subject to official certification by U.I.M." in as large and prominent type as that in which any part of the notification itself is printed.

Neglect to comply with the above regulations automatically entails the refusal to recognise the record. The National Authority have the right to penalise whoever is responsible.

615.08

U.I.M. issues annually a list of world records, changes being made known, when they occur, by means of newsletters.

U.I.M. keeps a registered list of all word records and delivers a certificate for each new record.

700 - ENVIRONMENT

701 - NOISE LEVEL

Every driver is responsible that his engine(s) does not exceed noise levels at all times as specified in Rule 504.

NOISE CONTROL

All race and record attempt organisers are responsible that noise control rule 504 is enforced.

702 - EMISSIONS

To reduce emissions as much as possible, all drivers are recommended to use lead free petrol and biodegradable lubrication oil.

703 - RE-FUELLING

Every care must be taken not to spill fuel or oil. An absorbent carpet to avoid any spillage on to the ground must be used.

704 - ENVIRONMENTAL CARE IN RACE AREAS

704.1

Every organiser must appoint a responsible person or persons to make necessary arrangements to control the environment of the whole race area including spectator areas.

704.2

There must be large containers in the pits where waste, such as cans, containers, oil waste can be placed.

One person must be responsible for this and to authorise the proper removal after the race meeting with the Local Authorities.

800 - CUPS AND TROPHIES

800.01 - INTERNATIONAL CUPS

The National Authorities must send to the Secretary of the U.I.M. a copy of their international Cup Rules, Year Books, official Statutes and other publications. These documents will be held at the disposal of all National Authorities.

801 - ALFRED BUYSSE TROPHY

The Alfred Buysse Trophy is perpetual.

The present regulations may be modified by the donor and after him by the General Assembly.

The holder of the cup must insure its safe keeping for a year and return it personally to the National Authority.

Two plaques, presented to winners, remain their property.

The Trophy goes to the driver who has the highest number of points from titled international meetings in the list established by the Sports Commission. The latter decides which of the classes entered will count for the competition.

Only the points obtained between 1st of November and 31 st of October by the first five drivers of each competition and of which the results have been properly communicated to the Secretary of the U.I.M. by the National Authority are taken into consideration.

The U.I.M. Secretariat publishes the results in the first bulletin following the receipt.

The interested National Authorities must indicate at the beginning of the year, which are the meetings held in their country counting for the Buysse Trophy.

All results counting for the trophy are to be sent to the U.I.M. Secretariat within a fortnight after each race.

802 - JOHN WARD TROPHY

The Trophy created in 1925 is a work of art worth 762 €. The funds were subscribed by several well-doers and the affiliated National Authorities.

It is attributed to the 0 500 class of racing outboards but the Council has the right to attribute the Trophy to another class subject to one year's notice.

The John Ward Trophy may be raced for only by drivers holding a driver's licence issued by their National Authority.

The John Ward Trophy is raced for annually during an international titled meeting.

There is only one race for the John Ward Trophy each year.

It is not allowed to combine this race with a cup or trophy bearing another name. However, the organisers may give cups and other prizes to the competitors.

The John Ward Trophy is perpetual. The organisers hand to the winner a cup bearing the inscription "John Ward Trophy - Anno..." as a souvenir.

Because of the fragility of the Trophy, the Union has decided not to dispatch it anymore to winners.

The race is in circuit, in two heats of 9 kilometres or 5nm each. Classification on points system as per rule 318.01 of the Racing rules. In case of a dead heat, the total elapsed times decide.

The Organising Committee must transmit the results to the Secretariat of the Union, without delay.

If the National Authority who has undertaken to organise the race cannot do so, they at once notify the Secretariat of the U.I.M. who may then offer the organisation to another National Authority.

The Trophy remains the property of the U.I.M.

In case the Union is dissolved, the extraordinary General Meeting held as per statutes decides what to do with it.

Any dispute concerning the attribution of the Trophy must be placed before the U.I.M. by the National Authority. The decision of the Union is final.

The present rules can be altered only by the General Assembly of the U.I.M.

The organisation of the meeting is decided by priority of the alphabetic rotation of the affiliated nations, the French names of these nations being considered to this effect.

Should a nation decline the organisation, the nation which comes next in rotation is taken into consideration.

Should a nation enter the race on the Calendar for a given year but fail to organise it, said Nation is considered as having held the race, as far as the rotation is concerned.

The attribution of the John Ward Trophy must be done under the same conditions as for the Championships.

803 - BOUCQUEY TROPHY

Same rules as Buysse Trophy.

The class is 0-850cc.

804 - DIETER KONIG MEMORIAL

Will be attributed to the F.500. The three first classified at the general classification will receive respectively a golden, silver and bronze medal.

805 - JOHN HILL MEMORIAL

A replica of the John Hill Memorial Trophy will be attributed each year at the last European race of the F1 World Championship. The criteria to allocate the Trophy will be “sportsmanship” and the allocation will be done by the F1 Committee. The original Trophy is kept at the U.I.M. office.

900 - U.I.M. RULES FOR RIVER MARATHON RACING

900.1 - GENERAL

1. River Marathon Race is a one day for classes: P and PR or a multiple day motor boat Race covering a predetermined distance per day with an overnight stop(s) at a designated area for refueling, repairs and rest if needed. The winner is the boat which uses the least elapsed time in covering the total prescribed distance. There will be an Overall Winner (first, second and third places), they will be the boats which start and finish all of the legs, under their own power, and they do it in the least amount of time, regardless of their class (the boat and/or the driver must not be changed).
2. International racing is open to the classes mentioned in Rule 900.15. Titles shall be given to these classes only. Other local classes can be invited by the National Authorities and or the organizers of River Marathon Races, if so, the classes and the capacity of the engines must be indicated in the Advance Program.
3. An entry fee may be set for all classes including those which may have been given a U.I.M. international Title and must be indicated in the Advance Program.
4. An Advance Programme must be sent by the organisers of an international River Marathon Race to other National Authorities and to the U.I.M. Office at least 90 days before the race.

900.2 - APPLICABLE RULES

1. River Marathon Racing is governed by these rules and by U.I.M. Circuit rule Groups 100, 200, 400.
2. In cases where these rules and the rules contained in the Circuit rule Groups are in conflict, these River Marathon rules will prevail.
3. Organisers of international races may prescribe deviations from the U.I.M. Group 100, 200 and 400 rules only when they are called for

by national legislation or by safety or environmental requirements, or when the organisers deem those rules unsuitable for River Marathon Racing in general. Such deviations can, however, be applied only if they have been published in the Advance Programme.

900.3 - RACE COURSE

1. The length of the race course must be such that it can be covered in a maximum of six days of racing, excluding possible lay days which can only be a maximum of 50% of the racing days. The minimum duration of a race is two days (for inflatable boats – one day **as per rule 900.1**), excluding a possible lay-day.
2. A day's race course can consist of more than one leg and it can be done in the same or different rivers. The organizers must allow sufficient time between each leg for the racers to get to the finish line, it should be proportional to the length of the leg. The time allowed must be indicated in the Advance Program and must be confirmed at the driver's briefing of each particular day and/or leg. If the race course is divided into a minimum of two legs, the distance to be raced in each leg must be such that it can be covered between official sunrise and sunset of the region in question.
3. The race course does not need to be registered or officially measured by the U.I.M.
4. A River Race can only be done on a rivers class I, II and III. Conditions on a river may vary from time to time during the year. In the Advance Program it must be indicated the class of the river for each of the legs of the race.
5. The Race Committee can **postpone** a day's race for 24 hours, if the weather conditions and / or river conditions are not adequate, according to Rule 309.

900.4 - CREW

1. Any **race boat** crew shall be comprised of no less than 2 members.
The boat driver must be in possession of a current racing license issued by his National Authority.
2. All crew members full names and home addresses must be communicated to the race organisers when the boats official entry to the race is made.
3. No change to the list of crew members shall be made between the official entry of the crew and the finish of the race, except when the race committee gives its explicit consent to the change.
4. Each crew shall have one crew member designated as Captain who is responsible for the boat and all actions of its crew. All matters related to the race shall be conducted through the Captain.
5. The minimum age of a crew member is 18 years, for younger crew-members (16years or older), they can race only with a written authorization from their parents.

900.5 - EQUIPMENT

In order to race, each race boat must be equipped with the following:

- a) A minimum of two oars or paddles
- b) A minimum of two ropes, one measuring 12 mm (1/2 inches) in diameter and 10 m (33 ft) in length to be used for towing, another rope measuring at least 10 mm (3/8 inches) in diameter and 15 m (50 ft) in length, it must have at one end a flotation device and the other end must be fastened to the structure of the racing boat.
- c) A set of flags: red and yellow, measuring each one at least 400 x 600 mm (16 x 24 inches) and attached to a pole 800 mm (31 inches) long.
- d) An emergency first aid kit
- e) A repair kit with necessary tools

- f) Two readily accessible fire extinguishers (ABC or equivalent) ***well secured to the boat*** having a minimum capacity each of 2.25 kg. (5 lb.).
- g) Life jackets for all crew members conforming to minimum specifications as stipulated in Rule 205.06.
- h) Crash helmets conforming to rule 205.07, for all crew members.
- i) *reserve letter*
- j) Clothing of any flammable material such as plastic or nylon, can be worn by crew members on top of the racing suit only in weather affected areas. The racing suit can be a one or two piece suit, it should be made of wool or cotton, any flame retardant material is highly recommended. Regular length pants and long sleeves are required.
- k) The use of helmet to life jacket back or side straps are highly recommended.
- l) The crew must wear eye protection during the race, it is recommended that it is in the form of shatter proof material (plastic goggles or it could be a full face protection).

900.6 - TECHNICAL SAFETY REGULATIONS

- 1. Fuel tanks must conform to the following when applicable:
 - a) Removable fuel tanks such as those used for outboards must have quick disconnect couplers to permit such tanks to be filled at a remote location, away from the boat
 - b) a fuel tank must be properly grounded. It must have a minimum 50 mm (2 inch) diameter spout to fit the fuel tender. The spout must be fixed to the boat.
 - c) fixed fuel tanks must be properly vented to the outside of the boat
 - d) all fuel tanks must be securely fastened.

2. Any water outlets exiting from the side of the boat must be directed horizontally or downward.
3. All propeller driven race boats must be equipped with a kill switch to stop the engine if the driver is separated from the boat.
4. For totally enclosed inboard engines the bilge must contain a blower or must be well ventilated.
5. It is recommended that all carburetors or induction inlets have flame arrestors, they should stay on the engine at all times when the engine is running.
6. All moving mechanical parts such as drive shafts, *flywheels*, belts, pulleys, etc., have a secure cover.
7. All Inboard engine boats and Outboard engine boats, must have a battery isolator switch. The switch should be accessible to the racing crew and must be marked by a red triangle 75mm (3 inches) sides.
8. There should not be any sharp elements or protruding elements in the boat were the crew is seating while racing the boat, as stipulated in Rule 509.09

900.7 - INSPECTION PROCEDURES

1. Motors, hulls and compulsory equipment subject to these rules shall be inspected prior to racing and after any of the legs have been finished to assure compliance with these rules. The technical inspection shall be conducted by designated inspectors who must report compliance or non-compliance with these rules.
2. The engine block may be given a unique identification mark to facilitate enforcement of Rule 900.8.13.
3. Each race boat and support boat, must go through a technical and safety inspection before the race. All equipment shall be available for inspection during the time and at the location specified for technical inspection. If any equipment is found not to be in compliance with these rules, or if the *captain* or driver has failed to

comply with the spirit or letter of these rules, the Race Committee may rescind the entry. Correction or substitution may be permitted if made prior to the start of the race. The start of the race shall not be delayed to allow compliance.

4. Immediately after crossing the finish line of the last leg of the race, contestants shall report with their boats and motors to the inspector at the location designated by the Race Committee. The boats and motors shall remain in the custody of the inspector(s) until dismissed. Any contestant(s) refusing to comply with the orders of the Chief Inspector or who violates the provisions of this rule may be disqualified by the Race Committee.
5. The Chief Inspector shall have the power, after the finish of the race, to order any contestant(s) to dismantle the boat or motor for inspection or measurement **at the designated area**. For the motor the **Chief** Inspector will be able to measure the engine capacity with a meter and/or gauge (if available), if the motor's capacity is on the upper limit, it will be to his discretion if the motor should or should not be dismantled. The cost of dismantling and assembling the boat or the motor should be paid by the contestant.
6. Any boat which after it has been inspected and according to its motor capacity should have raced in a bigger class, will be disqualified from the race by the Race Committee.

900.8 - GENERAL RACING RULES

1. Definitions:
 - a) Race Course: those rivers or waters, or segments thereof, between the first starting line and the last finish line as designated by the Race Committee
 - b) Leg: that segment of the race course between the starting line and finish line as designated for any particular part of the race.
 - c) Starting Line: the imaginary band, approximately one meter (three feet) in width, as drawn from the starting flag person to a

point across the river or water as described by the Race Director at the daily drivers meeting

- (1) To be ahead of the starting line implies being on the leg between the starting and finish lines so designated for that leg
 - (2) To be behind the starting line implies being out of the leg designated for that day.
- d) Staging area: that segment of the water immediately behind the starting line and continuing away from the starting line for a distance of approximately 50 meters (55 yards).
 - e) Holding area: that area behind the staging area and continuing for a reasonable distance away from the staging area.
 - f) Finish line: the imaginary band, approximately one meter (three feet) in width, drawn from the finish flag person to a point across and approximately perpendicular to the bank of the water. The finish line will be described at the drivers' meeting prior to the beginning of each leg.
 - g) Under its own power: The boat is in motion and not assisted by any other powered vehicle or craft. When starting a leg it can not be done by the motion generated from the water current and/or the rowing action of the crew. On finishing a leg it can be done by motion generated from the water current and / or the rowing action of the crew considering that the boat has run all of the leg without any external assistance.
This rule can not be changed in the Advance Program.
 - h) Under the control of its own crew: The crew is deemed to be able to control the destiny of its own boat.
 - i) Wilfully: a boat is under its own power and under the control of its crew.
 - j) Being in tow (taking a tow): the boat is put into, or is in motion with the assistance (in any way) of, another powered vehicle or craft.

- k) Allowed starting period: that period of time between a boat's designated starting time and the official closing of the starting line.
 - l) Pit area: that area of the parking lot designated by the Race Committee for parking of racing boats, trailers and support vehicles. Each entrant will have a designated parking space for one boat and trailer in addition to an adjacent space for a support vehicle. Also, any area where boats are parked between legs of the race will be considered a pit area.
 - m) Long block: the engine block, heads, valve train, crankshaft, rods, pistons, camshaft, and timing drive assembly.
2. A Drivers' Meeting will be held prior to each leg of the race at the advertised time. It is compulsory that the driver of each boat entered in the next leg attend the Drivers' Meeting. A roll call will be held and a 15 minute penalty assessed for drivers being absent when their name is called.
 3. The life jacket and helmet must be worn by everyone in a race boat at all time while the boat is in motion under its own power, this implies during racing, testing or tuning. Violation of this rule will result in assessment of a 15 minute penalty.
 4. Drinking alcoholic beverages or using intoxicating drugs by any contestant or race official prior to completion of racing is strictly prohibited. No boat (race or safety) driver shall operate a boat if alcoholic beverages or intoxicating drugs have been consumed. Violation of this rule will result in total disqualification or suspension of official duties.
 5. The use of offensive language by a crew member towards any other person or crew member **or race officials** will not be tolerated. Violation of this rule will result in assessment of a minimum ten minute penalty.
 6. Intentionally causing damage to another boat or intentionally causing bodily harm to a crew member of another boat will not be tolerated. Anyone failing to comply with this rule will be totally disqualified.

7. A crew member must obey the instructions of any **Race** official. Failure to do so will result **up to** total disqualification **from the race**.
8. The possession of firearms at any time during the event by any crew member is strictly prohibited. Violation of this rule will result in total disqualification.
9. No person shall provide or receive assistance from the air or by radio during the race. Failure to observe this rule will result in the contestant being disqualified for the leg. Global Positioning Systems (GPS) are allowed.
10. After the boat has passed inspection, the Race Committee may re-inspect it to insure that all of its compulsory equipment is present and technical specifications are in compliance with the rules.
11. Smoking is prohibited in the designated pit area and / or onboard a boat and / or in an area 3 m (15 ft) around the boat. Violation of this rule by any crew or member of the team will result in a penalty to the race boat. This penalty will be assessed by the race officials and can be of 15 minutes to disqualification of one particular leg.
12. The hull shall not be replaced during the race. Failure to abide by this rule will result in total disqualification from the race.
13. The organiser can decide to use Rule A or Rule B provided it is printed in the Advance Programme.

RULE A The engine block must **not** be changed during the race.

RULE B The complete engine can be changed once only during the race in the case of failure, provided he notifies the chief inspector who must authorize the change and inspect the engine to be replaced as well as the second one, for class homologation and give new identification numbers and / or mark. The second engine block cannot be replaced during the rest of the race.

The initial engine block cannot be rebuilt and used for the remainder of the race.

Failure to observe any part of this rule will result in total disqualification from the race.

Failure of the Advance Program to state any decision then rule A will apply.

14. A boat which has been disqualified for a leg, may continue in the race, for the disqualified leg it will have the same time as the boats which did not start (DNS) Rule 900.10.13.
15. Any issue not foreseen by these rules will be decided by the Race Committee, according to Rule 200.02.
16. The organizers must specify in the Advance Program if there will be fuel available for the race and if the fuel for the first leg is included, what type and / or quality of fuel and under what terms and conditions it will be supplied to the racing boats.
17. Fuel can be inspected by the Chief Inspector at any time during the race, this will be done according to the regulations and procedures indicated in Rule 508. Failure to let the fuel be inspected will result in total disqualification from the race. Regular Pump fuel refers to the fuel which anyone can buy on a regular basis in any fuel station at the race site, the octanage considered for the race, will be the highest available at these stations.
18. The race boat must have a set of race numbers on each side and one on the bow of the boat. The racing numbers size on the boat must comply with Rule 206.02. The colors can be according to this rule or they can be of another color with a contrasting surface surrounding them, or they could have an outline of at least 4 cm (1 7/8 inches) wide of a contrasting color.

900.9 - FLAG SIGNALS

- 1) Red Flag: raised and displayed by a safety boat **or a race boat** implies danger or emergency. All approaching boats must stop. It is the responsibility of a member of the safety boat, or race boat crew member if a safety boat is not present, to maintain a record of the time the boat is stopped. After stopping for a red flag, a race boat may not continue with the race until a safety boat has arrived and granted permission to continue the race even if the red flag was unjustly raised. Prior to granting permission to continue, the safety

boat must record all boats' times out and forward them to the finish line officials for the leg. If a boat fails to wait for a safety boat before proceeding with the race, the time out will not be acknowledged. The boat which makes improper use of the red flag, will be disqualified from the race by the Race Committee.

- 2) Yellow Flag: A safety boat member or race boat crew may raise its yellow flag only when they have a problem, when one of the crew is in the water, or they become an obstacle in the race course and the boat prevents or endangers the safe passage of oncoming boats or endangers the life of any or all the crew members. When a yellow flag is displayed the approaching boats should proceed with caution until they pass the problem area. The improper use of the flag will result in total disqualification of the offending boat for the leg.
- 3) Black Flag: do not leave the pit area - return to the pit area.
- 4) White Flag: It will be raised at the starting line, 2 (two) minutes before the starting time of the first race boat of that leg and lowered 10 seconds before the start
- 5) Green Flag: It will be raised for each boat at the starting line, 10 seconds before the starting time and lowered at the starting time. ***It will be raised again 10 seconds before the starting time of the following boat.***
- 6) Black and White Chequered Flag: raised with the arrival of a boat at the finish line and lowered when the boat crosses the finish line.

900.10 - STARTING

1. A ramp official will be present at the launching site, prior to the start of the race, to assure orderly and timely launching. Each boat may be given a launch time, each boat will have five minutes to clear the ramp / launch area. Failure to do so will result in a penalty of 15 minutes. Emergency situations requiring longer times will not be subject to the penalty at the discretion of the ramp official.
2. The starting order for other than mass starts, shall be determined by drawing lots. Subsequent starting orders for other than mass starts

will be determined from the cumulative elapsed times for previous legs. The starting order will progress from the fastest time to the slowest time. The starting line officials may, at their discretion, alter the order of the start if conditions so warrant. At the starting line the numbers of the next boat to start should be displayed to the racers, the size of the numbers should be at least of 300 mm (12 inches) high and they must be black on a white surface.

3. A leg is deemed to be open to racing on completion of the drivers meeting. Thereafter, and until the leg is officially closed as per Rule 900.12.3, the leg is deemed to be open to racing.
4. All boats must be in the water and behind the starting line when a leg opens to racing. Any boat which is in the water and ahead of the starting line at this time will be deemed to be wrongfully on the leg and can be assessed a penalty of 3 minutes. In order to get behind the starting line without incurring in this penalty, such boat must proceed in accordance with Rule 900.10.12.
5. *Reserve number*
6. While the leg is open to racing, if for reasons beyond the control of the crew, a boat crossed the starting line, prior to its designated starting time, it will be wrongfully on the leg, the crew must raise the yellow flag, if not it will be assessed a penalty of 2 minutes. When possible will have to return behind the starting line in accordance with Rule 900.10.12, in order to take his start.
7. A boat will be assessed a penalty of 1 minute per second if it wilfully crosses the starting line from the staging area prior to its designated starting time.
8. A boat is deemed to have started a leg if it wilfully crosses the starting line from the staging area at any time after the ten (10) second flag (for its designated start) is raised and before the starting line is closed.
9. Regardless of when a boat starts a leg, it will begin to accumulate a running time for that leg from the time that it was designated to start.

10. A boat that is unable to start at its designated starting time may remain in the staging area provided that it does not wilfully obstruct or impede a boat that is rightfully in the staging area. Failure to abide by this rule will result in assessment of a penalty of 15 minutes up to disqualification for the leg.
11. The boat that is unable to start at its designated starting time may cross the starting line from the staging area at any time within the allowed starting period provided that the ten second (**green**) flag **has not been** raised for another boat. Failure to abide by this rule will result in the assessment of a penalty of 15 minutes.
12. In order to return, or get behind the starting line when the starting line is officially open, a boat, wrongfully on a leg, must comply with the following procedures whenever applicable.
 - (a) If a boat is out of sight of the starting line, it may proceed with precaution at a reasonable speed and by no means at full speed against the direction of the race, navigating in the middle of the channel, this boat must display the yellow flag at all time. Failure to comply with this regulation will result in the assessment of a penalty of 15 minutes up to disqualification for the leg.
 - (b) If a boat is within sight of the staging area, it may proceed against the direction of the race only when another boat is not rightfully on the segment of the leg in the direction of the staging area, the returning boat must display the yellow flag at all time. Failure to comply with this regulation will result in the assessment of a penalty of 15 minutes up to disqualification for the leg.
 - (c) A boat may enter, or pass through, the staging area from ahead of the starting line only without obstruction or impeding a boat that is rightfully in the staging area and only when the ten (10) second flag is not raised. In the event of a mass start, a boat may enter, or pass through the staging area from ahead of the starting line only when there are no other boats rightfully in the staging area. Failure to comply with this rule may result in assessment of a penalty of 15 minutes up to disqualification for the leg.

13. A boat which is unable to start a leg including the last one (DNS) may continue to compete in subsequent legs. It shall be given a time for the unstarted leg equal to the Winner's time of his class for that leg, plus the time penalties according to the table in Rule 900.12.9

This rule can not be changed in the Advance Program.

14. ***If for any reason a boat will fully start on any previous minute, he will be assessed a penalty of 10 minutes per every minute that he advanced his start. In this case Rule 900.10.7 would also apply for the minute which the boat started in. For the time of the race leg, the jury will deduct the total amount of minutes which he advance his time and will add the corresponding time penalties.***

900.11 - RACE IN PROGRESS RULES

1. Any boat that is overtaking another boat must do so in a safe and courteous manner. Maintaining when possible a safe distance between the boats.
2. Any boat that is being overtaken must give way and let the faster boat through no later than the point at which the overtaking boat is within the peripheral vision of the driver. Encumbering a faster boat is not permitted and doing so will result in the assessment of a penalty of 15 minutes up to disqualification from the race.
3. Any boat that willfully travels against the direction of the race after it has started, must do so according to Rule 900.10.12.
4. The loss of any equipment required in Rule 900.5 during a race must be reported to a finish line official for that leg or a penalty of 5 minutes will be assessed.
5. A race boat might or might not be permitted to refuel on a leg which is open to racing. This should be indicated in the Advance Program, if it is not indicated, the race boats will not be permitted to refuel.
6. The crew of the boat must remain in their seats at all times during the race. It is not allowed for any member of the crew of the boat to

ride on the bow of the boat at any time during the race. Failure to abide by this rule will result in assessment of a penalty of 15 minutes.

900.12 - FINISHING RULES

For the purpose of the Rules 900.12, the winner is the race boat which finishes a leg under its own power in the least amount of time.

1. A boat must cross the finish line under its own power. All the boats of a class which cross the finish line within the time frame of the winner's time for that leg plus 40% will receive their actual time. The boats of a class which cross the finish line, after it has been officially closed, shall be given a time for the leg equal to the winner's time of his class for that leg, plus the time penalties according to the **adjustment factors** indicated in Rule 900.12.9. This will include the DNF boats Rule 900.12.4. This rule can not be changed in the Advance Program.
2. Once a boat has crossed the finish line, it may not re-enter an open leg, unless it is approved by race officials and it may only re-enter in order to get to the boat ramp. It will have to do it with precaution and displaying the yellow flag. By no means should cross the river if a racing boat is approaching the finish line. Failure to comply with this regulation will result in the assessment of a penalty of 15 minutes up to disqualification for the leg.
3. The leg's starting line and finish line for each class will be closed at the time indicated at the Driver's Meeting of that day, there will be a maximum of two hours allowed after the winner of a class arrives for them to be closed. If there are two or more legs which are run in a day, the closing times will be given at the previous Driver's Meeting(s). All the boats which cross the finish line after it has been officially closed will be given a time for that leg equal to the winner's time of his class for that leg plus the time penalties according to the **adjustment factors** indicated in Rule 900.12. 9, no matter how they got to the finish line by water or land. For the last leg if a boat has started the leg and was not able to finish within the time frame indicated in this rule it shall be given a time for the leg equal to the

winner's time plus the time penalties according to the **adjustment factors** indicated in Rule 900.12. 9.

This rule cannot be changed in the Advance Program

4. A boat which is unable to finish a leg (DNF) may continue to compete in subsequent legs, this boat must have started the leg under its own power and must have run at least 2 km (1.2 mi). It shall be given a time for the unfinished leg equal to the winner's time of his class for that leg, plus the time penalties according to the **adjustment factors** in Rule no. 900.12.9. If a boat arrives to the finish line after it has been officially closed Rule no. 900.12.3 will apply. This rule can not be changed in the Advance Program.
5. Official results will be determined by total elapsed time plus any **time** penalties incurred. ***They must be posted at the site and before the next day driver's meeting.***
6. Immediately after finishing a leg, the crew of the boat which has been picked for inspection should make his boat available to the Chief Inspector at the designated area and will be under the Inspectors custody until inspected. Failure to comply with this regulation will result in the disqualification for the leg.
7. If for any reason in one of the classes non of the boats finishes, the leg's finish line time for that class will be the time of the winner of the next smaller class plus 40 %. If there are no other classes to get this time from, the Finish Line Officials and the Chief Scorer will determine an Estimated Time, based on the distance of that particular leg at an average speed of that class, in this case for the boats which did not finish (DNF) this leg, they will have the Estimated Time plus the time penalties according to the **adjustment factors** in Rule 900.12.9, for the boats which did not start (DNS) this leg, they will have the Estimated Time plus the time penalties according to the **adjustment factors** in Rule 900.12.9.
This rule can not be changed in the Advance Program.
8. The time which will be given to the boats which do not start (DNS) or to the boats which finish after the finish line is closed or the ones which start and not finish (DNF), will the time of the winner of his class for that leg, plus the time penalties:

ADJUSTMENT FACTORS

DNF DNS

1.40 1.70

PROCEDURE

To calculate the time for a particular boat which did not start (DNS) or a boat which did not finish (DNF) will be the following: The time of the winner of the class for that particular leg must be multiplied by the corresponding factor.

900.13 - SUPPORT BOATS

1. The race committee shall provide sufficient boats to act as safety boats at critical locations along the race course. The safety boats shall be properly equipped to provide rescue and emergency first aid services. They will be subject to inspection by the Inspection Committee. All occupants in safety boats shall wear nationally approved type life jackets. The safety boat captain will be a Race Official. The safety boat captain and his crew must abide by the orders of the Safety Director.
2. The safety boats are not towing crafts, however, at the boat captain's discretion, he may provide a tow after the leg is closed to the racing boats, to the nearest boat ramp or either the starting or finishing lines.
3. The Race Committee shall provide a sweep boat which will have a Race Official on board. The sweep boat will be the last official boat to leave when the starting line is officially closed. The sweep boat captain will give directions to any race boats on the leg that are unable to continue under their own power.
4. The finish line will remain open until the sweep boat has crossed. Any race boat crossing the finish line after the sweep boat, and consequently after the leg has closed, will receive a time equal to the slowest time in its class plus a penalty of 10 minutes.
5. The sweep boat shall be clearly identifiable.

- 6. *The support boats of any of the race boats must pass a safety inspection as per Rule 900.7.3.***

900.14 - RACE OFFICIALS

Any Race Official may bring forth any infraction of these rules to the Race Director, the Race Official are:

- (a) Race Director
- (b) Safety Director
- (c) Safety Boat Captains
- (d) Chief Inspector
- (e) Chief Scorer
- (f) Starting Line Officials
- (g) Finish Line Officials
- (h) Ramp Officials

900.15 - INTERNATIONAL RACING CLASSES

1. **Series:** There should be a minimum of three boats registered per class, if there are less they will move up to the next available class.
 - A) Serie Inboard Boats, (I); classes SP2500, E, D, C, B, A, and Unlimited
 - B) Serie Outboard Boats (O); classes 2000 and 3000
 - C) Serie Inflatable Boats (P); classes 550V, 550, 850V and 850
 - D) Series Rigid Inflatable Boats (PR); classes 700, 750, 1000, 1500, 2000 and 3000
2. Inboard Engine Boat Classes:

Classes of Inboard Racing Boats: are determined solely on cubic capacity of the engine used and are distinguished as follows:

Hull: The hull design is free.

Length of the Hull: The minimum dimension are 3.96 m (13 ft).

Roll Bar: Any aluminum boat on classes E or above must have a well structured steel chrome molly roll bar. ***Aluminium boats on class SP2500 must also have a roll bar. The minimum specifications for the chrome molly roll bar are : 1 1/2 "in diameter with a wall thickness of 0.065". It is also recommended that the main "A" frame(s) be made of one piece and the structure of the roll bar be done with a set of triangles with front and back supports. It is also recommended that the primary elements of the roll bar be secured to the main engine bearers and the chine, if possible the seats and seatbelts should be attached on the roll bar (cage) structure.*** For the existing fiber glass boats it is recommended that a roll cage or a reinforced cockpit be put in. As of 2004 it will be compulsory that all fiber glass boats have one. The roll bar should be at least 100 mm (4") away from any side of the helmet of the racing crew when seated, as stipulated in Rule 509.05.

Seats: There should be a high back seat for each member of the racing crew, securely attached to the hull. The back of the seat should support the back of the helmet.

Seat Belts: The crew of any racing boat with a roll bar, must be strapped to the seat with four point seat belts system, at all time during the race, the straps should have a minimum width of 50.8 mm (2"). Two seat belts straps should run over the waist at the height of the hips and the other two straps should run over the shoulders. It is recommended that the wear of the straps should be checked periodically.

Engine Mounts: The engine on classes E and above must be attached to the hull, at least on four points, unless the set up of the jet and the engine allows it to only be attached in three points closed coupled. ***A safety device must be*** installed to the engine to avoid ***its*** forward movement in a case of a front collision.

Throttle: The throttle pedal should be attached to the hull and should be spring loaded, so it is capable of returning to the idle position.

Flotation: Its is recommended to put in the bow of the hull sufficient flotation material that would allow the recovery of the boat.

The engines on these class boats should be inboard, for each class there is a specific engine capacity range. All the boat must run with a propulsion of a jet pump and must not have any exposed propellers.

Classes	Capacity
SP 2500	Up to 2500 cc / Two cycle
E 3400	Up to 3400 cc
D 4200	3401 to 4200 cc
C 5100	4201 to 5100 cc
B 6000	5101 to 6000 cc
A 7700	6001 to 7700 cc
Unlimited	Above 7701 cc
Overall	Any of the above

Multiplication Factors:

- Forced Induction engines or those engines using combustion supporting additives (e.g. nitrous oxide, turbo charged, etc.), will be required to multiply their engine capacity by 1.8 to obtain their racing class.
- Rotary Engines will be required to multiply their engine capacity by 1.8 and if forced induction is utilized a further 1.4 to obtain their racing class.
- Diesel engines are not subject to any multiplication factors to obtain their racing class irrespective of whether forced induction is utilized.
- Turbine engines boats are allowed, any boat with one will run in the A / Unlimited class.

Exhaust : the use of an efficient manufacturer brand muffler is recommended. As of 2006, it will be compulsory that all boats have one at all times during practice and during the race.

For “SP 2500 Sport Class” the only engines allowed to race are two stroke engines up to 2500 cc and they can not be modified.

Hull: Any shape and material, the minimum length shall be 3.96 mt (13 ft).

Machinery: The engine must be of a “Standard Production Line” the engine must be two strokes, V 6 cylinder, with a maximum of 240 HP,

Engine limitations:

- Maximum oversize piston diameter allowed for overhauled engines is 0.020”
- No modifications in the Cylinder Block Assembly; Reed Block; Cylinder Head Assembly; Crankshaft; Pistons; and/or Connecting Rods are allowed; All these engine replacement parts or parts contained herein must be of a standard production “off the shelf line items” available from the manufacturer.
- The whole exhaust system shall be the “standard” as included in the manufacturer’s catalogue, no modifications are allowed.
- All boats must have a Muffler Assembly in the exhaust system.

Propulsion:

- The Jet Drive must be of a “Standard Production Line”
- All the Jet Drive replacement parts or parts contained herein must be standard production “off the shelf line items” available from the manufacturer.

Jet Pump limitations:

- No modifications in the transmission or in the housing of the jet pump are allowed.
- Impeller: shall be of a “Standard Production Line” off the shelf line item. No special production or impellers from other manufacturers are allowed.

- No modifications in the Nozzle and Rudder design, they should be kept as supplied by the manufacturer. The use of separators and/or wedges to modify the angle of the water output are allowed.
- The reverse system is NOT compulsory.

3. Outboard Engine Boats Classes

Classes of Outboard Racing Boats: are determined solely on cubic capacity of the engine used and are distinguished as follows:

Hull: The hull design for class O 2000 and class O 3000 is free.

Length of the Hull: The minimum dimension are 5.10 m (16 ft 9 in).

Racing Capsules: are not permitted

Roll Bar: Any aluminum boat must have a well structured steel chrome molly roll bar. For the existing fiber glass boats it is recommended that a roll cage or a reinforced cockpit be put in. As of 2005 it will be compulsory that all fiber glass boats have one. The roll bar should be at least 100 mm (4") away from any side of the helmet of the racing crew when seated, as stipulated in Rule 509.05.

Seats: There should be a high back seat for each member of the racing crew, securely attached to the hull. The back of the seat should support the back of the helmet.

Seat Belts: The crew of any racing boat with a roll bar, must be strapped to the seat with four point seat belt system, at all time during the race, ***the straps should have a minimum width of 50.8 mm (2")***. Two seat belts straps should run over the waist at the height of the hips and the other two straps should run over the shoulders. It is recommended that the wear of the straps should be checked periodically.

Engine Mounts: The engine must be attached to the transom of the hull, at least in four points.

Engine Cover: All engines must run with their cover on.

Transmission: The transmission used is free

Throttle: The throttle pedal should be attached to the hull and should be spring loaded, so it is capable of returning to the idle position.

Flotation: Its is recommended to put in the bow of the hull sufficient flotation material that would allow the recovery of the boat.

Propulsion: Only one propulsion devise is allowed and the total effort must be applied to the water

Machinery: The motor ***brand or make is open***, and outboard motor is a mechanical assembly which can be removed from the boat as a single unit, complete with its transmission, and which does not transmit the power through the hull at any point. The motor thus taken off and placed ashore, must be capable of being started, fed by its fuel tank. Any mechanism intended to modify the angle of attachment and/or the height of the motor is authorized.

The mounting supports fixed to the boat, the control levers, the tachometer with its connections, the battery with its conductors and the fuel tank and lines do not form part of the power unit.

Classes	Outboard	Capacity	Fuel
O 2000	2000	1501 to 2050cc	Sport Fuel Rule 508.02
O 3000	3000	2051 to 3000cc	Racing Fuel Rule 508.03

4. Inflatable Boats Classes:

Classes of Outboard Inflatable Racing Boats: are determined solely on cubic capacity of the engine used and are distinguished as follows:

Hull :

- Boats can be inflated only with atmospheric air and the number of independent compartments must be such that the boat can float even if one half of the compartments is deflated.
- The boat must be deflectable, collapsible or foldable so that no dismantled part has a length greater than two thirds of the full length of the inflated boat, excepting the floor boards.
- The keel, inflatable or rigid, must extend along the whole boat and be properly fixed to the transom.

- d) The transom can be modified only for reinforcement purposes.
- e) The boats must conform with a model of the official catalogue of the manufacturer published during the last quarter of the previous year.
- f) Boats with a rigid underwater body made of fibreglass or any other type of rigid material are not allowed.
- g) Only original features provided for by the manufacturer are allowed under the water line.
- h) No other material can be inserted or attached between the floor and the bottom of the hull except the keel.
- i) Class 1,850 V : the only shape allowed is a V shape hull. Boats that tend to have the shape of a multihull are to be excluded, even if they are included in the official catalogue of the manufacturer. The transom must be made of rigid material to adequately support the motor.
- j) Length of the Hull: The minimum dimension are 3.50 m (11 ft 5 in).
- k) Racing Capsules: are not permitted.

Engine Mounts: The engine must be attached to the transom of the hull, at least in four points.

Engine Cover: All engines must run with their cover on.

Propulsion: Only one propulsion device is allowed and the total effort must be applied to the water.

Transmission: An efficient gear changing system giving forward, neutral and astern movement is compulsory. The gear control handle must be within easy hand reach of the driver.

Starter: An electric or manual starter must assure quick and easy start without external aids.

Machinery: The outboard motor is a mechanical assembly which can be removed from the boat as a single unit, complete with its transmission, and which does not transmit the power through the hull at any point. The motor thus taken off and placed ashore, must

be capable of being started, fed by its fuel tank. Any mechanism intended to modify the angle of attachment and/or the height of the motor is authorized.

To be homologated as a sports motor, an outboard motor must be sold and advertised by an industrial firm as being manufactured in standard production series (with all identical dimensions, weights and material).

The mounting supports fixed to the boat, the control levers, the tachometer with its connections, the battery with its conductors and the fuel tank and lines do not form part of the power unit.

The motor must be able to function at reduced speed. When the motor is in the water, the cooling must be **done** by the water circulation pump.

Classes	Outboard	Capacity	Fuel
P 550	550	up to 550cc	Sports Fuel Rule 508.02
P 550 V	550 V	up to 550cc	Sports Fuel
P 850	850	up to 850cc	Sports Fuel
P 850 V	850 V	up to 850cc	Sports Fuel

The engines that comply with EPA regulations will be allowed to compete with a 30 % greater capacity in their corresponding class.

5. Rigid Inflatable Boats Classes

Classes of Outboard Rigid Inflatable Racing Boats: are determined solely on cubic capacity of the engine used, type of hull and are distinguished as follows:

Classes : as per UIM rule 566

Hull : as per U.I.M. rule 567 Except 567.01 and 567.03 Hull is monohull

Machinery: as per U.I.M. rule 568

The engines that comply with EPA regulations will be allowed to compete with a 30 % greater capacity in their corresponding class

901 - RULES AND CODE OF PRACTISE **FOR JETSPRINT RACING**

CONTENTS

1. General Rules.....	3
Course Design.....	3
Event Administration.....	4
Promoters Responsibilities	4
2. Boat and Safety Equipment.....	4
Safety Harness	4
Engine Mounts.....	4
Fuel Tanks and Fitting	5
Batteries	5
Fire Extinguishers.....	5
Steering	5
Engine Control.....	5
Personal Safety Equipment.....	6
3. Roll Cage Specifications	6
Roll Cage Construction Materials.....	6
Welding and Forming	6
Configuration	7
Method of Securing	7
Anchor Points	7
Bolting.....	7
Crew Protection	8
Drawing 1	9
Drawing 2	10
Drawing 3.....	11
4. International Jet Sprint Classes.....	12
Superboats	12
International Group A	12
5. Testing Procedures.....	13
Testing Apparatus.....	13
Method of Testing Group A Class.....	13
Workshop Capacity Testing	13

6. Sportsmanship and Competitor Code of Conduct.....	14
Penalties.....	15
Overview.....	15
Appeals.....	15
7. Event Rules	15
Definitions.....	15
Signals.....	16
Race Procedure.....	16

1. GENERAL RULES

1. BOATS Any boat size, type and configuration, with a reciprocating engine, will be permitted, providing the boat is solely propelled by a water jet unit/s, carries two crew persons and complies with all safety requirements as specified under Boat and Safety Equipment rules including an approved Roll Cage.

2. HULL Hulls must be of metal construction up to the deck line; the deck line may not go down to the chine. All decks must be fitted in such a way as to add strength to the hull, and be permanently fixed.

3. EXHAUST The use of an efficient manufacturer brand muffler is compulsory. All boats will be required to meet supplementary track regulations for noise. These limits shall be notified by the race organisers/promoter 30 days prior to the departure of any international crew. (Opportunity must be given for crew to meet noise restrictions prior to any race)

4. CONFIGURATION Driver and crew will sit side by side with a difference in seat placement fore or aft, of not more than 300mm to each other. Crew shall have a clear and unhindered escape route through the front of any roll cage.

5. All boats must carry two crewmembers (driver and navigator).
6. The timekeeper's word is final and no protests will be considered. The timekeeper's van/area is out of bounds to all competitors and their crew at all times except by direct invitation of the Sprint Controller. This also applies to the race control area.

PENALTY: Unauthorised entry to race control area and timekeeper's van/area or contact with the timekeeper - may result in disqualification for the remainder of the day's racing.

7. Any boat may have a maximum of two crews. Each driver is to have his or her own navigator. Doubling up is prohibited (ie. no driver to navigate, navigator cannot drive, navigator can only navigate for one driver, driver can only compete in one class).
8. No driver or navigator may compete in more than one boat or class on race day. However, if a boat is rendered unable to continue racing, the sprint controller may permit the crew to continue racing in another boat, providing they can record a qualifying time in that replacement boat before the elimination rounds commence. Times previously recorded in the disabled boat will be disregarded. Only one boat change per crew per meeting is allowed.

PENALTY: Boat substitution or change without approval - disqualification for the day or remaining rounds.

9. All items named in Boat and Safety Equipment shall be available for inspection at the time and place nominated for scrutineering.

PENALTY: Race-day non-starter.

10. Scrutineering shall be at a time and place as nominated by the event organiser on the entry form and must be attended by all boats and crews unless prior arrangements are made with the organiser. The scrutineering area is declared an alcohol-free zone.

PENALTY: Not attending scrutineering without prior approval - race-day non-starter.

PENALTY: Consuming alcohol in any alcohol-free zone - race-day non-starter.

11. Entry forms submitted without payment or incomplete will be deemed invalid and disregarded. Late entries will only be accepted at the sole discretion of the promoter and may be subjected to a late entry fee and/or may be placed on a cancellation list. If a competitor wishes to withdraw an entry prior to the event, refund of the entry fee is at the discretion of the promoter. Entrants who are a “no-show” on race day are not entitled to a credit or a refund of their entry fee.
12. All drivers in order to compete, must be full financial members of their respective country's association or jet sprint authority, financial crew members from affiliated clubs and associations and licensed international crew members are deemed to meet this requirement.
13. Points accrued in any international series will be deemed to have been earned by a driver, not a particular boat or crew combination.
14. No driver shall compete with an injury or disability, which, in the opinion of the sprint controller could adversely affect his/her ability to safely and effectively control the boat. This includes the ability of either crewmember to safely and quickly exit the boat following an accident. Medical clearance in writing may be required before a driver or navigator is allowed to race.

COURSE DESIGN

International Events will only take place on suitable tracks homologated by the IJF, or its duly authorised agents. For more information contact the: International Jet Sprint Federation

PO Box 1456
Taupo 2730
New Zealand
Email-emoore@xtra.co.nz

EVENT ADMINISTRATION

1. A Sprint Controller will be appointed to control each event.

2. The Sprint Controller is the final arbiter for any disputes other than official protests on the day.
3. An approved host country authority member who has had at least two years experience in Jet Sprinting shall carry out Scrutineering. Any queries, which cannot be satisfactorily resolved by the scrutineers, shall be referred to the sprint controller for a decision the Sprint Controller is the final arbiter.
4. The IJF reserves the right to alter any of the above rules so as to accommodate any unusual or unforeseen circumstance or errors that may arise from time to time or country to country

PROMOTORS RESPONSIBILITIES

1. Promoters of all events must provide adequate first aid equipment, for any likely emergency, and ensure the attendance of qualified first aid personnel and an ambulance available at trackside.
2. Promoters of all events must provide adequate fire extinguishing equipment for any likely emergency. (Race gas and Methanol). It is preferable to engage the services of a trained fire crew.
3. Promoters of all events must engage the services of at least 20 able bodied people to act as boat pushers. Although the primary requirement is to assist with refloating of boats, which have become grounded, an essential aspect is an ability to quickly react and assist with any craft in difficulties that may endanger the crew. (i.e. a roll-over) The Pushers must be given a safety briefing prior to commencement of the event, so that they are conversant with the requirements regarding the safety of a crew in the event of a roll-over under water and that they are aware all other safety requirements concerning their own safety in the handling of boats.

2. BOAT AND SAFETY EQUIPMENT

SAFETY HARNESS

1. Must be fitted for both crew and be securely attached to the roll cage or the seat base bearers. They must be a motor sport type approved and a minimum of 75mm web, a 5 attachment point

harness is the minimum, a 6 point is recommended both shall have a lever action quick release buckle and be within their use by date as set by the manufacturer. (See roll cage specs) Push lock or twist action buckles are not permitted.

2. The mounting point for the shoulder straps behind each crew member must be between a line horizontal to the shoulders, and a line drawn downward from the shoulders at an angle of 25 degrees to the horizontal. If the shoulder straps join prior to a single common mounting point then that junction shall be at least 150mm behind the crew members neck.
3. weakened due to the actions of chemicals abrasion or sunlight etc. Safety belt harness hardware must be replaced if it is bent, deformed or rusted.
4. The lap and crotch straps may not pass over the sides of the seats but through the seats in order to wrap and hold the pelvic region over the greatest possible surface. Care shall be taken that the webbing of the harness is protected against chaffing in any area.
5. Shoulder straps must wrap around the horizontal cross bar of the roll cage as the only means of attachment.
6. All Drivers and Navigators must be scrutineered with their nominated boat so their safety equipment can be checked for correct size.
7. Motor sport type approved arm restraints must be worn on the outside wrist of each crew member and shall be of a length which will prevent the arm from protruding from the boat in the event of a roll over. They must be attached to the safety harness in such a way as to release freely when the buckle is released.
8. Head restraints for both driver and navigator are compulsory, these may be either motor sports approved mounted from the shoulders to the helmet or the fixed type from to the seat back head rest protruding out each side of the helmet in a line drawn forward past half the forward length of the helmet when the head is rested to the back of the seat in the sitting position. Head restraints must have the ability to absorb the side to side impact of the head under racing

conditions without being left permanently bent out of shape after normal racing.

ENGINE MOUNTS

1. Engines are to be solid mounted to the main engine bearers. Where an engine mount is built to create a frame there shall be a minimum of four, 3/8 in. bolts or metric equivalent to secure that frame to the main bearers of the boat. Where engine mounts create four separate attachment points there shall be a minimum of eight, 3/8 in. or metric equivalent.
2. Flexible mounts will only be permitted if they have security through bolting and it can be demonstrated to the scrutineers' satisfaction that the through bolt is designed to restrain the engine should the flexible portion fail.

FUEL TANKS AND FITTING

1. Fuel tanks must be securely mounted and be fitted with a non spill sealing cap. The tank must not be mounted in such a way as to be a stressed or a load bearing part of the boat structure.
2. Adequate breathers are to be fitted to the fuel tank, the breather tube must include a non return valve and be vented to the outside of the boat.
3. A fuel filler must be fitted in such a manner that no fuel can spill into the hull during refuelling.
4. All fuel and vent lines must be resistant to the type of fuel used. All fuel lines must be attached with proper fittings no hose clips or clamping of fuel lines is permitted. (With the exception of the fuel filler to the scrutineers satisfaction)
5. A fuel shut off tap or control must be fitted and identified by its own red triangle (75mm sides) and must be located between the seats or on the dash and able to be operated by either crew when harnessed in their seats.

BATTERIES

1. The battery should be of a non-spill, (ie. manifold-vented or gelled type) and firmly secured to the hull or bearers.
2. An operating battery isolating kill switch must be installed and located between the seats within easy reach of both crew whilst harnessed in their seats. Metal constructed boats should have the isolating switch to the earth terminal.

FIRE EXTINGUISHERS

1. Approved fire extinguishers must be carried in all boats. They must be easily accessible and well secured in a bracket capable of holding them in the event of an accident.
2. Approved Types: The extinguisher fitted to any boat must be of a type designed to extinguish the fuel type used in that boat.

Capacity 1 kg minimum. (sodium/potassium bicarbonate base, BE type)

All types must carry standard approval label and a certificate showing that it has been serviced in the last 12 months.

Aerosol types not permitted.

STEERING

1. All shackles and turnbuckles in the steering system must be lock-wired. (Nylon TY wraps are acceptable)
2. Chain guards/guides must be fitted to each side of the steering chain sprocket. All open pulleys must be fitted with keepers. Where steering is a lever system any shackle attaching steering cables shall be lock wired. Especially designed taper lock cable joiners may be accepted at the scrutineers' discretion. All steering cables must be from a braded type wire rope, with a minimum 750 kg breaking strain.
3. Jet unit steering shafts where fitted shall have Nylok type nuts, the thread of any bolt must protrude through the nylon locking device or have been locked by fitting two standard nuts.

4. An operational reverse is mandatory for all boats.

ENGINE CONTROL

1. Two throttle return springs, either of which is capable of closing the throttles, must be fitted in addition to any internal carburettor throttle shaft springs. Where fuel injection is fitted there will be at least one spring on the accelerator pedal in addition to the factory fitted springs on the throttle shafts. Where this is not possible the spring will be fitted to the main operational lever of the fuel injection where the throttle cable is attached. At all times for any boat the throttle must be able to return to the idle position unaided.
2. Any other items that in the opinion of the scrutineer are unsafe must be rectified to the scrutineers' satisfaction.

PERSONAL SAFETY EQUIPMENT

1. Safety equipment must be properly and securely fitted to the crew, prior to the boat lining up at the start line and all must be in place when the boat crosses the finish line. No other items of clothing shall be worn over the fire retardant suit.
2. A bow-towing eye with at least a ten mm hole must be fitted with a d-shackle to suit, to aid the rescue crew or towboat to attach a safety line.
3. The driver shall be responsible for checking that his or her crew is wearing the required safety equipment and that it is correctly adjusted, before proceeding onto the start line. The starter will visually check safety equipment prior to allowing any boat to cross the start line, but will not be responsible for any item not being in place during competition.

PENALTY: Incorrect fitting or missing safety equipment during run – DNF

4. All crew in boats must wear Motor sport approved flame retardant balaclavas and hand protection. Balaclavas with openings from eyebrows to above the chin are acceptable. Balaclavas must not be of the peephole type, the mouth and nose must be easily cleared.

5. All competitors whether they are competing or practicing must wear a correct fitting approved helmet; motor sports type neck brace, arm restraint, enclosed footwear and fire retardant suit. Cotton workman overalls will not be accepted.
6. Any brand of crash helmet is allowed provided they meet the standard specifications applied by the Jetsprint authority in the Country of origin of the boat and crew concerned. Approved Internationally manufactured helmets may be eligible for competition use, consult an IJF representative for approval. Open face types only. All helmets must be in excellent condition with no frayed chinstraps or rusted anchor points and must correctly fit the person who wears them. Full-face or types with removable face pieces are not acceptable.
7. All boats must be fitted with a working bilge pump.

3. ROLL CAGE SPECIFICATIONS

1. A roll cage, which meets these minimum specifications, must be fitted to all boats.
2. Boats not complying with these standards will not be permitted to race.
3. The intention of these specifications is to provide the best possible protection to drivers and navigators, taking into consideration the accidents, which have occurred within the sport, and best practices in other motor sports. No guarantee is implied or stated, nor is any responsibility taken, regarding the degree of protection or safety afforded by any roll cage constructed to these specifications. Owners are encouraged to seek the advice of a structural engineer qualified in roll cage design.
4. All jet sprint boats are to have roll cages constructed so that all parts of the crew's bodies above the deck line are contained within the roll cage structure. At the same time, the roll cage should provide minimum restriction to the driver's vision to the sides or front and must allow quick and easy access and exit, especially in the event of an inverted accident. While different designs of roll cages will be

permitted, each design must satisfy the requirements stated above and be constructed to the following minimum specifications.

ROLL CAGE CONSTRUCTION MATERIALS

1. Only round tubing is deemed acceptable.
2. Mild steel tubing minimum diameter 41.2mm-1 5/8"; DOM, 1020/26, mechanical (Drawn over mandrel, round carbon steel tube) 3.0mm- .120 wall thickness minimum.
3. Dimensions for mild steel roll cage as per drawing three.
4. Chrome molly tube; minimum diameter 38.1 mm, wall thickness 2.1 mm.

WELDING & FORMING

1. All joints must be fully welded by a competent welder with all welds being of good external appearance and remain un-ground using the TIG or MIG process with the 4130 filler wire, for chrome Molly.
2. Joints should be preheated and welded in 90-degree increments to avoid brittleness. (Chrome Molly)
3. Tube must be contoured and shaped for a close fit prior to welding.
4. Tubes cannot be flattened in order to make a joint.
5. All bracing bars should be of straight tube between attachment points.

CONFIGURATION

1. All bracing and the rear A frame must be straight between attachment points. (Drawing one)
2. The main A frame roll cage structure and brace bars must be of one-piece continuous tubing. No welds permitted other than at attachment points and capping plates.
3. The top corners of the overhead framework must be formed with one 90 degree formed on a bender suitable for bending the chrome molly size used.

4. The two A frames must be spaced apart a minimum of 100 mm between centres.
5. Capping plate (3 mm) must be welded to each corner of the A frame covering the entire bend. (May be mild steel)
6. A minimum of one (3 mm) plate or tube must be welded in between the capped A frame corners between the top bars.
7. The mounting points on the cage for the shoulder straps behind each crew member must be between a line horizontal to the shoulders and a line drawn downward from the shoulders at an angle on 25 degrees to the horizontal.
8. The two A frames must have a brace on the side so as to form an A section on the side of the cage and should where possible also be attached to the side panel (gunwale) as well as the chine.
9. Helmet Clearance; 5 or 6 point harness, a minimum clearance of 100 mm from the top of the helmet to the underside of the cage top bar must be maintained for all crew.
10. An X styled brace going from the upper outer A frame to either:

The back cross bar and then down to the engine bearers or outer chine area. (The bottom part of the X brace can be removable in this configuration provided suitable hardware is used)

Goes directly to the engine bearers or chine and joined where they intersect. (If the lower part of the X brace goes to the chine it must have suitable load distributing plates on attachment points)

11. ***Seats must mount to the one piece cross bars shown in drawing three and be attached at all manufacturer located mounting points or a minimum 4 points at the base and 1 at the back above the shoulder line of any crew.***
12. ***All attachment points to the seats must be a minimum of 3 mm mild steel and able to support the entire weight of the crew throughout any crash without distortion to the mount between the seats and roll cage crossbars or cross members.***

METHOD OF SECURING

1. Attachment plates or load-spreading flanges must be welded to the tube ends to secure the cage to the boat and must be at least 5 mm minimum thickness.
2. For rectangle attaching plates:
Minimum width of attaching plate = tube diameter.
Minimum length of attaching plate = tube diameter x 2.
3. For circular attaching plates:
Minimum diameter of round flanges should be tube diameter x 2.
The roll cage may be bolted or welded to the boat as defined below.

ANCHOR POINTS

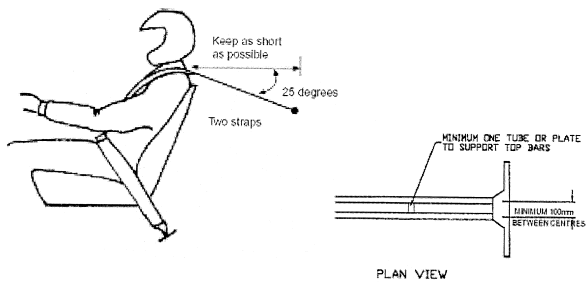
1. The primary elements of the roll cage must be secured to the main engine bearers and the chine.
2. In the absence of chine bearers in an alloy boat, the cage should be attached in the chine area of the bottom skin with load-spreading flanges as above.
3. In addition to the above, the main roll bar structure may be attached to the fore or side deck, provided that a brace bar of the same type and size material as the roll bar continues the load path through to the bottom of the boat. Provided the primary elements of the roll cage structure attach to the boat bottom as stated above, then braces may be attached to the roll cage in order to strengthen the boat sides or deck structure.

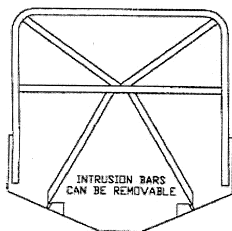
BOLTING

1. Through bolting with back-up washers.
2. Minimum of two bolts per attachment point.
3. Minimum bolt diameter 3/8in for two bolts, four bolts - 5/16, six bolts - 3/8 etc or metric equivalents

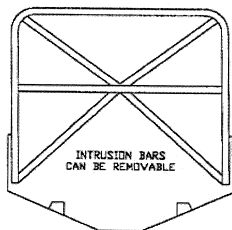
CREW PROTECTION

1. The driver and navigator must be able to exit an inverted boat through the front area of the roll cage with no frame members in such a position as to impede their exit or rescue.
2. The frame of the cage must protect the crew from any frontal or inverted impact to their head and torso and must be able to restrain the engine from moving forward far enough to cause injury by having suitable cross-members in place.
3. No cage should project more than 7in/17.8cm above the helmet of the tallest crewmember.
4. A minimum clearance from the top of the crews' helmet to the underside of the roll bar will be 100 mm (4 inches) for a five or six-point harness

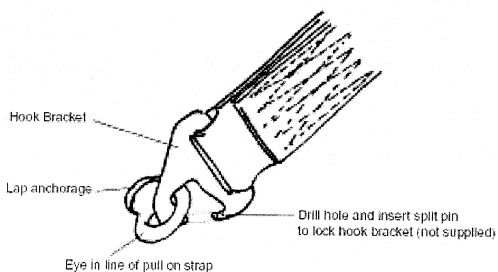




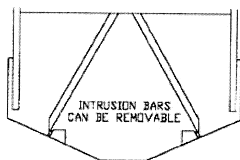
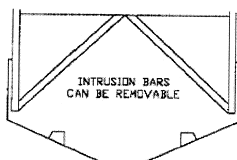
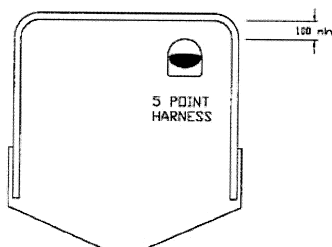
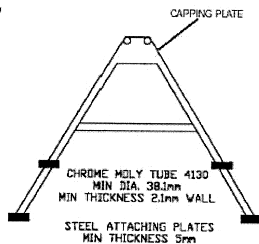
BACK VIEW

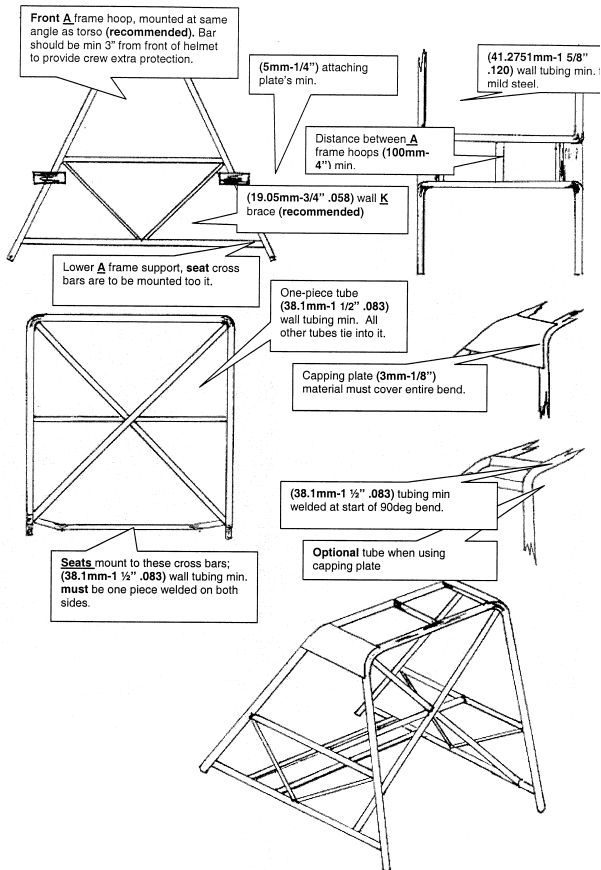


BACK VIEW



A split pin must lock the seat belt hook to the lap anchorage.

BACK VIEWBACK VIEWHELMET CLEARANCESIDE VIEW



4. INTERNATIONAL JET SPRINT CLASSES

SUPERBOATS

1. All naturally aspirated engines must be a minimum of 6.5 litres displacement, and if any naturally aspirated engine is under seven litres displacement, it must be equipped with alloy heads, use methanol fuel and have multi carburettors or fuel injected induction system.
2. Forced induction engines must be a minimum of 3.8 litres displacement.
3. Under special circumstances it may be possible by request, to have boats that do not comply to the above requirements, to be awarded Super boat status.
4. If Superboat status is granted, the boat will only be eligible to compete in the Superboat class for that season then would need to re-apply.
5. The use of fuels other than petrol is permitted. The only Methanol (alcohol) fuel is Mobil No 5 and equivalent Oil Company brand Fuels. The use of any nitrous oxide or nitro Methane type fuels is prohibited.
6. The minimum age for Super class competitors shall be 18 years of age for drivers and 16 years of age for navigators. Proof of age will be required on demand.

INTERNATIONAL GROUP A

1. The minimum age for Group A competitors shall be 16 years of age for drivers and 16 years of age for navigators. Proof of age will be required on demand.

JET UNIT

Single unit only, direct drive only (no gearboxes) otherwise no restriction.

ENGINE CONFIGURATION

2 valves per cylinder, push rod operated, maximum of 8 cylinders,

	single engine and internal combustion only.
OILING SYSTEM	Wet sump only.
CONRODS / CRANK	No Titanium or alloy.
ENGINE BLOCK	Cast iron only.
CYLINDER HEADS	<p>Cast iron only exceeding 365 cu inches. Engines not exceeding 365 cu inches maximum swept capacity may use aluminium cylinder heads conforming with these rules only.</p> <p>Cylinder Heads. Chevrolet. Valve angle 23 degrees to engine block face, OEM production position only. All Port and bolt locations OEM production position only, intake port size template FELPRO gasket-part No 1206.</p> <p>Cylinder Heads. Ford. Valve angle 10 degrees to engine block face, OEM production position only Part. Number. M-6049-N351. Template Intake port size FELPRO gasket-part No. 1262. All port and bolt locations OEM production positions only.</p> <p>All other engines within these regulations but outside the specified cylinder head configurations must apply for full homologation or run the NASCAR approved Carburettor. Holey Part Number 0-80507. Barry Grant Part Number. 30600300T. 1 and 1/16th inch Venturi and 1 and 7/16th inch Throttle Bore.</p> <p>Camshaft. Single camshaft only.</p>

ENGINE CAPACITY	412 cu inch (6,752 cc) maximum swept capacity, minimum 345 cu inch capacity.
INDUCTION SYSTEM	Single 4 barrel carburettor, with a maximum 4 venturis, none of the throttle body bores or the butterflies on the carburettor may exceed 1-11/16 inch in diameter. No fuel injection or electronic fuel-metering device permitted. No additives may be discharged into the intake tract.
CONTROL FUEL	Will be provided at all international meetings by the promoter and paid for by competitors of the class involved.
FUEL	Will be 100 octane Race gas. The control fuel will be dispensed either from a central point to which each boat must come, or by smaller containers refilled from the main source. The scrutineer will be in charge of and oversee all refuelling operations. Fuel tanks are not required to be drained prior to the addition of control fuel provided that a tank has maximum capacity of not more than 30 litres.

5. TESTING PROCEDURES

1. At any meeting, any boat's engine capacity may be tested, at random, without notice. The decision to conduct testing shall be made by the committee prior to the commencement of that meeting. Other boats may be tested, time permitting, as the chief scrutineer or his appointed deputy sees fit, but only after they have been eliminated for that day.

2. The crew of any boat found to have an illegal engine capacity will be disqualified and will lose all points and placing's gained since that boat's last capacity test.
3. The crew holding lower placing's will move up in the finishing order one or more positions as required. Where boats are disqualified and the finish order reshuffled, the top three must still be tested prior to presentation.
4. Testing will be conducted by the chief scrutineer or appointed deputy and in the continual presence of the boat owner or designated representative.
5. Refusal to allow an engine to be tested will be taken as evidence of over-capacity and the stated penalty applied immediately.
6. The results of the capacity testing as detailed will be final and no protest will be entered into provided the equipment falls within its manufacturer set tolerances.

Note: All testing methods must be approved.

TESTING APPARATUS

1. A measuring flask of 1.000cc's certified capacity (known brand name) and have sufficient height so as to have distinct and well-separated graduations.
2. A length of flexible hose 300mm to 500mm long with one end attached to the bottom of the measuring flask, the other end able to accept either a large or small spark plug hole fitting, drilled to allow fluid to pass.
3. One litre of testing oil, to be a recognised upper-cylinder lubricant.

METHOD OF TESTING GROUP A CLASS

1. Engine must have: - All spark plugs removed, one rocker cover removed, both rockers on one cylinder backed off, so that the valves remain closed at all times.
2. Bring the piston to approximately TDC on the cylinder being tested.

3. Screw the measuring flask connecting hose into the spark plug hole.
4. Fill the measuring flask with testing oil and raise the flask above the cylinder, keeping the connecting hose as vertical as possible.
5. Turn the engine over slowly by hand until no more air bubbles are expelled.
6. Turn the engine over slowly by hand three times, recording the maximum and minimum level readings each times. (At all times, the oil level must remain within the graduation range of the flask). The oil level may be topped up in between reading sets if necessary and the process should be repeated until two readings are within 8cc of each other.
7. Taking the difference between the two readings and multiplying by the number of cylinders will yield the total engine displacement. For Group A boats, this may not be more than 6752 cc or 412 Cubic inches.
8. Maximum allowable capacity for one cylinder is $844\text{cc} + 1\% = 852\text{cc}$ for Group A boats. (8-cylinder engine)
9. The engine will be classed over-capacity if: Measuring flask reading is more than 852cc (for Group A) per cylinder, the engine cannot be turned over one full revolution.
10. Oil leakage from the cylinder is such that no two readings are within 8cc (in this case, another cylinder must be tested. The number of cylinders to be tested is at the scrutineers' discretion).
11. Note: Competitors suffering a total engine failure, such that the engine cannot be tested will be treated as having an under-capacity engine for the day.
12. The method of testing Superboat engine capacities, will generally be those as indicated above for Group A class, however capacities should be substituted so as to comply with the class capacity rules.

WORKSHOP CAPACITY TESTING

1. Only one head of a "V" configuration engine need be removed to enable access to the cylinder bore for the purpose of accurately

measuring the bore and stroke of that engine. The resultant calculated swept volume shall not exceed 412 Ci. or 6752 c.c.'s for Group A.

2. There shall be no tolerance permitted when the capacity is measured in this manner.

6. SPORTSMANSHIP AND COMPETITOR CODE OF CONDUCT

1. There is a total commitment to promote and foster good sportsmanship within the jet sprinting movement. This is to assist in the creation of a strong, positive public image of a group of competitors with a professional attitude, providing exciting, spectacular entertainment.
2. All competitors, their crew and race officials are expected to understand and display, in public, all those qualities which define good sportsmanship.
3. Obvious public displays of bad sportsmanship by any member will be considered a breach of the code of conduct and subject to disciplinary action.
4. The code of conduct requires not only a public display of good sportsmanship, but compliance with the rules of racing and the contractual obligations of the individual competitors and their teams via the signed entry forms carrying conditions relating to behaviour and general codes of practise as outlined in these rules.
5. During a pit walk by members of the public at least one member of the boat crew shall remain with the boat to foster good relations and public image by interacting with spectators as they view the boats in the pit area.
6. All personnel involved with an event, competitors, their crews and officials are required to function within the code of conduct.
7. The pits are a controlled-access area and an alcohol-free and smoking-free zone. Entry is confined to competitors who are

entered on the day, pit crew, officials and those holding pit passes. Non-competing, financial members may not use their licence as a pit-pass, and not as a free event entry pass. Children must be under the control of a responsible adult at all times.

PENALTY: Abuse of licence. Cancellation of licence for six months.

PENALTY: Smoking or drinking alcohol in the pits - exclusion from the pits for the remainder of the meeting. Competitors or officials will be suspended for one meeting. Repeat offence - 12 months suspension.

8. There is a total ban on the consumption of alcohol and any other intoxicating substances by all boat crews and all event officials, including safety crews, for 8 hours prior to the commencement of briefing and for the duration of the meeting, or until they have completed their racing activities for the day. No crewmember, or support crewmember may consume alcohol whilst wearing racing apparel. Crews are required to have a zero blood alcohol content prior to racing and to comply with insurance conditions. Breathalyser testing of crews may occur, prior to commencement of racing. This ban applies to prescribed drugs and other performance-affecting substances. No alcohol may be consumed in the pit area by any person during racing.
9. A declared alcohol free zone refers only to the consumption of alcohol and/or prescribed drugs by any person in that area, and does not presume to enforce civil law relating to the possession or presence of it in a vehicle which may be parked in that area. However, any member who willingly breaks a civil law concerning prescribed substances in a manner which brings the sport into disrepute, may be deemed to be in a breach of the code of practice and can be penalised as provided for in the general rules of IJF.

PENALTY: Consumption of alcohol during meeting: minimum one meeting ban.

PENALTY: Competitors registering any blood/alcohol content prior to racing: - start denied until breathalyser registers zero.

10. Smoking is prohibited in the pit area. Any person smoking in the pit area may cause the disqualification of their associated boat. A crew smoking area may be established adjacent to the pit area at each track.

PENALTY: Disqualification for remainder of the days racing.

11. No competitor may abuse a race official. Any competitor, who has a dispute with any official on race day, must present the details of the problem to a class representative who may consult with the Sprint Controller for resolution. Likewise, any competitor who has any complaint concerning the running of the race day must firstly present the details in person to a crew committee member. If no committee members are present, then the complaint may be presented to a race official.
12. No official may abuse a competitor. If there is a perceived problem with the behaviour of a competitor, the matter must be brought to the attention of the sprint controller. (The disputes committee will be comprised of the sprint controller, host country representative, and IJF representative)

PENALTY: Abuse of official: disqualification for remainder of the day. Further penalty may be applied.

13. No competitor or crew shall be involved in any instance of dangerous behaviour, or conduct which deliberately causes or could cause disruption to a race meeting, nor may they take any action, verbal or otherwise which may be deemed to bring the sport into disrepute.

PENALTY: minimum: disqualification for the day - further disqualification possible at the discretion of the U.I.M.

PENALTIES

1. For any offence occurring on race day or as a result of scrutineering, the maximum penalty, which can be imposed by the Sprint Controller, shall be disqualification for that day. Additional penalties can only be imposed by the disputes committee following

race day and after consultation with all parties concerned with the incident.

2. A penalty for a breach of the rules may, at the discretion of the sprint controller, be in the form of a suspended penalty and shall remain current for a period of up to one year from the time of the offence. Any further breach of the rules within that time shall cause the suspended penalty to be immediately applied.
3. Suspension penalties for one or more meetings shall apply from the next listed meeting and if more than one, shall be consecutive.

OVERVIEW

1. The Sprint Controller shall have certain discretionary powers to deal with disputes and may make a ruling concerning a dispute.
2. The ruling will be given after due and careful consideration of the issues involved and will be final. It will not be open to further discussion on the day.
3. Any ruling, which affects the results of the day, may not be challenged on that day, but can be subject to appeal by affected competitors as per the appeals rule.
4. The disputes committee will formally advise affected persons that they have been reported for a breach of the rules or the code of conduct.

APPEALS

1. The disputes committee will be comprised of the Sprint Controller, host country representative and the Commissioner.
2. Whenever possible and practical, the Sprint Controller will consult with the disputes committee when considering disciplinary action.
3. The sprint controller, may request a competitor to attend a meeting of the disputes committee, either during the event or at some other suitable time, to provide good reason why they should not have disciplinary action taken against them for a breach of the rules or

code of conduct, which in the opinion of the sprint controller, could bring the sport into disrepute.

4. Failure to attend such a meeting will result in the immediate imposition of the appropriate penalty without right of appeal.
5. Notwithstanding the listed penalties, it is policy to apply penalties progressively. A warning may be issued followed by the prescribed penalty for a repeat offence.
6. A non-time related protest must be submitted in writing by a competing crewmember and accompanied by a US \$100.00 protest fee (or equivalent). It may be lodged immediately or at latest, within 15 minutes of the occurrence and handed to a class representative and the sprint controller. The disputes committee will hear the protest as soon as possible and if upheld, the fee will be refunded.
7. Any competitor who protests the eligibility of the engine of a competitor, shall be liable for all costs associated with the testing of that engine, including parts and labour if necessary, should it be proved to be legal. If the protest is upheld, the owner shall bear all associated costs.
8. Members seeking information on rule interpretations must first consult with a class representative, but if none are available, then the sprint controller may be approached.
9. Any member subjected to disciplinary action may lodge a formal appeal within 7 days of a penalty being imposed. Refer to the U.I.M. appeal rules.

7. EVENT RULES

DEFINITIONS

Commissioner- The U.I.M. appointed representative.

Class representative -The selected person to represent the drivers of a particular class on race day

Disputes committee -The sprint controller, a host country representative and the commissioner.

DNS - did not start.

DNF - did not finish.

Qualifying –The rounds in which a competitor can take their best time to secure a place in the elimination.

Elimination – Meaning the rounds where a competitor has only one opportunity to progress forward with their fastest previous time. i.e. qualified time to fastest 16, fastest 8, fastest 4, fastest 2.

Run – The completion of the prescribed track by following the course directions as handed out on an official map of the race course at a given event.

SIGNALS

1. Flags will normally initiate starts, however light signals may be used. A boat may not start until the starter gives the appropriate signal. This is a safety requirement and the driver and navigator must be satisfied that the previous boat is back in the pit area and clear of the spinout pool, the towboat is back in the pit pool and all safety crew groups are in position. (Note that the timing is triggered by the boat at a point some distance after the start lights and therefore does not commence with the display of the green light).
2. Each group of safety crew may have a red and green flag. In this instance, unless the track is clear of boats, crew or unplanned obstacles, a red flag will be displayed and a start is not permitted. A green flag signifies that the particular section of track is clear and the safety group is in position. Where it can be demonstrated that the event is efficiently controlled by two-way radios the requirement for the flag system may be waved in consultation with the sprint controller.
3. Signals - Due to the noisy environment, signals will be utilised by the starter and the safety crew. The starter has control of the start lights or flags which will be:

- RED** You may not start or pass this point under any circumstances.
- AMBER** A start is imminent, you should be idling toward the start line and are under the control of the starter.
- GREEN** You are cleared to start. The two and ten minute time limits do not apply for a normal start. (Time does not start until the timing beam is broken)
- BLACK** You are disqualified for that run.

PENALTY: For jumping the lights or start signal or being out of position - 10 seconds added to the time for that run. If a dangerous situation is caused, a disqualification will occur.

RACE PROCEEDURE

1. Each race event will commence with a roll call and crew briefing, the time of which will be advised to all drivers entered. All competitors, both drivers and navigators, must attend the pre-race briefing, the track walk safety inspection and sign any liability waiver form before racing. Competitors must be in attendance at roll call and/or briefing. Competitors who suffer an incident outside their control, and who in the opinion of the sprint controller were making every reasonable effort to attend the event on time, may be allowed to compete. In this case the competitors will be given a full and separate briefing prior to competing. As part of the briefing all crewmembers must be invited to raise any concerns over course safety.

PENALTY: Late to roll call on race day - disqualification for the day.

2. Event, track or boat sponsors who compete as guest navigators must sign the waiver form prior to their run and wear all specified safety equipment.
3. While racing is in progress, each boat is to have one crewmember at the boat or close by in the pit area, in case of a change to the running order.

4. There is a speed limit of 5kph or minimum idle in the pit pool and on entrance thereto, where a spin-out pool is provided each finishing boat must use this facility to reduce speed to idle, prior to entering the channel approaching the pits. If there is no spinout pool, an alternate procedure will be advised either at the crew briefing, or in the track supplementary regulations.

PENALTY: Speeding in pits and/or creating a wash hazard or other danger - disqualification for the remainder of the race day.

5. No person other than official, licensed entrants and the towboat driver shall drive or compete on the track on race day without the consent of the sprint controller.

PENALTY: Disqualification for a period of up to one year for the crew involved.

6. Boats will start from idle power in forward thrust and may not accelerate until given the start signal which will be when they are wholly within the start gate as indicated by the starter. Cones, on the bank adjacent to the start channel, will normally mark the start gate. A third cone will indicate the 10m-aborted start limit.
7. If upon initial acceleration a problem occurs with the engine or jet unit, the driver may request one re-start immediately by reducing power and raising an arm. The boat must return to the pit-pool via the shortest possible route to the spinout pool at reduced speed before rectifying the problem, e.g. weed in intake. Use of this rule to obtain an advantage will result in a DNF (Note that time limits may be applied to restart).
8. Only one re-start will be permitted per boat per round and the abort signal must be made within 10m of the start. (A marker will be positioned to mark this limit). This allowance is not meant to compensate for a poor start and will be at the discretion of the sprint controller.

PENALTY: Aborting the run more than 10m from the start - DNF (see also event rule 14).

9. If a boat becomes grounded during a run, the crew may attempt to re-float their boat, only by manipulation of the engine and jet unit. They may not receive any outside assistance and no item of safety equipment, including safety harness, may be removed during this attempt. Timing will continue until the crew is successful or the driver turns off the engine and calls for assistance by raising an arm. If the attempt is successful, they may continue racing and can record a time. (There will be a 20 second time limit on re-floating your boat in this manner).

PENALTY: For removal of safety equipment or receiving outside assistance - DNF.

10. When a boat is grounded and the assistance of the safety crew is required, the following procedure must be adhered to: The driver must shut off the engine and raise one arm to signify that assistance is required.
11. Assist the safety crew to re-float the boat by getting out if required by safety crew. Once back in the water, ensure that you have permission from the safety crew before you restart your engine and move back to the pits. Once re-started, drive back to the pit pool by the shortest route to the finish line at a speed only fast enough to prevent further grounding in shallow channels. Boats may not return to the pit pool via the start line channel unless indicated by the starter.

PENALTY: failure to comply DNF for next round.

12. If the tow-craft is called for, but then not required, the boat may not proceed to the pits until the tow-craft has been turned around and returned to the pit pool. If a dangerous incident is created a penalty will be imposed. Once re-started, a crew may not continue to drive the rest of the course as practice.

PENALTY: DNF for the next round.

13. Whilst racing, a boat may not pass through the finish line at any time other than at the conclusion of it's run.

PENALTY: DNF

14. During a run, if a crew notice any dangerous circumstance, distraction or an unusual object in the track, eg. a floating tyre, they can abort their run by slowing down, raising an arm and immediately returning to the finish via the quickest and safest route. If the safety crew can verify that there is an obstacle they will remove it and the crew will be allowed to re-start. The race controller is to decide on circumstance or unfair distraction. This allowance may not be misused to compensate for a bad run.

PENALTY: If no obstacle or circumstance or unfair distraction found - no re-run and DNF.

15. In the event of no primary or back-up time being recorded by the timekeepers for a particular run, that crew will be allowed a second run, which should be made as soon as possible and may be added to the next round. If a boat is unable to make a re-run due to mechanical problems, then it's previous best time at that meeting and on that track direction, will be recorded.
16. Once a crew has made their first run of the day they may not change their roles, that is, a driver cannot change places with the navigator. In the event of a crew member being unable to continue racing due to illness or injury and at the discretion of the sprint controller, substitute crew may be used provided that:

The substitute driver; May having completed all pre-competition formalities and already a competitor on the day can race another boat providing he/she is able to complete at least one qualifying round before commencing the elimination rounds. Any previous times recorded by that driver will be disregarded.

Once eliminations have commenced, a substitute navigator may be used, but only from a previously eliminated crew and only after it is established that the previous navigator is unable to continue. A driver may not be substituted once eliminations have commenced.

PENALTY: Role changes or unauthorised substitution of crew: disqualification for the remaining rounds.

17. A crew must be ready to start in the correct running order in each round. If there is a problem, the starter or pit Marshal must be

notified of the problem in advance of their start order. If sufficient notification is given, the crew will be allowed to start later in that round.

18. In the case of a "B" crew near the end of the running order, they will be allowed a start in the following round, with their time counting for the previous round.
19. If a problem with a boat occurs in a qualifying round, there is a two-minute time limit upon repairs. This time limit will commence from when the last boat in that particular round enters the pit pool at the conclusion of its run and the time will be displayed on the main timing system display (where practical). The time count ceases when the subject boat is moving toward the start line, under its own power, with both crew on board, all safety equipment in place and under control of the starter. From the top 16 down, the time limit shall be ten minutes and will apply from the time the affected boat is due to start in order, other boats will continue to race until the time has elapsed. If the affected boat cannot continue after ten minutes has elapsed, the timer will record a DNS. (The time count ceases when the subject boat is moving toward the start line under its own power ready to race). The sprint controller will be responsible for keeping time in this instance. At all times, the pit marshal or starter must be kept informed as to the status of the boat. A time-out is declared if the scrutineer is required to inspect a damaged boat and he/she must be completely satisfied that the boat is safe and fit for racing. No further work may be carried out during a scrutineering "time out".

PENALTY: Not notifying Pit Marshal or Starter of a problem or an inability to start in order - DNS.

PENALTY: Exceeding the two or ten minute time limits - DNS.

19.1 In the event that a Dummy grid is to be used to stage boats for launching, the boats must be at the dummy grid within 5 minutes of their "Last Call". In the event that a crew notifies the race controller of a problem with their boat they will make good their repairs within the allocated time frame (see rule 19) at the dummy grid under the supervision of the race controller or his steward without impedance to any other crew.

20. Any boat, which suffers a heavy impact or obvious damage in a racing accident, must be re-scrutineered before further competition. The re-scrutineering process will take place after any work to repair damage to the boat has been completed, but shall have time constraints such as meeting the appropriate time limits or qualifying rounds. The boat may not be returned to the water until the scrutineer has declared it safe for competition. In the absence of a scrutineer the sprint controller will re-scrutineer the boat. Likewise, the crew must be checked by the ambulance or first aid crew and declared fit to resume racing.
21. On race day, there will be a minimum of two (2) qualifying rounds, but normally three. If the qualifying rounds are reduced for any reason once racing has begun, the first elimination will include all entrants affected. (Elimination rules apply to such rounds). Formation of the elimination rounds will be advised on the event programme, or at drivers briefing. The cut progression will be advised prior to the running of that segment, but will normally be 16-8-4-2. The running order from the top 8 down must be slowest first. Where there are insufficient numbers to make up an elimination round, the round will still be treated as such, until the relevant cut can be made. (Elimination rules apply to such rounds).
22. Due to a number of variables such as weather, accidents, crew and boat numbers, the sprint controller retains the right to vary the progression of the elimination cuts. Crews will be informed of variations in advance.
23. Regardless of the number of crews available, no crew may be included in the elimination rounds without recording a qualifying time.
24. During the eliminations, if two crews record equal times in a particular round and that time qualifies for the last spot in the next round, they shall both be included in that next round, even though it adds an extra boat to that round. The following cut however, will be as previously established.
25. In the elimination rounds, if more than one crew records a DNF in any elimination round, their respective positions in that round will be

considered equal. In all instances, a DNF will score higher than a DNS.

26. In the top 2, if both crews record equal times they shall have a run-off in the same order as before, to determine the winner. If necessary, this process shall be repeated until a result is achieved.

902 - NEW RULES FOR CIRCUIT ENDURANCE CHAMPIONSHIPS

1. ORGANISATION

All races will be controlled by and remain the property of the U.I.M. In cases where International regulations do not exist then the national authorities will judge according to the general spirit of the event, considering U.I.M. circuit rules in groups 100, 200, 300 and 400. Any local rules must be accepted by the teams provided they are in the advance programme. The World Championship will be ran over 4 events in different countries with the possible addition of a 5th round outside Europe, except pneumatics.

2. THE CIRCUIT

The overall shape of the circuit is not binding but must not be less than 3000 metres, and a maximum of 6100 metres. All circuits must be registered with their N/A and with the U.I.M. (Min. 3 months prior to a race.) The turn buoys will be divided into each class so as not to have boats in different classes turning on the same turn buoy. There must be at least 750 metres between the main turn buoy.

For Pneumatics : The overall length of circuit must not be less than 2500 metres. The turn buoys for all classes will be the same. There must be at least 500 metres between the main turn buoy.

3. CLASSES

Catamarans classes: There will be three classes for the World Championship, ***based on the corresponding S classes.***

Class 1. Up to 1050cc

Class 2. Over 1050cc and up to 2050cc

Class 3. Over 2050cc and up to 3100cc

Engines that comply with EPA regulations will be allowed to compete with a 30% greater capacity in their corresponding classes.

V Classes : There will be 2 classes for the World Championship.

Class 1. Up to 1050cc

Class 2. Over 1050cc and up to 3100cc

Pneumatics classes : There will be 5 classes for the World Championship.

Class 1. Up to 770 cc

Class 2. Over 770 cc and up to 1060 cc

Class 3. Over 1060 cc and up to 1550 cc

Class 4. Over 1550 cc and up to 2050 cc

Class 5. Over 2050 cc and up to 3150 cc

Engines that comply with EPA regulations will be allowed to compete with a 30% greater capacity in their corresponding classes.

V classes and pneumatics must NOT run with Catamarans.

4. DURATION

The race will be for a minimum time period of 6 hours and maximum time of 24 hours continuous racing (see Part 17 also)

5. ENTRIES

All entries must be on the organising club's official form and entries must be sent in by the date as requested. Late entries may be accepted providing all paperwork is in order and that their inclusion does not affect the water capacity as homologated. Entry fee for the championship may be charged. Late entries may be fined.

6. VALIDITY OF DRIVERS

All drivers must be in possession of a current licence issued by his N/A for the class of boat that he wishes to race. He must also have the N/A permission to race. All other aspects refer to IUM rule 120. A driver must also have a current immersion test certificate if required by the U.I.M. for that class.

7. INSURANCE

Any driver who does not carry insurance to meet the requirement of the country in which they are racing, will have to pay, per boat, the required amount to a maximum of 1250ff. (euros)

8. TEAMS

For races up to 12 hours there must be a minimum of two pilots and a maximum of 4 pilots, and for races from 12 hours to 24 hours there will be a minimum of 3 pilots and a maximum of 4 pilots. Teams may practice more than the required number of pilots providing they have all paper work and licences in order. Team managers must notify the Control, 1 hour prior to the race as to their final pilots names and no alteration of the pilots list will be accepted for whatever reason. Failure to do so will lead to D/Q of the team. A pilot must not drive continuously for more than 2 hours. Any laps over the period will not be included. Minimum rest period for drivers is one hour.

9. TEAM MANAGERS

All entries must have a Team Manager who must not be a pilot. In a case where there is more than one boat in a team then the Manager can represent all entries in that team. All penalties and other race documentation at the race site will be forwarded to the Team Manager. The Team Manager must be present at all meetings and scrutineering.

10. ADVANCE PROGRAMME

The organisers of the race will forward the advance regulations to all N/A's in English and their own language within 48 days of such race. All aspects of the advance regulations will have at least all those details as listed in U.I.M. rule 202.

11. DRIVERS MEETING

It is mandatory for all drivers and Team Managers to attend driver's briefing. It will be in English and held after scrutineering and at least one hour prior to practice.

12. PRACTICE

All new pilots to the course must complete at least 2 laps of practice.

13. STARTS

The start will be a flying start behind a pace boat. The signal to start will be via a Green flag.

14. FINISHES

The finish of the race will be after the lead boat has completed the time duration. This will be denoted by a chequered flag flown from control and the final positions will be at the time the other boats complete the required time period.

15. CLASSIFICATION

To be classified as a finisher and to gain points you must have completed at least 40% of the class winner's laps and take the chequered flag. Boats not running but complete at least 90% of the class winners distance will be scored after all boats that take the chequered flag.

16. POINTS

Points will be awarded in each competing class as follows

1st	20	9th	7
2nd	17	10th	6
3rd	15	11th	5
4th	13	12th	4
5th	11	13th	3
6th	10	14th	2
7th	9	15th and onwards	1
8th	8		

17. INTERRUPTED RACE

A race that has to be curtailed for one reason or another and it is impossible for the race to recommence then points will be awarded as follows:

Up to 25% of race time 20%

40% of race time 50%

60% of race time 75%

90% of race time 90%

Over 90% full points.

If a race is interrupted and can be recommenced within a reasonable period of time then the recommencement procedure will be as the start of the race less completed time. The time period remaining will be the total race time less 15 minutes and less completed time. The distance

covered by each part of the race will be added together for the final result. The distance covered, the lap prior to the stoppage will be the laps taken. If the race has to be brought under a reduced speed then the pace boat will take the boats round in order there will be no overtaking. The restart will from the pace boat.

18. REPAIRS

All repairs must be carried out within your designated pit area.

19. FUEL

Fuel for racing must be provided by the organisers to a minimum standard of 95 Ron Unleaded fuel. All competitors must use the fuel supplied by the organisers at cost as indicated in the advance programme, any competitor failing to use this fuel will be disqualified. No race fuel can be stored at the boat pit position. All boats must be refuelled at the fuelling depot. Each pit area may have a maximum of 25 litres stored in a shock proof container and carry the race number of the boat. The sole purpose of this is for generators etc. and must not be used for the race boats except in the case where repairs to a boat necessitate the fuel tank being drained. In this case a max of 25 litres may be placed into the fuel tank of the boat to fire up the engine in the pits and to drive the boat to the refuelling station. The maximum amount of fuel that a boat can carry is 200 litres for Catamarans and 100 litres for V.Boats

The use of replaceable plastic tanks for fuel with the capacity of no more than 50 litres is allowed for Pneumatics.

For Pneumatics the maximum amount of fuel the boat can carry is as follows:

CLASS 1	60 liters
CLASS 2	100 liters
CLASS 3	150 liters
CLASS 4	200 liters
CLASS 5	200 liters

20. REFUELLING

Must be carried out at the refuelling station and with the engine off. No repairs of any kind can be undertaken at the refuelling point.

21. FUEL DEPOT

All personnel entering the fuel depot must not carry the following: Cameras: Mobile Phones: Electrical items: Metal Objects: any device that can be ignited i.e. Lighters. All personnel must have rubber soled shoes.

22. PENALTIES

Any sanction against a drive (Yellow cards or disqualification) will be reported to the U.I.M. Should a driver be disqualified from that event under any rules of the U.I.M. then the team may continue providing they have the minimum number of drivers? All other penalties will be reported to the Team Manager and displayed at race control.

Fuel infringement	8 laps
Race infringements	5 laps
Light infringements	(after 2 laps of notification) no laps will be scored.
Craning infringements	8 laps
Failure to attend scrutineering or drivers briefing	5 laps
Flag Infringement	5 laps
Priority of boats	5 laps

23. PROTESTS

The Team Manager on behalf of his driver can make a protest. No collective protests will be accepted. They must be written in English with full details and if appropriate the U.I.M. rule. The fee for a protest is fixed

at 150 euros and for a protest against an engine is fixed at 380 euros. Protest will be heard if possible within one hour of the protest being accepted and the result of any such protest will be given to the Team Manager and driver if possible. If a protest is upheld and laps have been deducted at the next posting of positions the laps will be re - introduced. If a protest comes within the last hour of racing then the protest will be heard as soon as possible and the final result will be not be posted until the protest has been heard. Protest on the final results must be made within half of one hour.

24. YELLOW/RED CARDS

Yellow cards can only be given by the OOD or the U.I.M. commissioner for offences on the water. They must be issued to the Team Manager or the driver on the official U.I.M. form. Other offences are covered within rule 406.06 of the U.I.M.

25. APPEALS

Appeals against any protest not upheld see U.I.M. rule 405.

26. POSTING OF POSITIONS

Positions will be posted every half hour for the first 2 hours then hourly and every half hour for the last 2 hours.

27. BOATS BROKEN DOWN ON COURSE

Any boat that breaks down on the course cannot refuse a tow and must be towed in the direction of the course. It is forbidden to be towed against the race direction on the race course.

28. PRIORITY OF BOATS

Rescue and pace boats have priority at all times. Entering the pit land and refuelling lane must be done at reduced speed and no overtaking

unless a boat has become stationary. All boats entering the pits or refuelling area have priority over those departing.

29. RESCUE

There must be at least one rescue boat for every 250 metres of course and that the course must be visible at all times from at least two rescue boats. There must be at least one rescue boat every 1000 metres capable of carrying a stretcher or that the boat must be a drop front dory.

30. SCRUTINEERING

All boats must be scrutineered before entering the water. It is mandatory for all drivers to be present and the Team Manager. All drivers must be able to prove that they fit within the craft as per U.I.M. rules. The OOD and U.I.M. commissioner will rule upon any such cases that infringe U.I.M. rules on cockpits. Each pilot must have his own helmet and life support vest and this must be produced at scrutineering. All boats must be presented to scrutineers with pickle forks dismantled and boats must have a spare pair that must be presented to the scrutineers.

31. CRANES

In the event of a crane being used for launching there will be a minimum of one crane per 15 boats. If launching is by slipway then one launch vehicle per 15 boats. The slipway must be large enough to allow the number of launch vehicles to work at the same time.

32. FLAGS

Red: Race terminated return to the holding area and await instructions

Yellow: Danger on circuit

Black: With race number fixed – driver to return immediately to the pits

Chequered black & white: Race completed

Yellow: Pace boat on circuit – no overtaking and reduce speed

Green: Race commenced/recommenced

33. TURN BUOYS

Turn buoys will be of a single colour either Orange or Red unless there are turns of both left and right then for left hand turns the buoy will be Orange and for right hand turns the buoys will be yellow.

For hitting a turn buoy no penalty.

For dislodging or destroying the first turn buoy – 5 lap penalty

For dislodging or destroying the second turn buoy – 10 lap penalty

Destroying a turn buoy – the cost of replacement will be charged to the offending team. Maximum cost 1500ff (euros) per buoy. Each turn will have 2 turn buoys.

34. POST INSPECTION

The first three boats in each class will render themselves to the “parc ferme” for inspection immediately after the end of the race. Any boat failing to do so will be D/Q from that race. the only persons allowed in “parc ferme” are the drivers, the Team Manager and two mechanics. No other personnel are allowed.

35. MACHINERY

Only homologated single engines can be used. The engine block must remain the same as started. All other parts may be changed. All engine blocks will be marked one hour prior to the commencement of the race. more than one engine can be used in practice.

Use of power trim is allowed for pneumatics. The height of the motor must remain fixed while racing for pneumatics.

36. HULL

The hull as started is the only hull to be used. More than one hull can be used in practice providing it passes all scrutineering. Running repairs can be carried out to hulls at their own pit area. All repairs must be inspected by the scrutineers before re-entry to the race.

37. NAVIGATION LIGHTS

Boats racing during night hours must have navigation lights as follows:

Single red to Starboard min 5 watt

Single Green to port min 5 watt

Two sets of red lights separate installed to the stern of the boat min 5 watts each with back-up batter supply.

2 spot light minimum to the front of the boat 36 watts minimum. These lights must not be more than 2 metres from the front of the boat. Drivers must carry a water-proof torch within the boat.

38. SLINGS

Each boat must carry its slings at all times if launch is by crane, whilst on the water and must comply with the safety rules and regulations of rule 205.12 of the U.I.M. Drivers or mechanics must not ride in boats whilst the boat is being craned.

39. NATIONAL FLAGS

All boats will carry the national flag of all competing drivers.

40. U.I.M. PENNANTS

It is mandatory for all boats to carry the U.I.M. Logo to both side of the boat.

41. BUOYANCY

All boats must carry sufficient buoyancy to keep their boat afloat.

42. FIXING EYE

All boats must have a fixing eye not less than 25mm fixed to the bottom of the nose of the boat and not further than 250mm from the front. The

fixing shall be strong enough to tow a boat partially submerged. A tow rope must be fixed. It must not be longer than the length of the hull.

432. PUBLICITY

The organising club and national authority can request that any offences advertising or any that conflicts with the sponsor or sponsors may request it to be covered throughout the duration of the race. All other aspects – see rule 203.051.

44. MEDICAL COVER

The minimum cover for the duration of the race and practice is for two ambulances on station at all times and two doctors, one to be trauma trained. It is advisable to have two stations.

45. DRUG TESTING

All drivers can be subjected to drug and alcohol tests. Any refusal will be deemed as a failure and the drive disqualified. Any tests that prove positive, the drive will be disqualified and reported to the U.I.M. All other aspects – see rule 205.02.02.

46. PRIZES

The U.I.M. will provide Gold, Silver and Bronze medals at the completion of the series of races and the Champions will receive their medals at the last race. The organisers will provide trophies for the first three in each U.I.M. class at each event. Prize giving must be within one hour of the completion of the race or, if protests are being conducted then it will be 30 minutes from completion of protests.

47. RESULTS

The results will be forwarded to the U.I.M. offices within 24 hours of the completion of the event.

The general classification will be drawn on additional points of all rounds per pilot(s).

In case of an ex æquo the points obtained in the longest endurance race will determine the winner(s) (there could be various ex æquo positions if it concerns one same team).

In all events, the pilots will be classified and not the boats.

48. U.I.M. COMMISSIONER

A U.I.M. Commissioner will be present throughout the event and the costs will be borne by the organising club. The club must make Hotel arrangements for all the nights that the Commissioner will be present. For events of more than 12 hours, 2 Commissioners will attend, one of which is the Senior. The U.I.M. will decide on seniority.

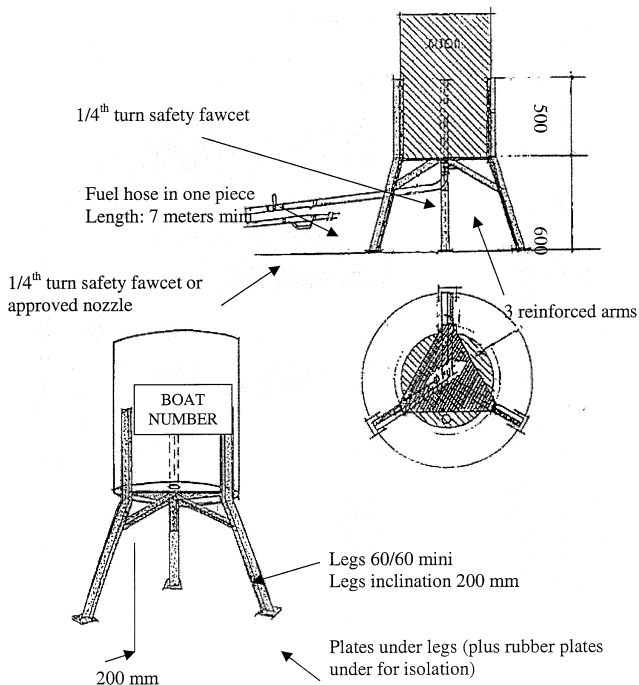
49. PACE BOAT

It is mandatory that a pace boat be launched read for use throughout the duration of the event and should there be a need for the race to be brought to a much reduced speed, then the pace boat will lead the leading boat flying a yellow flag until such time as the course is clear to race. All boats must remain in the order that they are in with no overtaking permitted. When the race is due to be restarted the yellow flag will be dropped and the green flag raised. When the pace boat marshal is happy that all is in order to restart he will drop the green flag. This is the only signal to recommence racing.

50. RACE NUMBERS

In the event that two numbers are identical, then the team who enter first can keep their race number. The Scrutineer will issue a further number to the Team and it is the team's responsibility to replace the number. Race numbers must be on the race trailer. An organised will, if possible, notify any team of a clash of numbers prior to scrutineering. Number one is reserved for the winner of the U.I.M. world championship. For all night races the numbers must be in luminescent white on a black

background 40 x 40. All numbers must be fixed to both sides of the boat on the cell, for Catamarans only, just behind the drivers head position. For V. Boats the number must be vertical and behind the drivers head.



A

Absence of race number	206.03
Advance-programme	202
Aerial (boats with aerial propeller)	590
Age limit	203.04
Aid (first aid station)	205.08.02
Altering course	312.12
APBA recognised series	106.01
Appeal (court)	405.01
Appeals (protests and)	403/405
Approaches to the starting line	306.08
Authority (change of National)	120.02
Awarding of prizes	322

B

Base	300-604
Before the start	306.06
Boat starting before time	306.09
Boats (inflatable)	560
Boats (equipment)	503
Boats (fouling competing boats)	314
Boucquey Trophy	803

Buoy and course officers	201.07
Buyse Trophy	801

C

Calculation of speed	605
Calendar	101
Cancellation of licence	120
Cancelled (race)	308
Cards (yellow)	406.03
Certificate (record)	615.08
Certificate (measurement)	501
Certificates (performance)	614
Championship (world)	107
Championships (continental)	108
Championships (general rules)	106
Change of date	101.04
Change of National Authority	120.02
Circuit	302
Circuit races (rules for)	300
Circulation of the result (record)	615.07
Classes (international series and classes)	500
Classes, sports outboards	540
Classes racing inboards	510-515

Classes eligible for records	601
Classes racing outboards	520
Classes (monohull, outboards)	550
Classes Sport outboard	530
Classes Osy 400	525
Classes Pneumatic	565
Classes Diesel	570
Classes Turbo Sets	580
Classes Aerial Propellers	590
Classes Electric Powered Boats	595
Classes Inflatable	560
Clock	306
Cockpit (reinforced)	509
Clothing protection	205.11
Commissioner (U.I.M.)	102
Committee (powers of the race)	201.01
Committee (organising, race)	201.01
Cost of measurement	501.01
Course (marks of the)	303
Cups (international)	800

D

Date (change of)	101.04
------------------	--------

Deadline dates	101.03
Dead engine start	307
Dead-Heat	319
Deformable pickle-forks	503.02
Deputy Officer of the Day	201.04
Discs	306.02
Disqualification	406.04
Distinctive marks	206
Drag racing	112
Doping test	205.02.02
Duties of the timekeepers	201.09
Duties of the Technical officers	201.07
Duties of the race Officials	201.06
Duty of the Deputy officer of the day	201.04

E

Electric powered boats	595
Endurance races	109-902
Entries	203
Entry level racing classes	500.04
Environmental care	704
Equipment of boats	503
Exclusion	406.07

F

Fee (entry fee)	203.03
Finishing line	318
Finishing a race	317
First aid station	205.08.02
Flag signals	304.02
Fouling marks	313
Fuel	508-514-519-524-529-534-544-554-564-569-574-584-594

G

Gun	306.03
-----	--------

H

Handicap	111
Heats (races in several)	318.01
Helmet	205.07
Homologation of records	615
Homologation of engines	505-506
Homologation sheets	505.04
Hulls (inspection of)	321-607

I

Immersion Test	205.05
Inspection of engines & hulls	321-502-607
Insurance	205.09
Interested parties	402.02
International races recognised by the U.I.M.	103-104
International series and classes	500
International cups	800
International officers	201.02
Interpretation of the rules	200.02

J

John Ward Trophy	802
Jet Sprint Rules	901
Jurisdiction	400
Jury	201.01-402
John Hill Trophy	805

L

Licences	120
Life jacket	205.06-509.04

Lifting of race boats	205.12
Log book	501.11
Low emission engines	541.02-542.12

M

Margin required (records)	606
Marks of the course (definition)	303
Marks of nationality	206
Marks (rounding)	312.11
Measurement certificate	501
Medical examination	205.01, 02, 03, 04
Medical Officer	201.04.01
Minimum age	203.04
Modifications allowed	528.10-533.10-543.10-553.10 562.10-563.10-568.12-528.10-568.10
Motor shut off switch	509.12

N

National Authority (change of)	120.02
Nationality of the drivers	203.05, 206.01
Noise reduction	504
Non approved races	104
Number and value of the prizes	322

Number (race)	206.02
---------------	--------

O

Obstacle (definition)	300.01
Officer of the Day	201.03
Organisation of records	600
Organisation of races	200
Organising committee	201
Overlap, overtaking	312.09-10

P

Passing marks	312.11
Penalties	406
Performance certificates	614
Piston motors	503.07
Pickle-forks	509.02
Posting of the results	320
Postponed (race)	309
Prizes (number and value)	322
Production changes (engines)	507
Programme (race)	204
Protection (technical)	205.10
Protests	403

Protests (and appeals)	405
Pseudonym	120.07
Publicity	203.051

R

Racing instructions	204
Race number	206.02
Race (stopping of a)	311
Race cancelled	308
Race postponed	309
Race shortened	310
Race (finishing a)	317
Races (rules for circuit)	300
Races recognised by the U.I.M.	103
Races in several heats	318.01
Racing licences (rules)	120
Records	600
Record certificate	615.08
Record (circulation of the result of a)	615.07
Report of the Commissioner	102.04
Reports	323
Result (circulation of the result of a record)	605.07
Results (position of the)	320

River Marathon Rules	900
Rotary motors	503.07
Rounding marks	313
Rules of the road	312
Rules (interpretation of the)	200.02

S

Safety zone	306.05
Safety of the driver	205
Safety boats	205.08
Scoring system	318
Signals from the shore	304
Signals (starting)	304.02
Start (flying)	306
Start (dead engine)	307
Start	305
Starting (boat starting before time)	306.09
Starting in groups	304.03
Starting line (approaches to)	306.08
Stopping of a race	311
Super charging	503.09
Super-licence	120.08
Suspension	406.05

T

Technical officers	201.07
Technical protection	205.10
Throttle cut-off device (automatic) ⁵	03.03
Timekeepers and timing devices	201.09-301
Timing	301
Titled International events	105
Towing boats	205.08.01
Trophies	800

U

U.I.M. Commissioner	102
---------------------	-----

V

Value and number of the prizes	322
--------------------------------	-----

W

Water registration	106.02
Windscreen	503.02
World championship	107

Y

Yellow cards

406.03

U.I.M. FORMULA 1 WORLD CHAMPIONSHIP

1. ORGANISATION
2. RACE FORMAT
3. START
4. STOPPED RACE
5. INTERRUPTED RACE
6. RACE COURSE
7. CLASSIFICATION
8. EQUIPMENT
9. FUEL
10. SUPERLICENCES
11. YELLOW CARDS
12. PROCEDURE FOR PROTESTS AND APPEAL
13. SAFETY
14. GENERAL

INTERPRETATION OF THE RULES - 200.02

In all cases unforeseen by the international rules, the National Authority is to adjudicate, bearing in mind the spirit of the rules.

In the case of conflicting rules, these rules concerning the Championship as published in the blue pages of the Circuit Rules Handbook, shall prevail.

1. ORGANISATION

- 1.1. The rights of organisation of Formula 1 races and championships are owned and ruled by the U.I.M. which has granted the management of the competition to the Council.
- 1.2. The Council may update these rules whenever necessary.

- 1.3. The Formula 1 daily Management Committee is responsible for the championship daily management.

The Formula 1 World Championship is governed solely by the U.I.M. international rules for circuit racing, as published in the white and blue pages of the Circuit Rules Handbook.

- 1.4. The permanent Formula 1 U.I.M. commissioner must check new venues in due time before an event can be sanctioned.

As well as the permanent Formula 1 U.I.M. commissioner, a technical commissioner must be appointed for each race. Both commissioners are members of the Formula 1 management committee.

- 1.5. A Formula 1 Protest Judge will be nominated for each event and will adjudicate on all protests.

The protest judge must be named in the advance programme.

- 1.6. The Officer of the Day must speak fluent English.

The following officials have to be Club, NA or U.I.M. officials:- OOD, deputy OOD, safety officer, technical officer/scrutineer, time keeper, pit marshal; pontoon marshal and starter.

- 1.7. It is strictly not permitted to grant club-membership for the duration of the race meeting to interested outside parties.
- 1.8. The U.I.M. flag must be flown from a prominent position at each event.

2. RACE FORMAT

2.1. Each Formula 1 meeting counting towards the drivers World Championship will take place over a period of two or three days in accordance with the full U.I.M. Circuit Rules for racing, classification, protests and appeals and the specific rules listed below.

Friday	12.00 - 13.00 hrs	Documentation and registration
	12.00 hours	Technical scrutineering

Boats and all racing equipment (including racing gear of the driver) must be in the pits at 12.00 hours (fine if not present)

Saturday 0900 hours	Drivers' meeting (compulsory for drivers and radiomen and team managers of each boat) at race headquarters (fine if not present at the roll call)
1000-1100 hrs	Free practice
1300-1345 hrs	First timed trials Each boat entitled to only 15 laps excluding their first lap.
1500-1545 hrs	Second timed trials Each boat entitled to only 15 laps excluding their first lap.
1630-1700 hrs	Timed trials for the 6 fastest boats
Sunday 0930 hours	Drivers' meeting (compulsory for drivers and radiomen and team managers of each boat) at race headquarters (fine if not present at the roll call)
1015-1100 hrs	Free practice
1430 hours	Formula 1 Race (approx. 45 minutes duration, but with a limit of 90 minutes including any re-starts)

If there is a change to the racing course layout, there may be extra free practice.

- 2.2. No entry shall be considered official until the boat has passed scrutineering and the documents are checked.

Only one boat per driver is allowed to be entered for any championship meeting. Spare hulls and engines are allowed but may be used only as foreseen in the Formula 1 rules.

- 2.3. If force majeure causes one of the two time trial sessions to be cancelled the results of the other session will still stand. In the event that less than 50% of the other session is run, then the grid

positions will be established using the current championship positions.

- 2.4. Each boat is allowed to pass the timing control a maximum of 15 times during each time trial session.

2.5. Establishment of start-grid positions from timed trials.

2.5.1. All drivers are obliged to participate in both timed trials.

2.5.2. Each driver's start-grid overall position will be based on the average of the fastest time from both timed trials.

2.5.3. In the case of any drivers participating in one timed trial only, at their own choice ; all drivers with averaged times from both timed trials will be placed ahead of them on the start-grid.

2.5.4. Single timed result drivers will then be positioned in the order of their times.

2.5.5. The starting grid positions for the six fastest boats from timed trials will be further adjusted by taking the order of best times achieved in a subsequent shoot-out session. The previous results of the timed trials will no longer be valid.

2.5.6. If an officially declared force majeure or a driver decision prevents a driver from participating in the shoot out, he will automatically take the last of the first six positions in the starting grid. In the case of more than one driver being so affected, they will be positioned relative to each other in accordance with their timed trials times.

2.5.7. The six fastest engines in time trials must be sealed, the boats weighed, their fuel checked, **and, if so equipped, their restricted induction system integrity checked.** A procedure of random weighing and fuel testing during timed trials will also be enforced.

2.5.8. Any engine changed after qualifying time trials or the shoot-out, will be sealed by the U.I.M. technical commissioner and will be checked for legality.

- 2.6. The official practice schedule will be posted at race control.

Testing outside the race circuit and before or after authorised hours is prohibited during the event.

- 2.7. In case of force majeure, this format may be amended by the OOD and the two commissioners.

If a race is shortened due to force majeure, it is at the discretion of the OOD and the U.I.M. commissioner to alter the time table, and if necessary the course layout accordingly.

If the race is declared to be on rough water, the number of laps should ideally be maintained, but may be reduced if seen fit by the U.I.M. permanent Commissioner. If time permits, the circuit layout can be changed, as agreed by the permanent commissioner and the drivers representative.

- 2.8. If they change boat for rough water conditions, they take last position on the pole grid.

QUALIFICATION PROCEDURE

- 2.9. The maximum number of boats will be 24 for the race, unless decided differently by the U.I.M. Permanent Commissioner and the Promoter, and announced at the first drivers meeting.

If more boats are entered for a race, the series contracted boats will automatically have access to the official practice and time trials. All other boats will participate in a 30 minutes pre-qualifying timed practice session which will take place on Saturday morning one hour before free practice; the fastest boats will be allowed to participate in the official practice and time trials, bringing the numbers up to the maximum of 24. If necessary, the non-qualified boats must leave the paddock area.

- 2.10. The U.I.M. Commissioner reserves the right to place one non-qualified boat in the final event. National drivers from the country hosting the event or a driver selected by the promoter will be eligible. The wild card driver will be announced at the first drivers' meeting.

- 2.11. On the afternoon before the race, a meeting must be held to examine the main points of the race. The attendees of the meeting, and the agenda, must be in accordance with the rules of the U.I.M.
- 2.12. The U.I.M. Commissioner must ensure that there is a roll-call of names of drivers, radiomen and team managers and each one must respond with his assigned racing number. If the driver is not present to respond when his name is called, a financial penalty will be imposed.

3. START

- 3.1. The start shall be a dead-engine jetty start as described in rule 307, bearing in mind the following
 - To increase the length of the pontoon to come close to 5 metres space per boat. This would leave more space to maintain the lanes.
 - To incline the pontoon (or change the location of the first buoy) sufficiently not to have the pontoon straight on the first buoy.

In case these points are not feasible, the alternative will be a rolling start upon decision of the U.I.M. Commissioners. Also the Commissioners will apply stronger penalties for drivers not maintaining their lane.

- 3.2. Boats should be drawn up with dead engine, stern first to a jetty, floating pontoon, or beach.

Boat which do not line up with the boat next to them within 20 seconds of being requested to do so will be penalised financially.
- 3.3. The distance between the starting line and the first turning buoy must be at least 300 m considering all safety aspects.
- 3.4. When the Pontoon Marshal is satisfied that all drivers are ready to start, with engines stopped, he will raise a green flag and the OOD will initiate the start procedure, by instructing the starter to raise the 30 seconds board (min. 80 x 80 cm, figures 60 x 10 cm).

Once the 30 seconds board has been raised those drivers starting their engines or leaving the pontoon before all red lights are switched off, will be penalised one lap. This lap will be deducted at the end of the race.

30 seconds after this board is raised all red lights will be switched on. Between 5-15 seconds after that, all red lights will be switched off and the race starts.

After a period of time the red light will be shown again.

- 3.5. Late starters: A driver who has failed to start before the red light is shown again, must wait until the whole field has passed the start pontoon before he may join the race through the entrance lane to the course.
- 3.6. The starter may decide, if he judges it necessary, to cancel the start by showing the red flag and recommence the starting procedure.
- 3.7. The start jetty or pontoon must be minimum 75 m long or be equal to 5 metres multiplied by the maximum number of boats permitted to race, (whichever is the greater) and must be at least 1.5m wide.

The alignment of the start pontoons must be such that the pole position has the maximum advantage towards the first turn buoy, compared with the last positions on the starting grid. The plan of the circuit (including the position of the start pontoon) must be sent to the U.I.M. office 60 days in advance of the race date for approval of the permanent commissioner.

- 3.8. Any boat not starting must be returned to shore by the driver's own efforts. Any crew member swimming out to a boat will cause his entry to be disqualified.

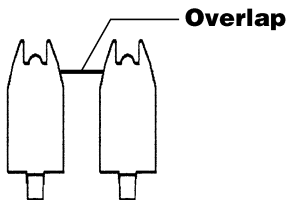
Only uniformed crew who are Formula 1 team members with Formula 1 passes designating pit starting pontoon admittance will be allowed on the starting pontoon.

- 3.9 The start procedure may be changed in 2005 in order to separate the boats in two groups from the start until the first buoy by guiding the drivers through 2 different gates marked with 2 large buoys

ahead of the first turn buoy. The daily management committee will decide whether to apply this system in 2005 and full details on the starting system will be given at the drivers briefing.

3.10 An overlap is established only when two boats are on the same course, or approximately the same course, and the cockpit of the overtaking boat, if overtaking from the inside, is even with the cockpit of the overtaken boat. When overtaking from the outside, an overlap is established when the overtaking boat has passed the overtaken boat sufficiently to change lanes without contact. The following rules shall apply to an overlap :

- **The lead boat has the right of way until an overlap is established.**
- **A safe overlap is the responsibility of the overtaking boat**
- **Once an overlap is established, the overtaken boat must give the overtaking boat room to clear any course markers.**
- **The overtaking boat cannot force an overlap after the lead boat has begun to alter course to negotiate a turn.**
- **An overlap can occur from port or starboard.**



4. STOPPED RACE

- 4.1. In the event of the race being stopped, boats must return under their own power to the start pontoon [or other position on the

circuit notified by the U.I.M. Commissioner during drivers' briefing]. Alternatively, the yellow flag may be shown and all drivers will continue at slow speed with no overtaking.

- 4.2. Drivers receiving outside assistance at any point during the race (whether the race has been stopped for an incident or not) will be disqualified.
- 4.3. Boats that have broken down and received outside assistance by being pulled to the infield or off the race course will be towed to the launching pontoon only during a «race stop» condition and if pick-up boats are available.
- 4.4. Repair or maintenance work may only be carried out on boats that have returned to the launch or start pontoons under their own power. Repaired boats may rejoin the race at any time as directed by the pontoon Marshal. Further definition of the repair areas will be given at drivers briefing.
- 4.5. No repairs are allowed on the start pontoon during the line-up and start or re-start procedure.
- 4.6. Drivers may carry-out their own repairs on their boats in accordance with the rules. The driver's two nominated pontoon mechanics and the official crane driver do not count as outside assistance.
- 4.7. In the event of a restart, a boat being repaired must be ready to take its position on the grid as directed by the pontoon Marshal. If the boat is not ready to take its position, it must start at the end of the pontoon.
- 4.8. The re-started race distance will be the original race distance minus the total number of laps completed by the leading boat at the time of the stoppage, minus also 1 lap per re-start.
- 4.9. Starting positions for restarts, or final classification if there is no restart, will be determined by the order at the end of the lap prior to the race being stopped.

- 4.10. The race organiser may at any time determine who was responsible for a race stoppage, even after the race is finished; in which case the guilty driver[s] will be disqualified from that race.
- 4.11. It is mandatory that all boats have remote controlled red and yellow lights in their cockpit. It is the drivers' responsibility to ensure that these lights are functioning and that they are clearly visible to the driver whilst racing.
- 4.12. In case of a race stop, the rescue boats and race control will show the red flag and race control will switch on the red light in the boats.

At this signal all drivers must immediately slow down and return to the starting pontoon, or the previously defined re-start area.

5. INTERRUPTED RACE

- 5.1. The position of the boats for the restart will be that of their passing order on the lap before the yellow signal was given. Boats which do not restart in their correct position, will be given a one lap penalty.
- 5.2. When the yellow flag is shown, the leading boat or a designated boat will act as pace boat, and run at slow speed. The yellow flag will be used by race control and the rescue boats to indicate a hazard when displayed stationary, and extreme caution to be exercised by drivers when waved vigorously.
- 5.3. The Organiser may interrupt the race for any reasons of safety or emergency services access, and in which case the rescue boats and race control will show yellow flags and race control will switch on the yellow lights in the boats.

At this signal all drivers must immediately slow down and not overtake. Any overtaking, 5 seconds after the yellow signal has been given, will be penalised by 1 lap ***unless instructed by the OOD or UIM Commissioner to change position of the rolling restart.***

- 5.4. When the race course is clear, only the yellow flag at race control will be continued to be shown.
- 5.5. All boats maintain their position at slow speed (approx 4000 rpm) until race control raises the green flag. It is the responsibility of the team managers to ensure that their boat(s) are at the correct position at the restart.
- 5.6. When a rolling re-start is used the U.I.M. Commissioner can show the green flag if the first 10 boats are in their correct positions. Boats which do not restart in their correct position, will be given a one lap penalty.
- 5.7. During the yellow signal situation, a boat may leave the race course but must re-join the race behind the boat running in last position.
- 5.8. The OOD or UIM commissioner may – up till one hour after posting of the results, determine who was responsible for an interrupted or stopped race, even after the race is finished ; in which case the guilty driver(s) will be disqualified from that race.

6. RACE COURSE

- 6.1. The length of the straight between any 2 turning buoys is to be a maximum of 850 metres.
- 6.2. There must be a minimum of 2 cranes for launching boats.
- 6.3. The boundaries of the water on which practice and racing are allowed to take place must be defined in the advance-programme.
- 6.4. The pit area should be sufficiently large to prevent boats being craned into the water over other boats.
- 6.5. Safety rules shall be those of the U.I.M. applicable on the date of the race unless specified otherwise by these rules.
- 6.6. Right hand turns can be inserted on the race course.

In order to avoid any confusion, left hand turns will be marked by orange buoys and right hand turns by yellow buoys.

7. CLASSIFICATION

- 7.1. The boat placed first will be the one having covered the scheduled distance in the shortest time.
- 7.2. All boats will be classified by taking into account the number of complete laps that they have covered and for those which have completed the same number of laps, the order in which they crossed the finish line.
- 7.3. The number of laps completed during restarts will be taken in account for the classification.
- 7.4. Points will be awarded to the first 10 classified drivers on the condition that they have completed at least 70 % of the number of laps covered by the winner (rounded down to the nearest whole number of laps).
- 7.5. Boats that complete 90% or more of the race distance, but do not cross the finishing line after the winner has crossed the finishing line, will be classified after those boats that have done the same number of laps but that also cross the finishing line after the winner.
- 7.6. To be awarded full points, the leading boat must have completed 70 % of the number of laps counting for the race.
- 7.7. If a stopped race cannot be restarted due to «Force Majeure», points will be awarded in accordance with the following scale:

70% or more of the total race completed	Full points
less than 70 % completed	Half points
less than 35% completed	No points
- 7.8. Finishing points scale:

First	20 points
Second	15 points
Third	12 points
Fourth	9 points
Fifth	7 points

Sixth	5 points
Seventh	4 points
Eight	3 points
Ninth	2 points
Tenth	1 point

- 7.9. The overall classification for the Formula 1 Drivers' World Championship shall be established by the addition of all points obtained by each driver in each of the races counting towards the championship.

The points are personal and are not allocated to the team. Each driver scores his own Championship points. If a driver changes team during the course of the season, he keeps his points.

- 7.10. In the case of a tie, the number of first places in these races shall be considered, then the second places, etc..

- 7.11. Immediately after the race:

The winner will complete a lap of honour

- 7.12. Presentation of trophies to first 3 drivers on the official podium. This presentation is only based on a provisional classification, and is subject to confirmation after scrutineering and consideration of any protests or appeals.

- 7.13. At the last race of the series U.I.M. will provide 3 medals (gold, silver, bronze) to be given to the first three in the overall classification of the World Championship.

These medals do not replace the normal prizes (art. 108.01 and 322).

- 7.14. Using the points scoring scale of the U.I.M. Formula 1 World Championship, a pole position classification will be established.

- 7.15. A Hull constructor's classification will be established where only the best place per brand will score points.

- 7.16. An engine constructor's classification will be established where only the best place per brand will score points.

- 7.18. The «John Hill Memorial Trophy» will be attributed at one of the Formula 1 World Championship rounds as decided by the Formula 1 Committee.

8. EQUIPMENT

HULL

- 8.1. The hull is free for each of the three capacities but must conform to the Formula 1 rules.
- 8.2. Hull, sponsons, and powerhead replacements and other parts are allowed after scrutineering, up until the start of the race, but any changes to be made must be immediately notified to the U.I.M. technical Commissioner for re-scrutineering and sealing and technical inspection of units removed.
- 8.3. Any replacement of boat or sponsons after qualifying time trials will result in all previous times achieved being cancelled, and the replacement boat or sponsons will be considered as a new entry, requiring full scrutineering. If no timed practice opportunity remains, the boat will start from the end of the grid.
- 8.4. Repairs are allowed at any time during the race weekend with the exception that the replacement of hull, sponson, crankcase, or cylinder block, is not permitted after the start of the race. Repairs are not allowed on the race course using outside assistance.
- 8.5. All craft must have an isolation switch as close to the positive terminal of the battery(ies) as practicable (negative terminal if positive earth system is employed).

The operating handle of the switch must be easily accessible on the deck or other surface of the boat on the port side between 1 to 3 meters forward of the stern.

This switch must be marked with a red flash + and the ON/OFF positions shown.

- 8.6. The weight of the boat, including residual fuel and oil, the driver with personal equipment, but excluding loose water is-

2.0 litres	461 kilogrammes
2.5 litres	525 kilogrammes
3.0 litres	561 kilogrammes

The first six classified boats will be weighed with scales after time trials and the race. The weighing of the boats cannot be protested. The scales will be available before the meeting commences.

Upon request of the U.I.M. Commissioner, drivers must allow to be weighed separately.

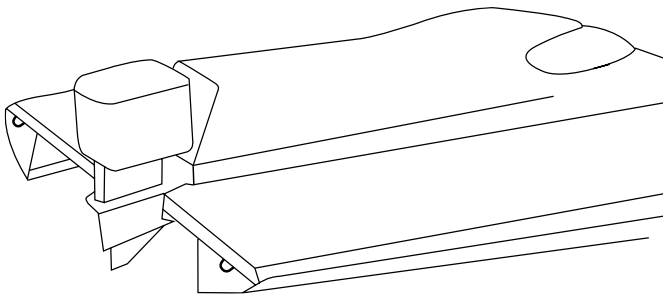
- 8.7. The minimum length (measured along the sponson) is 4m80 for all three capacities
- 8.9. Radios in boats are mandatory and a full radio check of each boat will be part of technical scrutineering in order to guarantee that all radios are working. The radioman must be present at race control whenever his boat is on the course. In order to guarantee clear communication between the Team Managers and the U.I.M. Commissioner, all Team Managers must speak English.
- 8.10. Each wing mirror must have minimum size of 60 sq. cm and be bolted on 2 points to assure proper mounting.

Within one team, two identical boats must have different colours of mirror bodies as follows:

Odd number: yellow fluorescent colour

Even number: orange fluorescent colour.

- 8.11. Mooring eyes must be placed on the back of each sponson no more than 20 cm in from the outside of the sponson -see drawing



ENGINES

TECHNICAL RULES UNTIL 2009

ENGINES

1. ***A grandfather rule applies for all Formula 1 outboards that were homologated and eligible to compete up to and including the 2003 season. These engines are :***
2. ***Mercury 2.5 EFI Homologation sheet No: F-1/00496***
3. ***Johnson 3.0 EFI Homologation sheet No: F-1/00497***
4. ***These engines will continue to be eligible for as long as they are competitive; they will be governed by their original homologation sheets supplied when new. These grandfather units will be taken as the performance benchmark for newcomers, with an objective of a nominal 350 hp performance ceiling.***
5. ***Any four stroke engine being submitted for homologation under these rules will have its performance limited by means of an orifice plate, 36mm bore diameter with a parallel length of at least 5mm. This plate may be incorporated into an airflowed induction system for atmospheric induction***

engines, and immediately prior to the compressor on forced induction engines; as designed and integrated by the manufacturer, and approved by the UIM management committee. The restrictor element will be a single metallic piece as detailed on UIM drawing number F1 – 001.

- 6. Any new two stroke engines submitted for homologation under these rules will have an induction restrictor that will be of a different diameter from the four stroke one, to compensate for the different combustion cycles.*
- 7. Except for the grandfathered engines, air can only be admitted to the engine for combustion via the restrictor orifice.*
- 8. The Formula 1 management committee reserves the right to review four stroke or new two stroke engine performance at mid-season in the event that one or the other is too performant within the meaning of these rules. If deemed appropriate, they will modify the diameter of the engine's induction restrictor. A four week grace period will be allowed for modifying and fitting the restrictor plates, from the date of announcement of the change.*
- 9. These technical specification for Formula 1 racing engines permit the engine [comprising of the power-unit, drive-line, propshaft, mounting system, steering, hydraulic, and electrical controls], to be installed as an outboard, inboard, or inboard / outboard, in any drive configuration.*
- 10. New manufacturers are admitted provided they are prepared to make complete engine units available and sell them in any quantity on demand. Orders by teams must be placed and deposit paid by the end of June in any particular year, and delivery must be made by the end of December the same year. The maximum retail price for a complete engine unit is to be in the order of US\$ 75,000 and will be reviewed for approval by the Formula 1 management committee at time of homologation.*
- 11. Formula 1 boats must be water propeller driven, no airscrews are permitted.*

12. *Formula 1 engines must have a fixed ratio direct drive. Variable ratio gearboxes, friction clutches, and torque multipliers are not permitted.*
13. *Atmospheric induction engines with a maximum of 8 cylinders, will be admitted up to 3.0 litres nominal capacity; grouped into the following measured engine capacity ranges :*

<i>2.0 litre</i>	<i>measured capacity 1950 to 2100cc</i>
<i>2.5 litre</i>	<i>measured capacity 2101 to 2550cc</i>
<i>3.0 litre</i>	<i>measured capacity 2551 to 3100cc</i>
14. *Forced induction engines with a maximum of 6 cylinders will be accepted with an equivalent capacity calculated by multiplying the measured capacity by 1.4. The maximum measured engine capacity admitted will be nominally 2 litres, hence:*

<i>2.0 litre</i>	<i>measured capacity 1200 to 1500cc equivalent to 2100cc</i>
<i>2.5 litre</i>	<i>measured capacity 1501 to 1821cc equivalent to 2550cc</i>
<i>3.0 litre</i>	<i>measured capacity 1822 to 2100cc equivalent to 2940cc</i>
15. *Power units may be of any internal combustion design, of limited or series production.*
16. *Rotary motors will be rated in accordance with rule 503.08.*
17. *Diesel, external combustion, or gas turbine engines are not eligible.*
18. *Only single crankshaft/powerhead configuration is permitted.*
19. *'Green engines' are to be encouraged [with respect to emissions] through incentives determined from time to time by the Formula 1 committee.*
20. *Engines must be capable of being silenced. If a particular race organiser requests it for local reasons, rule 504 will be applied and engines must not exceed a maximum decibel rating of 98 +6 db(A) when measured as rule 504.03.*
21. *If silencers are required to conform to the maximum noise*

rating, it is mandatory to have them available at all races. Compliance is required for scrutineering clearance.

- 22. To reduce environmental pollution, running-in of engines at the launch jetty or other specified area, will only be permitted by the Technical Commissioner and, if required by the Technical Commissioner, effective mufflers must be used*
- 23. Non-compliance with noise regulations will be financially penalised.*
- 24. All new Formula 1 engines must be homologated by the UIM in accordance with rules 505 and 506, and will be subject to these rules. Technical or component restrictions, mandatory in the UIM rules and homologation form, will apply to all engines as raced. For all engines, the crankcase and cylinder block, cylinder heads, induction system, injection system, engine management control unit including the ignition system, must be Original Equipment, corresponding with dimensions and/or part numbers on the homologation form.*
- 25. Crankcase cover and exhaust tuner pipes for two stroke engines will be included in the homologated details.*
- 26. Four stroke engine valve trains including cams, valves and mechanisms must be homologated, together with any supercharger and drives, or turbo blower and wastegate settings.*
- 27. Engines will either be tested for induction system integrity by running them at a steady 4000 rpm and timing the period they take to stop when the induction restrictor is blocked by a spherical plug; or by means of UIM approved alternative instruments.*
- 28. Non homologated OEM parts, or those parts not restricted by these rules may be changed for any after-market parts during engine race-preparation, but may not be included in the engine name unless the homologated engine manufacturer is specifically in agreement.*

29. *New engines will be accepted for homologation provided the engine/drive units are submitted for verification, the spirit and letter of the performance target of these rules is respected, and they are freely available to be purchased at a price approved by the Formula 1 committee.*
30. *To qualify for homologation, the manufacturer must present a complete power-unit assembly for verification by the UIM technical officer or delegate nominated by the Formula 1 daily management committee. All driveline and mounting units must be manufactured by the power-unit manufacturer, unless satisfactory proof of contractual agreement can be provided for mixed units, in which case both manufacturers names are to be included in the engine name and on the engine cover logo. Third paragraph of rule 505.01 does not apply.*
31. *Applications for homologation of a new engine, which will include a fully completed homologation form with recognition photographs, must be submitted no later than October 15th of the year preceding the first season of use. UIM verification and approval will be completed no later than 6 weeks after receipt of the application.*
32. *Manufacturer's logos and colour schemes must be used as homologated.*
33. *Running production changes will be accepted only once per year for the crankcase / cylinder assemblies, and notification must be received by October 15th of each year. Running production changes for all other components may be submitted for approval at any time. Legality for use will be considered by the Formula 1 committee, taking into account any affect the change will have on performance. If accepted, changes will be effective immediately after approval, and will be conditional that the affected components are freely available at a reasonable price. All interested parties will be notified by UIM.*
34. *In the case of an engine proving to be illegal, penalties will be imposed in accordance with rule 406 - Penalties*

POST-RACE SCRUTINEERING

1. *The measurements registered on the homologation sheet will be used as a reference for technical verification of the engine's legality. All dimensions given are absolute maxima [or minima, as the case applies] and no tolerance is given. The Original Equipment Manufacturer will build engines that are within the dimensions and conform to the homologation. The absolute responsibility will be passed onto the driver, at time of sale, to ensure that the engine maintains its legality irrespective of whatever maintenance or setting-up work is carried-out on it. Technical scrutineering will be carried-out using manufacturer produced gauges handed over at time of homologation to the U.I.M. These gauges will be used on a 'GO', 'NO-GO' basis. If the gauge does not go the dimension is legal.*
2. *The technical commissioner at each event will decide the degree of dismantling for inspection to be carried-out on the first three classified entries, and his decision will be final.*

9. FUEL

- 9.1. An official fuel will be supplied by the Promoter / organiser at each race venue.

It will be of a continuously consistent specification and quality ; conforming to the data sheet and samples issued at the beginning of the season and agreed between the U.I.M., the Promoter and the Engine Manufacturers.

Only this fuel will be used; teams bringing their own fuel to the race venues will be penalised.

- 9.2. Fuel samples taken at random from competitors during the event will be tested at the race venue and may also be analysed afterwards at a laboratory.

Drivers disqualified for fuel infringements will have to pay all laboratory testing and analysis costs.

- 9.3. The only permitted addition to the fuel is lubricating oil for two stroke motors. The use of additives aimed at increasing the power of the motor is prohibited to either the fuel (carburant) or the air intake (comburant).
- 9.4. No chemical starter aids are permitted on the start pontoon.
- 9.5. All participants are subject to thorough inspection at any time. The entire fuel system may be disassembled, removed from the boat and inspected and/or fuel samples taken. The presence of illegal fuel, at any time, on the onboard fuel system will result in immediate disqualification from that part of the race.

The legality of the fuel is to be adjudicated solely by the U.I.M. commissioner, based on all tests and information available to him.

- 9.6. No refuelling will be allowed when the boat is not on its trailer.

10. SUPERLICENCES

- 10.1. A super licence is required for Formula 1 together with the NA international licence. No other membership is required.
- 10.2. The maximum age for a driver to qualify to hold a U.I.M. F1 Superlicence and to compete in a F1 World Championship race is 50 years old.
- 10.3. The nationality of a driver is the nationality of the N.A. which issued the international racing licence to the driver. Medical and immersion tests can be done in other countries if the issuing N.A. agrees. The flag is the one of the issuing N.A. of the international licence as above. The request for the U.I.M. Superlicence must come from the N.A. which issues the international racing licence.
- 10.4. Drivers must hold a U.I.M. superlicence before entry will be accepted.
- 10.5. Any new driver must be vetted during practice by the two U.I.M. Commissioners and the Formula 1 drivers representative to the U.I.M.

10.6. To obtain a superlicence, a driver must have participated in 6 circuit races using a catamaran type hull (750cc minimum) or a hydrocat (500cc). Definition of a race during a race weekend, even if several heats are taking place, is that the race weekend with all heats or races is counting for one race only. The National Authorities are fully liable for the sports c.v. they submit for their drivers. The full race results of each race must be supplied since the drivers must have done at least 6 races with 70 % of the total distance of each race event.

The superlicence is provisional for the first 10 races and can be withdrawn at any time during the first 10 races by a committee composed by the OOD, the permanent Formula 1 Commissioner and the Drivers representative. When a provisional superlicence is withdrawn, re-application can only take place for the next season. The decision on all matters concerning superlicenses is final and cannot be protested or appealed.

10.7. The application for a superlicence must be made by the National Authority of the driver concerned and lodged with the U.I.M. 4 weeks before the first event the driver wants to participate in. The superlicence fee of 120 Euro will be doubled for applications received beyond the 4 weeks deadline.

The application must be accompanied by:

- The sports career of the driver, mentioning the results obtained in the qualifying events
- A U.I.M. medical document, as per appendix to these rules, giving the information required by the U.I.M. rules 205.03 / 205.04 / 205.04.1

General Recommendation for F1 superlicence holders concerning physical fitness

- Cardiovascular training (endurance training) as jogging, cycling, rowing etc. twice a week – minimum 20 minutes without any interruption. Any sports activity which has “stand still” during the activity such as soccer, tennis or any ball sport

is not suitable for the endurance training, but recommended as additional training.

- Training to build up muscles for neck and back at least twice a week.
- The immersion certificate (ref 205.05)
- A photograph
- The fees will be fixed by the Council
- A current rule book will be given to the driver along with the superlicence.

The National Authorities are responsible for the information given.

- 10.8. Superlicence application will be examined by Formula 1 Management Committee who will decide on its acceptability .
- 10.9. The superlicence will be valid for 1 year (from January 1st or from the date of issue to December 31st).
- 10.10. For renewal of superlicences, the National Authorities must, before the end of December, send the U.I.M. a list of the drivers for whom a renewal for the following year is required.
- 10.11. Each team will appoint one team manager who will be responsible for obtaining a team licence from the U.I.M. for his team, as well as ensuring that the drivers obtain their superlicences from their N.A.'s. He will also be responsible for enrolling his two boat team in the Championship and for all dealings (contractual, financial) with the U.I.M. and IDEA MARKETING.

He will also be responsible for the behaviour of his team members and his name will be mentioned on the U.I.M. Team Licence.

11. YELLOW CARDS

- 11.1. All yellow cards issued will be confirmed on a form which will be posted with the results. The driver has 60 minutes to protest to the Protest Judge. If the protest is not upheld, he can appeal. Protests and appeals must be lodged in accordance with U.I.M. rules.

Yellow cards given in other Formulae Series (**Boat GP**) will be taken into account on the yellow card listing.

11.2. Yellow cards can be given for the following reasons:

- up to two yellow cards at a time for dangerous driving.
- Red card for extremely dangerous driving

A driver who has a red card or three valid yellow cards will lose the right to take part in the following round of the World Championship (current season or following season), but his boat must participate in the race with a substitute driver. When a penalty of not racing at the next race has been satisfied, the 3 yellow cards or the red card are deleted by the application of that penalty.

11.3. All cards will remain valid for the four events of the Championship following the date on which the last card was delivered.

Participating in training, timed trials, and final race will be considered as having completed one event.

11.4. An organiser/N.A. who has given one or more yellow cards to a driver must inform U.I.M. by fax or mail within 3 days. U.I.M. will inform the next organisers/N.A.'s. of the World Championship races about the delivery of such yellow cards. When a driver has completed 4 races without yellow cards, his previous yellow card(s) will be deleted from the listing.

11.5. Yellow cards issued must be mentioned in the commissioner's report which must be distributed to all members of the relevant U.I.M. Formula commissioners panel.

11.6. Penalties, fines and yellow cards will be issued in accordance with the following list:

INFRINGEMENT FINE (IN EURO).

Boat and racing gear too late in pits	420 €
Registration after specified time	420 €
Not present at drivers meeting roll call	840 €

Not respecting dress code	135 €
Not respecting exit lane	1 yel or 200 € decision OOD/ U.I.M. Comm.
Not respecting entrance lane	1 yel or 200 € decision OOD/ U.I.M. Comm.
Radio man not at timing control	420 €
Not lining up in 20 seconds	340 €
Not in correct position on parade lap	420 €
Jumping the gun	1 lap
Destroying first buoy	1 lap and 420 €
Destroying second buoy	disq. and 420 €
Overtaking under yellow flag	1 lap
In pit during yellow flag	last position
Not in correct position under yellow flag	1 lap
Dangerous driving	1 or 2 yellow
Extremely dangerous/dirty driving	red card
Driver refusing to be weighed separately	405 €
Not using red/yellow light system in boat	420 €
Taking outside assistance	disq
Responsible for stopping the race	disq
Taking technicians on boat (not applicable to the race winner)	340 €
Noise / silencer violation	510 €
Bringing own fuel to race venue	840 €
Not respecting instructions of U.I.M. Comm / and or Officials	200 €
Not connecting fuel pump lanyard to life jacket	1000 €
Closed valve of airbag system :	1500 €
No closing safety belts during time trials, free practice and the race :	5000 €

Non respect of rules whilst overlapping : the radio man will be show a yellow card – if his boat is still not out of the way, the red card will be shown and his boat will be disqualified.

The Daily Management Committee may decide to apply a “drive-through” immediate-penalty in cases that carry a 1 lap penalty. This will be announced in the advance programme and/or the driver’s meeting ; it will be communicated to the infringing driver’s radio person by race control and the driver must compete the drive through within three laps of the announcement.

The F1 Committee has the right to fine a driver (money or suspension) for any infringement of the rules or any abusive or unsportsmanlike behaviour.

Any financial fines due will be collected by the Race Secretary and held by the Promoter. They will be added to the general prize fund which is distributed at the end of the season.

12. PROCEDURE FOR PROTEST AND APPEAL

- 12.1. A protest can be lodged in accordance with the 400 rules either by the driver or the team manager

Protests lodged by any driver or his team manager must be handled by the Protest Judge according to the rules.

The protest fee for F1 is 250 Euro.

- 12.2. An appeal must be submitted in writing (fax, mail) to the U.I.M. office for the attention of Secretary General within the specified time limit and signed by the driver or his team manager.

- 12.3. The time limit for appeal submission is 48 hours, and is counted from the moment of the communication of the protest judge’s decision to the interested parties

The 24 hours of the Sunday do not count within the 48 hours to appeal. Address, telephone, e-mail, and fax should be clearly mentioned on the appeal so that the U.I.M. can send any correspondence directly to the driver, with a copy to his N.A.

- 12.4. The driver may agree to forego an Appeal Committee meeting and for the appeal to be handled through telephone and fax, if possible, in order to minimise the costs of the appeal.
- 12.5. The Formula 1 Committee will select an Appeal Committee of 3 persons, who are neutral and independent parties; the chairman will be legally qualified.

The decision must be given within 7 days and in any case before the start of the following race.

13. SAFETY

- 13.1. Following an accident, the driver must undergo a physical examination and be passed fit by the event medical staff before being permitted continue in that racing event.

Following any accident, damaged boats must be completely rechecked and approved by the U.I.M. Commissioner before being permitted to rejoin the event.

- 13.2. Immersion tests at race venues will be carried-out by Osprey at least once during the season, starting with the first event in the calendar. Drivers may alternatively have a test in UK, by arrangement. Drivers entering the series part way through, will be accepted with their own National Authorities immersion test certificate, which will be valid only up to the time of the next immersion test organised by Osprey.

An example of the certificate and instructions of how to perform an immersion test are available at the U.I.M. Secretariat.

- 13.3. The deformable pickle forks are mandatory according to rule 503.02.
- 13.4. Reinforced cockpits are mandatory according to rule 509
- 13.5. Life jackets incorporating pants, are mandatory.

Drivers must wear a life jacket so constructed at the shoulders and the legs, that a driver can be easily pulled out of the cockpit.

Ballistic material in life jackets/ driving suits is not required.

- 13.6. The homologated delayed operation buoyancy airbag is mandatory.

The maintenance of the airbag system is the responsibility of the team.

- 13.7. U.I.M. reserves the right to introduce safety rules effective in a reasonable time following notice.

- 13.8. There will be an official rescue team contracted by the promoter for the entire season.

All local services provided by the organiser will work in conjunction with the official rescue team under the overall control of the OOD, his substitute or the medical officer.

- 13.9. It is not permitted to take technicians on the boats (financial penalty will be applied).

The winner can take his technicians for the victory parade lap only. A rescue boat will follow the winning team at close distance.

- 13.10. Seats are mandatory for F1 cockpits – for description see 509.08

- 13.11. Hans device is **mandatory** for F1 – see 509.08

14. GENERAL

- 14.1. Drivers who have not committed to participate in all races of the Formula1 World Championship, have to pay an entry fee of 670 € for each race.

All entries must be made by the team to the U.I.M.

All entry forms will include the text of the eligibility conditions and the following declaration will have to be signed by all team members « understanding that as a team member / driver I am participating in an U.I.M. Formula 1 Powerboat World Championship event which has ongoing international and sporting significance and in consideration of the acceptance of my

participation therein, I agree to be filmed, televised, photographed and otherwise recorded during the World Championship under the conditions and for the purposes now or hereafter authorised by the U.I.M. and the Formula 1 promoter.”

14.2. All drivers uniforms and boats must feature series sponsors logo and badges.

14.3. The official film crew of the Formula 1 World Championship, will be contracted by the Formula 1 promoter.

All TV, film, media and commercial rights are the property of the U.I.M.

The U.I.M. contracted promoter handles the rights on behalf of the U.I.M.

Any organisation wishing to film or to use existing film must contact the promoter.

14.4. The official Formula 1 passes are the recognised access to race events.

14.5. The official U.I.M. Formula 1 logo must be prominently displayed on both sides of all participating boats.

14.6. The U.I.M. flag shall be flown from the podium if other flags are flown

Enlarged U.I.M. Formula 1 logo has to be prominently displayed on top of the podium during prize giving or during any other public function.

Any correspondence regarding the Formula 1 must be sent to the following address:

UNION INTERNATIONAL MOTONAUTIQUE
STADE LOUIS II - ENTREE H
MC 98000 MONACO
Fax: (377) 92-05-04-60
E-mail: uim@uimpowerboating.com
Website: www.uimpowerboating.com

UIM BOAT GP WORLD CHAMPIONSHIP

1. ORGANISATION
2. RACE FORMAT
3. START
4. STOPPED RACE
5. INTERRUPTED RACE
6. RACE COURSE
7. CLASSIFICATION
8. EQUIPMENT
9. FUELS
10. SUPERLICENCES
11. YELLOW CARDS
12. PENALTIES
13. PROCEDURE FOR PROTEST AND APPEAL
14. SAFETY
15. GENERAL
16. INSURANCE

INTERPRETATION OF THE RULES - 200.02

In all cases unforeseen by the international rules, the National Authority is to adjudicate bearing in mind the spirit of the rules.

In case of conflicting rules, those rules concerning the Championship as published in the blue pages of the Circuit Rules Handbook are to prevail.

1. ORGANISATION

- 1.1. The rights of organisation of Boat GP races and championships is owned and ruled by the UIM which has granted the management of the competition to the Council

- 1.2. The Council may update the rules whenever necessary.
- 1.3. The Boat GP daily management Committee is responsible for the championship daily management. This Committee is composed of the Promoter, the UIM permanent Commissioner and the Formulae Chairman.
- 1.4. The Boat GP World Championships are governed solely by the UIM international rules for circuit racing, as published in the white and blue pages of the Circuit Rules Handbook.
- 1.5. A permanent UIM Commissioner will be appointed for the Boat GP World championship series which will be held over a minimum of three rounds.
- 1.6. Any additional rule to these specific rules must be justified by national legal requirements and notified in the advance programme
- 1.7. Other Continental Championships will be run subject to the contract between the World Championship Promoter and the UIM.

2. RACE FORMAT

- 2.1. Each Boat GP meeting counting towards the Drivers World Championship will include four separate parts to take place over a period of two consecutive days in accordance with the full UIM Circuit rules for racing, classification, protests and appeal and by the specific rules listed below.
- 2.2. In case of force majeure the meeting may take place in one day.
- 2.3. If, through Force Majeur, less than 25% of the programmed total timed trial period is run, the grid positions will be established using the current championship order.
- 2.4. The programme for the event must include:
 - 2.4.1. Free practice (minimum 1 60 minutes);
 - 2.4.2. Two mandatory official timed trials each of 45 minutes duration.

2.4.3. Two sessions of timed trials must be run except in case of “force majeure”.

LARGE ENTRY QUALIFYING SYSTEM

A maximum of 24 boats will be allowed **in any** heat or race – if water registration is less , this will be specified in the advance programme.

If more boats are entered than the maximum water capacity, they will be qualified in accordance with the following method :

On the Sunday, a fixed number of drivers will automatically qualify for the World Championship round, based on the fastest time from their two timed trials.

Those not qualified will run in a single qualification race of 12 minutes on the Sunday morning, and a certain number will be qualified from that race. There will be no other qualifying races.

This system of automatic, and raced-for, qualification will be based on the water homologation maximum and the maximum permitted number of pre-registered entries accepted by the promoter, as indicated in the following table :

<i>Maximum boats permitted in water homologation</i>	<i>24</i>	<i>20</i>	<i>18</i>	<i>16</i>
<i>Maximum permitted drivers pre-entered and accepted by organisation</i>	<i>40</i>	<i>34</i>	<i>30</i>	<i>26</i>
<i>Automatically qualified from Timed Trials 1 & 2</i>	<i>16</i>	<i>14</i>	<i>12</i>	<i>10</i>
<i>Number of drivers in qualifying races</i>	<i>24</i>	<i>20</i>	<i>18</i>	<i>16</i>
<i>Number of drivers going forward from qualifying race into the World Championship round</i>	<i>8</i>	<i>6</i>	<i>6</i>	<i>6</i>
<i>Drivers entered but failing to qualify</i>	<i>16</i>	<i>14</i>	<i>12</i>	<i>10</i>

Depending on the number of drivers entered, and the water capacity, on Saturday there will either be :

Two free practices of 60 minutes, and four timed trial periods of 45 minutes each, with the entry divided into two groups after free practice by using odd and even order of the practice results.

or

Longer single sessions of free practice and timed trials will be organised for the whole entry to participate in at the same time.

In which case the Commissioner will reserve the right at any time to prevents boats leaving the jetty to participate in timed trials depending on the amount of traffic already on the circuit.

Boats may indicate their readiness to start practice runs by parking in a designated waiting area only, and their radio man will be informed when the driver can join the circuit. Boats wishing to make a practice run cannot wait in any area other than the designated one.

2.4.4. The Boat GP race [approx. 30 minutes duration, but with a limit of 90 minutes including any re-starts]

2.5. A typical programme would be :

SATURDAY

08.00 to 11.00 Registration and scrutineering

Compulsory drivers and radio men roll-call and meeting, at race headquarters, penalised if not present

Free practice period [60 mins]

First timed trial [45 mins] Each boat entitled to only 15 laps excluding their first lap

Second timed trial [45 mins] Each boat entitled to only 15 laps excluding their first lap

Match Races, covered by separate rules

SUNDAY

Compulsory drivers and radio men roll-call and meeting, at race headquarters. Penalised if not present

Warm-up [45 mins]

Qualifying ***race***

Presentation parade lap

Race

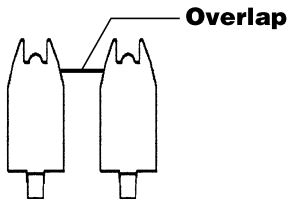
- 2.6. Detailed information about practice, timed trials, Match Races, and number of boats allowed on the circuit must be given in the advance programme
- 2.7. If a race is shortened due to force majeure, it is at the discretion of the OOD and the UIM Commissioner to alter the time table and if necessary the course layout accordingly. In the event of no qualifying times being achieved, the grid will be drawn-up in accordance with the current championship positions.
- 2.8. No entry shall be considered official until the boat has passed the scrutineering and documents controls

3. START

- 3.1. The fastest time recorded for each boat by the official timekeepers, during official timed trials, shall determine the starting positions of boats for the race or the qualifying heats if used.
- 3.2. The start shall be a dead engine jetty start as described in rule 307: Dead engine jetty start.
- 3.3. A course boat will be stationed at a convenient point for the last part of the parade lap, and will show a green flag; at which point the driver can use full revs to clear his engine prior to moving directly into his grid position on the start jetty

- 3.4. Boats should be drawn up stern first with dead engine, to a jetty, floating pontoon or beach. The start must be with the current. The distance between the starting line and the first turning buoy must be at least 300 metres.
- 3.5. When the pontoon marshal is satisfied that all drivers are ready to start, with engines stopped, he will raise a green flag and the OOD will initiate the start procedure. He instructs the starter to raise a 30 seconds board (min. 80 x 80 cm, figures 60 x 10 cm), **and when the remote operated start lights are used, blue flashing lights will be operated during the 30 second period.**
- 3.6. **After 30 seconds this board will be lowered the blue flashing lights switched off, and** the red light will be switched on. After 5-12 seconds the red lights are switched off, this is the signal to start.
- 3.7. These drivers rotating their engines (even with ignition switched off) prior to the red lights going off lose one lap.
- 3.8. This lap will be deducted at the end of the race.
- 3.9. The starter may decide if he judges it necessary, to cancel the start by showing the red flag and recommence the starting procedure.
- 3.10. The start jetty must have a minimum length of 70 metres or be equal to 4 metres multiplied by the maximum number of boats permitted to race, [whichever is the greater] and must be at least 1.5 m wide.
- 3.11. All start pontoons must be sufficiently angled (minimum 50 degrees) such that the pole position has a significant advantage towards the first turn buoy, compared with the last positions on the starting grid. The plan of the circuit (including the position of the start pontoon) must be sent to the commissioner 60 days in advance to get his approval.
- 3.12. Any boat not starting must be returned to shore by the driver's own efforts. Any crew member swimming out to a boat will cause his entry to be disqualified.

- 3.13. Drivers failing to start will wait at the dock until signalled by the pontoon marshal to fall in behind the field.
- 3.14 Before the start of the first race, each driver shall complete a lap of the course immediately after his name has been announced, so that he may be introduced to the public.
- 3.15 ***An overlap is established only when two boats are on the same course, or approximately the same course, and the cockpit of the overtaking boat, if overtaking from the inside, is even with the cockpit of the overtaken boat. When overtaking from the outside, an overlap is established when the overtaking boat has passed the overtaken boat sufficiently to change lanes without contact. The following rules shall apply to an overlap :***
- ***The lead boat has the right of way until an overlap is established.***
 - ***A safe overlap is the responsibility of the overtaking boat.***
 - ***Once an overlap is established, the overtaken boat must give the overtaking boat room to clear any course markers.***
 - ***The overtaking boat cannot force an overlap after the lead boat has begun to alter course to negotiate a turn.***
 - ***An overlap can occur from port or starboard.***



4. STOPPED RACE

- 4.1. In the event that a race is completely stopped rule 311 will apply.
- 4.2. Upon showing the red flag, drivers must return slowly under their own power to the start pontoon [or other position on the circuit notified by the UIM Commissioner during drivers' briefing].
- 4.3. Drivers receiving outside assistance at any point during the race (whether the race has been stopped for an incident or not) will be disqualified.
- 4.4. Boats that have broken down and received outside assistance by being pulled to the infield, or off the race course, will be towed to the trailer or the starting dock only during a "race stop" condition and if pick-up boats are available.
- 4.5. Repairs are allowed at any time during the race, with the exception that replacement of the crankcase and the cylinder block is prohibited. Repairs are not allowed on the race course using outside assistance.
- 4.6. Only if a boat reaches the pontoon on its own power, repairs can be carried-out either while the boat is completely in the water or on its trailer, at the sole discretion of the Pontoon Marshal. Further instructions for the repair areas may be given at drivers briefing.
- 4.7. No repairs are allowed on the start pontoon during the line-up and start or re-start preparations
- 4.8. Drivers may carry-out their own repairs on their boats in accordance with the rules. The driver's two nominated pontoon mechanics and the official crane driver do not count as outside assistance.
- 4.9. In the event of a restart a boat being repaired must be ready to take its position on the grid as directed by the Pontoon Marshal. If the boat is not ready to take its position, it must start at the end of the pontoon.

- 4.10. Hulls and motors must be sealed after scrutineering and before practice. In races with several restarts, substitution of hull or motors between the restarts or during the race is prohibited.
- 4.11. To qualify for points, a stopped race must be restarted if 70% of the total distance specified for the race has not been covered at the time the race is stopped.
- 4.12. The number of laps completed + 1 lap per restart will be deducted from the restarted race.
- 4.13. Should there be a restart, starting positions will be determined by the order of the end of the lap prior to the race being stopped.
- 4.14. All boats which are ready to race, may start in a restarted race.

5. INTERRUPTED RACE - PACE BOAT RULES

- 5.1. The Organiser may interrupt the race for any reasons of safety or emergency services access, and in which case the rescue boats and race control will show yellow flags.
- 5.2. At this signal all drivers must immediately slow down and not overtake. Any overtaking, 5 seconds after the yellow signal has been given, will be penalised by 1 lap. The leading boat, or a specially designated boat, will act as pace boat, and run at slow speed
- 5.3. When the race course is clear, only the yellow flag at race control will be continued to be shown.
- 5.4. All boats maintain their position at slow speed (approx. 4000 rpm) until race control raises the green flag. It is the responsibility of the team managers to ensure that their boat(s) are at the correct position at the restart.
- 5.5. When a rolling re-start is used the UIM Commissioner can show the green flag if the first 10 boats are in their correct positions. The position of the boats for the rolling restart will be that of their race positions on the lap before the yellow signal was given.

- 5.6. Boats which do not restart in their correct position, will be given a one lap penalty..
- 5.7. During the yellow signal situation, a boat may leave the race course but must re-join the race behind the boat running in last position.
- 5.8. In races run with the pace boat system in operation, a yellow flag will be used by race control and the rescue boats to indicate a hazard when displayed stationary, and extreme caution to be exercised by drivers when waved vigorously. No overtaking permitted during yellow flag status.

6. RACE COURSE

- 6.1. Course and safety rules shall be those of the UIM applicable on the date of the race unless specified otherwise by these rules.
- 6.2. The length of straight between any two turning buoys is to be a maximum of 850 metres. The boundaries of the water on which practice and racing are allowed to take place must be defined in the advance programme.
- 6.3. The pit area should be sufficiently large to prevent boats being craned into the water over other boats.
- 6.4. There must be a minimum of 2 cranes.

7. CLASSIFICATION

- 7.1. The boat placed first will be the one having covered the scheduled distance in the shortest time.
- 7.2. All boats will be classified by taking into account the number of complete laps they have covered and for those which have completed the same number of laps, the order in which they crossed the finish line.
- 7.3. The number of laps completed during the eventual restarts will be taken in account for the classification.

- 7.4. The points will be awarded to the first 20 classified drivers on the condition that they have completed at least 70 % of the numbers of laps covered by the winner (rounded down to the nearest whole number of laps). Boats having done 90% or more laps but do not cross the finishing line after the winner has crossed will be classified after those boats that have done the same number of laps but that also cross the finishing line after the winner.
- 7.5. The leading boat must have completed 70 % of the number of laps counting for the race.
- 7.6. If the race cannot be restarted due to “force majeure” then the points will be awarded in accordance to the following scale:
- 7.6.1. 70% or more of the total race completed full points
 - 7.6.2. less than 70% halfpoints
 - 7.6.3. less than 35% no points
- 7.7. The first six classified boats are to be weighed when coming out of the water. The weighing of the boats can not be protested. The scales will be available before the meeting commences.
- 7.8. Points will be awarded to the first 10 classified drivers in accordance with the following finishing points scale:
- | | |
|---------|-----------|
| First | 20 points |
| Second | 15 points |
| Third | 12 points |
| Fourth | 9 points |
| Fifth | 7 points |
| Sixth | 5 points |
| Seventh | 4 points |
| Eight | 3 points |
| Ninth | 2 points |
| Tenth | 1 point |
- 7.9. The overall classification for the Boat GP Drivers' World Championship shall be established by the addition of all points

obtained by each driver in each of the races counting towards the championship.

7.10. In the case of a tie, the number of first places in these races shall be considered, then the second places, etc...

7.11. Immediately after the race:

- The winner will complete a lap of honour
- Presentation of trophies to the first 3 drivers on the official podium. This presentation is only based on a provisional classification, and is subject to confirmation after scrutineering and consideration of any protests or appeals.

7.12 At the last race of the series UIM will provide 3 medals (gold, silver, bronze) to be given to the first three in the overall classification of the World Championship.

7.13. These medals do not replace the normal prizes (art. 108.01 and 322).

8. EQUIPMENT

8.1. HULL

- 8.1.1. Hull must be a catamaran type, based on twin hulls, but may be of completely free hydrodynamic and aerodynamic design.
- 8.1.2. Must conform to the general UIM rules 501; 502; 503; 504; 508; 509; 542.05 and these Boat GP rules.
- 8.1.3. Boats configured to work on the hydrofoil principal will not be eligible.
- 8.1.4. The minimum hull length is 4m80.
- 8.1.5. The minimum weight of the boat and driver as raced is 488kg
- 8.1.6. Measured in accordance with rule 542.12.

- 8.1.7. The front 305 mm. (12 inch) of the cockpit and pickles should not be made of a material which would not allow controlled deformation to reduce G forces on frontal impact (mandatory).
- 8.1.8. Each wing mirror must have minimum size of 60 sq. cm and be bolted on 2 points to assure proper mounting.
- 8.1.9. Radios in boats are mandatory and a full radio check of each boat will be part of technical scrutineering in order to guarantee that all radios are working. The radioman must be present at race control whenever his boat is on the course. In order to guarantee clear communication between the Team Managers and the UIM Commissioner, all Team Managers must speak English.
- 8.1.10. The homologated delayed operation buoyancy airbag is mandatory.

8.2. MACHINERY

- 8.2.1. Only engines qualifying to the SST120 rules will be eligible for Boat GP. Engines will be measured and checked against the SST120 data sheets.
- 8.2.2. Maximum engine displacement 2050cc
- 8.2.3. All cowling and engine graphics and colours shall be as OEM, with updating to later models allowed only as a complete OEM design.
- 8.2.4. Engine wiring can be changed to a 24-volt starting system.
- 8.2.5. Gearcases may have their outside surfaces re-profiled, however they must meet the minimum dimensional requirements of the Engine Specification sheets. The outside surfaces of the gearcases may be either painted or unpainted. If they are painted, the colour of the paint must be the same as the original factory motor colour. If the gearcases are unpainted, any surface finished is acceptable.

- 8.2.6. It is permissible to remove, or render inoperable, the engine fuel primer system and plug holes opened-up by removing the system.
- 8.2.7. A maximum of 3 damaged cylinders may be re-sleeved using sleeves from any manufacturer. Sleeve material and the machined port opening's shape and dimensions must all be as original equipment.
- 8.2.8. Reeds may be of any material or design.
- 8.2.9. Cylinder block exhaust port outlet openings may be remachined to the maximum dimensions shown in the Specification Sheet. Only machined openings are acceptable, machined in the same manner as the originals (prior to assembly) in situ hand grinding or blending of any sharp or misaligned edges is not permitted.
- 8.2.10. An Outboard motor is defined as a complete internal combustion power and propulsion unit that can be attached to a boat and which can be lifted bodily from the hull as one unit. Batteries used for ignition and starting, tachometer, throttle control and steering arrangement are excepted.
- 8.2.11. A manufacturer of outboard motors for **Boat GP** Racing is the one who filed specifications and is responsible for the original design and manufacture of the following: crankshaft, connecting rod, cylinder head, cylinder, crankcase, gears and valving arrangement, and who complied with such other requirements as provided by these rules.
- 8.2.12. Motors must be the product of a recognized manufacturer engaged in the production of outboard motors offered for sale to the general public.
- 8.2.13. There shall be no alterations of parts with the exception of spark plugs, carburettor jets, and propellers, or specific parts approved by the Technical Commission. The intention is to race the engine exactly as manufactured and without special performance boosting accessories.

- 8.2.14. No special parts or interchange of parts will be allowed, unless approved by the Commission.
- 8.2.15. The trim tab, thrust and locking washers may be altered or removed from the lower unit to permit the use of any propeller, thrust washer or nut.
- 8.2.16. Cylinder head and power head attaching studs may be substituted for bolts.
- 8.2.17. Electronic engine RPM limiting devices may be over-ridden or rendered inoperative .
- 8.2.18. Engines (Outboard) as referred to in these rules shall be defined as outboard models which:

Are manufactured in annual or model year quantities of more than 25 units.

Are not necessarily intended for sale and to be used by the general non-racing public.

Are not necessarily advertised through the same media, in the same publications and given the same prominence as the manufacturer's other engines.

Are not necessarily distributed through the manufacturer's normal channels.

Accessory gearcase, as submitted by the manufacturer, may be used in these engines provided that they are built in a total quantity of no less than 25.

- 8.2.19. Exhaust stacks not contained in the driveshaft housing will not be permitted.
- 8.2.20. Engines may only be run in the configuration that they were homologated in by the Technical Committee.

8.3. REPLACEMENT PARTS

- 8.3.1. Only such parts shall be permitted as are standard production OEM parts used on the motor as the purchaser may obtain it

from the dealer as a stock item or electrical or electronic products as approved by the Technical Commission, and published and available to the general public through dealers. These products cannot provide any performance advantage over original OEM parts.

- 8.3.2. No replacement part for current production motors shall be considered standard production until this part appears on current production motors which can be purchased from the dealers.
- 8.3.3. Any replacement part for motors no longer in production shall be approved only after the part being replaced is no longer available for the motor model from the manufacturer or is specifically approved by the Commission.
- 8.3.4. Final approval of non-OEM electrical or electronic parts approved for use in F-2 racing shall not be given until all provisions of the rule have been complied with, and until such time as the manufacturers have built at least 250 units of the part submitted for homologation.
- 8.3.5. For non-OEM electrical or electronic parts, the manufacturer must follow the same rules as specified for motor manufacturer registration with respect to submission of specifications to the Commission and the time period for which the parts are approved.

8.4. MOTOR MODIFICATIONS

- 8.4.1. There shall be no blueprinting or modifications whatsoever to the motor as furnished by the manufacturer other than:
- 8.4.2. Any carburettor jets, spark plugs or propellers may be used provided other parts are not altered to accommodate them with the exception of the trim tab.
- 8.4.3. Only such parts shall be permitted as are used by the motor manufacturer on the particular model as built on the production line. Modifications covered by a manufacturer's service or engineering bulletins are legal seven days after the date of any

such bulletin and approval by the Technical Commission, provided all direct purchasers of affected engines are notified by mail as well as all inspectors, followed by printing in the UIM Bulletin at the next opportunity. In the event the manufacturer is unwilling or unable to meet these criteria, modifications covered in such bulletins will be legal after printing in the Propeller.

- 8.4.4. Oversize pistons furnished by the manufacturer of an outboard motor may be used in the model for which they are furnished. The bore of the engine may be enlarged by the amount that the oversize pistons differ from the standard size pistons. An additional total 0.005 inch enlargement of the bore will be allowed for bore enlargement due to wear or re-matching errors, or measuring tolerance. Under no circumstances may the bore be enlarged more than 0.035 inch over the original dimension. It is not permissible to chromium plate cylinder walls unless the motor manufacturer uses chrome plating of cylinder walls as standard production procedure on that particular model.
- 8.4.5. Tachometer, fuel and water pressure, and temperature (including EGT) gauges may be installed if desired.
- 8.4.6. The attachment of a steering bar or its equivalent may be accomplished at any point or points on an engine. Any cowlings or plates removed may be cut and, must be replaced. Any openings created by this operation must be sealed to prevent the engines from pulling in air. Studs or bolts may be exchanged to secure steering bar.
- 8.4.7. Throttle return springs will be allowed on engines, however no modifications to the engine will be permitted to install same; no drilling of holes, etc. Mounting links may be attached with present fasteners.
- 8.4.8. Isolation Mounts: Only those supplied by the engine manufacturer as original equipment or through the service group are acceptable.
- 8.4.9. Any adjustments of mixture control, throttle and shift linkage will be allowed.

- 8.4.10. Any mechanical adjustments of spark advance will be allowed provided no modifications are made either to the spark advance system or the engine as manufactured.
- 8.4.11. Safety tilt switches and safety starting switches may be disconnected or by-passed.
- 8.4.12. Fuel pumps, filters, heat exchangers or auxiliary tanks may be installed in the boat provided the use of same entails no modification to the engine and fuel lines so used to connect to the fuel connector provided with the engine.
- 8.4.13. The attachment of a power trim system may be accomplished at any point or points on an engine. Only those parts of the production motor interfering with the attachment and function of the trim system may be altered or removed.
- 8.4.14. Bead blasting will be allowed at the block and crankcase interface only. No other internal parts will be allowed to be bead blasted.

9. FUEL

- 9.1. General fuel rules in 508 will apply
- 9.2. Fuel will be supplied by the organiser in conformity with rule 508.01

10. SUPERLICENCES

- 10.1. The nationality of a driver is the nationality of the N.A. which issued the international racing licence to the driver. Medical and immersion tests can be done in other countries if the issuing N.A. agrees. The flag is the one of the issuing N.A. of the international licence as above. The request for the UIM Superlicence must come from the N.A. which issues the international racing licence.
- 10.2. Drivers must hold a UIM super licence before entry will be accepted.

- 10.3. Any new driver must be vetted in practice by the UIM Commissioner and the drivers delegate to the UIM.
- 10.4. The super licence is required for Boat GP 2 together with the NA international Licence. No other membership is required.
- 10.5. To obtain a superlicence, a driver must have participated in 6 circuit races in any catamaran 0 or S class or hydrocat min 500 cc. Definition of a race during a race weekend: even if several heats take place, the race weekend with all heats or races counts as one race only. The National Authorities are fully liable for the sports c.v. that they submit for their drivers. The full race results of each race must be supplied since the drivers must have done at least 6 races with 70 % of the total distance of each race event.
- 10.6. The superlicence is provisional for the first 3 races and can be withdrawn at any time during the first 3 races by a committee composed by the OOD, the UIM Commissioner and the Drivers representative. When a super licence is withdrawn, next application can only take place for the next season.
- 10.7. The application for a super licence must be made by the National Authority of the driver concerned and lodged with the UIM 4 weeks before the first event the driver wants to participate in. The superlicence fee of 120 Euro. will be doubled for applications received beyond the 4 weeks deadline.
- 10.8. The application must be accompanied by:
 - The sports career of the driver, mentioning the results obtained in the qualifying events
 - A U.I.M. medical document, as per appendix to these rules, giving the information required by the UIM rules 205.03/ 205.04 / 205.04.1

General Recommendation for Boat GP superlicence holders concerning physical fitness

- Cardiovascular training (endurance training) as jogging, cycling, rowing etc. twice a week – minimum 20 minutes

without any interruption. Any sports activity which has “stand still” during the activity such as soccer, tennis or any ball sport is not suitable for the endurance training, but recommended as additional training.

- Training to build up muscles for neck and back at least twice a week.
- The immersion certificate (ref 205.05)
- A photo
- The fees will be fixed by the General Assembly
- A current rule book will be given to the driver along with the super licence.

- 10.9. The National Authorities are responsible for the information given.
- 10.10. The application will be examined by Formulae Committee who will decide on its acceptability.
- 10.11. The decision of the Formulae Committee on all matters concerning superlicences is final and cannot be protested or appealed.
- 10.12. The super licence is valid for 1 year (from January 1st or from the date of issue to December 31st).
- 10.13. For renewal of the super licences, the National Authorities must, before the end of December, send the UIM a list of the drivers for whom a renewal for the following year is required.

11. YELLOW CARDS

Yellow cards given in other Formulae Series (F1) will be taken into account on the yellow card listing.

- 11.1. Yellow cards can only be given for the behaviour on the water.
- 11.2. A yellow card will be confirmed on a form which will be posted with the results. The pilot has 60 minutes to protest to the jury.

- 11.3. If the protest is not upheld, he can appeal. Protests and appeals must be lodged according to UIM rules.
- 11.4. A yellow card can be given by:
 - * the Officer of the Day
 - * the UIM Commissioner
- 11.5. The yellow card can be given according to the following:
 - 11.5.1. Up to two yellow cards at a time for dangerous driving.
 - 11.5.2. Red card for extremely dangerous driving and unsportsmanlike behaviour
- 11.6. A driver who has been given three yellow cards or a red card loses right to take part in the next event of the World Championship or Continental Championship (current season or following season). When a penalty of not racing at the next race has been satisfied, the 3 yellow cards or the red card are deleted by the application of that penalty.
- 11.7. All cards will remain valid for the four following events of the Championship as from the date on which the last card was delivered.
- 11.8. Participating in training, timed trials and final race will be considered as having completed one event.
- 11.9. An organiser/N.A. who has given one or more yellow cards to a driver must inform UIM by telefax or mail within 3 days. UIM will inform the next organiser/N.A. of a World Championship race or the Continental Championship about the issue of such yellow cards. When a driver has completed 4 races without yellow cards, his previous yellow card(s) will be deleted from the listing.
- 11.10. Yellow cards issued must be mentioned in the commissioner's report which must be distributed to all members of the relevant UIM Formula commissioners panel.
- 11.11. Temporary suspension: see article 406.05

12. PENALTIES

12.1. Penalties will be issued to drivers in accordance with the following list :

INFRINGEMENT	FINE (IN Euro).
12.2. Not present at drivers meeting roll call	200
12.3. Not under crane when called forward	150
12.4. Not respecting exit lane	100
12.5. Not respecting entrance lane	100
12.6. Radio man not at timing control	200
12.7. Not lining up promptly on jetty	150
12.8. Not in correct position on parade lap	100
12.9. Destroying first buoy	1 lap and 200
12.10. Destroying second buoy	Disq. and 200
12.11. Overtaking under yellow flag	1 lap
12.12. In visiting pit during yellow flag	last position
12.13. Not in correct position under yellow flag/re-start	1 lap
12.14. Taking technicians on boat on the course (excl. the race winner)	200
12.15. Bringing own racing fuel to race venue	300
12.16. Not respecting instructions of UIM Comm / and or Officials	150
12.17. Closed valve of airbag system	1500
Not closing safety belts during time trials, free practice and the race	5000
Not connecting fuel pump lanyard to life jacket	1000
12.18. The Formula Committee has the right to fine a driver (money or suspension) for any infringement of the rules or any abusive or unsportsmanlike behaviour.	
12.19. The Daily Management Committee may decide to apply a "drive- through" immediate-penalty in cases that carry a 1 lap penalty. This will be announced in the advance programme and/or the	

driver's briefing ; it will be communicated to the infringing driver's radio person by race control and the driver must complete the drive through within three laps of the announcement.

13. PROCEDURE FOR APPEAL

- 13.1. Appeals will be handled according to the 400 Jurisdiction rules.
- 13.2. The delay to appeal is counted from the moment of the communication of the Jury decision to the interested parties and it is 48 hours.
- 13.3. The 24 hours of the Sunday do not count within the 48 hours to appeal. Address, telephone and fax should be clearly mentioned on the appeal so that the UIM can send any correspondence directly to the driver with copy to his N.A. The driver may agree to forego an Appeal Committee meeting and for the appeal to be handled through telephone and fax - if possible - in order to minimize the costs of the appeal.

14. SAFETY

- 14.1. Drivers must wear a floatation vest incorporating pants, and so constructed at the shoulders and the legs that a driver can be easily pulled of the cockpit.
- 14.2. The homologated delayed operation buoyancy airbag is mandatory. The maintenance of the airbag system is the responsibility of the team.
- 14.3. Drivers taking technicians on their boats will be penalised.
- 14.4. The winner can take his technicians for the victory parade lap only. A rescue boat will follow the winning team at close distance.
- 14.5. The winner can take his technicians for the victory parade lap only. A rescue boat will follow the winning team at close distance.
- 14.6.** Hans device is recommended for BOAT GP see 509.08

- 14.7. Seats are mandatory for F1 cockpits and very strongly recommended for all other cockpit classes.

15. GENERAL

- 15.1. The official UIM sticker is mandatory and must be visibly displayed on all participating boats.
- 15.2. All TV, film, media and commercial rights are the property of the UIM.
- 15.3. The UIM contracted promoter handles the rights on behalf of the UIM.
- 15.4. Any organisation wishing to film or to use existing film must contact the promoter.

16. INSURANCE

- 16.1. The advance programme must contain following information:
- 16.2. Terms and conditions of a mandatory insurance for personal and material damage on third parties, competitors included
- 16.3. The minimum limit of coverage required
- 16.4. The type of risks to be covered by the insurance
- 16.5. Whether insurance from other National Authorities are accepted and if they are, in which language they must be drawn up
- 16.6. The amount of insurance fees to be paid on the spot.

17. FINANCIAL CONDITIONS OF A BOAT GP WORLD EVENT

- 17.1. To confirm a calendar request we ask you to transfer to our bank account BEFORE FEBRUARY 28, the following amounts:
760 Euro: calendar fee

- 17.2. Prize fund and travel costs will be furnished by the promoter and agreed directly with the drivers
- 17.3. Any correspondence regarding Boat GP must be sent to the following address:

UNION INTERNATIONALE MOTONAUTIQUE

STADE LOUIS II - ENTREE H

MC 98000 MONACO

Fax: (377) 92-05-04-60

E-mail: uim@uimpowerboating.com

Website: www.uimpowerboating.com

U.I.M. FORMULA 3 WORLD CHAMPIONSHIP

**THE FORMULA 3 WORLD CHAMPIONSHIP TITLE
IS SUSPENDED UNTIL A VIABLE FORMULA
AND ANNUAL RACE FORMAT IS ESTABLISHED**

U.I.M. FORMULA 500 **WORLD CHAMPIONSHIP**

«Dieter König Memorial»

1. ORGANISATION
2. RACE FORMAT
3. CLASSIFICATION
4. STOPPED RACE
5. YELLOW CARDS
6. COMMITTEE OF APPEAL
7. SAFETY
8. GENERAL

INTERPRETATION OF THE RULES - 200.02

In all cases unforeseen by the international rules, the National Authority is to judge bearing in mind the spirit of the rules.

In case of conflicting rules, those rules concerning the series are to prevail.

1. ORGANISATION

The organisation of F500 races and championships is owned and ruled by the U.I.M.

The Council may update the rules whenever necessary, only if the Formula is promoted by a contracted promoter.

Meetings in Formula 500 are reserved to boats of the O series, equipped with 351 cc to 500 cc outboard engines.

The championship is open to all drivers (their number per National Authority being unlimited) who will produce evidence that they have participated satisfactory in at least 5 international or national races in the O series.

The official U.I.M. sticker is mandatory and must be affixed on all participating boats.

2. RACE FORMAT

The Championship includes a minimum of 4 and a maximum of 6 races organised in different countries, with a minimum interval of two weeks between races if possible. The U.I.M. reserves the right to allocate the Championship even if the minimum number of races are not held or awarded points.

Applications to hold a F500 event will be allocated in the normal way (rule 108.01, but using the English names of the country applying) except that one National Authority can be allocated two events provided the maximum number has not been requested by other National Authorities.

If 6 races are run with points allocated, the 5 best results count for the final classification.

It is recommended that all races use a jetty start (rule 307).

CIRCUIT:

The length of the lap will be included between 1500 and 2000 meters;

- The length of a qualifying heat will be included between 8 and 12 km;
- The number of boats authorised on the circuit must be announced in the advance-programme.

RACE:

Each race will include 3 final heats.

If the number of drivers present exceeds the number admissible on the circuit, qualifying heats will take place.

The composition of qualifying heats will be determined by the classification of official timed trials: drivers with an odd position (1, 3, 5, etc...) will be put together in one series, drivers with an even position (2, 4, 6, etc...) in the other series. Drivers who have not participated in trials will be split between the qualifying heats.

Each series will run a selection heat and certain drivers a supplementary heat.

Example based on the presence of 30 boats on a circuit authorised for 14 boats:

3 series of 10 boats will be formed:

At the end of the selection races for these series, the 4 first of each series will be selected ($3 \times 4 = 12$).

The supplementary heat will be run by the 14 non selected drivers with the greatest point totals, the two first of this heat will be selected to participate in the final ($1 \times 2 = 2$) giving a total for the final of $12 + 2 = 14$ drivers.

If necessary, after all the qualifying races, the organisers reserve the right to make up the number of boats in the final races to the maximum permitted from the repechage heat, when any qualified driver indicates that they cannot participate in the final heats. This cannot be done after the first heat.

An interval of at least one hour will be mandatory between final heats and between the last qualifying heat and the first final heat.

If the qualifying heats are of a different number of laps-positions on the jetty for the final races are from lap times - if not available then the positions in which they finish their heat will be intermingled - the first being the winner of the longest qualifying heat.

3. CLASSIFICATION

The classification of the race will be drawn-up by adding up each driver's points in every final heat (scale referred to in article 318.01).

In order to obtain a general classification of the European Championship, the first drivers classified in races taken into account in a championship will be given points as follows:

1st: 20 points	6th: 10 points	11th: 5 points
2nd: 17 points	7th: 9 points	12th: 4 points

3rd: 15 points	8th: 8 points	13th: 3 points
4th: 13 points	9th: 7 points	14th: 2 points
5th: 11 points	10th: 6 points	15th: 1 point

The winner of the Championship will be the driver who will have accumulated the highest number of points.

In case of a tie, the number of first positions, second positions etc. will be successively taken into consideration.

In case of a new tie, all U.I.M. points collected in all races counting for the Championship will be added up (scale referred to in article 318.01) applied to the general classification of each event.

At the last race of the series U.I.M. will provide three medals (gold, silver, bronze) to be given to the three first classified in the general classification.

4. STOPPED RACE

A stopped race is a race which has been interrupted by the Officer of the day after the start. Stopping the race is decided by the Officer of the Day for reason of force majeure of which he is the sole judge. A race must be stopped when any body is in the water as a result of an accident (blow over, roll, submarining, collision, etc...) as the continuation of the race constitutes a danger to those involved.

A stopped race may only be repeated twice.

- a) A stopped race must be repeated twice if necessary
- b) When the leading boat has completed less than 70 % of the required laps the race shall be re-started.
- c) First restart, full number of laps, refuelling is allowed.
- d) Only drivers scored on the lap before the stoppage, can restart.
- e) If a first re-started race is also stopped before 70 % of the required laps have been completed by the leading boat, the race shall have a second start.

f) A second restarted race does not allow refuelling.

g) Laps for a second restarted race shall be:

Add 1 lap to the full number of laps completed before the stoppage
- take these away from original number of laps - new race distance.

Example: restarted race stopped on lap 4 in an 8 lap race: 3 laps completed + 1.

8 laps - 4 = 4 laps for second restarted race

h) If a second restarted race is also stopped: results - up to 35 % of required laps 50 % points. 35 % and over = full points.

i) Any penalty incurred in any race is carried forward to any restart of that race.

If two repeated races are provided for in the advance programme, the first one must be a complete race (total number of foreseen laps) and the second one according to the following format.

If only one repeated race instructions will be given in writing to the drivers during a special drivers meeting.

No refuelling is allowed between a stopped race and a repeated race, except if the repeated race is a complete heat.

Only the drivers scored at the lap before the stop, are allowed to restart.

Points will be awarded according to the following system:

up to 35% of the distance	1/2 points
above 35% of the distance	total points

5. YELLOW CARDS

Yellow cards can only be given for the behaviour on the water. A yellow card will be confirmed on a form which will be posted with the results. The pilot has 60 minutes to protest to the jury. If the protest is not accepted, he can appeal. Protests and appeals can be lodged according to U.I.M. rules.

A yellow card can be given by:

- the Officer of the Day
- the U.I.M. Commissioner

The yellow card can be given according to the following:

- up to two yellow cards at a time for dangerous driving.
- red card for extremely dangerous driving

A driver who has been given three yellow cards or a red card loses his right to take part in the following competition of the World Championship (current season or following season).

All cards remain valid for the four following competitions of the Championship as from the date on which the last card was delivered. When the penalty of not racing at the next race has been applied, the 3 yellow cards are deleted through the penalty.

Timed trials, qualification race and final race count as one event.

An organiser/N.A. who has given one or more yellow cards to a driver must inform U.I.M. by telefax or mail within 3 days. U.I.M. will inform the next organiser/N.A. of a World Championship race or the Continental Championship about the delivery of such yellow cards. When a driver has done 4 races without yellow cards, his previous card(s) will be deleted from the listing.

Given yellow cards must be mentioned in the commissioner's report which must be distributed to all members of the relevant U.I.M. Formula commissioners panel.

6. COMMITTEE OF APPEAL

Appeals will be handled according to the 400 Jurisdiction rules.

The delay to appeal is counted from the moment of the communication of the Jury decision to the interested parties and it is 48 hours.

The 24 hours of the Sunday do not count within the 48 hours to appeal.

Address, telephone and fax should be clearly mentioned on the appeal so that the U.I.M. can send any correspondence directly to the driver with copy to his N.A. The driver may agree on having no Appeal Committee meeting but to handle the appeal through telephone and fax - if possible - in order to limit the costs of the appeal.

7. SAFETY

Races in Formula 500 are reserved only for boats conforming to the following specifications:

Minimum weight: 180 kg Minimum length: 3.85m Cockpit mandatory

Minimum weight mentioned above is the weight of the complete rig weighed directly after the race, without driver, residual water but including residual fuel.

Minimum weight with driver: 260 kg

Reinforced cockpits are mandatory according to rule 509 for F500.

Before racing a craft with restraint system, all drivers must have passed an immersion test in a restraint system to ensure that they can exit a reinforced compartment successfully.

The immersion certificate should be provided by the National Authority of the driver and includes an immersion test in a simulated reinforced cockpit. The immersion test and the certificate must be renewed every 14 months.

A model for the certificate and instructions of how to perform an immersion test are available at the U.I.M. Secretariat.

All catamaran type hulls must have the deformable pickle forks according to rule 503.02.

Drivers must wear of flotation vest so constructed at the shoulders that a driver can be easily pulled of the cockpit.

Seats are mandatory for F1 cockpits and highly recommended for all other cockpit classes. See 509.08

8. GENERAL

FINANCIAL CONDITIONS

To confirm a calendar request, the National Authority should transfer to U.I.M. bank Account before February 28 the amount of 8495 E, to be distributed as follows

3455 € prize money:

Distributed by the organiser to the drivers at the end of the race, before they leave the racing place (after the end of the protest time).

1st:	740 €
2nd:	485 €
3rd:	405 €
4th:	370 €
5th:	340 €
6th:	300 €
7th:	260 €
8th:	220 €
9th:	185 €
10th:	150 €

5040 € travelling money:

Travelling money for foreign participants is to be divided equally between the registered competing foreign drivers to a maximum of 340 €. Drivers who fail to send in their registered entry forms in due time, ***according to the advance programme/regulations***, may lose all their travel money.

All prize and travel money must be paid in Euro. Organisers unable/unwilling to pay in Euro will have to pay an increase of 10 % on all the amounts due to the drivers.

1310 € year end prize money:

To be deposited at U.I.M. BEFORE the end of December of the year preceding the organisation, for the prize money of the general classification of the Championship.

The year end prize money will be paid by U.I.M. after the last race, according to the general classification of the championship and after examining all possible protests: 10 € for each point accumulated by each driver in the general classification.

All TV, film, media and commercial rights are the property of the U.I.M.

If the U.I.M. has a contracted promoter, he handles the rights on behalf of the U.I.M.

Any organisation wishing to film or to use existing film must contact the U.I.M. or the promoter.

Any correspondence regarding Formula 500 must be sent to the following address:

UNION INTERNATIONALE MOTONAUTIQUE
STADE LOUIS II - ENTREE H
MC 98000 MONACO
Fax: (377) 92-05-04-60
E-mail: uim@uimpowerboating.com
Website: www.uimpowerboating.com

U.I.M. FORMULA 4 WORLD CHAMPIONSHIP

1. ORGANISATION
2. RACE FORMAT
3. START
4. STOPPED RACE
5. RACE COURSE
6. CLASSIFICATION
5. EQUIPMENT
6. ENTRIES
7. YELLOW CARDS
8. COMMITTEE OF APPEAL
9. SAFETY
10. GENERAL

INTERPRETATION OF THE RULES - 200.02

In all cases unforeseen by the international rules, the National Authority is to judge bearing in mind the spirit of the rules.

In case of conflicting rules, those rules concerning the series are to prevail.

1. ORGANISATION

The organisation of F4 races and championships is owned and ruled by the U.I.M.

The Council may update the rules whenever necessary, if the Formula is promoted by a contracted promoter.

Formula 4 races are reserved for boats of the S.850 class, governed by the rules 540.

The F4 World Championship is open to all drivers (their number is unlimited for each National Authority).

The Championship will be run in a maximum of 5 meetings organised in different countries, with a minimum of two weeks between races.

Provided that the number of applications is not sufficient, one single National Authority may be allowed to organise two meetings.

2. RACE FORMAT

It is recommended that each meeting of the F4 World Championship be composed of 4 parts that should take place within a period of two consecutive days, in conformity with U.I.M. rules on circuit races, further completed by the specific rules set hereafter.

The various parts of the meeting are as follows:

1) free practice (minimum 2 x 1 hour);

2) Mandatory timed trials:

Timed trials should cover 2 or 3 laps for each boat in each trial series;

2 series must be organised, except in a case of force majeure; All data concerning the trials and the number of authorised boats must be printed in the advance-programme.

Should the number of drivers at the race exceed the number of boats allowed on the circuit, a qualifying race will have to be organised. The composition of the qualifying series will be determined by the results obtained at the official timed trials; drivers in an odd-numbered position (1, 3, 5...) will be in one series, those in an even-numbered position (2, 4, 6...) will be in another series.

The drivers who have not taken part in trials will be distributed among the two series.

For each series there will be a selection race and some drivers will run a second chance race.

Example based on 30 boats on a circuit planned for 20 boats:

There will be two series of 15 boats.

At the end of the selection races for these series, the first eight boats in each series will be selected ($2 \times 8 = 16$).

The second chance heat will be run by 14 unselected drivers; the first four in this boat will be allowed to take part in the final race ($1 \times 4 = 4$).

That is to say total number of $16 + 4 = 20$ drivers for the final race.

A minimum one-hour interval is required between final heats as well as between the last qualifying heat and the final heat.

Championship race - from 50 to 60 km.

The organiser is required to mention in the advance-programme the number of laps which is determined by the length of his circuit.

A U.I.M. Commissioner is required at each race.

No other class of boats is allowed to take part in Formula 4 Championship.

The maximum length of a straight line between two turning buoys is 850 meters.

The shortest time recorded by official timekeepers for each boat will determine the position of boats on the starting grid. Should a boat starting a race be different from the boat that was used for trials, it will be put in the last position on the starting grid.

If a race is shortened due to force majeure, it is at the discretion on the OOD and the U.I.M. Commissioner to alter the time table and if necessary the course accordingly. In the event of no qualifying times being posted the grid will be drawn on current championship positions.

3. START

The start will be dead engine start, as follows:

Dead engine start

Boats will be drawn up stern first, engine dead, to a jetty, a floating stage or beach.

The distance between the starting line and the first turning buoy must be at least 300 meters.

When the pontoon marshal is satisfied that all drivers are ready to start, with engines stopped, he raises a green flag and the OOD starts the start procedure. He instructs the starter to raise a 30 seconds board (min. 80 x 80 cm, figures 60 x 10 cm).

30 seconds after this board is raised the red light is switched on.

After 5-12 seconds the red lights are switched off, this is the signal to start. These drivers rotating their engines (with ignition switched off) prior to the red lights going off lose one lap. This lap will be deducted at the end of the race.

The starter may decide if he judges it necessary, to cancel the start by presenting the red flag and recommence the starting procedure.

Before the start of the first race of each meeting of the Championship, when his name is called, each driver will lap the circuit to make it possible for the organiser to introduce him to the public.

4. STOPPED RACE

In the event of the race being stopped boats will return to the starting pontoon, under their own power.

Drivers taking outside assistance at any point during the race (whether the race has been stopped for an incident or not) will be disqualified.

Boats that have broken down and pulled to the infield or off the race course will be towed to the trailer or the starting dock only during a «race stop» condition and if pick-up boats are available.

Only if the boat reaches the pontoon on its own power, repairs can be made on the rear side of the start pontoon or on the launching pontoon, while the boat is completely in the water or on its trailer. Further instructions for the repair area may be given at drivers briefing.

In case of a restart the boat being repaired must be ready to take its position on the grid at the instruction of the pontoon marshal. If the boat is not ready to take its position, it must start at the end of the pontoon.

Hulls and motors have to be sealed after scrutineering and before practice. In races with several restarts, substitution of hull or motors between the restarts or during the race is prohibited.

To qualify for points, a stopped race must be restarted if 70% of the total distance specified for the race has not been covered at the time the race is stopped.

The number of laps completed by the leading boat + 1 lap per restart will be deducted from the restarted race.

Should there be a restart, starting positions will be determined by the order of the end of the lap prior to the race being stopped.

All boats which are ready to race, may start in a restarted race. After the finish of the race, the race organiser might find out who has caused the stoppage of the race and the driver or drivers responsible for the stoppage, will not score.

5. RACE COURSE

Water registration - see rule 106.02

Course and safety rules shall be those of the U.I.M. applicable on the date of the race unless specified otherwise by these rules.

The length of straight between any two turning buoys is to be a maximum of 850 metres. The boundaries of the water on which practice and racing are allowed to take place must be defined in the advance program.

The pit area should be sufficiently large to prevent boats being craned into the water over other boats.

There must be a minimum of 2 cranes.

Every turn on the race course must have two turn buoys.

If a driver

- touches the first turn buoy in a turn: no penalty
- damages or destroys the first buoy: one lap penalty
- touches the second buoy: no penalty
- damages, destroys or dislodges the second buoy: disqualification

The penalty lap will be deducted at the end of the race.

When passing a mark and an overlap has been established, both boats must maintain their lane, i.e. they must maintain their distance from the mark until the mark has been rounded.

6. CLASSIFICATION

All boats will be classified taking into account the number of complete laps they have covered and for those which have completed the same number of laps, the order in which they crossed the arrival line.

The number of laps completed during the eventual restarts will be taken in account for the classification.

The points will be granted to the first 15 classified drivers on the condition that they have completed 90% of the number of laps covered by the winner (rounded down to the nearest whole number of laps).

The leading boat must have completed 70% of the number of laps counting for the race.

In the event of the race being stopped, boats will return to the start pontoon.

Drivers taking outside assistance at any point during the race (whether the race has been stopped for an incident or not) will be disqualified.

Boats that have broken down and pulled to the infield or off the race course will be towed to the trailer on the starting dock only during a «race stop» condition and if pick-up boats are available.

Only if the boat reaches the pontoon on its own power, repairs can be made on the rear side of the start pontoon or on the launching pontoon,

while the boat is completely in the water or on its trailer. Further instructions for the repair area may be given at drivers briefing.

In case of a restart the boat being repaired must be ready to take its position on the grid at the instruction of the pontoon marshal. If the boat is not ready to take its position, it must restart at the end of the pontoon.

Only those drivers scored at the end of the lap before the stop are allowed to restart.

In case of a re-start, the positions on the grid are determined by the classification made at the end of the lap preceding the race stoppage.

If there is no re-start, the classification is the one established at the end of the lap preceding the stoppage of the race. The driver(s) responsible for the race stoppage are not classified.

Points will be granted according to the following schedule:

70 % or more of the total race total points

less than 70 % half of the points

less than 35 % no points

The points awarded will be as follows:

1st: 20 points	6th: 10 points	11th: 5 points
2nd: 17 points	7th: 9 points	12th: 4 points
3rd: 15 points	8th: 8 points	13th: 3 points
4th: 13 points	9th: 7 points	14th: 2 points
5th: 11 points	10th: 6 points	15th: 1 point

The general classification of the F4 World Championship will be established by adding-up the points collected in each race of the Championship. In case of a tie, the number of first places and then 2nd places, 3rd, etc... will be taken into account. In case of a last tie, all U.I.M. points (schedule 318.01) collected during all the races of the Championship will be added-up and will decide which of the drivers is the winner.

At the last race of the series U.I.M. will provide three medals (gold, silver, bronze) to be given to the three first classified in the general classification.

Immediately after the race, the first three in the meeting will be introduced to the public and to the media on the official podium. This introduction is only a provisional classification subject to modification after the developments of the race and the protests, if any, have been examined.

The first six classified boats are weighed with scales when coming out of the water. The weighing of the boats can not be protested. The scales will be available before the meeting commences.

7. EQUIPMENT

ENGINES

F4 driver's World Championship is reserved to boats equipped with one single naturally aspirated outboard engine complying with the requirements contained in rules 540 regarding class S.850.

FUEL

- a. General fuel rules 508 will apply
- b. Fuel will be supplied by the organiser in conformity with rule 508.01
- c. Unleaded sports engine fuel in rule 508.02 will apply.

HULL

All boats must be equipped with an isolation switch placed as close as possible to the positive pole of the battery(ies) (negative pole if the earthing is done via the positive pole). The handle of this switch must be easily reached on the deck or any other surface of the boat (preferably on the portside, 1 to 3 meters from the stern). The switch should be indicated by a red arrow and clearly show the ON/OFF positions.

The weight of the boat, driver and personal equipment included: 330 kg minimum (250 kg minimum for the boat plus 80 kg for the driver).

The minimum length is 3.90 m measured along the sponson.

Reinforced cockpit is mandatory.

No entry shall be considered official before the boat has passed the scrutineering and the document controls.

8. ENTRIES

Each National Authority is fully responsible for its driver's qualification. Registration forms will have to bear the authorisation to start delivered by the National Authority of the competitor (U.I.M. 203.01 - 1st paragraph).

All holders of an international licence will be allowed to take part in races.

9. YELLOW CARDS

Yellow cards can only be given for the behaviour on the water. A yellow card will be confirmed on a form which will be posted with the results. The pilot has 60 minutes to protest to the jury. If the protest is not accepted, he can appeal. Protests and appeals can be lodged according to U.I.M. rules.

A yellow card can be given by:

- the Officer of the Day
- the U.I.M. Commissioner

The yellow card can be given according to the following:

- up to two yellow cards at a time for dangerous driving.
- red card for extremely dangerous driving

A driver who has been given three yellow cards or a red card loses his right to take part in the following competition of the World Championship or Continental Championship (current season or following season). All cards remain valid for the four following competitions of the Championship as from the date on which the last card was delivered.

When the penalty of not racing at the next race has been applied, the 3 yellow cards are deleted through the penalty.

Participating in training, timed trials and final race will be considered as having done one event.

An organiser/N.A. who has given one or more yellow cards to a driver must inform U.I.M. by telefax or mail within 3 days. U.I.M. will inform the next organiser/N.A. of a World Championship race or the Continental Championship about the delivery of such yellow cards. When a driver has done 4 races without yellow cards, his previous card(s) will be deleted from the listing.

Given yellow cards must be mentioned in the commissioner's report which must be distributed to all members of the relevant U.I.M. Formula commissioners panel.

Temporary suspension: see article 406.05

10. COMMITTEE OF APPEAL

Appeals will be handled according to the 400 Jurisdiction rules.

The delay to appeal is counted from the moment of the communication of the Jury decision to the interested parties and it is 48 hours.

The 24 hours of the Sunday do not count within the 48 hours to appeal.

Address, telephone and fax should be clearly mentioned on the appeal so that the U.I.M. can send any correspondence directly to the driver with copy to his N.A. The driver may agree on having no Appeal Committee meeting but to handle the appeal through telephone and fax - if possible - in order to limit the costs of the appeal.

11. SAFETY

Reinforced cockpits are mandatory according to rule 509

Before racing a craft with restraint system, all drivers must have passed an immersion test in a restraint system to ensure that they can exit a reinforced compartment successfully.

The immersion certificate should be provided by the National Authority of the driver and includes an immersion test in a simulated reinforced cockpit. The immersion test and the certificate must be renewed every 14 months.

A model for the certificate and instructions of how to perform an immersion test are available at the U.I.M. Secretariat.

The deformable pickle forks are mandatory according to rule 503.02.

Life jackets incorporating pants, are mandatory.

Seats are mandatory for F1 cockpits and highly recommended for all other cockpit classes. See 509.08.

Drivers must wear of flotation vest so constructed at the shoulders and the legs that a driver can be easily pulled of the cockpit.

12. GENERAL

FINANCIAL CONDITIONS

Each National Authority wishing to organise a Formula 4 race will have to guarantee the amount of 2000 € to be distributed as follows:

2000 € prize money:

Distributed by the organiser to the drivers at the end of the race, before they leave the racing place (after the end of the protest time).

1st:	340 €
2nd:	290 €
3rd:	250 €
4th:	220 €
5th:	200 €
6th:	180 €
7th:	160 €
8th:	140 €
9th:	120 €
10th:	100 €

500 € year end prize money:

To be deposited at U.I.M. BEFORE the 28th February of the year of the organisation, for the prize money of the general classification of the Championship.

The year end prize money will be paid by U.I.M. after the last race, according to the general classification of the championship and after examining all possible protests: 4 € for each point accumulated by each driver in the general classification.

All TV, film, media and commercial rights are the property of the U.I.M.

The U.I.M. contracted promoter handles the rights on behalf of the U.I.M.

Any organisation wishing to film or to use existing film must contact the promoter.

All correspondence related to the F4 World Championship should be sent to the following address:

UNION INTERNATIONALE MOTONAUTIQUE
STADE LOUIS II - ENTREE H
MC 98000 MONACO
Fax: (377) 92-05-04-60
E-mail: uim@uimpowerboating.com
Website: www.uimpowerboating.com

U.I.M. FORMULA 1000 INTERNATIONAL

1. DENOMINATION
2. HULL
3. MOTOR
4. PROPELLER
5. FUEL
6. YELLOW CARDS
7. COMMITTEE OF APPEAL
8. SAFETY
9. GENERAL

TECHNICAL RULES

1. DENOMINATION

The Formula 1000 European Championship is an outboard monobrand class.

The Championship includes up to 4 races organised in different countries.

The final classification of each of these four events will be calculated using the drivers' best three heats and the 25 % reducing points system (400, 300, 225 etc).

The overall classification of the European Championship will be calculated by awarding points for positions in the final classification of each of the four events using the 20, 17, 15 etc points system.

The heat distance will be 20 to 25 km.

2. HULL

The hull must be of a catamaran type provided with safety cockpit and pickle-forks in accordance with the U.I.M. rules. The minimum dimensions must be the following:

minimum length 3,90 m

minimum weight of the boat 250 kg (325 kg with driver) immediately after the race with residual fuel and without residual water.

3. MOTOR

The motor must be a SELVA FORMULA 1000 homologated by U.I.M.

The electric supply of the system must be 12 Volt (including trim system).

The lift system is mechanic and must be not modified. Any machinery aimed at modifying the height of the motor during the race is forbidden.

Carburettors must remain standard in any of their parts.

Cylinder sleeves cannot be substituted and cannot undergo any remarking.

The cooling water must circulate through the water pump and be fed only through the standard water intake, neither the position nor the shape of which may be modified.

The use of non SELVA parts are forbidden.

Sole modifications allowed:

- It is permissible to use any ignition wire , spark plugs (not exceeding into combustion chamber), propeller nut.
- It is permissible to use any fuel line or fittings. A fuel filter may also be used.
- Is it possible to add a fuel supply electric pump without changing the original system of engine supply.
- It is permissible to replace original carburettor jets for another size.

Reworking of the parts is allowed within the manufacture **geometrical dimension** tolerances as from the technical schedule provided by the manufacturer.

In order not to modify the crankshaft balancing of the standard production engine it is not allowed to remove or add material, in

any way, to the components of crankshaft, connecting rods, complete pistons, bearings and pins assembly.

Apart from the above provisions, it is absolutely forbidden to make any alterations to the engine manufacturers standard components either by removal or addition of material. In particular this includes components being subjected to any form of machining, grinding, hand tooling, reworking, reshaping, fettling, polishing etc. unless it is specifically permitted.

The driver is responsible for the condition of the engine as raced. Errors on the part of one manufacturer, mechanic or previous owner is no excuse.(528.10.19)

Post race inspection should follow rules 502.2

4. PROPELLER

The propeller must not have more than 4 (four) blades.

5. FUEL

The fuel must be of commercial type used for automobile. Lubricating oil for cylinder head or for 2-stroke motors may be added to the fuel.

The use of additives aiming at increasing the power of the motor is prohibited either to the fuel (carburant) or to the air intake (comburant).

No refuelling will be allowed when the boat is not on its trailer.

When fuel is available at race site, minimum 92 octane, and the specification and price is printed in the advance programme, all competitors must use the specified fuel.

6. YELLOW CARDS

Yellow cards can only be given for the behaviour on the water. A yellow card will be confirmed on a form which will be posted with the results. The pilot has 60 minutes to protest to the jury. If the protest is not

accepted, he can appeal. Protests and appeals can be lodged according to U.I.M. rules.

A yellow card can be given by:

- the Officer of the Day
- the U.I.M. Commissioner

The yellow card can be given according to the following:

- up to two yellow cards at a time for dangerous driving.
- red card for extremely dangerous driving

A driver who has been given three yellow cards or a red card loses his right to take part in the following competition of the World Championship (current season or following season).

All cards remain valid for the four following competitions of the Championship as from the date on which the last card was delivered. When the penalty of not racing at the next race has been applied, the 3 yellow cards are deleted through the penalty.

Timed trials, qualification race and final race count as one event.

An organiser/N.A. who has given one or more yellow cards to a driver must inform U.I.M. by telefax or mail within 3 days. U.I.M. will inform the next organiser/N.A. of a World Championship race or the Continental Championship about the delivery of such yellow cards. When a driver has done 4 races without yellow cards, his previous card(s) will be deleted from the listing.

Given yellow cards must be mentioned in the commissioner's report which must be distributed to all members of the relevant U.I.M. Formula commissioners panel.

7. COMMITTEE OF APPEAL

Appeals will be handled according to the 400 Jurisdiction rules.

The delay to appeal is counted from the moment of the communication of the Jury decision to the interested parties and it is 48 hours.

The 24 hours of the Sunday do not count within the 48 hours to appeal.

Address, telephone and fax should be clearly mentioned on the appeal so that the U.I.M. can send any correspondence directly to the driver with copy to his N.A. The driver may agree on having no Appeal Committee meeting but to handle the appeal through telephone and fax - if possible - in order to limit the costs of the appeal.

8. SAFETY

Before racing a craft with restraint system, all drivers must have passed an immersion test in a restraint system to ensure that they can exit a reinforced compartment successfully.

The immersion certificate should be provided by the National Authority of the driver and includes an immersion test in a simulated reinforced cockpit. The immersion test and the certificate must be renewed every 14 months.

A model for the certificate and instructions of how to perform an immersion test are available at the U.I.M. Secretariat.

All catamaran type hulls must have the deformable pickle forks according to rule 503.02.

Drivers must wear of flotation vest so constructed at the shoulders that a driver can be easily pulled of the cockpit.

Seats are mandatory for F1 cockpits and highly recommended for all other cockpit classes. See 509.08

9. GENERAL

All TV, film, media and commercial rights are the property of the U.I.M.

If the U.I.M. has a contracted promoter, he handles the rights on behalf of the U.I.M.

Any organisation wishing to film or to use existing film must contact the U.I.M. or the promoter.

Any correspondence regarding Formula 1000 must be sent to the following address:

UNION INTERNATIONALE MOTONAUTIQUE
STADE LOUIS II - ENTREE H
MC 98000 MONACO
Fax: (377) 92-05-04-60
E-mail: uim@uimpowerboating.com
Website: www.uimpowerboating.com

GENERAL FOR ALL FORMULAE RACES

CODE OF PRACTICE FOR SAFETY

1) RESCUE BOATS

Should be high speed, drop front (dories or similar) which produce a minimum disturbance of the water.

There must be a minimum of two rescue boats along the circuit so placed that they can reach in the minimum possible time any point of the course. Their locations and their co-ordination must be decided in a meeting among the OOD, the Medical Officer, the rescue Teams responsible and the U.I.M. Commissioners before first practice.

These boats shall be equipped as a minimum as follows:

- a floating stretcher,
- belt cutters,
- one fire extinguisher,
- ropes (including tow ropes),
- an anchor,
- an haemostatic strip,
- bandages
- a set of flags,
- red flares.

All these boats shall be in contact with each other, with the medical post and the OOD with a radio equipment that has to be used without interfering with the driving and the rescue operations.

These boats shall not be allowed to carry on towing duties during the practice sessions and the race.

2) CREW MEMBERS

Each boat shall carry the following persons:

- an experienced driver,
- two crew members prepared and equipped to enter the water to support any injured driver and who are familiar with any safety equipment used on racing boats.

No pilot shall be pulled or assisted into any rescue boat; no pilot shall be removed from the water unless the whole body is supported by a stretcher. No other boat shall be allowed to pull or assist any injured driver.

3) MEDICAL ASSISTANCE

A minimum of three doctors are required at each event:

- The Medical Officer, a doctor well introduced in powerboat racing problems acting as the co-ordinator of the medical team;
- a doctor graduated in resuscitation responsible for the medical post where he has to be available during the practice sessions and the race;
- a doctor graduated in resuscitation operating on one of the three rescue boats in addition to the existing crew. He shall carry on the boat all the equipment necessary for first aid assistance (a water sucker, intra tracheal tubes, Mayo cannulae, oxygen supply and so on).

4) GROUND ORGANISATION

Apart from the medical post reserved for the public each event must have a private post (minimum a tent) where an injured driver may be taken after initial resuscitation before being moved to the nearest hospital.

This medical post has to be located as close as possible to the rescue boat landing site. This has to be in a reserved area (not accessible to the public) and must be adequate for an easy and safe of the transfer operation injured driver. It is very important that the landing site and the medical post are located close to the race course even if far from the pits area. A nearby closed area for an helicopter is recommended.

A minimum of two ambulances is required. At least one of them must be equipped as a mobile resuscitation centre. They will be located close to the medical post and must have easy and constantly open access from the medical post to the road leading to the hospital.

A general hospital shall be informed about the Event and prepared to take care of an injured driver.

The Medical Officer, according to any specific situation, may ask the OOD to adjust the rescue and medical organisation (increasing the number of rescue boats, ambulances and doctors) in order to maintain a minimum standard of assistance.

The presence and the employment of local teams able to work with the required equipment and efficiency are recommended.

ELECTIONS OF THE DRIVERS' DELEGATE FOR F1, BOAT GP

The election must be held in the presence of a U.I.M. official. The candidate must be a real driver (4 races or 40% of the total rounded up to the next unit); he is elected for a three year period provided he keeps within the limits of eligibility. The drivers' assembly may intervene if the delegate is not suitable any longer.



U.I.M. SUPERLICENCE MEDICAL CERTIFICATE FOR ALL FORMULAE DRIVERS

I, Doctor..... Specialist in

Hereby declare that Mr

Born in On

Nationality Passport number

Has carried out the medical examination as in 205.03 / 04 of the U.I.M. Circuit Rules.

In addition, Mr has carried out the additional items indicated
at rule 205.04.1 of the U.I.M. Circuit Rules.

The results of the tests are normal and I therefore declare the driver eligible for racing.

The reports of the tests carried out can be consulted in the next five years at my surgery.

This certificate is valid for one year from today's date.

Date

Stamp with address of the doctor
Or of the Clinic

Signature of the Doctor

GENERAL RECOMMENDATION FOR ALL FORMULAE SUPER LICENCE HOLDERS

- Cardiovascular training (endurance training) as jogging, cycling, rowing etc. twice a week – minimum 20 minutes without any interruption. Any sports activity which has “stand still” during the activity such as soccer, tennis or any ball sport is not suitable for the endurance training, but recommended as additional training.
- Training to build up muscles for neck and back at least twice a week.

Achevé d'imprimer sur les presses de
MULTIPRINT - MONACO
en mars 2005



The General Operating Guidelines, Technical Restrictions and Championship Regulations are supplementary to, and must be used in addition to, the U.I.M. Class II rules & UIM General rules, which govern ProVee.

The U.I.M. ProVee Working Group reserves the right to amend these General Operating Guidelines, Technical Restrictions and Championship Regulations as and when necessary providing the amendments are within the spirit of the class structure.

ELIGIBLE HULL

- Monohulls only are permitted.
- RHIBS. built and /or first (UIM ProVee Championship) measured before 31/11/03 will be permitted.
- Canopies are mandatory and must comply with U.I.M. regulation 508 in full.
- Fluorescent Orange Bows.
Rule no.508.28 does not apply to boats in the 2005 UIM ProVee Championship providing the registered boat contains only two crew members. Any deviation from this crew number will require reversion back to the applicable 508.28 rule.
- All boats must be capable of fitting (excluding propeller and or rudder where necessary) inside a 'standard' 12.19 m. (40 ft) container on a cradle or trailer. Where necessary inflatable RHIB tubes may be deflated to fit.
- Minimum Hull length: 9.55 mtr.
- Min.Weight: (Post Race, including residual fuel)
 - 2600kg. Petrol engine boats first UIM ProVee Championship measured prior to 31/11/04 and using Mercury Speedmaster Outdrives MkIII, MkIV, MkV or MkVI.
 - 2700kg. Petrol engine boats not complying with the above or first UIM ProVee Championship measured after 31/11/04.
 - 3000kg. Diesel engine boats.
- All hulls must have embossed onto the outer side of the transom, a HIN (Hull Identification Number) which must be clearly visible.
(Before or at time of measurement for the 2005 UIM ProVee Championship series, the UIM ProVee Championship Permanent Technical Officer will advise the number).
- To be eligible to compete in the 2005 UIM ProVee Championship, all boats must be measured and issued with a '2005 UIM ProVee Championship Measurement Certificate' prior to the Championship season.

ELIGIBLE ENGINES

- Single inboard engines only are permitted.
- Only 2005 U.I.M. ProVee Working Group approved Petrol or Diesel engines of the following type are permitted:
 - Normally aspirated petrol engines
 - Supercharged petrol engines.
 - Turbocharged petrol engines.
 - Normally aspirated diesel engines.
 - Supercharged diesel engines.
 - Turbocharged diesel engines.
- The maximum engine displacement is unlimited.
- The maximum power of any engine used must not exceed:
 - 950 HP (ISO Standard) Petrol engines
 - 1000 HP (ISO Standard) Diesel engines
- Prior to the 2005 UIM ProVee Championship series, all engines proposed for use in the series must be submitted via the 2005 UIM ProVee Permanent Technical Officer for dynamometer power evaluation. Engines found to produce in excess of the specified maximum horsepower will be rejected until satisfactory adjustment has been effected.
- Engines found to produce power within the specified maximum horsepower will be approved for declaration on the relevant UIM ProVee Championship measurement certificate, and will be sealed to prevent alteration of critical settings at time of evaluation.
- Unauthorised removal (not under the scrutiny of the 2005 UIM ProVee Permanent Technical Officer) of engine seals will deem the engine illegal, and the responsible crew during the event will become liable to the appropriate penalty as listed below.
- During the 2005 UIM ProVee Championship series, random dynamometer evaluations may also be made to establish engine power output.
Crews found to be using engines producing in excess of the permitted figures will be liable to the appropriate penalty as listed below.

Penalties:

- Unauthorised removal of an engine seal:
Crew disqualification from the event.
- Engines exceeding the specified limit by less than 10%:
Crew disqualification from the event and exclusion from the remainder of the 2005 series.
- Engines exceeding the specified limit by more than 10%:
Crew disqualification from the event and exclusion from the remainder of the 2005 series and exclusion from the 2006 series.
- During the 2005 UIM ProVee Championship series, the use of a maximum of two (2) engines is permitted without penalty. Both engine serial numbers must be declared and listed on the 2005 UIM ProVee Championship measurement certificate
- The use of more than two (2) engines during the 2005 UIM ProVee Championship series will result in the deduction of 25% of any championship points attained after the use of the second engine has been exceeded.
- Fuel to 2005 U.I.M. regulation 504. (Pump Fuel)

VISIBLE EMISSION

- Under no circumstances will Visible Emission be tolerated during any practise or race session.
- UIM Rule 704.1 will be rigidly enforced at all times.
- The OOD and/or UIM ProVee Championship Permanent Technical Official will be the sole judges of any 'visible emission'.
- Penalty: Disqualification of the controlling crew from the event.

DRIVE TRAIN

- No multi-speed gearboxes or transmissions are permitted.
- Reverse gear facility is not mandatory within the transmission.
- Maximum of one (1) fitted drive unit per boat.
- Maximum of one (1) fitted propeller per boat.
- All propellers proposed for use in the championship must be submitted to the PTO (prior to finalisation of the measurement certificate) for verification of blade thickness compliance and for identification marking. The complying propeller serial numbers will then be annotated to the relevant measurement certificate.
- Non compliant propellers are prohibited during any racing.
- Penalty: Crew disqualification from the event and exclusion from the remainder of the 2005 championship season.
- The use of a maximum of two (2) alternatively dimensioned propellers per championship series is permitted without penalty.
- The use of more than two (2) alternatively dimensioned propellers per championship series will result in the deduction of 25% of any championship points attained after the use of the second propeller has been exceeded.
- Under the scrutiny of the PTO, a broken or disabled propeller may be permanently replaced with a new propeller of the same dimensions and specification without deduction of any championship points. The identity marking of the broken or disabled propeller will be removed rendering it unsuitable for further championship use.
- Maximum diameter of propeller 44.45cm (17.5ins.)
- Maximum number of blades: Five (5)
- Permitted Propeller Dimensions are shown in *Addendum Chart No1*.

PARTICIPATION

- The boat/engine (combination) entry must be approved by the U.I.M. ProVee Working Group.
- ProVee Management *may* negotiate with non ProVee Championship registered teams to participate (without benefits) in any races of the Championship.
- Teams wishing to be eligible for championship points and benefits must register prior to 4th February 2005

RACE FORMAT

Each venue will have two races per event as described in the advance programme.

The duration of each of the two races will be a maximum of 45 minutes.

The course will be multi lap unless otherwise agreed by the UIM ProVee Working Group and announced in the advance programme.

Points from all heats of the Championship are to count toward final Championship positions.

Points scoring system: 20-15-12-9-7-5-4-3-2-1

The 70% rule for 2005 will be as 860.34.5.

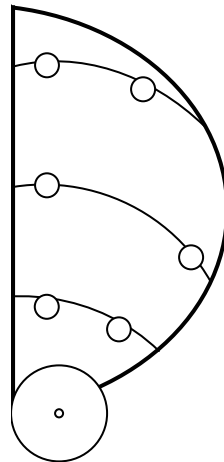
The above Guidelines, Technical Restrictions and ProVee Championship Regulations are supplementary to the UIM Class II rules and UIM General rules which govern ProVee.

'ProVee' refers to the 2005 UIM Class of ProVee

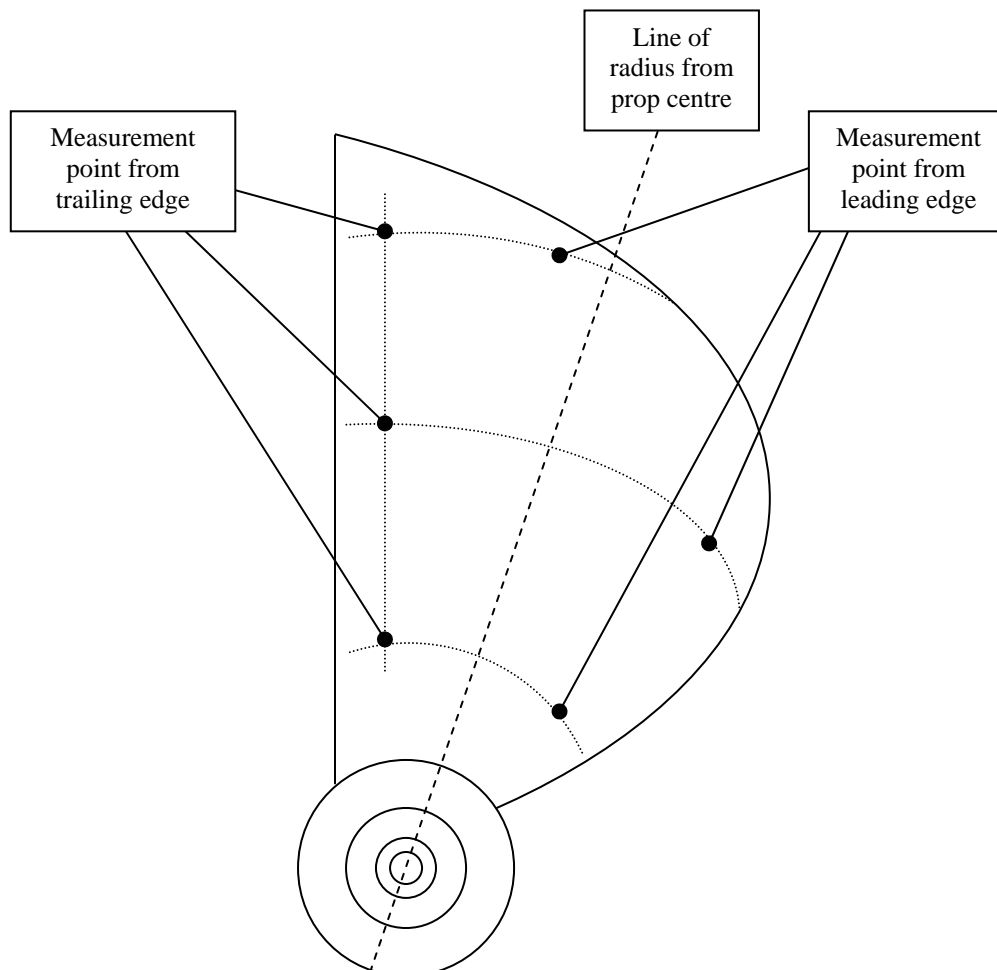
'championship' refers to the 2005 UIM ProVee Championship.

'PTO' refers to the 2005 UIM ProVee Permanent Technical Officer.

'measurement certificate' refers to the 2005 UIM ProVee Championship Measurement Certificate



2005 ProVee Championship Propeller Dimension Details and Points of Measurement





**UNION INTERNATIONALE
MOTONAUTIQUE**

OFFSHORE RULES

2005

2004 OFFSHORE WORLD CHAMPIONS

Class 1



Driver 1

GJELSTEN Bjorn - Norway



Driver 2

CURTIS Steve - UK

Class III 1,3I



Driver 1

GULBRANDSEN Espen - Norway



Driver 2

ENGEDAHL Nils Helge - Norway

Class III 2I



Driver 1

WRENKLER Thomas - Sweden



Driver 2

WRENKLER Annika - Sweden

2004 OFFSHORE WORLD CHAMPIONS

Class III 4l



Driver 1

BARONI Armando - Italy



Driver 2

PALLINI Mauro - Italy

2004 OFFSHORE EUROPEAN CHAMPIONS

Class 1



Driver 1

NASSER Ali - UAE



Driver 2

AL QAMA Ali - UAE

Class III 1,3l



Driver 1

TIIHONEN Tapio - Finland



Driver 2

KAARTINEN Kauko - Finland

Class III 2l



Driver 1

CUCURNIA Mauro
Italy



Driver 2

DUVAL Alessandro
Italy

Pro Vee



Driver 1

BRKOVIC R.
Netherlands



Driver 2

NICOLINI Luca
Italy

V 24



Driver 1

LITTLE Peter
UK



Driver 2

BROWN Gavin
UK

Class 1 Middle East



Driver 1

GJELSTEN Bjorn
Norway



Driver 2

CURTIS Steve
UK

GROUP 100

U.I.M. ADMINISTRATION

- 101 CALENDAR**
- 102 WORLD CHAMPIONSHIP**
- 103 TYPES OF RACES**
- 104 NON APPROVED RACES**
- 105 TITLED INTERNATIONAL EVENTS**
- 120 LICENCES**

GROUP 200

RACE ORGANISATION

200	RACE ORGANISATION
201	RACE CONTROL
202	ADVANCE PROGRAMME
203	ENTRY
204	PROGRAMME/ RACING INSTRUCTIONS
205	SAFETY RULES
206	ASSISTANCE IN ACCIDENTS
209	CRANE AREA

GROUP 300

RACING RULES

- 301 TIMING**
- 303 MARKS OF THE COURSE**
- 305 PROCEDURE FOR U.I.M. OFFSHORE STARTS**
- 306 QUALIFICATION**
- 309 RACE CANCELLED**
- 310 RACE POSTPONED**
- 311 RACE SHORTENED**
- 312 RACE STOPPED**
- 313 RULES OF THE ROAD**
- 314 ROUNDING AND FOULING MARKS**
- 315 FOULING COMPETING BOATS**
- 316 RUNNING AGROUND**
- 317 ANCHORING DURING A RACE**
- 318 MEANS OF PROPULSION**
- 319 OUTSIDE ASSISTANCE**

- 320 FINISHING A RACE**
- 321 SCORING SYSTEM**
- 323 POSTING OF THE RESULTS**
- 324 INSPECTION OF HULLS AND ENGINES**
- 326 AFTER THE MEETING (REPORTS)**

GROUP 400

JURISDICTION

- 401 DEFINITIONS**
- 402 JUDICIAL BODIES**
- 403 PROTEST PROCEDURES**
- 404 HEARING OF THE PROTEST**
- 405 THE APPEALS PROCEDURE**
- 406 PENALTIES**
- 407 PRIORITY OF THESE RULES**
- 408 POWERBOAT PROTEST FORM**

GROUP 500

INTERNATIONAL CLASSES

- 501 MEASUREMENT CERTIFICATE**
- 502 INSPECTIONS**
- 503 EQUIPMENT**
- 504 FUEL**
- 505 HOMOLOGATION**
- 506 HOMOLOGATION PROCEDURE**
- 507 RUNNING PRODUCTION CHANGES**
- 508 CREW SAFETY**

GROUP 600

RECORDS

- 600 RECORDS AND PERFORMANCES**
- 601 CLASSES ELIGIBLE FOR RECORDS**
- 602 DEFINITION OF UNITS**
- 604 THE COURSE**
- 605 CALCULATION OF SPEED**
- 606 MARGIN REQUIRED**
- 607 MOTORS**
- 608 REQUEST TO ORGANISE**
- 609 OFFICIALS**
- 610 SPEED RECORDS**
- 611 DISTANCE RECORDS**
- 612 HOUR RECORDS**
- 613 WORLD COMPETITION RECORDS**
- 614 PERFORMANCE CERTIFICATES**
- 615 HOMOLOGATION OF RECORDS**

GROUP 700

INTERNATIONAL OFFSHORE RULES

- 701 GENERAL**
- 702 CREW**
- 703 HULL**
- 704 INBOARD ENGINES**
- 705 ENGINES**
- 706 ENGINE CUT-OUT**
- 707 ENGINE AND FUEL COMPARTMENT
BULKHEADS**
- 708 PROPULSION**
- 709 GYROS**
- 710 RACES NUMBERS**
- 711 NATIONAL FLAGS**
- 712 RACING VESTS**
- 713 HELMETS**
- 714 PROTECTIVE CLOTHING**
- 715 SAFETY EQUIPMENT**

CLASS 2

720 ENGINES

721 HULL, DIMENSIONS AND MINIMUM WEIGHTS

CLASS 3

- 730 HULLS AND CLASSES**
- 731 MINIMUM/MAXIMUM LENGTH AND WEIGHTS**
- 732 HULL DIMENSIONS**
- 733 MINIMUM FLOTATION**
- 734 OUTBOARD ENGINES**
- 735 OUTBOARD ENGINES MODIFICATIONS
PERMITTED**
- 736 INBOARD ENGINES**
- 737 INBOARD ENGINES MODIFICATIONS
ALLOWED**

GROUP 800

GENERAL RULES FOR OFFSHORE WORD AND CONTINENTAL CHAMPIONSHIPS (EXCEPT WPC)

GROUP 855

ADVISORY REQUIREMENTS FOR U.I.M. CLASS 1 AND 2 CHAMPIONSHIP EVENTS

GROUP 856

RECOMMENDED MINIMUM REQUIREMENTS FOR U.I.M. CLASS 3 CHAMPIONSHIP EVENTS

GROUP 860

U.I.M. CLASS 1 WORLD POWERBOAT CHAMPIONSHIP (WPC)

GROUP 900

OFFSHORE CIRCUIT RACING GROUP 950 U.I.M. V24 CLASS

- 951 GENERAL**
- 952 MODIFICATIONS ALLOWED**
- 953 FUEL**
- 954 IDENTIFICATION**
- 955 SAFETY**
- 956 EQUIPMENT**
- 957 CREW**
- 958 ENGINE SEALS**

GROUP 960

U.I.M. PRO-VEE CLASS EXPERIMENTAL

101 - SPORTS CALENDAR

101.01 - INSCRIPTION FEE

Registration of International meetings and races on the annual calendar of the U.I.M. is compulsory and is subject to the payment of a fee the amount of which is fixed annually by the General Assembly.

Payment of these fees must be made within three months after the fixing of the International calendar. It is compulsory even if the meeting does not take place.

Permission to organise these meetings is only delivered by the U.I.M. upon payment of these fees. No permission is to be granted to National Authorities in arrears of payment.

International events organised by clubs affiliated to a National Authority and which have not been entered for reserved dates on the Sports Calendar may have to pay a fine up to five times the usual fees.

The registration fee is to be doubled for any meeting requested after the deadline dates set by the rules.

All requests to organise an International championship must be sent to the U.I.M. Secretariat before the time set by the deadline in the rules.

When compiling the Offshore Calendar due attention is to be paid to the confidential reports of the U.I.M. Commissioners before granting the authorisation to organise titled meetings.

The organisers must name the series and classes which may participate and also give the name and address of the organisers and/or the responsible person (s) who are in a position to supply information about the event. They must also name the type of races: titled, championship, etc., the locality, as well as the date of the inscriptions.

Such information must be provided as soon as possible.

Fees for multi-class events :

For Championship events held for one class or sub-class, there shall be one inscription fee.

For Championship events for two or more classes or sub-classes, the fee shall be doubled.

Championship events for OCR 1.3 and 2 litre classes or Offshore 1.0 and 1.3 litre classes, when raced together shall require only one fee.

These “events” may have up to three races (heats), providing they are all run at the same venue and do not exceed the time interval between races (heats) as allowed for in the relevant Championship series rules.

101.02 - OTHER MEETINGS

It is to be noted that the offshore Calendar also lists the registered ordinary International meetings.

The calendar of national or local events is not to be submitted to the U.I.M. The interested National Authorities take responsibility for their own calendar.

The organisation of an event cannot be granted if the title, the locality and the date of said event are not given.

The U.I.M. does its utmost to avoid clashes between ordinary meetings. However, whether the interested parties agree or disagree, the U.I.M. lists all requests for dates, leaving the organisers to sort out their responsibilities.

The titled meetings have priority over all other meetings before the closing of the registrations for the Offshore Calendar.

However, when the Offshore Calendar is finalised all the motor boating meetings are on an equal footing.

A titled meeting which has been cancelled does not count any longer for the title. If the organisers decide to run the race in spite of the cancellation, the race must be considered as an ordinary International race.

No national championship race may be run at the same week-end as a U.I.M. World, Continental, or Regional Championship race, in the same continent, for the class concerned.

For a World Championship, no national championship race may be run on the week-end before the World Championship, run in the same continent.

This is providing the championship race has been announced in accordance to U.I.M. dates.

101.03 - DEADLINE DATES

10th September : the deadline date for the U.I.M. Secretariat to receive from the N.A., the dates for the championship races and ordinary International meetings they wish to enter in the Offshore Calendar for the following year. Also at this time, requests may be lodged for any World or Continental Championship not yet allocated or cancelled before this date.

15th November : the deadline date for the U.I.M. Secretariat to send, to all concerned, the final Offshore Calendar for that calendar year.

1st March : the deadline date for the U.I.M. Secretariat to receive from the National Authorities, requests for Continental or World Championships 2 years hence. It is not necessary to add the locality; the date and the nation suffice. These championships will be allocated by Cominoff at the March General Assembly.

Requests in writing or by fax to the U.I.M. Secretariat will be accepted.

Any change of date of a Continental or World Championship, notified after 28th February, in the year that the event is to be run, may be penalised by the payment of a fine up to five times the amount of the registration fee (to be determined by Cominoff).

101.04 - CANCELLATION

If the event is cancelled 30 days or more before the calendar date, 50% of the inscription fee will be refunded. For later cancellations there will be no refunding.

However, if cancellation is due to insufficient participation and is not fault of the organiser, Cominoff may review this.

102 - WORLD CHAMPIONSHIPS

World Championships are open to all U.I.M. classes as well as A.P.B.A. classes. There can be only one World Championship per class, per year. Cominoff will allocate the Championships.

103 - RACES RECOGNISED BY THE U.I.M.

The U.I.M. recognises the following events:

- Titled International meetings
- Ordinary International meetings
- National and local meetings
- Private meetings or club meetings
- International cruises
- Titled offshore races
- Ordinary offshore races

104 - NON APPROVED RACES

All races not organised according to U.I.M. rules are not to be recognised and all officials and drivers who take any part in such races are suspended or excluded if offence recurs.

The same applies to motor boating events not approved by National Authorities.

Any driver having competed in a race not approved by the National Authority of that country or forbidden by the National Authority may be suspended for a period to be fixed by the National Authority.

In the case of an International meeting being organised by a non-affiliated club which has not asked permission to organise such a meeting the National Authority must:

- Notify the organising club that the drivers can be suspended.
- Notify the drivers that they will be suspended in their own country if they take part in International races not approved by the National Authority; the latter may request the U.I.M. to extend this suspension to other countries.

A national race, or a race by “invitation” in which a foreign driver is “invited” to take part must be entered in the U.I.M. Calendar otherwise it becomes an unauthorised event. Invited drivers entering such unauthorised races, may lose their International licences. Remember that drivers having International licences, are only allowed to enter a meeting outside their country provided their National Authority gives them permission to do so.

105 - TITLED INTERNATIONAL EVENTS

The word “International” defines the type of race. It is impossible to know beforehand the number of the countries which will participate.

There are three categories of titled meetings:

- World Championships
- Continental Championships and Hemispheric Championship
- Special events as approved by U.I.M. Cominoff

No meeting is allowed a title unless authorised to do so by the U.I.M. who draw up the annual list.

The U.I.M. has the right to refuse a title if it thinks that the amount of prizes or the organisation are not acceptable.

Titled meetings must recur as far as possible at the same time of the year.

U.I.M. aims to protect titled meetings, the Cominoff may refuse to sanction any meeting which takes place at the same time as a titled meeting. Such refusal is only notified at the specific request of the National Authority lodging a protest.

In order to promote new series and classes, new hulls, motors or engines, new ideas regarding propulsion and new concepts of racing, the Cominoff may, provided the request to organise be accompanied by an amount equal to double the fee of an ordinary meeting, approve the organisation of International contests for such boats, motors or engines, type of propulsion and races of a kind not yet known to the U.I.M. provided such events do not clash with other U.I.M. titled meetings.

120 - RACING LICENCES

120.01 - COMPULSORY LICENCE

No one can take part in a race, establish or improve on a record, or test drive at any titled event as a driver or crew in International class boats, without being in possession of a licence from his National Authority.

Anyone responsible for controlling a boat in any competition and holding the necessary driver's licence issued by the National Authority is considered to be the driver.

Any driver or crew member that rides in a boat fitted with restraints must partake in a suitable Driver Immersion Test annually prior to being issued with his current licence. Such licensed persons shall have their licenses endorsed as "Driver Immersion Tested : expiry date/....../.... (day/month/year)".

120.01.01

In order to obtain an International licence or Super Licence, following disclaimer must be signed by the licence holder :

"The undersigned ... licensed driver (licence n° ...), participating to U.I.M. sanctioned events, yields the media rights of any coverage by the promoter taken during the event, to the U.I.M. for further use press, radio or television. The right however remain the property of the undersigned".

The signed disclaimer can be requested by the U.I.M. commissioner at any U.I.M. sanctioned event.

120.02 - REGISTRATION

The National Authority establishes each year a list of people to whom licences have been delivered and this is sent to the U.I.M. Secretariat.

The list must include: the name and surname, the address, the class(es) and whether National or International Licence.

These lists may be sent to others National Authorities upon request.

Licence numbers are given annually by the National Authority to the owners and drivers entered on the list.

Any person wishing to obtain a licence must apply to the National Authority through his club.

The licence is granted by the National Authority and is marked "International" as decided by the National Authority. Licence holders must buy a rule book.

Any National Authority has the right to deliver licences to:

- its nationals;
- nationals of another country affiliated to the U.I.M. but with the agreement of the National Authority of that nation.

With the approval of the U.I.M. a National Authority may also deliver licences to persons belonging to a country which is not yet affiliated to the Union.

Nobody is allowed more than one driver's licence.

A National Authority may refuse to give a licence without having to state the reason. Notification of this is sent to all affiliated clubs and to U.I.M.

120.03 - VALIDITY OF LICENCES

Licences are valid from the 1 st. January until the 31 st. December each year, unless local circumstances require other dates, but validity should always be shown on the licence itself.

An International licence is valid in any country affiliated to the Union and entitles the holder to enter or drive his boat in all competitions organised in any affiliated country, provided he complies with what is stated in the U.I.M. general racing rules.

However, for any event held under special rules, the licence holder must abide by special rules.

First and second named drivers competing in the World Offshore Championship for U.I.M. Class 1 must obtain a U.I.M. Offshore Super Licence as described in 860.12.

120.04 - COST OF LICENCE

The cost of the yearly licence is to be fixed by the National Authority.

120.05 - EXAMINATION OF LICENCES

At any meeting, the holder must produce his licence at the request of any official of the meeting.

120.06 - CANCELLATION OF LICENCE

Anybody who takes part in an unauthorised meeting loses his licence.

However, if the unauthorised meeting is held in waters of a country other than the one which has delivered the licence, both National Authorities must agree as to the duration of the suspension. The U.I.M. decides finally in case of dispute.

120.07 - PSEUDONYM

The use of a pseudonym must be requested from the National Authority and the National Authority marks the pseudonym on the licence.

A licence holder, so long as he is listed under a pseudonym, may not take part in any sports event under any other name.

Changing a pseudonym is subject to the same formalities as the adoption of it. Anyone who has chosen a pseudonym may not return to his actual name until the National Authority has agreed and has delivered a new licence.

120.08 - ELECTION OF DRIVERS REPRESENTATIVE

There may be a Driver's Representative from each International racing class.

The election of each representative must be held in the presence of an U.I.M. Official. Candidates must be real drivers. To be eligible, they must have obtained the following minimum experience :

Have competed in International races and be regularly racing. Candidates must be nominated by their National Authority, prior to the mid term meeting and selected by Cominoff.

Driver's Representatives are elected for a 1 year term, beginning April and ending the following March. They should be available to attend Cominoff meetings at least annually to report and advise on the consensus recommendations and requests from the drivers'. Otherwise a full written report must be forwarded to the U.I.M. Cominoff 30 days prior to the General Assembly.

120.09 - MODEL APPLICATION FORM FOR A LICENCE

The undersigned _____

Date of Birth : / /

begs to apply for
a licence

National
International

Driver's
Owner's

I have read the U.I.M. licence rules and will abide by same.

The pseudonym chosen is _____

Enclosed please find the licence fee _____

the _____

signature _____

address _____

Member of _____

To be sent to the Secretary of the National Authority through the Club.

120.10 - MODEL OF LICENCE

20 _____

The National Authority's
Licence Book)

N° _____

LICENCE
NATIONAL OR
INTERNATIONAL

OWNER'S
DRIVER'S

Delivered to M _____

Member of _____

Valid until _____

Pseudonym _____

"Driver Immersion Tested : (expiry date / /)"

(Here name of National Authority)

Secretary,

President

Signature of Licence Holder

200 - RACE ORGANISATION

200.01 - GENERALITIES

The following bodies have the right to organise any power boating event: National Authorities and their Affiliated Clubs, Corresponding Members of the U.I.M., U.I.M. contracted promoters (with the agreement of the National Authority).

All events have to be sanctioned by the National Authority.

Affiliation to the National Authority implies acceptance of the National Rules and those of the U.I.M.

A club, a club member, or a driver who is suspended, is not allowed to take part in any power boating event in his country, or in any other country neither as a driver nor as an official, as long as the suspension lasts.

A member or a driver who takes part in any power boat race in a foreign country affiliated to the U.I.M., undertakes ipso facto to abide by the rules of the National Authority of that country.

All clubs, race organisers, officials and drivers are obliged to know the general rules of the U.I.M. as well as the rules related to races or series (local, National or International). A copy of the current U.I.M. Offshore rule book, including any addendum, must be available for every organisation.

All offshore races organised by National Authorities or their affiliated clubs must be organised in accordance with the U.I.M. Offshore rules. All advance programmes and racing instructions must mention this.

The general racing rules and the rules related to records are applicable to all the International series.

Any member or driver who infringes any of the rules of the U.I.M. or any rules of the responsible National Authority is liable to be penalised.

All organising bodies, members, drivers or owners who take part in a race pledge themselves :

- to know current U.I.M. Offshore rules and National rules of the responsible National Authority, if any, and also any particular event rules imposed by the event organiser or National Authority.
- to bind themselves to submit without restriction to any consequence following the application of these rules.

200.02 - INTERPRETATION OF THE RULES

In all cases not foreseen by the International rules or seemingly inaccurately defined, the National Authority, or Race Committee, or Race Jury or the Appeal Board, is to judge bearing in mind the spirit of the disputed rule.

The rules for series and classes are to be considered as being supplementary to the Offshore general rules.

In case of conflicting rules, those concerning a specific case/occasion (i.e. series and classes) are to prevail over the general rules.

National Authorities may impose special event rules relating to exceptional local requirements for National events or International events including World or Continental Championships. These special event rules prevail over, but must not conflict with the U.I.M. Offshore rules.

English is the working language.

200.03 - MONETARY PENALTIES

At any titled International event, the Race Committee shall be entitled to impose monetary penalties for breaches of the Rules; either the U.I.M. Offshore Rules or special event rules.

The monetary amount shall be scaled according to the severity of the "breach of the rules".

All monetary fines shall be based on a standard monetary value. For the purpose of these rules the value shall be expressed in Euros and converted at the current rate of exchange to the currency of the country where the event is being held.

Examples of penalties :

Crew or team members smoking in pit area	110 €
Un-authorised transference of fuel	110 €
Late for briefing or drivers meeting	110 €

Other infringements may attract monetary fines based on the above scale.

The fines must be paid before the start of the race ; otherwise the driver is disqualified.

The Race Committee should take care not to substitute monetary fines for infringements of a technical nature or infringements that would normally result in disqualification or suspension.

Any fines collected shall be deposited in the Race Organiser's bank account and may eventually be used to defray organisational expenses.

Absence from Drivers meeting or briefing

Complete absence from briefing or drivers meeting entails disqualification.

201 - RACE CONTROL

201.01 - ORGANISING BODIES

All races are under the control of an Organising Body approved by and affiliated to the National Authority of the country in which the race is run. The Organising Body nominates :

- An **Organising Committee** which organises the general and administrative aspects of the event.
- A **Race Committee** which organises and supervises the actual racing, determines any special event rules, determines the course, decides whether competitors are eligible or not and is responsible for safety. The Race Committee is composed of the following :

- * the Officer of the Day
- * the Safety Officer
- * the Race Secretary
- * the Officials in charge of the Technical Inspections, Course, Wet/Dry Pits, Starting and Time keeping
- * Any other special official appointed by the Organising Body.

-A **Jury**. Foreign nominees must be authorised by their National Authority.

The Race Committee must inspect and verify all entrants documents required prior to racing, i.e., Measurement Certificates, Licences, Insurances, etc.

The Jury decides on any protest submitted by the Race Committee. The names of the members are to be printed in the official programme.

No officer, official, nor member of the Jury nor member of a National or International Committee or Commission, is allowed to intervene in the judgement of a problem in which he is involved himself or is an interested party.

The composition of the Race Jury must be announced at drivers briefing and published in a race bulletin.

The decisions of the Race Committee and the Jury must be based upon the U.I.M. rules as far as they may apply, but as no rules can be devised capable of covering every case, the Race Committee and the Jury should discourage all attempts to win by means other than fair racing, superior speed and skill.

The following are the Race Officials :

- * the Officer of the Day
- * the Safety Officer (deputy to the OOD)
- * the Race Secretary
- * the Technical Officials

- * the Course Officials
- * the Starting Officials
- * the Timekeepers
- * the Pit Officials
- * the Medical and Rescue Officials
- * the Communication Officials.

The U.I.M. Commissioner is a U.I.M. Official.

201.02.1 - THE OFFICER OF THE DAY

The Officer of the Day must direct the organisation of the races on behalf of the Organising Body. He supervises the drivers briefings, maintains order on the course, ascertains that all services work properly, ensures that all correspond to the rules and observe the Racing Rules according to the Programme. He analyses the written reports of the Officers and Timekeepers and any other documents that will enable the results to be compiled. He verifies any decision to stop or curtail a race. He is not allowed to sit on the Jury.

For all the World Continental Championship events, the Officer of the Day must be a U.I.M. approved person. To qualify, he/she must meet the following minimum standards :

Have had previous experience in controlling at least 3 important races.

Have a good knowledge of the U.I.M. Offshore rules.

Have the ability to speak and understand the English language.

Be nominated by his/her National Authority for "U.I.M. International Officer of the Day" approval

A list of approved persons is held by the U.I.M. Secretariat and reviewed each year by Cominoff (considering also any reports from U.I.M. Commissioners). Nominations for International OOD approval shall be sent to the U.I.M. by the nominee's National Authority, accompanied by details of his/her qualifications and previous experience.

National Authorities must keep a list of their approved persons up to date and notify the U.I.M. accordingly of any changes.

National Authorities are strongly encouraged to arrange for their Officers to attend training sessions.

201.02.2 - THE SAFETY OFFICER

The Safety Officer is deputy to the Officer of the Day. He/She coordinates (in harmony with the OOD and the medical and rescue officials) the safety services on shore, on the water and in the air.

At the scene of an accident a qualified doctor can take command of emergency services and actions.

201.02.3 - THE RACE SECRETARY

The Race Secretary is responsible for the general organisation of the event. He/She must insure that all the various officials know their respective duties and that they have the necessary documents and equipment to fulfil their duties. He/She must collate all documents following the race.

201.02.4 - THE TECHNICAL OFFICIALS

The Technical Officials are to make sure that all drivers have their Measurement Certificates and Homologation Sheets (or production verification) available for inspection and shall verify that the hull and engine(s) conform to the measurement certificate, homologation sheets and the rules.

They check control and safety devices for boats and drivers as provided for in the rules. Scrutineers should be specifically delegated for this duty.

Special attention must be paid to engine and fuel shut-off devices (where applicable), driver protection and safety devices, fire protection equipment and that the racing number is clearly visible.

After the race, they must carry out all the necessary checks.

For all the World Continental Championship events, the Officer of the Day must be a U.I.M. approved person. To qualify, he/she must meet the following minimum standards:

Have had previous experience in controlling at least 3 important races.

Have a good knowledge of the U.I.M. Offshore Rules.

Have the ability to speak and understand the English language.

Be nominated by his/her National Authority for "U.I.M. International Officer of the Day" approval

A list of approved persons is held by the U.I.M. Secretariat and reviewed each year by Cominoff (considering also any reports from U.I.M. Commissioners). Nominations for International OOD approval shall be sent to the U.I.M. by the nominee's National Authority, accompanied by details of his/her qualifications and previous experience.

National Authorities must keep a list of their approved Officials up to date and notify the U.I.M. accordingly of any changes.

National Authorities are strongly encouraged to arrange for their Officers to attend training sessions.

201.02.5 - THE COURSE OFFICIALS

The Course Officials must ensure that the drivers and boats conform to the racing rules. At the end of the race, they draw up and sign a statement certifying that all participants conduct during the race and at the turn marks was according to the rules. This statement is handed to the Officer of the Day who will apply the International rules in the case of infringements. The Course Officials monitoring turning marks must have a clear un-obstructed view.

201.02.6 - THE STARTING OFFICIALS

The Starting Officials are responsible for the safe and orderly execution of the race start. They must closely observe all boats during the starting procedure to ensure that the rules are adhered to and report any infringements.

One Starting Official must be on the start boat.

201.02.7 - THE TIMEKEEPERS

The Timekeepers are responsible for recording the start time, all lap times and the finish time and finish order of a race. They must be located at a position where they have clear un-obstructed viewing of the start and finish lines. In some races they may be required to carry out lap timing at turning points other than just the start and finish lines. Timekeepers must work in teams of at least two persons for each timing point. They must use reliable synchronised timing apparatus. They must keep detailed records of the timing of each boat and hand these to the Race Secretary at the completion of the race.

201.02.8 - THE PIT OFFICIALS

The Pit Officials maintain order in the wet and dry pits. They are responsible for the parking of boats, the fuelling of boats, the launching and retrieving of boats and all safety procedures within the pit areas including fire safety.

201.02.9 - THE MEDICAL AND RESCUE OFFICIALS

The Medical and Rescue Officials are responsible for all first aid and rescue procedures. The Medical personnel must be fully trained in emergency accident procedures with emphasis on resuscitation, spinal type injury care and trauma treatment. They must have facilities available for emergency treatment and facilities for rapid transportation of seriously injured persons to the nearest hospital.

The Rescue Officials must ensure that adequate rescue boats and rescue helicopters when required and available with trained personnel at strategic points of the race course and that they are equipped for any rescue eventuality.

201.02.10 - THE COMMUNICATION OFFICIALS

The Communication Officials are responsible for maintaining radio or other alternative means of communication between all key race officials

with emphasis on communication between rescue crews and race headquarters.

201.08 - U.I.M. COMMISSIONERS

The presence of a Commissioner of the U.I.M. is necessary at any International titled meeting, except in case of force majeure. The Commissioner is appointed by the U.I.M. Cominoff.

The function of the Commissioner must be taken very seriously.

The U.I.M. Commissioner :

- * ***ensure that U.I.M. rules and regulations are adhered to***
- * assists the local organiser, especially the Officer of the Day
- * is a member of the International Jury with the right to vote
- * signs late alterations of the Advance Programme
- * surveys the signing of the declaration paper by the drivers using a safety cockpit and restraint system
- * takes part in important decisions with the OOD and the Safety Officer during the event
- * approves and advises on safety procedures with the rescue teams and the Safety Officer (especially if a foreign crew is participating)
- * if an emergency decision is required on general organisation, the following committee will be responsible :
 - The Officer of the Day
 - The U.I.M. Commissioner
 - A representative of the National Authority, if present
 - A representative of the Promoter/Organiser
 - A drivers elected representative

The U.I.M. Commissioner is entitled to take action with the race organisation if safety conditions are not met. In some cases, he will

even be entitled to request that an International Championship/event be suspended if safety conditions are not approved.

After the event, he sends his report to the U.I.M. Secretariat using the forms he has received for the purpose within 10 days of the race and these must be accompanied by the official results of the race.

The reports from the Commissioner are to be considered as confidential documents and are not to be used for aims which are contrary to those of the U.I.M., nor be communicated to the press nor otherwise revealed for any reason whatsoever. Should the Commissioner not have received all necessary data and results for transmission to the U.I.M. Secretariat, he notes the fact in his report and insists that the organisers send every document required, without delay, to the Secretariat of the U.I.M.

A copy of the Commissioner's report shall be sent by the U.I.M. Secretariat to the President and Secretary of the U.I.M. Cominoff and be available to all members of Cominoff. These reports will be reviewed at the next Cominoff meeting.

National Authorities are requested to send to the U.I.M. Cominoff, nominations for Commissioner with the following information :

Full name

Complete address

Telephone - Fax numbers

Languages read and understood

Offshore power boating knowledge and experience.

Cominoff may also nominate candidates for Commissioners. The approval of Commissioners will be made by Cominoff.

A Commissioner cannot be appointed to an event organised by his own National Authority, unless by reason of force majeure.

Following confirmation of appointment, the U.I.M. Secretary writes to the National Authority to announce that such and such Commissioner will come to such event.

201.09 - TRAVELLING EXPENSES AND ACCOMMODATION

The U.I.M. must ensure that the travelling expenses for the Commissioner from his town of residence to the locality where the event is taking place are paid. Air, rail and/or road travel expenses are to be granted at economy rates.

The expenses stated above must be refunded to the Commissioner by the event organiser, or if not, by the U.I.M. within 30 days of receipt of vouchers.

The Commissioner delegate is to be accommodated in a good hotel for the whole duration of the race and as long as is necessary for the accomplishment of his mission, at the expense of the organisers. The organisers must offer daily meals and local transportation to the Commissioner for the whole duration of the event.

202 - ADVANCE PROGRAMME

202.00 - ADVANCE NOTICE

To promote International titled events, an advance notice should be published at least 6 months ahead of the event. The advance notice should contain the following information :

- The championship title and class(es)
- The date and place of the event
- The name, address, phone, fax, e-mail, website (if any) of the organiser
- Any other information that is available for promoting teams to attend the event.

202.01 - ADVANCE PROGRAMME

45 days before any International race, the organising committee must forward at least one advance programme on the official form to the U.I.M. Secretary, the U.I.M. Cominoff Secretary and the National

Authorities. Should an organiser fail to meet this requirement, the U.I.M. Secretary shall deem this International race to be cancelled. The U.I.M. shall notify all National Authorities of the cancellation. The inscription fee shall remain due.

The allocation of future International events to that organiser shall be given lowest priority by the U.I.M. Cominoff.

The above advance programme must be written in the national language and English. It must contain the following information

- 202.02.0 The name, the address and fax number of the organiser.
- 202.02.1 The classes eligible.
- 202.02.2 The race status.
- 202.02.3 The venue and the date of the race(s).
- 202.02.4 The provisional course(s).
- 202.02.5 The races are held under the U.I.M. Offshore rules.
- 202.02.6 The special conditions which in accordance with the U.I.M. Offshore rules may be added or modified.
- 202.02.7 The closing date and place to which entries must be sent, and whether by post or by fax.

An entry form must be included in the advance programme.
- 202.02.8 The amount of the entry fee and details of whom payment is to be made to.
- 202.02.9 All information regarding prizes for each race.
- 202.02.10 Where and when the racing instructions can be obtained from the organising committee.
- 202.02.11 An entry form must be enclosed to the advance programme.
- 202.02.12 Transportation facilities and freight concessions.
- 202.02.13 Details of compulsory insurance for personal and material damage to third parties including other entrants, where

required by National Authorities. The amount of the insurance fee to be paid prior to racing to the organising committee.

- 202.02.14 The choice and efficiency of a racing vest is the sole responsibility of the wearer.
- 202.02.15 The wearer is entirely responsible for the choice of his helmet.
- 202.02.16 Should no publicity be allowed during a race, or there be a restriction on any advertising, this is to be mentioned in the advance programme.
- 202.02.17 Dates and times for registration and scrutineering.
- 202.02.18 The protest fee

The maximum amount for a protest fee in an International event will be decided from time to time by the U.I.M. General Assembly.

202.03 - MODIFICATION TO ADVANCE PROGRAMME/ RACING INSTRUCTIONS

No modification is to be made to any particular rule after the advance programme or racing instructions are posted, except if decided by the Race Committee and for cases of force majeure only. Such modification shall be communicated to each and every driver and crew person in writing at an official drivers meeting.

203 - ENTRIES

203.01 - RATIFICATION

The National Authorities must ratify in writing any entry for drivers wishing to race abroad.

Direct correspondence between the Organising Committee and foreign drivers is permitted, but no entry is to be accepted unless approved by the driver's National Authority.

The Organising Committee will keep blank entry forms at the disposal of the drivers.

An entry is not valid unless it reaches the Organising Committee within the time prescribed in the advance programme and the entry fee is paid.

Entry forms must include the names of all drivers, including ratified test drivers.

203.02 - ADMISSION

No National Authority, or Organising Committee, organising an International race has the right to refuse the entry of a foreign driver duly commissioned by his National Authority, provided said driver is not within a period of expulsion and he conforms to the U.I.M. Offshore rules.

The closing date for entries should be at least two weeks before the event. Owners need only to supply such particulars as are applicable to the class for which their boat is entered.

203.03 - ENTRY FEE

Each Organising Committee fixes the amount of the entry fee for a race.

The National Authorities or organiser have the right to establish a rule whereby higher fees are imposed on late entries. No late entry may be accepted after the closing time of registration for an event.

203.04 - MINIMUM AGE

Each National Authority has the right to fix a minimum age for the drivers allowed to race on its waters. Such age restriction is to be shown in the advance programme.

203.05 - OWNERSHIP OF BOATS

All boats entitled to race must be owned by a person or a group of persons who are members of a club affiliated to a National Authority.

The nationality of a boat is the nationality of the owner. The nationality of the driver only must be taken into consideration in the classification.

A boat is allowed to race only under its own name. The owner and the driver may only race under their own name or the pseudonym written on the licence.

203.06 - PUBLICITY ON BOATS

Advertisements may be freely applied to the entire surface of the boat, but there must be a free distance of space of no less than 0.5 metre around the race number.

No owner or crew can be compelled to carry any advertising whatsoever on their person, clothes or upon the boat that is being raced and his/their refusal to do so cannot in any way be an obstacle to their participation in any race. If an entrant refuses to display sponsor advertising, this must be noted on his entry application.

Penalties may be applied to any driver or crew member who, in the course of any offshore racing event, has made any statement contrary to the truth or has distorted the facts.

Political publicity or immoral advertisements are not allowed.

Should restricted publicity or no publicity be allowed for an event, this must be announced in the advance programme.

Any National Authority has the right to fix its own rules regarding publicity for its own boats and venues.

203.07 - TEAM REPRESENTATIVE

At any titled event, each boat must designate its representative for use in case of an accident or force majeure.

204 - PROGRAMME / RACING INSTRUCTIONS

204.01

The Organising Committee must clearly state the following points in their programme / racing instructions :

- 204.01.1 The venue and the date(s) of the race(s).
- 204.01.2 The date and times of driver registration.
- 204.01.3 The dates and times for scrutineering.
- 204.01.4 The date(s) and time(s) for drivers briefing.
- 204.01.5 Exact, complete description of the race course(s), including all turning marks, known hazards and any designated spectator areas.
- 204.01.6 Starting area and its marks.
- 204.01.7 Finishing area and its marks.
- 204.01.8 Starting time of the race(s), time limits for late starters and finishers.
- 204.01.9 Time when the control closes for each race (sign off time).
- 204.01.10 Signals of race cancellation, postponement, recommencement of a race, curtailment of a race, or stopping a race.
- 204.01.11 When helmets and racing vests must be worn.
- 204.01.12 The choice and efficiency of a racing vest is the sole responsibility of the wearer.
- 204.01.13 The wearer is entirely responsible for the choice of his helmet.
- 204.01.14 Special harbour conditions and local laws, if any.
- 204.01.15 Full particulars of prizes for each race.

204.01.16 Time and place of the prize giving.

204.01.17 Time limit and place for the handing in of protests.

204.01.18 Composition of the Race Committee and the Jury.

204.2 - DRIVERS MEETING

A drivers meeting must be held before the race.

It is mandatory for drivers to attend the drivers meeting(s).

At least one of the crew, or as described in the Race Instructions, shall attend the briefing(s), otherwise the crew shall not be allowed to start.

It is the drivers' responsibility to read the Race Instructions and have full knowledge of its content before attending the drivers meeting(s).

Any infringement to the rules specified in the Race Instructions may incur a penalty or disqualification.

The drivers briefing shall take place at least two hours (except in cases of force majeure, one hour) before the start of the race and shall be recorded on audio or video.

The briefing must cover :

- * name call
- * important items not mentioned in, or changes from Race Instructions which must be in writing
- * important local conditions and rules
- * special safety routines
- * weather forecast
- * telephone number of Race Secretary.

The members of the Jury and the Commissioner(s) should attend.

205 - SAFETY RULES

205.01 - NATIONAL AUTHORITY

Each National Authority must submit all drivers under its jurisdiction to an annual medical examination and where applicable, an immersion test annually.

Each National Authority must adopt a form to be used for the medical examination to which every driver who competes Internationally must be submitted every year.

Each National Authority must submit to a special examination any handicapped driver, any driver who has had a serious accident, or any driver who during two or three races has shown odd behaviour when driving his boat. This check is compulsory before participation in an International race and must be certified by a commission appointed by the National Authority of the interested driver. The commission will be composed of a well known medical specialist and two other experienced offshore racing administrators who are members of the National Authority. The commission may give or refuse authorisation as to the eligibility of the driver(s).

Each National Authority which organises an International race must abide by any recommendations of Cominsafe regarding the medical organisation.

205.02 - MEDICAL EXAMINATION

The medical Officer who is to be present at any race has the right to submit any driver entered to a physical or psychiatric examination, or an alcohol and/or drug test. These examinations/tests may be performed at any time during the event.

The result of such examination/test is immediately communicated to the Race Committee, who basing themselves on the report of the medical officer, may suspend any driver from the event.

205.02.01 - CONSUMPTION OF ALCOHOL

Competitors shall not be under the influence of alcohol while competing or practising.

A competitor shall, if requested by an authorised officer, submit to an alcohol test. Written notification of selection for testing will be handed to a competitor by an authorised official. Refusal or failure to do so may be taken as if a positive test had been ordered and dealt with accordingly.

Testing when carried out shall be using an Alcometer.

For International Races, any competitor found to have more than BAK 0,15 0/00 (0,15 g/L blood // AAK 0,075 mg/L air) 1 hour before and during testing or race shall be immediately suspended and disqualified from the whole event. A format report shall be forward to the N.A. of the competitor.

If in an organising country, the legal requirements of the above numbers are lower, then these numbers have to be announced in the advance regulations and prevail.

Any competitor found to have an alcohol level more than 50 % of that permitted by the Road Traffic Act governing domestic motoring regulations of the country which is organising the race shall be immediately suspended and disqualified from the whole event. A formal report shall be forwarded to the National Authority of the competitor.

If no limit is specified by the country organising the race in that country, then the level should be zero +. Note : This final figure of zero + is to be clarified by the U.I.M. Medical Commission which is responsible to the U.I.M. for these “specialist definitions”.

205.02.02 - ANTI-DOPING REGULATIONS

Anti-doping regulations based upon the world anti-doping code.

As per separate publication.

Aptitude assessment

205.04 - MEDICAL EVALUATION SHEET

Established at _____ The _____

Name _____ Surname _____

Born in _____ The _____

Address _____

Status _____ Profession _____

Date of first involvement in the sport _____

Other sports practised _____

PRESENT STATE OF HEALTH AND FORMER DISEASES

Disease _____

With special mention of

Cardiopathy

Coronary troubles

Diabetes

Epilepsy

High blood pressure

Operations undergone previously _____

Dates of these operations _____

Accidents (when, where, under which circumstances) _____

Nutrition _____ Tobacco _____

Digestion _____ Alcohol _____

Abdomen _____ Medicines taken _____

Signature of the interested person

General Recommendation for Class 1 superlicence holders concerning physical fitness

Cardiovascular training (endurance training) as jogging, cycling, rowing etc. twice a week - minimum 20 minutes without any interruption. Any sports activity which has "stand still" during the activity such as soccer, tennis or any ball sport is not suitable for the endurance training, but recommended as additional training.

Training to build up muscles for neck and back at least twice a week.

205.05 - INJURED DRIVERS

If a driver has an accident needing medical care, he must immediately inform his National Authority.

It is the responsibility of his N.A. to have the driver examined and verified recovered before the driver is allowed to compete in an International race.

If his N.A. does not manage to verify this (lack of time etc.), the N.A. must notify the organiser and the driver must be examined by the Medical Officer of that race.

205.06 - RACING VEST

Racing vests (for boats without canopies). Each person aboard a boat must wear, during the race and practise, an efficient racing vest.

The racing vest shall be of full jacket type, with collar and leg straps. All straps shall be at least 40 mm wide and have a minimum breaking strain of 500 kg.

The vest shall be at least 70 % orange or yellow, have lifting straps at the front or on the shoulders and have impact protection material covering the back.

The disposition of the buoyancy must be such as to ensure that an unconscious person will float face up in the water. The racing vest must have a minimum of 7.5 kg buoyancy.

The choice and efficiency of a racing vest is the sole responsibility of the wearer. Race organisers are required to repeat this important clause in the Advance Programme and the Race instructions.

For boats with canopies, refer to rule 508.26.

205.07 - PROTECTING HELMET

Any person aboard any boat partaking in races must wear an adequate helmet of orange colour throughout the race and during practise runs. Helmets may only be removed when effecting repairs and the boat is "off plane".

Each crew member's helmet must be identified with their boat race number. This number shall be placed on the top surface of the helmet and shall be read from the rear. The numbers shall have a minimum height of 7.5 cm and shall be of the same style as the boat race number. These numbers shall be painted in waterproof black paint or be made of black coloured self adhesive material of adequate strength.

The wearer is entirely responsible for the choice of his helmet. The organisers are requested to repeat this important rule in their particular rules, as well as in their Advance Programmes and Race Instructions.

Each National Authority may require that their nationals wear a helmet of a type laid down in their own National rules.

The use of neck collars is strongly recommended.

205.08 - SAFETY BOATS

Safety Boats in sufficient number must be standing by during official practise and for the duration of all races.

Each boat must carry :

- * a minimum of two persons who can enter the water and carry out rescue procedure
- * necessary signal flags
- * a large capacity fire extinguisher suitable for fuel fires
- * ropes suitable for towing and a boat hook.

All safety boats must have radio communication with race headquarters. A first aid station with qualified medical attendants and an ambulance must be stationed at a central on-shore location where injured drivers can be easily transferred from a safety boat to shore. It is recommended to have a medical doctor present, preferably qualified in handling trauma type accidents.

205.09 - INSURANCE

All drivers must be properly insured, either by themselves, with the policy to be produced before the start, or by any other insurance agency as required by the National Authority, the fee being paid by the driver.

The following insurance requirements are to be included in the Advance Programme :

- * Terms and conditions of an insurance for personal and material damage to third parties, and to other competitors if available.
- * The minimum limit of insurance cover required.
- * The type of risks to be covered by the insurance.
- * Insurances from other National Authorities are acceptable and must be written in the English language as well as in the language of the country where the boat is registered.
- * The competitors must have a racing third party insurance. The maximum amount of cover must not be inferior to that required by the laws of the countries where the races will be held.
- * If a team wish to use their own insurance, a copy of their policy must be sent to the relevant National Authorities at least 28 days before the race, so that the National Authority can consider whether the cover is acceptable. If the cover is acceptable, the local insurance will not be required to be taken out.
- * The amount of insurance fees to be paid at the race site if appropriate.
- * All insurances must cover the whole period of the practise testing and races from beginning to end of the event.

205.10 - TECHNICAL PROTECTION

It is forbidden to start a motor with the propeller rotating in the air.

After launching a boat, it is forbidden to start the motor(s) or the engine(s) with the boat elevated and the propeller(s) rotating.

205.11 - PROHIBITED SMOKING

All smoking of any type is prohibited in pit areas. Adequate signs will be posted to this effect and the rule will be rigorously applied by the pit officials.

205.12 - USE OF SAFETY EQUIPMENT

Whilst on the water at an event, all the relevant safety equipment, i.e., restraints, racing vests, helmets and racing suits must be worn whilst the race boat is on the plane.

205.13 - PROTECTIVE CLOTHING

All crew members whilst racing must wear suitable protective clothing that covers the torso and all limbs to the wrists and ankles.

Protective clothing used must be durable enough to provide bodily protection and it is recommended to be fire retardant.

205.14 - FUELLING

Any team who dispenses fuel, either in or out of the boat, other than in the designated fuelling area, or by prior approval of the Pit Officials, will be subject to a penalty as decided by the Race Committee.

206 - ASSISTANCE IN ACCIDENTS

It is mandatory for competing boats to stop and the drivers to provide assistance at the scene of an accident involving other competing boats, if no other suitably qualified rescue assistance is present.

Placing and championship points shall be awarded to any boat/driver that stops at the accident scene and provides assistance. Such placing shall be related to the last confirmed position it held prior to the time of stopping at the accident scene after deleting all non-finishers from the placing list.

However, except in extreme circumstances as decided by the Race Committee, the boat/driver that gives assistance must, when relieved of their duties by suitable rescue personnel, continue in the race until the completion.

Providing such competitor does finish at the completion of the race, at racing speed, the 70 % rule shall not be applied to that boat/driver.

Should such boat/driver improve its position, it will keep its better place.

Any prize money must be paid for the position awarded.

206.02 - HAND SIGNALS

The following hand signals are to be used by drivers and/or crew members at an accident scene to advise rescue personnel of their requirements.

HANDS CLASPED OVER HEAD

OK - No help needed at this time



THUMBS - UP

OK



BOTH HANDS WAVED OVER HEAD

Need immediate medical assistance



ONE HAND WAVED OVER HEAD

Need immediate tow or fire extinguishing assistance



NO SIGNAL

Immediate response by nearest personnel

ONE HAND RAISED WITH CLENCHED FIST WHILE ON PLANE

Caution - Debris or accident ahead !



Slowing or avoidance maneuver to follow.

All drivers, before being issued with their annual licence, must be tested on these signals.

208 - RESERVED NUMBER**209 - CRANE AREA**

When race boats are being craned in and out of the water prior to, during or after a race meeting, the boat shall be craned without any person on board. The arrangements for craning shall be so organised that on no account will the craned boats pass over the head of any person.

This area must be cordoned off and marshalled.

300 - RACING RULES

300.01 - DEFINITIONS

French	English	Italian	German
Course	Race	Gara	Rennen
Manche	Heat	Prove	Lauf
Tour	Lap	Giro	Runde
Circuit	Circuit	Circuito	Rundestrecke
Parcours	Dist.covered	Percorso	Rennstrecke
Base	Base	Base	Messstrecke

Circuit

A circuit is a closed course defined by buoys where races are to be held.

Distance covered

Distance covered is the total distance to be covered per heat or race.

Base

A base is a course expressly measured for record attempts.

Marks of the course

Any objects specially indicated as such in the race circular, are to be considered as marks of the course.

Obstacle

Any non specified object is considered as being an obstacle to free navigation.

Race

A race is a competition in speed between boats held in one or more heats.

301 - TIMING

In all International races and national championships the timing is carried out by officially appointed timekeepers using certified timing devices graduated to one tenth of a second. The devices must have a split second system one part of which must be capable of being stopped and restarted.

303 - MARKS OF THE COURSE

303.01 - DEFINITIONS

The Marks of the Course are any objects (boat, buoys...) that are specifically designated as such in the racing instructions. The objects that are not designated specifically as such are to be considered as obstacles.

An Official is posted near the turning marks. He must observe the turning of the marks and report to the Race Committee.

Should any mark be removed from its proper position either by accident or otherwise, the Race Committee shall, if possible, have it replaced. Should it be impossible to replace the mark in time for the boats to round it, the Race Committee shall decide whether the race is to be restarted or not.

The distance between the starting line and the first turning buoy must be at least one nautical mile. In class 2, in any start where there are ten or more boats starting together, the distance between the starting line and the first turning buoy must be at least three nautical miles.

303.02 - SPECTATOR AREAS

Race Organisers should designate spectator areas.

If a race boat finds itself in a designated spectator area, it must come off the plane immediately and proceed "off plane" with caution until back on the race course to avoid disqualification.

305 - PROCEDURE FOR U.I.M. OFFSHORE STARTS

305.01

A safe area shall be established for milling. In this area, boats must be off plane (bow down, no wake) and must turn in counter clockwise direction.

305.02

Distinct areas in which each starting group can mill (safe area) will be established. See diagram for area and location.

305.03

A control line on the race side of the milling area which is effectively where the start boat turns to pick up each class to start will be established.

305.04

The referee - wearing orange gloves and an orange vest (optional) over his life jacket on the start boat may use hand signals to control the fleet prior to the start.

305.05

The use of a starter boat is mandatory for all classes. There must be a safe interval between each class start with the start boat headed towards the start line to one side or the other of the chute.

305.06

The minimum width of starting chute is 280 metres with a minimum length of 1600 metres. The chute must be straight.

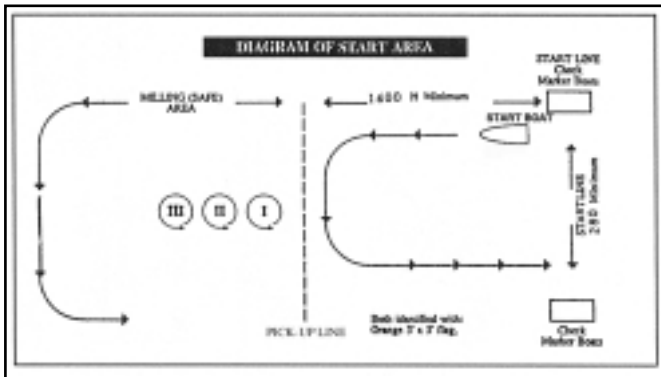
305.07

A single white smoke or rocket flare will be fired 2 minutes before the beginning of the starting run-up for the first class start.

305.08

Two minutes following the firing of the start signal flare, the start boat shall raise and continue to display a yellow flag whilst running across the “pick-up” line. When the start boat reaches the opposite side of the starting chute it shall turn and begin its run towards the start line. All race boats may then begin their run towards the start line, following the start boat, at the 30 metre minimum safety distance, until the raising of the green flag.

The minimum dimensions recommended for the yellow flag are 1.5 m x 1.0 m.



305.09

During the start procedure the race boats must run at the safety distance of at least 30 metres behind the start boat and must observe a safe spreading distance between boats.

305.10

The start boat should approach the start line at a speed which allows boats which have difficulty in getting on plane to catch up.

305.11

When the starter is satisfied that all boats are in a satisfactory line, at a satisfactory speed, he raises the green flag and lowers the yellow flag simultaneously. The raising of the green flag determines the actual start.

The minimum dimensions recommended for the green flag are 1.5 m x 1.0 m.

305.12

The race only starts when the green flag is raised. However, for time keepers, the official start is when the leading boat crosses the actual start line. The actual start line shall be announced in the racing instructions.

One, or two start line/check marker boats may be used in conjunction with other fixed or laid marks help define the actual start line.

305.13

If at any time during the start procedure after the white flare has been fired, a red flag is raised, the start has been aborted and all boats must return off plane to the milling area for restart or follow specific instructions of the start boat.

305.14

A disabled boat or one which cannot get on plane with its class, must not interfere with the start of the next class in any manner. The craft must go to one side of the chute and wait for a clear lane to commence their start. Should a boat fail to start after its respective class has started, it may start, in a normal fashion within a half hour (30 minutes) of its respective class time of start.

305.15

It is recommended that the start and/or pace boat personnel be limited to seven persons.

305.16

Penalties for infraction of the starting procedure are:

- a) Failing to respect a safety distance of 30 metres (305.05) : 3 minutes;
- b) Turning wrong direction (305.01) : 3 minutes;
- c) Interference with starting procedure : disqualification;
- d) Excess speed in the milling area, or faster than 10 knots : 10 minutes penalty. If the driver has caused danger to other boats, he may be disqualified;
- e) Should the bow of any boat be in front of the transom of the start boat when the green flag is raised : 10 minute penalty.

An outer-limit pace boat on the other end of the penalty line may be used to help define the actual penalty line from transom to transom.

305.17

Safety takes precedence over racing. Pilots should do nothing to endanger crew, spectators or other participants.

305.18

If, through lack of entries, it is desirable to start two or more starting groups together, this may be done provided it is fully covered in the racing instructions (and/or drivers briefing).

305.19

It is recommended that the same start boat starts each starting group and in which case the start boat, having started the first group should return on the diagonal to its original position at the opposite end on the control line and immediately repeat the starting procedure outlined in

305.08 for the next starting group until all the groups have been started. Alternatively, if separate start boats are used for secondary starting groups they should utilise the procedure described in Rule 305.08 (or 305.18) at time intervals fully described in racing instructions.

305.20

Should sea conditions in the milling area be such that boats ship water when turning while off plane, procedure will be modified as follows:

The start boat will lead race boats to the milling area at which point it will raise yellow flag slowly to allow race boats to get on the plane in safety before accelerating towards the start line. Race boats will follow the start boat at the thirty metres minimum safety distance, on the side and in the manner described in the racing instructions or at race briefing, until the raising of the green flag.

305.21

The start boat speed should be a minimum of thirty five knots ***for Class 2*** when the green flag is raised.

306 - QUALIFICATION

A timed qualification session may be held the day before the race. The qualification course must be from 10 to 15 miles, with at least one turn. There must be 2 minutes of interval between each timed running start.

The Race Committee or the Officer of the Day can decide to split the start of one class in case of more than 10 participants. Results of the qualification session will determine the starting grid. The second flight shall start no more than one minute behind the first start.

309 - RACE CANCELLED

The Race Committee has the power to cancel any race should unfavourable weather or other serious circumstances render such action necessary. A red flag raised indicates that such race has been cancelled.

310 - RACE POSTPONED

310.01

The Race Committee has the right to postpone a race, in which case the red flag must be raised from the start boat and/or a patrol boat. Race starts thus postponed may re-commence the starting procedure with the raising of the yellow flag as described in 305.08.

The Race Committee may postpone the start, even after the first signals have been given, but only in cases of force majeure or if an error in the starting signals has been made.

310.02

In both cases, signalling is to be recommenced. When the need for postponement has passed, the Race Committee signals the start according to the instructions contained in the programme.

New entries are not to be accepted for postponed races.

Only drivers who duly entered and were present when the race was postponed are admitted.

311 - RACE SHORTENED

A shortened race is a race over a course which has been shortened by the Race Committee before the start.

The decision with instructions about the shortened course must be handed in writing to the drivers and the timekeepers.

312 - RACE STOPPED

A red flag raised indicates the race has been stopped.

A stopped race is a race which has been interrupted by the Race Committee after the start.

Stopping the race is decided by the Race Committee for reason of force majeure of which it is the sole judge.

The Race Committee classes the boats according to their position at the time or place when and where the race was stopped.

312.01 - RACE CURTAILED

A curtailed race is a race shortened by the Race Committee after the start.

The signal to be used to announce it, is the waving of the chequered flag at the finish line.

The OOD or other responsible official can implement a curtailment decision at any time during the event, even in a multi-class race, provided the curtailment signal is broken out on the start/finish line. Once the curtailment signal has been broken out, all boats will stop racing at the end of their current lap.

Raising a retirement flag will not preclude a driver from getting finishing points that would otherwise be awarded if the race is curtailed.

Arriving at a finishing sequence - The first boat in each class, or subdivision, to be stopped by the curtailment signal becomes the "key" boat in that class. Then -

- I) All boats that subsequently take the curtailment signal and have then completed the same number of laps as the "key" boat are listed, in their finishing sequence behind the "key" boat.

- II) All boats that subsequently take the curtailment signal and have completed one less lap than their “key” boat are listed, in their finishing sequence, behind all the boats listed under (I) above.
- III) Boats that break down on their last lap shall be placed in the finishing order, in the sequence which they last crossed the start/finish line and in accordance with the number of laps which they actually completed.
- IV) All boats that subsequently take the curtailment signal and have completed one more lap than their “key” boat are listed, in their final finishing sequence, ahead of all boats listed under (I) and (II) above, and probably ahead of all boats listed under (III).
- V) Boats that break down on their last lap having completed less laps than any other finisher (under the above rules) shall be regarded as non-finishers.

313 - RULES OF THE ROAD

313.01

The International regulations for the prevention of collisions at sea, shall apply at all times.

313.02

Where by any of these rules one of the two boats is to give way, the other must keep her course and speed.

313.03

Every boat, which is directed by these rules to give way to another boat must, if the circumstance of the case admit, avoid crossing ahead of the other.

313.04

Every boat which is directed by these rules to give way to another boat must on approaching her, if necessary, slacken her speed, or stop or reverse.

313.05

Any boat overtaking any other must give way to the overtaken boat.

313.06

In obeying these rules, due regard must be made to all dangers of navigation and collision, and to any special circumstances which may render a departure from the above rules necessary in order to avoid immediate danger.

313.07 - OVERLAP

An overlap may only be established when two boats are on the same course or approximately on the same course and the overtaking boat has no longer a free choice on which side he shall pass.

The inside boat may gain the right of way by establishing an overlap even after the outside boat has altered her helm for the purpose of rounding the mark.

Once an overlap is established during the approach to or the rounding of a mark it cannot be broken even if the inside boat is clear astern.

An overlap exists when one boat is not clear astern of another.

A boat is clear astern of another when her hull is abaft a line projected abeam from the aftermost part of the other.

313.08 - PASSING OBSTACLES

Should an overlap exist between two or more boats when they are about to pass an obstacle, then the outside boats shall give the inside boats room to pass clear of the obstacle.

313.09 - PASSING MARKS

Should an overlap exist between two or more boats when they are about to pass a mark on the required side, then the outside boats shall give the inside boats room to pass clear of the mark, leaving it on the required side.

313.10 - ALTERING COURSE

When one of the boats is obliged to keep clear to avoid risk of fouling, the other shall not alter her course.

A boat must not alter her course so as to hinder another in passing to starboard.

313.11

When an overlap exists but neither boat is an overtaking boat, the one which has the other on her starboard side must give way.

314 - ROUNDING AND FOULING MARKS

314.01

Rounding marker buoys must be in the direction and on the side stipulated in the race instructions.

314.04

A driver who is forced on to a mark by another driver may lodge a protest against that driver.

315 - FOULING COMPETING BOATS

If a boat, in consequence of her neglect of any of these rules fouls another boat, or compels others to foul each other, she is disqualified.

316 - RUNNING AGROUND

Any boat, running aground, fouling a buoy, vessel or other obstruction, may use her own anchors, warps, spars or other gear to clear herself.

317 - ANCHORING DURING A RACE

A boat may anchor during a race but must weigh and recover her anchor again, not slip it.

318 - MEANS OF PROPULSION

All methods of propulsion other than motor, and paddle are prohibited.

The action of the wind and current will not entail the disqualification of the competitor.

319 - OUTSIDE ASSISTANCE

319.01

Unless specified in racing instructions, no outside assistance (including refuelling) is allowed during a race.

For all classes it is permitted to use electronic steering compasses and/or electronic position fixing equipment. It is expressly forbidden to interface any of this equipment to the steering system.

GYROS : All movable surfaces allowed on the boat shall be under the control of the onboard crew without any sensors in the control loop (i.e. no auto pilots, gyros, accelerometers or other attitude or acceleration sensors may be in the control loop).

The Race Committee has the right to decide whether there was outside assistance or not.

No changes to the boats systems may be made from outside races (i.e. no radio command links to adjust onboard parameters to engine management systems, either air/fuel ECUs or ignition ECUs).

320 - FINISHING A RACE

320.01 - THE FINISH

The finish of a race is signalled to the driver of the winning boat by waving a black and white flag at the finish line.

Where two or more classes are running together, a different flag may be used for each class.

The description of the flag used for signalling the finish of each class must be clearly specified in the Racing Instructions.

When the leading boat completes the course and crosses the finishing line, the chequered flag will be waved. It designates the “winner” of the race and the start of the finish procedure of the race.

Any boat crossing the finishing line after the “winning boat” will be classified considering its time and the number of laps completed, provided that it has covered the minimum number of laps announced at the race briefing.

When calculating the number of laps completed by a boat, its last lap completed (after the arrival of the winning boat) must not be slower than 50 % of its fastest average lap speed.

The race may be curtailed or stopped at the finish line at any time by waving the chequered flag. This designates the end of the race. To be designated a “finisher”, a minimum of 70% of the laps completed by the winning boat must be completed, rounded up or down to the nearest full lap.

320.02 - STOPPING THE RACE

In case of force majeure or accident, the Race Committee may stop the whole race, waving a red flag. This signal will be given from the turn

mark control boats. Additionally this signal may be given by other official safety boats.

320.03

A driver who finishes a race or heat must withdraw from the course without hindering the boats that are still in the race. This constitutes an exception to rule "Right of Way".

A driver who has finished that race and cuts across the course may be disqualified.

A vessel is timed as completing a race when her bow crosses the finishing line. After finishing the race, she must continue to observe any special regulations prescribed by the Race Committee as to keeping clear of the finishing line and the course.

320.04

The Race Committee has the right to stipulate the maximum duration of each race.

321 - SCORING SYSTEM

321.01

Unless otherwise specified, the following scoring system is to be adopted:

1 - 400 pts	11 - 22 pts
2 - 300 pts	12 - 17 pts
3 - 225 pts	13 - 13 pts
4 - 169 pts	14 - 9 pts
5 - 127 pts	15 - 7 pts
6 - 95 pts	16 - 5 pts
7 - 71 pts	17 - 4 pts
8 - 53 pts	18 - 3 pts
9 - 40 pts	19 - 2 pts
10 - 30 pts	20 - 1 pts

321.03

The official results must be posted on the notice board within an hour of the end of the races.

321.05

National class boats may race at International meetings.

321.06

If boats of different series or classes start together every boat must be classified within its own class.

323 - POSTING OF THE RESULTS

The results of each race must be posted in a clearly visible position, specified during the driver's meeting.

The officer in charge must sign the results and state the time of posting.

324 - INSPECTION OF HULLS AND ENGINES

324.01

Boats, and engines taking part in a race which are subject to checking, must not leave the boat park until one hour after the positing of the results.

324.02

The Technical Officers may postpone the inspection and stipulate the time limit and place where the engines must be available.

324.03

In the meantime, the motor must remain sealed to prevent any alterations being made.

Refusing to produce the motor or the hull entails disqualification.

324.04

It is compulsory for a driver to make arrangements for tools, special tools and mechanics so that his motor can be disassembled if the Technical Officers at a post race inspection want to check any part of the motor. Both the driver and/or the mechanics shall work as efficiently as possible so that inspection duration can be kept to a minimum.

Motors and boats will be placed pending inspection in a well-delimited place, which remains under the care of the “ boat park “ Official. This area is called the “ park fermé “.

324.05

After the pre-race technical inspection, the boats must not leave the boat park without permission except for practising or racing.

324.06

If any repairs can only be effected outside the boat park, they must be carried out in the presence or with the permission of a Technical Officer.

324.07

Officers allowed to attend the inspection:

- The Officer of the Day.
- The delegate of the interested National Authority
- The measurers named by the organising committee
- The driver and a mechanic if necessary
- The U.I.M. Commissioner

The presence of other persons is only allowed if authorised by the Race Committee.

326 - AFTER THE EVENT (REPORTS)

326.01

At the end of the event, the Secretary of the Race Committee must send to the Offshore Commission of his National Authority a report including:

- 326.011 Classification and distribution of prizes (minutes of the jury)
- 326.012 Report of the timekeepers
- 326.013 List of drivers
- 326.014 Penalties
- 326.015 Documents on any records beaten in conformity with the U.I.M. rules
- 326.016 List of protests and decisions relating to these protests.
- 326.017 All necessary documents in order to evaluate the event or the race.
- 326.018 These documents must be sent to the Offshore Commission of the National Authority within 8 days of the end of the event or the race.
- 326.019 After each titled event the organisers must send the results to the Secretariat of the U.I.M.
- 326.020 If the results are taken into consideration for the granting of a trophy or a prize, the points must be sent to the U.I.M. by telefax within 48 hours.
- 326.021 days after a competition, the organising club must send the final result to the National Authorities of all the participants and to the Secretariat of the Union.

400 - JURISDICTION

401.01 - DEFINITIONS

Jurisdiction applies to :

The U.I.M. Offshore, Circuit, Aquabike Rules.

402 - JUDICIAL BODIES

402.01 - THE INTERNATIONAL JURY

The Jury hears any protest raised by a driver.

1. Composition

The Jury must be composed of at least three members, the Chairman included. The Organisers of the event appoint the Chairman. Each competing country, including the hosting country, may appoint one member delegate who can represent only one country. These appointments must be made in writing, authorised and signed by a National Authority Official and delivered to the event Secretary.

If there are not enough N.A. appointed members to form a Jury of three persons, the Organisers must appoint additional members, but only enough to reach three persons.

The Jury composition will be announced at the first drivers meeting. The jury must be present at the first drivers meeting.

Race Officials can not be Jury members.

The U.I.M. Commissioner appointed to the event is a full member of the Jury.

In an event comprising races of several classes there may be a different Jury for each class.

A U.I.M. registered drivers' representative is allowed to attend any International Jury Meeting only concerning his relative class. He has no vote.

2. Authority and Duties

The Jury sits as long as the event lasts, and hears all protests relating to that event.

The jury must decide as soon as possible on all protests presented to it during the event.

402.02 - INTERESTED PARTIES

No member of a Jury who has taken part as a competitor or Official in the race concerning which a judgement is to be given, or knows himself to have directly or indirectly an interest in one of the parties involved, may judge the protest.

However, a member of an International Jury shall not be disqualified even if one of the parties is from his N.A.

402.03 - THE U.I.M. APPEALS BOARD

The U.I.M. will decide on Appeals arising from International events according to the provisions laid out in Rule 405.1.

In each case, the U.I.M. Executive Committee, or in case of an appeal lodged against an Executive Committee's decision, the appointed neutral Council member will nominate three members, one to be Chairman after consultation of the relevant commission. The members must not be from the National Authorities of the parties to the case.

One member will act as Clerk to the Board.

403 - PROTEST PROCEDURES

403.01 - VALIDITY OF PROTESTS

Only a driver may protest against facts pertaining only to the races in which he takes part.

Joint protests signed by several drivers will not be considered.

All protests must be in writing - type written or hand written in printed characters in English.

It must state the reason for the protest and be accompanied by any relevant documents that shall provide evidence within the given time and any protest fee as stipulated by the Race Organiser.

All protests must be signed by the protester.

The protest must be handed to the Race Secretariat who must, in the presence of the protester, record the time of receipt.

Any costs involved in verification are to be borne by the losing party.

A written protest can be withdrawn by the protester ; he then loses the protest fee.

403.02 - PROTEST FEE

The amount of the Protest fee must be decided by the National Authority and it must be published in the advance programme. The maximum amount for a Protest fee will be decided by the U.I.M. General Assembly. It will be 80 € or an equivalent sum in the local currency.

The Jury must determine whether the protest presented to it, has been lodged according to the rules. If found to be invalid, the reasons must be given to the protester.

If a protest is upheld by the Jury, the fee must be returned to the protester.

403.03 - RIGHT OF PROTEST

A protest can be lodged against the posted provisional results or against any decision made by the Race Committee or against one or several competitors.

A protest can only be lodged on a subject which directly concerns the protester himself. A protest which complies with these rules can not be refused by the Jury.

403.04 - TIME OF LODGING A PROTEST/ PROTEST PERIOD

A protest regarding the qualification of a boat, a motor, or a driver must be made before the drivers briefing. Starting in a race is considered as acceptance of the conditions and the eligibility of the other competitors. The only exception to the above is when the protester can prove the facts were not given to him before the drivers briefing, then a protest can be lodged up to one hour after the posting of the results.

Any other protest must be lodged within one hour of the first results being posted with the following exceptions:

A protest against new results can be lodged within one hour of these new results being posted, but only concerning the decision, which lead to the changing of the results.

A protest against any other decision, posted after the posting of the first results can be lodged within one hour after the posting of this decision.

403.05 - JUDGEMENT

Decisions by the Jury shall be reached by simple majority of votes. In the case of equal votes, the Chairman's vote will be the casting vote.

Any judgement pronounced by the Jury stays in effect until altered by the Appeal Board.

403.06 - NOTIFICATION OF DECISION

All decisions by any jury must be notified to the concerned parties in writing, including their right of appeal.

403.07 - AVAILABILITY OF DOCUMENTS

All documents relevant to any decision by the Jury must be filed with the organising N.A. and held available for a higher authority until the time limit for an appeal has expired.

404 - HEARING OF THE PROTEST

404.01 - RIGHT TO A HEARING

Any person being party to a protest, or being charged for an offence against the rules shall have the right to be heard before the Jury in order to defend himself.

The Jury must give notice to all parties of where and when the hearing will take place.

If any party duly notified, fails to appear without giving an acceptable reason, judgement can be rendered by default.

404.02 - WITNESSES

Each party may bring witnesses to the hearing.

Any costs incurred by the appearance of witnesses shall be borne by the respective parties unless decided otherwise by the Jury.

404.03 - THE HEARING PROCEDURE

The following procedure must be followed in all hearings unless otherwise stated elsewhere in these rules :

- Written minutes must be taken.
- All parties to the case are entitled to be present and to hear and question all evidence at the hearing up to the time the Jury makes its decision. The protest, shall be read out to the parties.
- The parties shall be invited to state their cases. The parties may call witnesses. Each witness, after having given his account of the case, may be questioned by all parties and by the members of the Jury.
- Any member of the Jury who is familiar with the case may give his evidence
- The parties shall be entitled to question each other and any member of the Jury who may have given evidence.

- The witnesses shall withdraw and the parties shall be invited to make a final statement of their cases.
- The Jury may recall any party, previous witness or new witness and call on other evidence.
- The parties must be present during the whole of the recall and must be given the opportunity to question any new evidence after which they may re-make their final statements.

405 - THE APPEALS PROCEDURE

405.01 - APPEAL BOARD

The members of the Appeal Board may hold other sports functions and consist of a Chairman and two members.

The Appeal Board sits whenever the Chairman calls a meeting.

405.02 - INTERESTED PARTIES

No member of an Appeal Board who has taken part as a competitor or Official in the race concerning which a judgement is to be given, or knows himself to have directly or indirectly an interest in one of the parties involved, may judge the appeal.

405.03 - RIGHT OF APPEAL

A decision by a Jury may be appealed by the parties involved when they consider that an injustice has been made against them and/or fresh evidence has been made available that may alter the decision of the Jury.

A driver can also appeal against :

- a penalty, imposed by the U.I.M. Executive Committee, or imposed by the OOD for the reason of post race technical scrutinising

- new results, but only concerning the decisions, which lead to the changing of the results,

without the prior lodging of a protest, but only if this was not possible, because the race jury has had dissolved itself before or within the protest period after the finish of the event.

405.04 - NOTICE OF INTENTION TO APPEAL

The intention to appeal must be notified in writing to the Secretariat of the U.I.M. by the interested party within four days of the day following the Jury decision or in the case of a decision against which a driver can appeal without the prior lodging of a protest within four days of the day following the notification of this decision to the NA of the driver or if earlier to the driver himself.

405.05 - TIME LIMIT

All appeal documents and fees must be received within ten days from the date when the notice to appeal was received by the U.I.M. Secretariat.

405.06 - LODGING OF AN APPEAL

The appeal must be sent by telefax, letter or email and it must be signed by the appellant. The reasons for the appeal must be stated.

Address, telephone, email and/or fax should be clearly mentioned on the appeal, so that the U.I.M. can send any correspondence directly to the driver with copy to relevant N.A.

405.07 - NOTIFICATION OF THE PARTIES OF THE APPEAL

The U.I.M. Secretariat must within two days from receiving the appeal, notify by telefax or letter the other parties that an appeal has been lodged and that they may send a rejoinder.

Eventual rejoinders must be received within ten days from receiving this notification.

405.08 - TIME LIMIT FOR DECISIONS ON APPEALS

All appeals must be determined no later than forty-five days after the date when the appeal documents and fees were received by the U.I.M. Secretariat.

405.09 - APPEAL BOARD DECISIONS

The Appeal Board takes its own decision based on every information available. The decision of the jury may be upheld, changed or not upheld. The decision of the Appeal Board is final.

405.10 - COSTS

The appeal fee is 1525 €. In addition the driver has to pay 2290 €. to the U.I.M. for possible costs of the appeal committee (meetings, telephone or fax costs). The appeal fee for a single championship/invitation race (not a Formula race, or a WPC race or similar) is 760 €, in addition the driver has to pay 760 €, to the U.I.M. for possible costs of the appeal committee.

The driver who appeals has to pay for the meeting costs anyway if the appeal is upheld or not. If the costs of the appeal committee is less than 2290 € then the difference will be paid back to the driver.

The appeal fee of 1525 € will be refunded if the appeal is upheld. If the original decision is changed by the appeal board (405.09), it can decide to refund a percentage of the appeal fee if appropriate.

The driver may agree to forego an Appeal Committee meeting and for the appeal to be handled through telephone, email, and fax, if possible, in order to minimise the cost for the appeal.

406 - PENALTIES

406.01 - GENERAL

Any proven breach of applicable Rules may be penalised.

If the rules do not determine a special penalty for the relevant breach, the penalty to be given has to be proportional to the seriousness of the breach. The proportionality is at the discretion of the decision maker.

The U.I.M. Executive Committee or the relevant National Authority shall only impose penalties, when they deem the relevant infringement to be so serious, that it has to be penalised although the deadlines for penalising or lodging a protest have already expired. After the expiry of the aforementioned deadlines, a penalty for an ordinary breach of the rules shall in general not be imposed.

The Race Jury or the Appeal Board following a protest or an appeal can (only) impose penalties, which can be imposed by the OOD.

406.02 - DEADLINES FOR PENALISING

The OOD and the U.I.M. Commissioner can only impose Penalties until the moment, the first results of the race are posted.

The National Authority and/or the U.I.M. Executive Committee can only impose penalties within 3 months from the day, the infringement occurred.

These deadlines are not valid for penalties imposed by the OOD for the reason of post race technical inspection. In this case, the penalty has to be imposed as soon as possible after the receipt of the inspection results.

406.03 - UNACCEPTABLE BEHAVIOUR

In order to protect the interests of the sporting community, the following actions may also be penalised by the OOD, by the U.I.M. Executive Committee or by the N.A. of the Licence Holder.

- any deliberate act taken to gain unfair advantage.
- any false act made or statement given with the intention of suppressing facts required for the proper conduct of the race.
- any attempt to bribe or the taking of a bribe.
- any abusive or unsportsmanlike behaviour.

406.04 - REPRIMAND

A reprimand is a notice of disapproval of an unacceptable action. It must be recorded by the Race Secretariat. A reprimand must be witnessed. A reprimand automatically constitutes warning that if the offence recurs, a heavier penalty will be given. A Reprimand is valid for 12 Months.

A reprimand can be given by the OOD, or the National Authority of the Licence Holder.

406.05 - YELLOW CARD

A yellow card may be given by the OOD or U.I.M. Commissioner for behaviour on the water.

A yellow card will be confirmed on a form, which will be posted with the results. The pilot has 60 minutes to protest to the Jury. If the protest is not accepted, he can make an appeal. Protests and appeals can be lodged according to U.I.M. rules.

A card is valid for 12 months. A driver who has been given three yellow cards is excluded from all races run under U.I.M. rules for a period of two calendared racing months in the current or following season.

Yellow cards must be notified to the U.I.M. and National Authority of the driver concerned within three days.

The U.I.M. will notify all National Authorities if a driver obtains three yellow cards and is suspended from International competition.

All yellow cards including reason for the card given must be mentioned in the U.I.M. commissioner's report.

406.06 - DISQUALIFICATION

Disqualification deletes a competitor from the results of the heat or race where the offence occurred.

Disqualification is done by the OOD.

406.07 - TEMPORARY SUSPENSION

Temporary suspension suspends a competitor, a competitor together with crew, a crew member, or an Official from all or part of an event. Temporary suspension may be given for serious indiscipline.

Temporary suspension will be imposed by the OOD.

406.08 - PROLONGED SUSPENSION

Prolonged suspension can be imposed on a competitor, an Official or an organisation for deliberate fraud, repeated or very serious indiscipline or very serious misconduct.

Prolonged suspension can be imposed only by the N.A. of the licence holder or organisation, or by the U.I.M. Executive Committee.

A Prolonged suspension is always International. The N.A. must immediately inform the U.I.M. of a Prolonged Suspension and the U.I.M. must inform all other N.A.'s.

406.09 - EXCLUSION

Exclusion means a permanent loss of all rights to take part in any activities falling under the U.I.M. and its affiliated organisations. A person or an organisation who has committed a moral or sporting offence of extreme gravity is liable to be excluded.

A sentence of exclusion can be pronounced only by the N.A. of the licence holder or organisation, or by the U.I.M. Executive Committee.

Exclusion is always International. The N.A. must immediately inform the U.I.M. when it has imposed a sentence of Exclusion and the U.I.M. must inform all other N.A.'s.

In the case of suspension, prolonged suspension or exclusion, all National Authorities and therefore their affiliated clubs bind themselves to respect the decision and apply it in their own country.

407 - PRIORITY OF THESE RULES

The course of the U.I.M. has to be exhausted, before the dispute can be brought before civil court.

408 - POWERBOAT PROTEST FORM

PART A

Is for you, the driver, to complete and hand in within the specified time limit.

PART B

Is for you, the driver, to complete and give to the Protest Committee Chairman at the beginning of the Protest Meeting.

PART C

is for the Protest Committee to complete.

DRIVERS PROTEST FORM

PART A

SECTION 1

NAME OF EVENT: _____ DATE OF RACE OR HEAT: _____

HEAT NUMBER: _____ CLASS: _____

SECTION 2

YOUR NAME (PROTESTOR) _____

YOUR BOAT NUMBER: _____ BOAT NAME: _____

SECTION 3

I (the Protestor) am protesting against: (tick ✓ as appropriate)

☐ To seek redress from the Race Organising Committee for actions or omissions.

☐ Another driver, name: _____ Boat No: _____

☐ The Results as posted

SECTION 4

Which rule has been infringed _____

Time of incident: _____

On which lap was the incident _____

Where was the incident _____

SECTION 5

Your signature _____

Time of signature _____

SECTION 6

You must see this section completed by the official receiving this Protest:

500 - TECHNICAL RULES, CLASSES

500.01 - GENERAL

The 500 group rules contain general technical information.

For specific class rules see:

- Offshore General Rules RULE 701 - 715
- **Offshore 1** ***RULE 860.43 – 860.55***
- **Offshore 2** ***RULE 720 - 721***
- Offshore 3 RULE 730 - 737
- O.C.R. RULE 900
- V24 Class RULE 950
- Crews Safety RULE 508

By word “boat” it is understood to mean any vessel used in powerboat racing.

When a matter is strongly recommended, the driver (pilot) choosing a different solution may be asked to explain to the measurer or scrutineer the advantages of his solution.

500.02 - INTERNATIONAL OFFSHORE CLASSES

The request to create any new International class has to be presented by at least three Nations.

All classes for which the rules have been drawn up by U.I.M. are International.

All National bodies are bound to observe these rules without any alteration whatsoever. Only the U.I.M. Cominoff has the right to change the rules. Rule changes must be ratified by U.I.M. General Assembly.

500.03 - RECOGNISED CLASSES

All national classes for which the rules have been properly established by a National Authority are recognised by U.I.M.

501 - MEASUREMENT CERTIFICATE

A boat is not allowed to take part in a local, National or International race without a measurement certificate, issued by a National Authority, made up in the native language and in English according to the official U.I.M. model. ***The certificate is valid for one year. It can be renewed without re-measuring, provided the rules for the relevant class have not been changed.***

501.01

Each National Authority stipulates the cost of measurement and the duration of the validity of the certificate.

The request to obtain a measurement certificate must be accompanied by the homologation file of the motor, if such is requested for the devices being used.

UIM MEASUREMENT CERTIFICATE

NAME OF NA _____

NA ADDRESS _____

NA TELEPHONE NO _____ NA FAX NO _____

OWNER	RACE NO
ADDRESS	CLASS
	BOAT NAME
	UIM REG. NO
CLUB	NAME OF MEASURER
NOTES	DATE MEASURED
	DATE OF ISSUE
	DATE OF RENEWAL
	DATE OF EXPIRY

HULL

ENGINE(S)

BUILDER	MANUFACTURER
DESIGNER	TYPE YEAR
YEAR BUILT	NO. OF ENGINES
TYPE : MONO/MULTI	HOMOLOGATION NO
MATERIAL	
L.O.A. (M)	ADDITIONAL CLASS
BEAM (M)	RACE NO
MIN. RULE WEIGHT (KG)	ALTERNATIVE ENGINE(S)
RESTRAINTS : YES/NO	MANUFACTURER
CLOSED / OPEN CANOPY	TYPE YEAR
NO. OF CREW	NO. OF ENGINES
CREW CONFIGURATION	HOMOLOGATION NO.
FLOTATION SYSTEM	
DIRECTIONAL CONTROL SYSTEM	NA STAMP
DRIVE SYSTEM	
	CERT. AUTHORISED BY

DECLARATION BY OWNER : (Owner's Signature _____)

(I agree to be bound by the rules of the UIM and my National Authority in respect of boat measurement. I understand that non-compliance with any of the rules or regulations could result in disciplinary action by the UIM/National Authority).

NOTES FOR SCRUTINEER/MEASURER

DATE	
VENUE	

(Sign)

DEFECTS/REPAIRS CLEARED - DATE
SIGNATURE
PRINT NAME

DATE	
VENUE	

(Sign)

DEFECTS/REPAIRS CLEARED - DATE
SIGNATURE
PRINT NAME

DATE	
VENUE	

(Sign)

DEFECTS/REPAIRS CLEARED - DATE
SIGNATURE
PRINT NAME

DATE	
VENUE	

(Sign)

DEFECTS/REPAIRS CLEARED - DATE
SIGNATURE
PRINT NAME

DATE	
VENUE	

(Sign)

DEFECTS/REPAIRS CLEARED - DATE
SIGNATURE
PRINT NAME

DESIGNER'S DECLARATION

1. CRAFT NAME		RACE NUMBER	
2. CLASS		UIM MEASUREMENT NO.	
3. DESIGNER		4. DRIVER/CLIENT	
ADDRESS		ADDRESS	
TELEPHONE		TELEPHONE	
5. DESIGNER'S REF. NO		DATE DESIGNED	
6. DESIGNER'S DOCUMENTATION REFERENCES			
OWNER'S OPERATING DOCUMENTATION			
SAFETY INSTRUCTIONS (ATTACHED)			
MAINTENANCE INSTRUCTIONS			
Drawings to comply with UIM Offshore Rules (attached)			
Required as evidence of build standard; receipt does not imply approval of design)			
7. BUILDER		DATE COMPLETED	
ADDRESS			
TELEPHONE			
8. ENGINE(S)			
MANUFACTURER	MODEL	YEAR	

DECLARATION

I certify that the above craft has been designed (and built*) to comply with the UIM Rules appropriate to the Class, to be driven/by a qualified licence holder/only by the driver's specified below**. This Declaration is conditional on the (state number) supplementary qualifications/limitation below (it is the Designer's option to draft these qualifications).

Further, it is conditional that the craft is properly maintained to Schedule. Any alternation to the Craft and/or Declaration must be in writing and requires the Designer's prior approval and endorsement, added to this Declaration.

Designer _____ Date _____

Accepted by Registered Driver _____ Date _____

* This assumes a Designer Survey; if not, a Builder's Signature clause is also needed against the Designer's Documentation in (6) above.

** There is a real possibility of this type of clause (important for resale).

501.02

Any owner who has made alterations affecting the measurements in the certificate must notify his National Authority at once. Non-observance of this rule invalidates the certificate. The National Authority re-measures the boat at the owner's expense. Any boat that has been altered but not re-measured is not allowed to race.

The certificate will become invalidated on change of ownership.

501.03

The serial number of the motors or engines may be omitted in the measuring certificate.

501.04

The model of the International measuring certificate as described hereafter must be adopted and used by all National Authorities of the U.I.M.

The certificate contains the following data in English and in the language of the country issuing the certificate.

501.05

No blank certificate may be issued by National Authorities. Documents must be completely filled in with all particulars pertaining to the series for which the boat is measured. The certificate must also be signed and stamped by the National Authority.

501.06

Drivers need only supply, and measurers need only check such particulars as are applicable to the class of boat.

501.07

Anyone involved in the building of the hull or motor is not to act as measurer or to be a member of the Measurer's Committee as measurer where they have a vested interest.

501.08

All measurements are to be taken at least twice. All measurements of length are to be expressed to the millimetre.

501.09

The owner of a vessel pays all fees and expenses for rating or measuring his boat.

The measurement certificate is only to be handed over when all dues have been settled by the owner to the National Authority.

501.10

When a re-measurement is made at the request of the National Authority the expenses and fees for such re-measurement are paid by the National Authority if the first measurement is upheld.

501.11 - HULL MEASUREMENT

All hull measurements are to be taken while the boat is ashore.

The length must be measured between perpendiculars of the external moulded length of the boat, taken at the extreme bow and the rear most planning surface (the rear most part of the keel) including all hull planning steps (regardless of height), but excluding trim tabs, either fixed or movable.

Hull extensions beyond the transom shall be deemed to be "fixed trim tabs" and shall not be included in the measured length.

Hull extensions added to the bow and/or any extending parts, rubbing strakes, fenders, outboard motor spacing brackets, stabilising trim tabs and rudder assemblies are not to be included in the measured length.

When defining “extreme bow”, any form that constitutes the bow and its construction and contributes to the performance of the boat shall be included in the measured length. Any other attachment that is added to meet a minimum length requirement shall not be included in the measured length.

Should the trailing edge of the planning surface of a hull or sponson terminate with an angle greater than 15° measured between the keel and outer chine in plan view (birds eye view), such excess shall not be included in the measured length.

The beam measurement shall be taken at the widest part of the moulded hull, excluding any extending parts, rubbing strakes and/or fenders.

Trim tabs, either fixed or moveable, shall not exceed a length of 10% of the measured length of the hull.

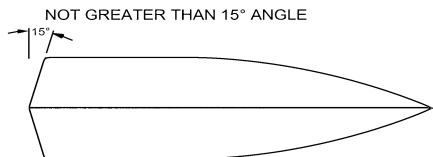
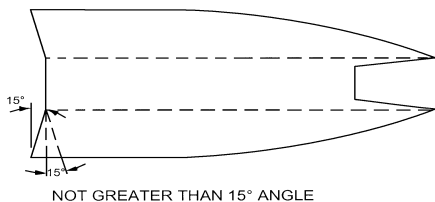
Multihull tunnel flaps shall not exceed a length of 10% of the measured length of the hull.

For Rigid Inflatable Boats (RIBs), the length measurement shall be taken at the extreme of the bow tubing (or rigid hull, whichever is the longer), to the rear most planning surface, excluding any trim tabs as for moulded hulls.

Tube extensions on RIBs extending beyond the rear most planning surfaces shall not be included in the measured length.

The RIB beam measurement shall be taken at the widest part of the inflatable tubes when fully inflated.

.. See drawings on page 276 of the U.I.M. Offshore Rule Book plus additional clarification drawing below.

TRANSOM MEASUREMENT CLARIFICATION**MONO HULL IN PLAN VIEW****CATAMARAN IN PLAN VIEW****501.12 - FLOTATION**

Description of the flotation (type, place(s) and volume) should be presented in the measurement certificate.

The owner may not change his flotation gear without having the corresponding changes noted in the measurement certificate, by an official measurer. However, additions to the certified flotation are allowed.

502 - INSPECTIONS

502.01.01

Hulls, motors, fuel, accessories and equipment, subject to restrictions regarding dimensions or other characteristics, must be submitted for verification.

At every race meeting the boats must be inspected before the race and practice due to safety reasons.

502.01.02

Drivers are at all times responsible for the condition of their boat (hull, motors, accessories, equipment, etc.)

502.01.03

Errors, if any, on the part of the manufacturer, builder, mechanic or even the previous owner do not justify in any way non-conformity with the rules.

502.01.04

Any driver refusing to comply with the decisions of the Technical Inspectors or who does not comply with the conditions of the rules, is not allowed to take part in a race or, should he have raced, cannot be classified and penalties may be applied.

502.01.05

Technical inspectors have the right, once races are over, to carry out all the checks they consider necessary, even when inspection has taken place before practice, they have the right to inspect as they think fit.

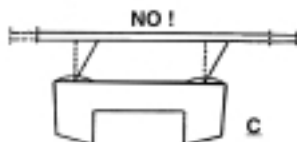
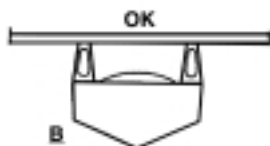
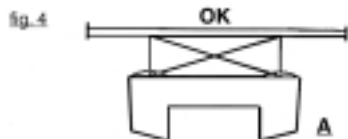
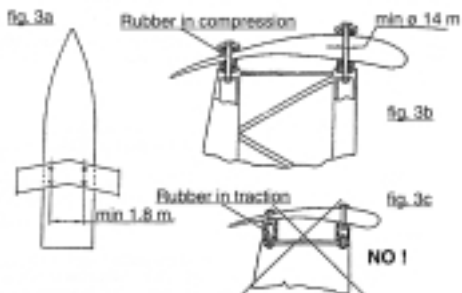
502.02- WINGS

A wing is defined as a device above the deck that has a downward plus or minus 60 degree lower surface, with an area in excess of 0.1 square metres (1.08 sq. ft).

The use of any wing, whether for lift, down force, or stabilisation is permitted, provided that it is fitted to good standards of workmanship. However, the following rules apply:

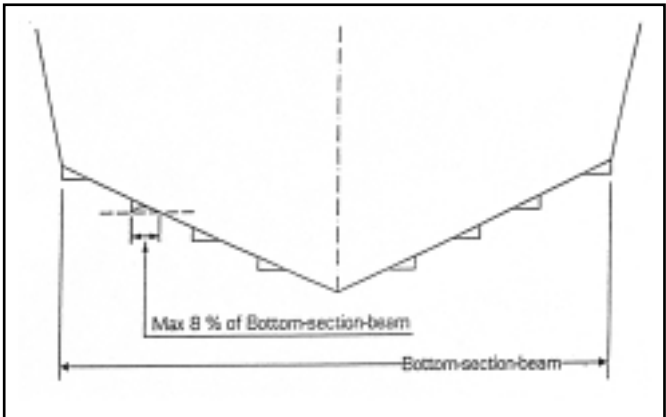
1. The width of the wing cannot be more than 1.5 times the width of the hull for single hull craft and more than the width of the boat for multi hulls.
2. The wings must be strong enough to bear a man's weight (100 kg) at all points on it up to 1.2 m from the ends.
3. The wing should be fixed to the hull at least at four points in groups of two, spaced no less than 1.8 m apart with steel bolts of a diameter of at least 14 mm (Fig.3.A). The use of shock absorbers, which work under compression only, is recommended (Fig.3.B). A section smaller than that of the bolts must not exist at any point in the support.
4. A suitable transverse arrangement must be provided to make the fixing of the quadrilateral hyper static (Fig. 4 A.B.C.).
5. Loss of a wing means disqualification from the race concerned and re-use of the wing is subject to technical documentary evidence that structural strengthening has been carried out.
6. The driver is entitled to decide whether or not to use a wing in case of each individual race, but the existence of the wing must be noted on the certificate of weight and the fitting procedure must be checked by the weight measurer.
7. The wing must not be ahead of the crew or the centre of gravity of the boat.
8. No trimable aerodynamic surfaces are allowed on the wing.
9. Wings of other designs than described in this rule; Drawings must be sent to Cominoff for approval before they are allowed to be used.

10. Wings are not allowed in Class 3, OCR or V Class.

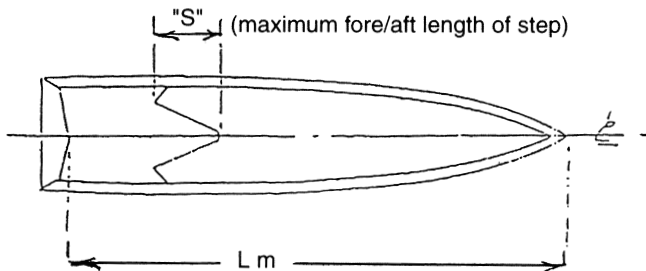


502.03 - MONOHULL DEFINITION

1. A monohull should be a boat with one hull.
2. A minimum distance of 80% of the "Measured Length" should, in the centreline, be the deepest part of the hull.
3. Transverse sections should, from the centreline, have positive angles up to the sections maximum beam, except in the following two cases:
 - (i) Each spray deflector may have a concave surface with a horizontal width from the hull to the outside of the spray deflector of no more than 8% of the hull's bottom-section-beam (see drawing).



- (ii) Each spray deflector must not have a total downward angle exceeding 20 degrees.
- (iii) Steps which are swept aft may have a distance "S" of no more than 25 % of "Measured Length" (see drawing)



502.04 - ENGINES

502.04.01

The total cylinder capacity of the motor or motors forms the basis for eligibility in the corresponding classes.

The engines may be of the two-stroke or the four-stroke types.

Fuel may be fed to the engine by a carburettor or by injection.

502.04.02

Rotary motors of the type covered by the NSU-Wankel patent are admitted on the basis of an equivalent cylinder capacity.

The rotary motor cylinder capacity equivalent is twice the volume determined by the difference between the greatest volume and the least volume of the working chamber.

502.04.03

Verification of motors are carried out according to the facts contained in the homologation file.

At World records runs, World Championships and Continental Championships, inspection of the winning motor(s) is compulsory.

Inspections may also be carried out whenever necessary at all International and National events.

The following procedure is mandatory for these inspections:

1. Check that the motor type code and visual appearance conforms to the motor homologation sheet.
2. The measurements stated in the homologation sheet must be checked on the motor. If the results are within the specified tolerances, the motor should be accepted as legal. Only adjustments to meet dimensions shown in the homologation sheet are allowed.
3. When ports in cylinders are adjusted to the dimensions specified in the homologation sheet, material may only be removed in the specified opening to a depth of 10 mm, to match the shape of the original adjacent connecting passage (channel) outside the adjusted port opening.
4. Parts not specified with measurements in the homologation sheet can only be checked by comparison with standard parts. As the actual manufacturing tolerances are not published, small differences between the measurements of the inspected part and the reference standard part must be accepted.
5. Cylinder dimensions are measured when the motor is cold. Allowance is made for the carbon and oil present in the combustion chambers. Checking is done with oil. A large scale graduated cylinder or burette with adequate orifice is to be used.

503 - EQUIPMENT

503.01

During a race, the boat must have onboard all the equipment required by the Race Committee in the detailed instructions contained in the advance programme.

503.02

All attachments, handholds, steering gear, pulleys, fair leads, cleats and anchor bits, etc., must be bolted to the hull.

503.03

Hatches and covers are to remain closed during racing, except for temporary checks.

504 - FUEL

N.B. Although International standards exist for petrol and for diesel, there remains a huge imbalance of specification from one country to another regarding the specification of 'pump' fuel available.

Due to these circumstances no specification of fuel is illustrated in the following rules.

Organisers are encouraged to supply a common fuel for use at International events in an effort to maintain equal opportunity for all competitors.

504.01

The only fuel to be used by any offshore powerboat must have the same characteristics as the fuel commercially available to the general public as dispensed from roadside pumps or marine fuelling facilities.

504.02

For petrol engines, the fuel used shall be unleaded petrol.

No additives are permitted except lubricating oil for two stroke engines, providing that such oil does not increase the octane or the water content of the fuel or enhance the fuel by any other means.

504.03

For diesel engines, the fuel shall be that intended for use in diesel engined road vehicles or automotive based marine engines (including red diesel).

504.04

Post race comparison testing may be used to compare petrol fuel samples taken from boats, to that of the original supplied fuel or a locally obtained sample if the fuel was not supplied by the organiser.

Post race testing shall check:

1. Oxygenates by checking the electrical conductivity using the Digatron DT-15 apparatus.
2. Specific Gravity by using a Petroleum Hydrometer.
3. Water Solubility for detection of Methanol and other water-soluble additives.
4. Ceric Nitrate re-agent test for the detection of Alcohol.

1. Oxygenates

Use of Digatron DT-15

This apparatus is to be used for comparison testing ONLY.

Comparison of 'bench mark' Petrol/Petrol with two-stroke additive

1. Competitors using two-stroke oil must declare the brand of oil and the ratio of oil used to the CTO. (Take firm note of 504.02)
2. A sample of the mixture must be made using a small quantity of the 'bench mark' fuel and the declared oil.
3. The 'bench mark' fuel (now containing the appropriate oil) can now be compared to that of the competitor sample.

4. The procedure for comparison testing is as per the petrol/petrol comparison.

Comparison of 'bench mark' Petrol/Petrol (no two-stroke additive)

5. Stabilise the electrode of the Digatron by immersion in the 'bench mark' fuel.
6. Set the indicator to Zero.
7. Remove the electrode and clean off any residue of 'bench mark' fuel.
Wait a few moments for any final residue to evaporate.
8. Immerse the electrode into the competitor fuel sample and observe the reading of the indicator.
9. It is very unusual to find an identical reading. (This is due to influences of differing temperatures, aeration, etc. of the sample)
10. An indication of between -010 and $+010$ is normal and indicates a satisfactory comparison.
11. An indication exceeding these parameters is justifiable reason for laboratory analysis.

2. Specific Gravity

The use of a Petroleum Hydrometer should be made to compare the specific gravity of the 'bench mark' sample against the competitor sample.

Both readings should be identical. The temperature of both bench mark samples and competitor sample should be the same.

3. Water Solubility

This test should be conducted as per the text of the Digatron instructions.

Both 'bench mark' sample and competitor sample readings should be identical.

4. Ceric-Nitrate re-agent

This test should be conducted as per the Digatron instructions.

Both 'bench mark' sample and competitor sample results should be identical.

504.05

If the results of the above tests show characteristics of similarity between the locally obtained sample (or fuel supplied by race organiser) and the sample taken from the competitor, then the fuel must be accepted as legal.

A judgment of illegality cannot be made on the results of the above tests alone.

Only the results from a laboratory analysis can be used to declare a fuel conclusively illegal or not.

If the results of the above comparison tests do not show characteristics of similarity and give cause for doubt, then the following detailed 'Fuel Sample for Laboratory Analysis' procedures shall be enforced.

504.06 - FUEL SAMPLES FOR LABORATORY ANALYSIS

Procedure

Fuel samples for laboratory analysis may be taken at any time and place during any event under the authority of the U.I.M. Such samples shall be taken at the discretion of the Chief Technical Officer and must be as per U.I.M procedure. Fuel Test Certificates (in quadruple) shall be made available to the Technical Officers responsible for collecting fuel samples.

Only nominated Technical Officials may take samples.

504.07

At the beginning of the event, the Chief Technical Officer must obtain sufficient fuel from the local fuel source (or the fuel supplied by the organiser) for comparison test sampling.

This fuel will be referred to as the 'Bench Mark' fuel, and will be the fuel to which all comparisons will be made.

504.08

It is mandatory to carry out fuel testing on the heat and/or race winners (to third place) at all World Championship events, and recommended at Regional Championship events.

In a multi-race Championship, the fuel testing may be carried out by testing 3 or more boats at each race.

504.09

Any competitor refusing to provide an adequate fuel sample or having insufficient fuel available in the boat's main fuel system or tanks immediately following completion of the race, will be disqualified.

504.10

Race Organisers must ensure that there is a supply of at least six one litre containers per class of boat competing for each heat/race of the event.

Such containers must:

1. Be clean and constructed of robust, fuel non-reactive, impermeable material
2. Be sealable
3. Have provision for identification.

504.11

Equipment used for the extraction of fuel from boats must be clean and constructed of non-reactive material.

504.12

Each sample must be divided into two and placed in separate one litre containers, being sample A and sample B. The containers must be completely filled up and immediately sealed and identified to the boat from which the sample was taken. This information must be entered on the Fuel Test Certificate and on the sample containers. The Fuel Test Certificate must record; the place and time of taking the sample, the identity of the boat from which the sample was taken and the identity of the driver of the boat.

504.13

Sample A must remain in the control of the Chief Technical Officer or his deputy and sample B must be given to the driver or a representative of the driver's team. The driver or the representative must sign the Fuel Test Certificate, acknowledging receipt of the sample.

504.14 - RESERVED NUMBER

504.15

A sealed sample of both the competitor's fuel and the benchmark sample must be sent to an authorised petroleum laboratory for full analysis.

The Chief Technical Officer must deliver all such samples to a courier authorised by the Organising Committee or the U.I.M. Commissioner.

The C.T.O. must return a copy of the Fuel Test Certificate, signed by the courier for the receipt of the samples, to the Organising Committee.

The authorised courier must deliver all “A” samples, together with copies of the relevant Fuel Test Certificate to the nominated laboratory, where they must be tested in accordance with standard International scientific procedures.

If the result of the laboratory analysis proves that the fuel is not within the same characteristics as the benchmark sample, the analysis must be paid for by the competitor. If the fuel is within the same characteristics as the benchmark sample, the organiser must pay for the analysis. The same rule shall apply for testing diesel.

504.16

The results obtained from such testing must be attached to the laboratory's copy of the Fuel Test Certificate and delivered to the U.I.M. as soon as practicable after the results have been obtained.

504.17

The U.I.M. must as soon as practicable after receipt of the results, notify the following:

1. the relevant drivers or team representative
2. the relevant N.A.
3. the Race Organiser.

504.18 - RESERVED NUMBER

504.19 - AIR

Only air may be mixed with the fuel as an oxidant.

504.20 - RESERVED NUMBER

504.21 - DEFINITION OF PETROL

Petrol described within these rules is defined as one of the following:

1. Petrol of a kind recognised by the U.I.M. as being on general and genuine sale to the public in a country where U.I.M. International events are held annually.
2. The fuel used in Offshore racing shall be pump petrol as this term is generally understood. The detailed requirements of these rules are intended to achieve this purpose whilst allowing the use of absolutely consistent petrol's for racing purposes.

Any petrol which appears to have been formulated in order to subvert the purpose of this regulation will be deemed to be illegal.

Fuel suppliers are invited to supply samples of their petrol's to be checked for conformity before use.

505 - HOMOLOGATION

505.01

Application for homologation of an engine must be received by the U.I.M. Secretariat by close of 15th October for consideration of validity commencing the following year.

The homologation of a new engine will first be evaluated by Comintech.

Applications for homologation of an engine proposed for use in Offshore racing will subsequently be evaluated by Cominoff.

The U.I.M. Commission Cominoff (Offshore) retains the right to refuse an application for an engine homologation if in the view of that commission the granting of such a homologation would be contrary to the interests of offshore power boating.

Any engine proposed for use in Offshore racing, must initially be approved for use by Cominoff (or WPC where relevant) before homologation application can be considered for offshore racing.

A refusal by Cominoff (or WPC where relevant) to approve the engine for use in Offshore racing, will result in the application for homologation being denied for offshore racing.

In circumstances where an engine is proposed for use in both Circuit & Offshore racing, approval from both Comintech & Cominoff is required for use in both disciplines prior to homologation.

Following approval of the relevant (or both) Commissions, the homologation file (copy) will be annotated indicating the specific U.I.M. powerboating discipline the engine is approved for. The annotations will be one of the following:

- CIRCUIT
- OFFSHORE
- CIRCUIT/OFFSHORE

Engines will be inspected to ensure that they conform to the declarations in the homologation application file at a time and place to be agreed between the Comintech homologation group and the manufacturer, but this must be before close of 1st April for the commencement of homologation validity to be considered in that year.

The required number of engines manufactured prior to homologation is to be verified by the manufacturer and endorsed by the National Authority of the country of manufacture. If insufficient numbers of engines have been manufactured at the time of homologation, a written certificate, witnessed by a practicing lawyer, of the firm intention to manufacture the specified numbers is required from the manufacturing company. This must be endorsed by the National Authority.

When the homologation specification is agreed, the manufacturing schedule to meet homologation number requirements must be presented to the homologation group for adoption.

505.02

The homologation becomes valid when all correct and completed documentation has been received by the U.I.M. secretariat. This must be before 1st April for the homologation to be valid for commencement in that year.

A homologation remains valid for ten years from April 1 of the first year of validity, or 1st April of the year of any subsequent running production change.

If an engine continues in common use after this date to the same specification as declared in the homologation file, then provided all spare parts are readily available, an extension of validity for 3 or 5 years may be applied for. If the engine continues to meet these criteria for a further period, an extension of validity may be applied for on a yearly basis.

The application for extension of validity must be applied for using the same dates as Rule 505.01.

The fee for 3 years extension of validity is 30% of the normal homologation fee.

The fee for 5 years extension of validity is 50% of the normal homologation fee.

The fee for 1 year extension of validity is 10% of the normal homologation fee.

All the above extensions are liable to expenses as in Rule 506.02

Extensions of an Offshore file must be approved by Cominoff before application for extension can be accepted.

No alterations are authorised outside these dates.

505.03

Validation by the U.I.M. is subject to the payment of a fee, the amount of which is fixed every year by the General Assembly.

505.04

Application for homologation must be on the relevant U.I.M. forms. These must be complete and, where necessary, accompanied by explanatory notes, where interpretation may lead to misunderstanding.

505.05

Homologation files are obtainable against payment of a fee, the amount of which is decided upon annually, at the General Assembly.

The copyright thus bought allows the National Authority to reproduce the file with as many copies as necessary for its drivers and technical services. Should the National Authority so wish, it may fix the fee to be paid, for copies provided.

505.06

If competitors are unable to obtain copies of homologation files from their relevant National Authority, the U.I.M. may supply the documents against payment of a fee. Such documents are individual and are not to be reproduced without authorisation.

505.07 - RESERVED NUMBER

505.08

For restricted classes a homologation sheet is required for engine and propulsion units.

This file is valid only for the model described. Any model deriving must be subject to homologation separately.

A copy of the homologation sheet must accompany any engine, boat or accessory used in the classes in which homologation is required.

505.09

Refusal of homologation: a National Authority has the right to refuse an application for homologation if the latter is considered as being contrary to the interests of power boating. The refusal may be appealed against before the U.I.M. Council after examination by Comintech/Cominoff.

505.10

Comintech shall refuse any sheet put forward by manufacturers via the National Authorities concerned that has not been properly completed.

506 - HOMOLOGATION PROCEDURE

506.01

Inspection of engines is to be carried out by the Homologation Group.

The Homologation Group consists of a Chairman and two other members appointed by Comintech. Two members of Cominoff to control Offshore requirements may be co-opted as required.

For any inspection, a minimum of one Homologation Group member plus either one other member or a member from the Technical Commission of the National Authority of the country of manufacture is required.

506.02

The expenses of inspection are to be borne by the motor manufacturing company requesting the homologation via its National Authority. These expenses must be refunded to the inspectors before they leave the country of inspection, against receipt of an expense report and supporting proof of expenditure.

For Homologation Group members, travel expenses from their town of residence to the place of inspection and back, hotel accommodation and meals and transfer cost between home, airport and inspection place

are to be paid. For a National Authority, Comintech member, the National Authority will reimburse the expenses. The National Authority is authorised to charge the manufacturer a reasonable administration fee to cover its secretariat expenses.

506.03

An application for homologation outside the deadline-dates set in Rule 505.02 will be considered on payment of one-and-a-half times the normal fee, plus inspection expenses. For the homologation to commence and be valid in that year all documentation must be complete and received at the U.I.M. office by the deadline in Rule 505.02.

507.01 - RUNNING PRODUCTION CHANGES

A "Running Production Change" may be requested by a manufacturer at any time of the year. The Homologation Group decides whether any requested change is acceptable as a Running Production Change, or requires the model to be completely re-homologated. Modifications to specifications dictated by manufacturing or structural requirements are admitted, but not those primarily aimed at improving the performance. (as a guide, any changes or accumulation of changes likely to increase the power by more than 5%, or the speed performance corresponding to more than 5% power increase above that given by the original homologation specifications, may be grounds for requiring re-homologation).

An approved Running Production Change is to be announced in the U.I.M. Bulletin, and is legal for use in racing 30 days after the issue of the Bulletin. The fee for running production changes will be 50% of the full homologation fee.

Cominoff must approve 'A Running Production Change' affecting a homologation file of an engine used in Offshore racing, before the application can be accepted.

508 - CREW SAFETY

All other 508 Rules apply to all categories of boats.

All Class 1 mono/multi, Class 2 mono/multi, Class 3 / 6 litre mono/multi and Class 3 / 4 litre multi only must be equipped with a Reinforced Cockpit(s) with Canopies for all riding crew members and buoyancy to ensure the boat floats. The crew, who must be seated, must have a restraint system comprising of and conforming with the following rules:

A Reinforced Cockpit with Canopies is defined as a containment area for crew and can be constructed as an integral part of the boat. This Reinforced Cockpit Area must be designed and constructed to a specification capable of withstanding the forces of a water impact when running at the highest design speed of the boat, and therefore protecting all members of the crew in the event of an accident. The various components that constitute the Reinforced Cockpit shall be properly maintained to ensure reliable operation of all components, with emphasis being placed on the canopy release mechanism, emergency air supply and restraint systems.

In any Class 1 multihull boat, Cockpits will not be allowed in the sponsons. It is recommended that Sponson Cockpits are not used in any other classes.

These rules also apply to any boat in any class using Reinforced Cockpits with Canopies.

508.01 - COCKPIT EVACUATION / IMMERSION TESTING

Before racing in a craft with restraint systems, all crews must have passed in the last fourteen months, an immersion test in a restraint system to ensure that they can exit a reinforced cockpit crew compartment successfully.

Prior to taking the immersion test, all crews must have a valid scuba certificate or have received suitable training (refer to current Mark Lavin Safety Foundation Guidelines). This alternative training should be approved by the National Authority.

An Immersion Certificate to certify the passed test, showing the expiry date, must be delivered by Experts recognised by a National Authority.

All riding crew members using restraints must sign the National indemnity form prior to competing in any race or practice.

508.02 - DRAWINGS AND MEASUREMENT

Three view drawings (plan, side and elevation) of the design of the Reinforced Crew Cockpits(s), the Bulkheads, the type of Canopy, the Buoyancy System and the Restraint System anchorage Points must be lodged with the National Authority of the measurer and verified at the time of craft measurement.

Drawings shall be provided showing canopy aperture dimensions for full or partial canopies, single or tandem arrangements. Arrangements shall describe whether fore and aft, or side by side seating is fitted.

Drawings shall show the method and construction of release devices. Drawings should show the material specification of the transparent areas.

Prior to Boat Measurement the drawing and material specifications shall be sent to the Measurer requested to measure the boat. On completion of measurement, the drawings and material specifications called for by the designer shall be lodged with the measurers National Authority before they issue a certificate of compliance and measurement.

508.03 - REINFORCED COCKPIT AREA AND CANOPY

All boats with restraints must have a Reinforced Cockpit Area with Canopy, which should be constructed entirely of materials equal to the strength of the running surface of the boat, or stronger. This area must be the sides, floor, decking and bulkheads fore and aft.

It is mandatory to close the canopy with a hatch, and the hatch to remain closed during all racing and practice.

It is strongly recommended that a “Floor Escape Hatch” is the secondary means of escape.

508.04

Canopies must be a composite structure with the following features.

508.05

Polycarbonate areas are strongly recommended to be as small as possible while still maintaining that the driver and co-driver have clear, safe and undisturbed visibility ahead at sea level whilst racing. For Class 1 and Class 2 it is strongly recommended that these polycarbonate areas are built using 12 mm thickness, or more.

The combined visibility of driver and co-driver must be through a horizontal arc of 225 degrees (112.5 degrees either side of the centre line of the boat).

These polycarbonate panels are to be recessed into the composite structure and may be bonded using a suitable bonding agent, and/or “bobbins”.

It is highly recommended that there is also a through bolted outer flange for the fitting of the polycarbonate panels. Refer to the Mark Lavin Guidelines.

508.06

Screen flanges should be a minimum of 50 mm and should be fastened every 100 mm if using “bobbins”; it is recommended to use metal “bobbins” with heads, as opposed to the recessed plastic type.

The outer polycarbonate area of the flange fitting must not be painted, so that the measurer/ scrutineer may monitor any discrepancies.

508.07

These Restraint Cockpits must be fitted with an internal roll bar, two in a tandem cockpit as a minimum. There must also be, between the two single cockpits, an anti-compression strut or structure of similar strength to the roll bar.

508.08

Hatch openings shall have a minimum of 25 mm flange.

508.09

Hatches should be recessed on the front and sides.

508.10

It is recommended the hatches are constructed to the same specification as the main Restraint Area. The hatches shall be fitted with a catch which has a positive open and positive close mechanism and should hold the hatch against lateral forces. These catches shall be able to be opened from both inside and outside the cockpit and must have a second emergency mechanism to allow the rescue team to easily remove the hatch from outside if necessary.

These hatches should be fitted with hinges with short release pins. This is important because long pins invariably bind the hinge.

508.11 - RESERVED NUMBER

508.12

There should be one or more divers grab handles fitted to the outside of each hatch.

508.13

Canopy hatch release handles, which must be provided both inside and out, must be painted fluorescent orange or have a fluorescent orange background panel to identify them and directional arrows to indicate the method of opening.

508.14

The canopy lid hinges and the canopy hatch covers release mechanism must not encroach within the canopy aperture area, and these hinges and release mechanisms must not in any way hinder the exiting of crew members when fully race fitted.

508.15

Canopy openings should have the entry/exit apertures located directly above the crews' heads.

The canopy aperture openings should be at least 0.55m in length and 0.55m in width. If the crew is seated side by side, then the opening should be at least 0.55m x 0.825m wide. In tandem configuration, the opening(s) should be 0.55m x 0.55m per crew member. The canopy apertures should be cut with all corners having a radius of 0.025m minimum or 0.25m maximum. The radius should be constant and have a smooth finish to relieve stress.

508.16

The canopy aperture must have a 20 mm wide (minimum) fluorescent orange band around the opening.

508.17

It is mandatory that one single air supply (not oxygen) and a bottle will be provided for each riding crew member. The air supply must be securely fixed adjacent to, or on each one of them. It is recommended that sufficient air be provided in each individual bottle for ten minutes.

Air bottles must have a pressure gauge fitted for visual checking at pre-race scrutineering. ***This gauge should be filled with liquid and be at least 5 cm in diameter for easy reading.***

Air supply bottles shall be “Turned On” before starting a race or taking part in practice and/or testing.

508.18

Reinforced Cockpits must have flood tubes or other means of flooding the cockpit to equalise the pressure quickly in an accident. The floor of the cockpit should be as air tight as possible to help the cockpit pressure equalise far more quickly when in an upturned position.

508.19

Boats with restraints must have stop buttons/switches located in the cockpit area, immediately accessible to driver, co-driver and rescue officers. The stop buttons/switches must be identified by a fluorescent colour.

These switches must shut off all fuel pumps as well as the ignition circuit.

In the case of diesel boats, the stop control cable for the fuel injection pump shall be a non-sleeved cable, so as to eliminate the cable being able to bond in a fire.

508.20

All boats shall have a White High Intensity Strobe Light fitted to indicate “coming off the plane” but not needing assistance. The strobe light must be able to be operated by the throttle man, and should be operated by the throttle man if a problem occurs, to enable any following race boats to take avoiding action. The strobe light shall be mounted on the top rear of the canopy. When dual canopies are used, the light may be on or behind either one.

This strobe light may also be used as a substitute for the orange retirement flag when returning to port under reduced power.

508.21

Cockpits with Restraints must be fitted with rear of head protection for each crew member. This must be an integral part of the seat, which must be attached directly to the structure of the Restraint Compartment. The head protection must be a minimum of 0.2m wide and extend at least 75% of the height of the safety helmet as worn by the crew whilst in the normal seating position. There must be a minimum of 0.12m vertical and lateral clearance between the canopy and each of the crewmembers when in the normal seating position.

508.22

The Restraint System must consist of a 5 or 6 strap harness and should utilise a 75 mm lap belt, a 50 mm strap over the shoulder harness rated at 4,100kg (9,000 lb.) and grommited to prevent chafing or cutting of the belt. Harness straps must be attached directly to the cockpit structure. Those straps close behind the driver's head and neck must be 100 mm to 150 mm apart at point of attachment. The shoulder harness should be installed at 90 degrees to the spine at shoulder line to minimise compression injuries under high "G" loading. All straps must be free to run through intermediate loops or clamps/buckles. All anchor point bolts must be fitted with backing plates of 10cm minimum width.

The driver harness attachment bolts in reinforced cockpits must consist of minimum grade EN8 bolts, with an 8 x 1.25 mm thread and locked nuts. There must be a spacer and plain washers on each bolt. The spacers must be glued to the cockpit structure. Intention of these spacers is to prevent buckling of surface material near bolts. This always leads to local delamination which easily spreads out over cockpit structure, when it is under stress.

On the sides of the structure, which has to take up the force on the attachment bolts, there must be a stainless steel plate (washer of minimum 3 mm thickness and 100 cm² area).

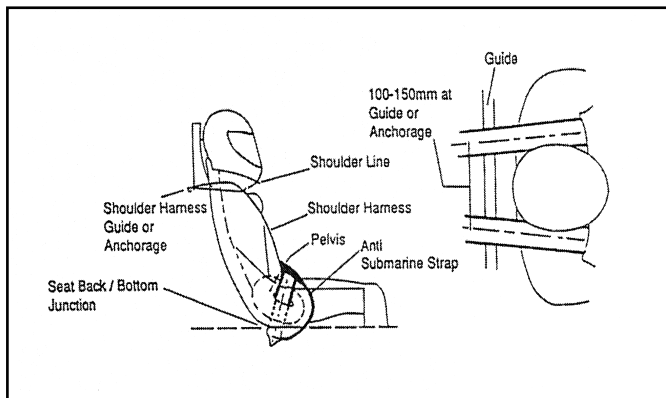
When using seats with suspension, and therefore not using a bulkhead restraint anchorage, drawings must be lodged with the National Authority of the measurer and approved prior to boat measurement.

All restraint systems must have a common method of release. The single lever method (sometimes called the NASCAR type) or rotary type, are both acceptable restraint release systems.

Both types of restraint release must be examined for satisfactory operation by the scrutineer before every race.

The harness system must comply with Drawing 2.

The shoulder harness should be installed 90 degrees to the spine at shoulder line to minimise compression injuries and the high "G" loading.



75 mm minimum/maximum to Centre line of Lap Belt at Seat Back, Seat Bottom junction. Lap Belt should continue in straight line to anchorage.

508.23

A quick release steering wheel may be fitted on a boat with personal restraints, but all drivers must be able to exit the cockpit without removing the steering wheel.

508.24

Rear view mirrors are mandatory, as well as a method of cleaning the canopy whilst under way.

508.25

Two fire extinguishers, each a minimum of 2kg must be carried and be readily accessible to the crew.

The flares described in U.I.M. Offshore Rule 715.10 may be placed in a shallow locker adjacent to the deck race number.

Should a life raft be carried, it may be placed in the same locker.

All crew containment areas of inboard engine 508 canopied boats must be fitted with a carbon-monoxide alarm.

508.26

Racing Vests - the efficiency of the racing vest is a matter of the exclusive responsibility of the wearer. Every crew member whilst on board, must wear a racing vest during the practice runs and throughout the race. Racing vests must be coloured high visibility orange or yellow. The racing vest must have epaulets/handles to help extract crew from the boat. The racing vest must have crutch straps or a method of ensuring that the vest does not "ride up".

508.27

Each Reinforced Cockpit Area shall have one or more water activated light(s) or similar.

508.28

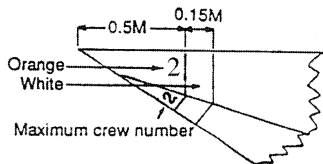
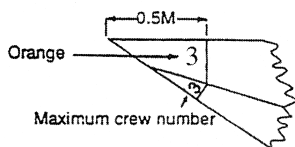
All boats with restraints must have their bows painted fluorescent orange for at least 0.5m. ***Only boats with restraints and closed canopies are allowed to use orange coloured bows.*** If the hull is of a similar colour, then there must be a white separating band of at least

0.15 m wide to ensure that the fluorescent orange band is obvious. ***If the number of riding crew exceeds two***, the number of riding crew members must be written in black in at least 0.25m high numbering on the orange nose in the following three locations:

- The lower running surface.
- The topside of the hull/sponson.
- The deck of the hull/sponson.

Should any boat be found to have contravened the riding crew number requirement, the penalty shall be disqualification from that event.

Note : Numbers shall be placed on the orange nose, not the white band area



508.29

It is mandatory that sufficient buoyancy is provided in the boat, or in the material used for its construction, to ensure that the boat floats if capsized or holed. If extra buoyancy is needed, the buoyancy system described by the designer should be verified by the Measurer. This added buoyancy must be in at least four separate flotation units.

It is recommended that the buoyancy should float the hull as parallel with the surface of the water as is practical, to help in rescue accessibility.

508.30 - RESERVE NUMBER

508.40 - RESERVE NUMBER

508.50 - CLASS 3 CRITERIA

Each National Authority shall decide if its Nationals shall be permitted to use Restraint Systems in Class 3 boats below Class 4 litre.

In Class 3 boats, all the 508 rules apply. Refer to U.I.M. Offshore Rule 715.10 for application of flares. These flares must be readily accessible from the deck.

508.60 - CLASS 3 CRAFT WITHOUT RESTRAINTS

Note: All Class 3 / 6 litre and 4 litre multihulls must be equipped with reinforced cockpit(s) and fully comply with Rule 508 - CREW SAFETY.

Reinforced Water Deflectors forward of the crew are mandatory for Class 3/ 2 litre and 1.3 litre Multihulls and all Monohulls without Reinforced Cockpits.

For 1.3 litre, the over-deck water deflectors only are mandatory.

The Reinforced Water Deflector must be designed and constructed of materials of sufficient strength to provide adequate crew protection, both over and under the deck and hull. The forward fairing on the deck must rise to a minimum height of the chin of the tallest crew member when in the normal driving position. It is recommended that lateral protection covering be provided up to the shoulders. The top 0.05m (5cm) of the Water Deflector must be at least 45 degrees from the horizontal, with a minimum 0.3m (30cm) width per person measured transversely in the horizontal plane.

The Reinforced Water Deflector must be designed and constructed so as to present no hazard if the crew is thrown forward and must be so designed that it would not restrict the crew from being ejected. In all cases, the visual requirements in Rule 508.05 must be adhered to.

Measurers and Scrutineers will examine boats with the crew in the normal driving position.

The flares described in U.I.M. Offshore Rule 715.10 must be readily accessible from the deck.

Rear of head protection is recommended. If fitted, it should be at least 0.3m (30cm) wide and the height of the padding must extend at least 75% of the height of the safety helmet as worn by the crew whilst in the normal driving position.

If a monohull has a canopy and/or restraints, it must conform to the same criteria as multihull boats as described in these 508 rules.

MARK LAVIN FOUNDATION GUIDELINES

It is strongly recommended to use the latest publication of “Reinforced Cockpit Guidelines” from “The Mark Lavin Safety Foundation” for the construction of cockpits for clarification of detail and all subjects that are not specified in the 508 rules.

1. It is strongly recommended that there will be at least two independent exits available from each fully enclosed cockpit.
2. The secondary exit (with hatch open) must at least conform with the guidelines for the construction of cockpits published by the Mark Lavin Safety Foundation.
3. These guidelines are intended to guide the design and construction of Reinforced Cockpits with Canopies.
4. The strength of all parts of the cockpit shall be at least as strong as the bottom running surface of the boat. The reinforced cockpit pod shall be built as a continuous unit.
5. It is strongly recommended that the transparent areas shall be built of 12 mm thickness polycarbonate or stronger. The transparent areas shall be attached by internal flanges that overlap by at least 50 mm. If attached by bolts, they shall be at least 6 mm diameter high strength bolts spaced not greater than 100 mm apart and properly installed with bushing to protect against thermal expansion stresses. The loads of the bolts on the outside of the transparencies shall be distributed and transferred via external flanges at least 50

mm wide and completely surrounding the transparencies. It is recommended that the external flanges also overlap the structure by at least 50 mm and be bolted or bonded to the structure. If the transparencies are bonded to the cockpit structure, the primary supporting flanges shall be internal and overlapping by at least 50 mm completely surrounding the transparencies. External flanges are also recommended to strengthen the bonded attachment.

6. It is strongly recommended that the hatch hinges, flanges and latches shall be strong enough to support the loads applied to the hatch by impacting the water at any realistic angle at the maximum speed the boat is capable of.
7. The condition of the reinforced cockpit pod is the responsibility of the drivers.

600 - RECORDS AND PERFORMANCES

600.01 - GENERAL

All record trials and their control are submitted to the following rules.

The record belongs personally to the driver who has established or beaten it. The record is entered under the name and nationality of the driver.

Any race against the clock, and called kilometre or mile trials or any similar name is prohibited if not conducted in accordance with these rules.

Combining speed records with distance records is not allowed.

No direct or indirect aid is to be provided to the driver who attempts a record.

600.02

World speed records, hour records, distance records and competition records are all records open to all boats and aquabikes of International classes adopted by U.I.M.

600.03

National records recognised by U.I.M. are the records established by boats of national classes duly recognised by the national authority (the rules of which have been approved by U.I.M.). The Certificate differs from those issued for World Records.

600.04

The World's Unrestricted Water Speed Record is awarded to the fastest boat in the world piloted by the driver, irrespective of the class of boat; this record is established in conformity with U.I.M. rules.

601 - CLASSES ELIGIBLE FOR RECORDS

601.01

World records may be attempted with the following craft :

- All U.I.M. series and classes
- American Powerboat Association classes
- Prototypes

601.02

A boat can establish a record in its own class only.

601.03

When a class is modified so as to reduce the performance of the class, for example by :

- reducing the cylinder capacity,
- restricting the tuning permitted,
- restricting fuel permitted,
- introducing a minimum weight,
- increasing the minimum weight,
- or any other means,

the records established before the modification comes into force will be frozen and a new series of records will be started.

All proposals for new rules, rule changes and U.I.M. General Assembly minutes shall state :

- whether or not records will be frozen,
- the classes to be frozen,
- the date(s) when the classes are to be frozen.

601.04 - QUALIFICATION

For offshore records, a boat must have completed the course, taken the chequered flag and finished first, second or third, in an U.I.M. World or U.I.M. Continental Championship race in the class to be attempted and after the date when the class was last frozen.

602 - DEFINITION OF UNITS

602.01 - TIME, DISTANCE AND SPEEDS

The hour is the twenty-fourth part of a terrestrial day. The symbol is letter "h".

The minute of time is the sixtieth part of one hour. The symbol is letters “mn”;

The second of time is the sixtieth part of one minute. The symbol is the letter “s”.

nautical mile	=	nm
statute mile	=	st.m
kilometre	=	km
nautical mile per hour	=	knot
statute mile per hour	=	mph
kilometre per hour	=	kmh

602.02 - STATUTE MILE, NAUTICAL MILE AND KILOMETRE

The statute mile is an Anglo-Saxon measure.

The nautical mile is used at sea.

The knot is not a unit of length; it expresses the speed of one nautical mile per hour.

One statute mile (st.m) = 5280 feet = 0.8690 nm = 1,609.3 km

One nautical mile (nm) = 6080 feet = 1,852 km = 1.1508 st.m

One kilometre = 3280.844 feet = 1000 metres

604 - BASES (THE COURSE)

604.01

Records may only be established on bases recognised by National Authorities and notified to the U.I.M. in accordance with these rules.

604.02

The base is measured and certified by an official surveyor holding a certificate of competence from a recognised institute, in the presence of a delegate of the National Authority.

The base must be measured by triangulation or electronic distance measuring equipment (EDM), which must have a first class certificate delivered by an Observatory or similar authority. This certificate must not be older than two years.

The official surveyor provides a certificate in duplicate in which the strict adherence to the above requirements is certified.

The certificate is signed by the surveyor and countersigned by the delegate of the National Authority. One of the copies is for retention by the National Authority for its special national records files and the other is sent to U.I.M. for its special World record files.

The base for a record must be a fixed base, as laid out for a National Authority, or must be measured for a special occasion in waters designated by the driver. It must be defined by fixed marks ashore.

604.03

No distinction is made between records established on the sea, rivers or lakes. Records must not be attempted during the period between half an hour before and half an hour after a change in direction in flow of water over the course (change of tidal flow).

604.04

Any expenses incurred in the laying of a temporary course are defrayed by the entrant attempting the record.

604.05

For speed records there shall be only one attempt on the course at any one time. An attempt must not start until the director of the trial is satisfied that effective rescue boats are on the course.

605 - CALCULATION OF SPEED

605.01

The National Authority calculates the speed from the data provided by timekeepers. The speed is to be shown in kilometres per hour (kph) and statute miles per hour (mph), even when the attempt takes place on a course of one nautical mile.

The duration of each run is obtained by taking the arithmetical mean of the times recorded by two sets of timing devices, the mean time being given to one tenth of a second.

Should one of the timing devices stop or one of the signals fail and only one duration be registered, this duration will be accepted.

605.02

The only duration so obtained serve to calculate the speed of each run. Only two decimal places are to be used.

605.03

The speed of the trial is the arithmetical mean of the speeds, two decimal places being used.

605.04

For speed records established on a distance of one nautical mile (1 nm = 1.852 km = 1.1508 st.m) the following constants are to be used :

Where t is the time in seconds :

the speed in mph = $4142.9 \div t$

the speed in kph = $6667.2 \div t$

606 - MARGIN REQUIRED

A record will be valid only if its speed at least equals the speed of the previous record multiplied by 1.003.

607 - MOTORS

607.01

All accessories and/or elements required by the racing rules for each class of boat attempting to establish or improve a record must be on board during said attempt.

607.02

A boat which holds two certificates for two different classes is entitled to establish World records in those two classes, but shall make two distinct trials, one for each class.

607.03

Immediately after the record trial, the motor(s) or engine(s) must be sealed with a distinctive seal in the presence of the officials who have witnessed the trial.

607.04

Within 48 hours of the trial, two measurers must check that the hulls, motors and equipment are in conformity with the specifications of their class, and this inspection must be carried out in the presence of the

Officer of the Day or an Official specifically appointed by the National Authority.

607.05

When attempting to break a U.I.M. class record, the noise level of the motor shall not exceed the authorised level (rule 735.14).

608 - REQUEST TO ORGANISE

608.01

All applications for records trials must be made in writing to the National Authority at least one week before the trial and must be accompanied by the fee stipulated by the National Authority.

The National Authority applies to the U.I.M. for homologation of records.

608.02

Only records established under the direct control of a National Authority affiliated to U.I.M. can be recognised.

608.03

Each National Authority decides on the application for records to be established on its territory. The National Authority may refuse to consider a record application, but must then explain the reasons in writing.

The National Authority indicates the validity of the record permit.

608.04

It is the duty of the driver to accept responsibility for any safety measures deemed necessary for this attempt.

The Officials will not accept any responsibility, they will only record the results and ensure that the rules are complied with.

609 - OFFICIALS

609.01

The National Authority shall appoint an Official Observer and other duly qualified persons to hold the official functions.

An Official so nominated shall not hold any other office than that to which he was nominated. An Official shall not himself establish a record.

609.02

The Official Timekeepers and Officials may receive payment for their services at rates fixed by the National Authority.

609.03

Time keeping may only be carried out by Timekeepers and Officials who have been recognised and authorised by the appropriate National Authority.

The stop-watches or any other apparatus employed must have a first-class certificate delivered by an observatory or similar authority. This certificate must not be older than two years.

Each Timekeeper hands to the National Authority a signed report of the record attempt, countersigned by the Official Observer.

609.04

Timing from aboard anchored boats or aboard competing boats is not allowed.

610 - SPEED RECORDS

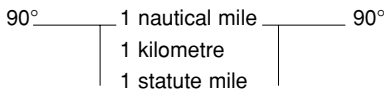
610.01

There is only one World record per class. This record can be established over the nautical mile, Statute mile, or the kilometre. For aquabikes the distance may be 500 metres.

610.02

The course is covered once in each direction consecutively.

The course is to be marked at either end by two posts in solid material, put up on shore with approved distinguishing marks affixed to them. The two lines uniting the two pairs of posts are parallel to each other and at right angles to the line of the measured course. Steering marks may be placed wherever needed.



610.03

In wide rivers and lakes, the width of the course may be marked off at each end by two buoys, moored at a distance of one hundred metres from each other. The boats must run between these buoys.

610.04

Timing is to be one tenth of a second up to 200 kph. Above 200 kph timing is to be by photoelectrical or similar apparatus to one hundredth of a second.

There must be a Timekeeper and a Timekeeper Observer at each position.

Individual timers may be used at each position or a single timer electrically activated from each position.

Where two timers are used at a position the time is the average of the two timers.

The times of day of each attempt must be recorded.

The time elapsing between the two runs must not exceed 20 minutes except for jet attempts, when the permitted interval is one hour.

610.05

A driver may re-start an attempt because of breakdown or other reason and may stop the engine or boat between runs.

If the attempt has not been completed within four hours of a starting time fixed by the National Authority, the Officer in charge may stop the trial.

611 - DISTANCE RECORDS

611.01

Distance records are established on a total course of twelve nautical miles for the outboard series, twenty-four nautical miles for other series, and six nautical miles for aquabikes.

611.02

In distance records, the base is covered up and down successively in even numbers and the turns are not included in the distance.

The course for distance records may be :

- a course established for speed records;
- a base of three nautical miles maximum, in straight lines with buoys placed at the corners if necessary, no corner to be less than 135 degrees.

The buoys placed at the corners must always be rounded.

611.03

Any base must be defined by two posts set in concrete at either end.

611.04

Timing is to be to one tenth of a second.

The timing of distance records is done by a Timekeeper with two timing devices at the starting line. He is assisted by two Officials, one with him and one at the other end.

611.05

Stopping the motor or the boat during a distance record attempt is prohibited.

612 - HOUR RECORDS

612.01

The following International and National records are recognised by U.I.M : 1, 2, 3, 4, 6, 9, 12 and 24 hours duration records for all the series recognised by U.I.M.

The homologation of a record of lesser duration than that applied for may be granted, but only within such limits as provided for by the above rule.

612.02

The base must not exceed six nautical miles from station to station, that is to say twelve nautical miles up and down. For aquabikes only, the distance may be 3 nautical miles from station to station, that is to say 6 nautical miles up and down.

612.03

The base is defined by two posts set in concrete at either end.

612.04

The boat may be driven by more than one driver, but their names must be filed in the record application.

612.05

Repairs and adjustments which can be carried out on board may be effected on the course.

The same motor or engine and hull must be used throughout the trial.

On the other hand, repairs that cannot be effected on board and refuelling and changing of drivers must be effected at the control station and are to be supervised by an Official.

612.06

Any time during which a boat stops during an attempt is included in the elapsed time for the attempt.

Turns are included in the course and are consequently not excluded from the timing.

612.07

Timing is carried out by one or several Timekeepers with two timing devices stationed at the start. The Timekeeper is assisted by two Officials, one stationed with him and one stationed at the other end.

612.08

A driver can claim several different hour records during one trial by selecting the best series of consecutive laps.

612.09

The boat must cross the finishing line at the end of the lap (the last lap) during which the record time has elapsed.

The average speed of the last lap is calculated and the distance which corresponds to the time still to be run is added to the distance covered at the end of the penultimate lap.

The result is shown as the distance covered in the record time and the average speed for that distance.

612.10

Example for a two hour record on a base of six nautical miles (nm), twelve nautical miles per lap :

boat completes 8 laps in 1 h 54 mn 40 s = 110.48 st.m

ninth lap completed in 13 mn 50 s = 830 s

time balance 5 mn 20 s = 320 s

distance balance = 13,81 st.m \div 830x320 = 5.32 st.m

Total distance in 2 h = 115.80 st.m

Average speed = $115.80 \div 2 = 57.90$ mph

613 - WORLD COMPETITION RECORDS

613.01

The U.I.M. recognises World records of 5, 10 and 15 statute miles in races for all U.I.M. circuit classes.

No other records may be established during a race.

Unlimited hydroplanes only are entitled to statute mile records established on :

- single lap course (two laps for APBA Gold Cup races) while qualifying or racing ;
- total heat average speed while racing.

614 - PERFORMANCE CERTIFICATES

614.01

U.I.M. may issue Performance Certificates for the best heats made during races for the principal International trophies.

614.02

Application for these is to be made by the National Authority of the country where the race took place and the accuracy of the results, timing, course and class is to be certified by them.

614.03

U.I.M. only recognise the results which beat previous performances.

614.04

The amount of the fee for Performance Certificate is decided upon annually by the General Assembly.

614.05

U.I.M. may issue a Performance Certificate for speed trials made by boats not included in U.I.M. classes. These trials must, however, be made in accordance with U.I.M. rules.

U.I.M. award Performance Certificates for boats of the Production Boat Series having established or broken speed, duration, distance or competition records according to the Record Rules.

614.06

For these performances, it is not permissible to remove any of the boat's accessories, or items of comfort, provided by the builder and these are compulsory for racing.

U.I.M. may award Performance Certificates for any of the offshore classes, International and National, established at sea on properly measured runs.

615 - HOMOLOGATION OF RECORDS

615.01

Only records homologated by U.I.M. are considered as World Records.

Only records established under the direct control of a National Authority affiliated to U.I.M. can be recognised.

615.02

If a record has been established on the territory of two National Authorities, it will be entered as a record by the National Authority on whose territory the start was made.

As soon as a record trial has taken place, the National Authority communicate the results to the Secretary General of U.I.M. by fax and confirm it by letter.

The information has no official character and is only transmitted to such National Authorities as apply for it, in order to avoid useless trials.

615.04

The Timekeepers and Measurers must complete the official U.I.M. record application form, which must be signed as correct by the Senior attempt Official (President, Chairman, OOD etc.) and be sent to the National Authority within three days of the trial, seven days for APBA controlled classes.

The National Authority transmits the application for homologation, together with other necessary papers, to the U.I.M. no later than four weeks after the trial, sixteen weeks for APBA controlled classes.

615.05

For a request for homologation to be considered by the U.I.M., the National Authority must send :

- a plan of the course if not already in possession of the U.I.M.,
- the completed official U.I.M. application form which must include any National Authority calculations,
- a copy of the boat's measurement certificate,
- a photograph of the boat under way with the colours, inscriptions, etc. noted on the reverse ; also to be noted on the reverse, the driver's name, class and speed achieved,
- a list of the full names of the Officials with, at least, the names of the Timekeepers, Measurers, Rescue Officer, OOD and Observer,
- offshore boats, a copy of the results in which the boat qualified, as per U.I.M. rule 601.04,
- for attempts on tidal waters, the calculation stating the times of the changes of direction of water along the course (changes in tidal flow) and the source of the information,
- a cheque for the amount due

615.06

The recognition fee is fixed annually by the General Assembly.

615.07

Pending the official confirmation by U.I.M., neither the driver, builder, nor any other shall publish or cause to be published, circulate or cause to be circulated, the results of the trial, until the delegate of the National Authority has given the actual results in writing, after checking the calculations in the presence of Timekeepers.

Even when such authorisation has been received, results may only be published with the addition of the words "Subject to official certification by U.I.M." in as large and prominent type as that in which any part of the notification itself is printed.

Neglect to comply with the above regulations automatically entails the refusal to recognise the record. The National Authority have the right to penalise whoever is responsible.

615.08

U.I.M. issues annually a list of World records, changes being made known, when they occur, by means of newsletters.

U.I.M. keeps a registered list of all World records and delivers a certificate for each new record.

700 - INTERNATIONAL OFFSHORE RULES

CLASS 1 - INBOARD ENGINES

HULLS/LENGTH/WEIGHTS

From 1st January 2003, all Class 1 rules will be aligned with the 860 series rules.

701 - GENERAL

All races shall be run in accordance with the general sporting rules and bye-laws of the U.I.M. as applicable to Offshore.

Rule changes are only made following approval by U.I.M. Such changes taking effect not less than six months after approval. Changes affecting safety may be made at any time, according to their necessity.

701.01

The following basic rules and definitions are for Offshore powerboats which have been designed and are suitable for racing in open and unprotected waters.

701.02

Organisers shall not be permitted to make any additions to these rules which by their nature would affect the eligibility of any boat EXCEPT in circumstances specifically affecting safety or in respect of special laws which may apply to the particular locality of the race. Organisers shall seek the approval of their National Authority to make such additions.

701.03

Organisers shall have the right to refuse any boat which they deem to be unsuitable and/or which does not conform to the requirements of these rules.

701.04 - RESERVE NUMBER

701.05

The finishing order is determined by the order in which the boat finish in its Class.

Points for Championships are awarded separately for the Classes.

701.06

Organisers are free to organise prize categories for varying types, such as cabin boats, diesel engines etc., provided the craft comply with fundamental rules.

701.07

Organisers may also organise separate races for the different classes and/or prize categories.

701.08

Championships: Races included in championships are governed by individual rules of the particular championships as defined under their own rules.

701.09

All boats entered for racing shall be subject to the direction and control of the Race Committee.

701.10

It shall be the sole responsibility of each boat's driver(s) to decide whether or not to start or to continue the race.

701.11

Unless specified in racing instructions, no outside assistance (including refuelling) is allowed during a race.

In an International Championship race no outside assistance (including refuelling) is allowed.

The Race Committee has the right to decide whether or not there was outside assistance or not.

It is permitted to use electronic steering compasses and/or electronic position fixing equipment.

It is expressly forbidden to interface any of this equipment to the steering system.

702 - CREW

702.1

All boats will carry a minimum of two crew members with valid driving licences and of minimum age 18 years.

For Classes 3: 1, 1.3, and 2 litre the minimum age is 16 years.

702.2

Only licensed drivers are permitted to control the boat when under the jurisdiction of the race organiser.

702.3

Members of the crew under their age of majority must be required to produce evidence of parental consent to participate in the race.

702.4

Breach of the above rules shall entail immediate disqualification.

702.5

In championship races involving points qualification, the points will be awarded to the driver designated as the first driver on the entry form and who shall be aboard throughout the races.

702.6

It is the responsibility of each National Authority to ensure that before any driver or throttle man has his licence endorsed for competing in International Championship Class 2 events, he has to be proved to be competent and has competed satisfactorily in at least 3 races and finished in 2 at national level races in Classes 2, 3 or one design classes.

703 - HULL

703.1 - DECK

The deck must be able to bear the weight of a standing man (100 kg) at any point.

703.2 - DECK, OUTBOARD MOTOR PROPELLED CRAFT

Wherever the motor is, the hull shall be fully decked to the stern, incorporating a well formation if desired.

Any holes for the passage of cables, fuel pipes, etc., shall be fitted with sealing grommets and be above the lowest point of the stern structure.

703.3 - STEERING POSITION

The steering arrangements shall be sited so that the safe handling of the boat shall not be prejudiced by interference to the driver's line of sight across the bow at any state of trim in calm water.

703.4 - GUARD RAILS

Rails or wires or hand holes shall be fitted which shall extend fore and aft to enable crew to proceed from the cockpit to the whole deck for the working of mooring, anchoring and towage and access to deck hatches.

No ropes can be used as rails or hand holds.

703.5 - MOORING CLEAT

All boats shall be fitted with a well secured cleat or sampson post on the fore-deck adequate for anchoring in a seaway and for towing at sea over a prolonged period.

703.6 - TOW HOOK ON CATAMARANS CLASS 2

A Tow Hook identified with an orange 0.2m x 0.2m x 0.2m triangle painted around the hook must be fitted to the underpart of the tunnel, centreline at 1/3 of the distance aft from the front edge of the tunnel. This hook must be easily accessible for attaching a line during recovery of the boat in the event of a capsize. The hook is to be fitted on a reinforced area designed for the purpose of towing a water laden hull.

703.7.1 - CRANAGE

Boats must be fitted with a minimum of three lifting eyes, either through deck or topside hull cleat type, suitably engineered to withstand lifting the boat fully equipped and fuelled with an adequate safety margin.

Each boat shall be equipped with a detachable strop assembly providing a single lifting point.

703.7.2 - CRANAGE SAFETY

The Race Committee shall cordon off, restrict and supervise the crane area. Access to this area shall be limited to authorised race officials and designated race boat crews.

703.8.1 - CANOPIES

All craft in Class 1 mono/multi, Class 2 mono/multi, Class 3 / 6 ltr mono/multi, and Class 3 / 4 ltr multi only, must have canopies and comply completely with the 508 rules and latest version of the Mark Lavin Memorial Safety Foundation Guidelines.

703.8.2 - WINDSHIELD

Windshields, if fitted, shall be strong and well supported. All edges of glass, plastic material and framing shall be effectively padded. Transparent windscreens are to be made of non-splintering glass or plastic material.

703.9 - OPEN COCKPITS

703.9.1

The cockpit opening shall provide seating positions for at least two drivers, with the following minimum dimensions; length 0.76 m, width 0.53 m per driver, with a round-off radius of 0.25 m.

703.9.2

There shall be a minimum cockpit clear depth of 0.38 m everywhere, with the exception of seats, steering wheel and controls. Measurement is done from the upper part of the cockpit edge or from the top of the cockpit coming if it is substantial.

703.9.3

All cockpit seats must be securely fixed to the main hull structure. A seat for each crew member must be provided. The crew should be seated at all times when racing or attempting the race start.

703.9.4

The structure of cockpit fairings must at least equal the strength of the main deck i.e. be able to be walked on at any point as per Rule 703.1.

703.9.5

For craft without canopies and restraints, also refer to 508.60.

703.9.6.

Only boats with restraints and closed canopies are allowed to use orange coloured bows.

703.9.7

In open cockpits, the cockpit edge or the type of a substantial coaming is not to be higher than the top of the crew persons' shoulders.

704 - INBOARD ENGINES

704.1 - EXHAUST

The engine exhaust manifolds, pipes and exhaust driven turbines, from the engine outlet to the point of exit must be water cooled by the water jackets or shielded. The external surface of the shield must not exceed 150° C at any time. The exit must be located in such a position whereby the crew cannot be effected by exhaust fumes.

704.2 - SHAFT GUARDS

Inboard propelled craft must have a protected shield to each input drive shaft which shall include at least two rings which completely encircle the shaft and a 180° scatter shield over each universal joint.

In the event of a failure this protection shall be capable of containing the shafts and couplings from causing damage to the hull skin, fuel tanks, any other installation and or component, and from causing any danger to the crew.

704.3 - ENGINE COMPARTMENTS

Inboard engines are to be installed in a compartment separated from the cockpit and other accommodation which is to have adequate ventilation. Vent holes in bulkheads or covers shall not be in close proximity to the drivers and crew without flame trap protection. The compartment(s) shall have rigid covers.

704.4 - FIRE EXTINGUISHERS

For all inboard engine installations, it is mandatory to have a fully automatic fire-extinguisher system, though it is permitted to have a separate manual system.

705 - ENGINES

705.1

Turbo and/or Supercharged petrol engines are not permitted.

When a diesel engine includes a device for supercharging or turbocharging, the nominal cylinder capacity is to be multiplied by 1.4 and the boat will pass into the class corresponding to the effective volume resulting from this multiplication.

705.2

A dynamic air inlet for ducting the atmospheric air toward the engine's air intake is not considered a supercharging device.

If approved for use in offshore by Cominoff and annotated on the Homologation Certificate, EPA 2006 compliant, outboard engines utilising "air pumps" as part of the induction system are not considered as supercharged.

705.3

Turbo Compound engines and Gas Turbines are not permitted.

A Turbo Compound Engine is defined as a process in which effective power is produced not only by expansion in the working cylinders, but also in a second stage. In compound engines, the exhaust turbine, or one of the exhaust turbines, is coupled to the engine output shaft.

706 - ENGINE CUT-OUT

706.1

- a) Engine cut off switches for connection to all crew are mandatory (first man out shuts off engine). An emergency override system to restart engines shall be mandatory. The lanyards used must not exceed 120 cm between the driver and the boat. The lanyards shall

be attached to all crewmembers at all times when the boat is at racing speeds.

- b) The lanyards must be able to support ten times the strength necessary to operate the device or to resist a load of ten kilos. Having chosen the biggest value, they must be capable of operating the device in any direction on a horizontal level.
- c) Lanyards are not necessary when restraint systems are used.
- d) If the engine is a Diesel, the action of the device should operate on the injection pump gear.

707 - ENGINE AND FUEL COMPARTMENT

BULKHEADS

707.1

Bulkheads of engine and fuel tank compartments must be sealed to the hull across the bottom and have sufficient height above the bottom to prevent fuel and fumes flowing throughout the boat. The separate compartments thus formed must have provision for separate pumping out of bilge's.

Fuel containment systems (tanks, cells, etc.)

In all new boats built from 30th November 2001, and all existing canopied craft registered for racing after that date, boats must have fuel filling and fuel venting capabilities outside of the crew containment area. Canopied craft are to have fuel tanks contained in sealed compartments from the hull and crew area to avoid leakage of liquid or vapour being released into the area being used by the crew. Fuel filling / fillers are to be outside of the crew compartments.

707.2

Closing of the fuel flow must be done by means of a device, the control of which must be in the cockpit and within reach of every crewmember.

No sleeved cables are allowed, so as to eliminate the cable being able to be bonded in a fire.

707.3

The fuel tank or tanks must be grounded to enable the discharge of static electricity.

707.4

All new Class 1 gasoline boats measured after 1st January 1995 and all new Class 2 gasoline boats measured after 1st January 1999 must have all fuel tanks filled with any suitable material that will avoid gas explosions.

Fuel tanks may be constructed as an integral component of the hull construction, or may be fabricated material suitably secured to the hull.

Purpose designed and constructed rubberised collapsible fuel cells may be used.

Fuel may not be transferred during a race except by means of permanently installed fuel lines connected to the tanks.

708 - PROPULSION

708.1

One hundred per cent of the propulsive effort shall be derived from the water while proceeding at racing trim in calm water.

708.2

All craft must be able to be manoeuvred ahead and astern and have neutral capability, operated by controls at the helmsman's position. This facility must be a permanent installation and able to be demonstrated as practical at any time.

708.3

Boats running two or more propulsion systems must have either an external tie bar to stabilise out drives or some other protective device that will prevent drives colliding should a failure occur.

708.4

No multi-ratio transmissions, torque converters, or variable ratio drives are permitted in Class 3.

709 - GYROS

709.1

All moveable surfaces allowed on the boat shall be under the control of the onboard crew without any sensors in the control loop (i.e. no auto pilots, gyros, accelerometers or other attitude or acceleration sensors may be in the control system).

710 - RACE NUMBERS

710.1

All boats are to be allocated a race number by their National Authority. These numbers shall be painted in waterproof black paint or to be made of black coloured self-adhesive materials of adequate strength. They shall be applied to a white or yellow background. No number may begin with a zero. All numbers shall be plain and up-right.

710.2

On all boats the numbers shall be positioned as far forward as possible whilst adhering to the requirements of rules 710.3 and 710.4.

On catamaran boats the numbers must be placed on:

- a) The foredeck within front 1/3 of boat.
- b) The tunnel underside within front 1/3 of boat.
- c) The outside of both sponsons within front 1/3 of boat.

On all other boats the numbers must be placed on:

- a) The foredeck within front 1/3 of boat.
- b) Both sides of the hull within front 1/3 of boat.

710.3

Those numbers displayed on the foredeck and on the underside of a tunnel must read correctly from the transom and shall be underlined by a black bar.

710.4

The individual numbers shall conform to the following minimum dimensions, the only exception being where the size of the boat does not allow the minimum size to be carried:

Height	Width	Thickness	Spacing
30 cm	23 cm	5 cm	13 cm

The background shall extend at least 23 cm in front of and beyond the end numerals and at least 7.5 cm above and below the numerals.

The beam numbers may also be painted on slightly curved non-vertical surfaces, provided that the whole number is within 30 degrees from the vertical position. The height must be to the minimum height measured vertically.

710.5

Race Organisers are empowered to require a number to be changed in the event of identical numbers being issued by different National

Authorities. Except in exceptional circumstances the visiting craft has priority.

710.6

The class letter will precede the numbers and they will be separated by a dash. The class letter must conform with the same minimum dimensions as the race number. Class letters are:

Class 2 M

Class 3 6 litre S

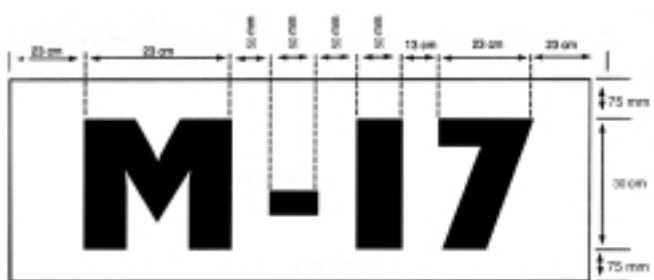
Class 3 4 litre D

Class 3 2 litre C

Class 3 1.3 litre B

Class 3 1 litre A

Dimensions are as per 710.4



711 - NATIONAL FLAG

711.1

All boats are to fly their appropriate national flag ***of the designated driver as determined by the entry***, with minimum dimension 0.45 m x 0.30 m, throughout the race.

711.2

Alternatively, the flag must be painted, or glued, on a panel of not less than 0.45 m x 0.30 m on both sides of the hull.

712 - RACING VESTS

Refer to rules 205.06 and 508.26

713 - HELMETS

Refer to rule 205.07

714 - PROTECTIVE CLOTHING

Refer to rule 205.13

715 - SAFETY EQUIPMENT

Refer to rule 205.12

715.01

The following equipment shall be carried and stowed to the satisfaction of the Race Committee.

715.02

Boats meeting the criteria of U.I.M. Offshore Rule 508 (restraints and canopies) must comply with the requirements regarding storage of safety equipment contained in those rules.

715.03

When additional electric pumps are fitted to the fuel supply a fuel cut-off switch for the pump shall be fitted within reach of either crew member.

715.04

A manually operated bilge pump with the capability of pumping out any one of the watertight compartments.

In addition to the above minimum it is highly recommended to have any additional electric bilge pumps that appear appropriate.

715.05

Suitable gear for anchoring and berthing, including one anchor, not less than 30 metres of suitable chain or warp.

Two towing lines of sufficient strength, one twice the length of the boat.

715.06

Two (2) fire extinguishers to be immediately accessible to the crew, and must be minimum of 2kg each.

For Class 3 / 1, 1.3 and 2 litre there is only a requirement for one fire extinguisher of 2 kg.

715.07

An orange flag measuring at least 0.60 x 0.40 metres and the means of hoisting it, to indicate retirement.

715.08

A fitted steering compass.

715.09

Up-to-date charts covering the whole course of the race.

715.10

Two red hand-held distress flares and two orange smoke flares.

715.11

One whistle attached to each crew member.

One compresses attached to each crew member.

It is strongly recommended that one hand held flare is attached to each crew member.

715.12

A sharp knife suitable for cutting webbing, clothes or rope. The knife shall be suitably sheathed so as to protect crew members against accidental injury. It may be located in a fixed position in the cockpit adjacent to all crew members or, be separately carried on each crew member.

715.13

One diving-mask.

715.14

For Classes 1 & 2

A radio capable of transmitting on the International distress frequency or as may be applicable to the locality of the race, must be carried in full operational condition.

In addition, a “certified waterproof by the manufacturer” hand held unit which shall be carried at all times whilst racing.

715.15

For Class 3

A pail or bucket with a minimum capacity of 5 litres.

715.16

Each Organiser can, in the Advance Programme, specify additional safety equipment necessary for their race.

720 - ENGINES

720.01 - CLASS 2 ENGINES

Outboard or inboard reciprocating engines, whether fuelled by petrol or diesel, used in the offshore series boats, must be produced from a basic cylinder block and cylinder head (original or manufactured as spare parts) of a marine, automobile or industrial engine. All engines must be homologated by U.I.M.

For Class 2, the minimum number of engines built prior to homologation is 100.

720.03 - CLASS 2 ENGINE LIMITATIONS

Petrol Engines

Up to a maximum of 8.2 litre.

Engines must be normally aspirated.

No forced induction is permitted.

Diesel Engines

Up to a maximum of 14.75 litre.

Engines may be abnormally aspirated but will be subject to a penalty of 1.4 times the displacement of the engine.

It is only permissible to have single or twin inboard engines, or multiples of outboards.

In Class 2, all engine modifications are permitted, except for the following which has to remain as homologated:

- stroke
- the number of valves per cylinder
- type of fuel system (carburettors, injection, turbo).

The cylinder block and cylinder head may be modified provided the resultant modifications do not exceed the maximum displacement of the class limit.

All other components are free.

720.04 - CLASS 2 CHAMPIONSHIP CLASSES

In Class 2, there will be individual Championship classification between Inboard engine and Outboard engine boats.

Organisers are free to run both Inboard and Outboard engine boats together in International, Continental or World Championships.

All Championships for the Inboard engine class allow the use of gearboxes with a maximum of "two forward" speeds.

Dispensation as approved by Cominoff on March 2000 :

The use of two-forward speed transmission gearboxes is allowed for all International racing in Class 2.

721 - CLASS 2 HULL DIMENSIONS AND MINIMUM WEIGHTS

721.1

U.I.M. Measured Length:

	Minimum	Maximum
Class 2	9.75m (32ft 0in)	12.00m (39ft 4in)

721.2

Class 2 Minimum Weights:

	Monohull	Multihull
Petrol Outboard	2250 kg	2500 kg
Petrol Inboard	2700 kg	2950 kg
Diesel Inboard	3000 kg	3250 kg

The boat weight is checked at the end of the race. The weight will be an all-up weight including engines, residual fuel, drives and safety equipment as stated in the racing instructions.

At measurement, the following is not included in the all-up weight:

Fuel, bow tank ballast, crew, racing vests, protecting helmets and movable ballast.

Weight must be an integral part of the boat.

CLASS 3

730 - HULLS AND CLASSES

Offshore Class 3 racing shall permit the use of boats powered by either Outboard or Inboard engines.

Outboard powered boats will be classified within their sub-division independent from Inboard powered boats.

Inboard powered boats will be classified within their sub-division independent from Outboard powered boats.

Outboard and Inboard sub-divisions may be run together in the same event but will not be classified collectively.

730.1

Class 3 subdivisions are established in accordance with the following

	3A	3A EPA	3B	3B EPA	3C	3C EPA	3D	3D EPA	3S	3S EPA
Cubic Cap.	390- 1000cc	up to 1000cc	1001- 1350cc	up to 1600cc	1351- 2050cc	up to 3000cc	2051- 4010cc	up to 6000cc	4101- 6000cc	up to 6000cc
Max HP	.....	60 hp	.....	90 hp	.....	200 hp	.....	400 hp	.....	400 hp
Min Length	4.8m	4.8m	5.0m	5.0m	6.2m	6.5m	7.2m	7.6m	7.6m	7.6m
Max length	6.2m	6.2m	6.8m	6.8m	8.4m	8.4m	9.2m	10.0m	10.0m	10.0m
Mono Min kg	350kg	365kg	595kg	635kg	625kg	695kg	1000kg	1350kg	1350kg	1350kg
Multi Min kg	.....	.....	670kg	710kg	775kg	845kg	1250kg	1600kg	1600kg	1600kg
Add Kg per m	40kg		50kg		65kg		100kg		110kg	
Cube										
Length	1.20m	1.20m	1.20m	1.20m	1.20m	1.20m	1.20m	1.20m	1.20m	1.20m
Width	2.35m	2.35m	2.35m	2.35m	2.75m	2.75m	3.25m	3.60m	3.60m	3.60m
Height	0.46m	0.46m	0.46m	0.46m	0.46m	0.46m	0.46m	0.46m	0.46m	0.46m
Bouyancy										
Mono GRP	250l	250l	270l	270l	370l	370l	470l	500l	500l	500l
Mono Comp	130l	130l	140l	140l	150l	150l	250l	280l	280l	280l
Multi GRP	260l	260l	280l	280l	380l	380l	500l	550l	550l	550l
Multi Comp	160l	160l	180l	180l	200l	200l	280l	300l	300l	300l

In the 1 litre class, only monohulls will be accepted.

From 2006 a transition period for the use of EPA/CARB allowed engines will commence. Where non EPA/CARB engines represent less than one third of the participating boats in a world championship event, as described in U.I.M. Offshore rules 800.3, that event will run. However, in the following U.I.M. calendar year non EPA/CARB engines will not be allowed in the specific class.

731 - MINIMUM/MAXIMUM LENGTHS AND WEIGHTS

For Class 3B only there is an additional minimum weight of 170 kg for the combined weights of driver and co-driver, including racing vests and protecting helmets. The weight of the driver and co-driver are to be checked separately pre-race with dry race suits, racing vests and protecting helmets.

If the combined weight of the driver/co-driver does not reach the minimum of 170 kg, the difference is to be added to the minimum boat weight.

When checking weights of boats post race, residual fuel (spare fuel) in the tanks will be included in the minimum weight, together with all safety equipment. Slings carried in the boat during the race will count as safety equipment. The boat must be empty of water.

For all other Class 3 categories, the minimum weight mentioned is for a boat with the mentioned minimum length. For boats longer than the minimum, the weight shall be increased according to the formula shown above.

The weight is ready to race dry, including all safety equipment (except the crew and their helmets and life vests). For control after a race, residual fuel in tanks will be included in the minimum weight. Slings carried in the boat during the race will count as safety equipment. The boat must be empty of water.

732 - HULL DIMENSIONS

732.1

For monohulls, the outer surface of the hull excluding chine-rubbers and spray rails, must contain a cube of the dimensions mentioned in the 730.1 chart.

733 - MINIMUM FLOTATION

All boats shall contain a minimum volume of buoyancy permanently installed in the hull.

The minimum volumes measured in litres are mentioned in the 730.1 chart.

If a boat is constructed of wood, the wooden weight content of the boat may be accepted as buoyancy as per the following formula : Wood weight in kg x 2 = litres of buoyancy.

ENGINE ELIGIBILITY

734 - (NON EPA/CARB) OUTBOARD ENGINES

734.1

Class 3 outboard engines shall be strictly production engines such as the standard marine production engines sold by manufacturers to leisure boating (or touring) customers.

734.2

For a newly homologated engine to be eligible for Class 3, Manufacturers must certify that their motors meet the U.I.M. requirements on noise when fitted to an appropriate boat. Engines already homologated do not require this certification. The noise level is to be measured by means of a phonometer which conforms to the specifications laid down by IEC and ANSI, a precision sound meter using the "A" weighting and set to slow response. The registering microphone is placed at a distance of 25m from the boat. The boat must pass it at top speed. Measurements must be made on open water free of all obstructions over a minimum distance of 25m in all directions. Two measurements must be carried out, while the boat approaches and while moving away. Neither of the measurements is to exceed 91db (A) with a 4db (A) tolerance.

For Mercury XR2 and 2,5 EFI the exhaust plate must be fitted.

734.3

The only outboard engines eligible for use in Offshore are those approved (at time of homologation application) by Cominoff.

734.4

No. of manufactured engines up to	1000 cc = 1000 units.
	1001-2000 cc = 500 units.
	above 2001 cc = 250 units.

734.5

An outboard engine shall be able to run at reduced speed.

734.6

The engine being placed in water, the cooling shall be effected only by its own standard circulation pump. The intake to the circulation pump may be modified for cooling purpose only. Bearing carriers and prop shafts may be changed from original. Refer also to 735.17 for water intake definition.

734.7

An electric or manual starter must assure a quick and easy start without external aids. It shall be used as supplied by the manufacturer, no adjustment or alterations are allowed.

ENGINE ELIGIBILITY (EPA/CARB) OUTBOARD ENGINES

734.8

EPA/CARB approved engines used in Class 3 shall be strictly standard marine production engines as marketed by manufacturers to leisure boating (or touring) customers.

734.9

At time of homologation, manufacturers must certify that their motors meet the U.I.M. requirements regarding noise when fitted to an appropriate offshore powerboat. The noise level is to be measured by means of a phonometer which conforms to the specifications laid down by IEC and ANSI, a precision sound meter using the "A" weighting and set to slow response. The registering microphone is placed at a distance of 25m from the boat. The boat must pass it at top speed. Measurements must be made on open water free of all obstructions over a minimum distance of 25m in all directions. Two measurements must be carried out, while the boat approaches and while moving away. Neither of the measurements is to exceed 91db (A) with a 4db (A) tolerance.

734.10

The only EPA/CARB outboard engines eligible for use in Offshore are those approved (at time of homologation application) by Cominoff.

734.11

No. of manufactured engines up to	1000 cc = 1000 units.
	1001-2000 cc = 500 units.
	2001 cc and above = 250 units.

734.12

The engine being placed in water, the cooling shall be effected only by its own standard circulation pump. The intake to the circulation pump may be modified for cooling purpose only.

Bearing carriers and prop shafts may be changed from original.

Refer also to 735.17 for water intake definition.

735 - MODIFICATIONS PERMITTED **(NON EPA/CARB) OUTBOARD ENGINES**

Only the following modifications are allowed:

735.01

The propeller is free, together with its washer(s) and nut(s).

735.02

Spark plugs may be changed for any other.

735.03

Revolution counters, tachometers, water pressure and temperature gauges may be fitted to the engines.

735.04

Thermostats of the cooling circuit may be removed.

735.05

Over-speed switches may be blocked.

735.06

Re-boring and fitting of oversize pistons is allowed as made available by the manufacturer for that model.

735.07

The steering bar and brackets may be reinforced as a safety measure. It is also permitted to modify the standard engine cowling in order to facilitate the fitting of steering assemblies, provided that any openings are sealed so the engine can not pull in any additional air.

735.08

The rubber mounts of the engine may be changed or substituted.

735.09

A spring may be added to the butterfly valve of the carburetor.

735.10

Mechanical carburetor and ignition advance controls may be adjusted out of their normal setting range as long as the original parts are retained.

Carburetor fuel jets and air jets are free if they can be changed without alterations to the carburetor body of float housing.

735.11

All external (not contained within the powerhead) studs, screws, nuts, bolts and their washers are free as well as the method of locking them, providing there is no performance advantage.

735.12

When twin engines of the outboard type are installed, but the particular type is not available with opposite hand propeller rotation, it is permitted to modify the drive of one unit to obtain contra-rotation provided that:

- the standard underwater unit casing is retained
- the final propeller ratio to the crankshaft is unaltered.

It is also permitted to change the rotation of one unit if an alternative opposite hand rotation underwater unit, or a kit of parts to modify the unit is offered by the manufacturer or his concessionaire. In that event the gear ratio of the single altered engine may vary from standard provided it is an incorporate part of the alternative unit or kit.

735.13

The use of thrust blocks mounted on the lower unit and or on the boat is permitted.

735.14

Power Trim: a properly engineered system for altering engine trim whilst under way is permitted.

735.15

In the process of modification, minor optional alternatives as listed and priced by the manufacturer of the homologated engine, may be permitted provided they do not affect engine performance, but improve convenience and safety.

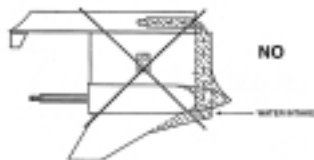
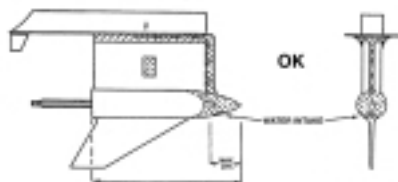
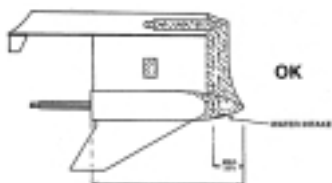
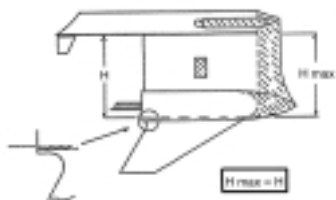
735.16 - RESERVED NUMBER**735.17**

The water intake may be repositioned; it must however remain on the gear case torpedo. The width or height of the gear case torpedo must not be changed. The length fore and aft may be increased. The rear edge of the water intake opening must not be further back than 20% of the total length of the modified gear case torpedo.

The original intake to the circulation pump may be filled in and faired. For cooling purposes it is allowed to add material in front of the union leg, as long as the union leg width is not increased, except where the union leg meets the lower side of the anti-cavitation plate.

The modification shall be for cooling purposes only and shall form a radius. The radius shall be lengthwise parallel to the anti-cavitation plate. It is also permitted to add material to the centre section exhaust housing for the purpose of reducing noise.

Remote water pick-ups are not allowed.



Method of measuring maximum depth of water intake.

735.18

Lighter flywheels as made available by the manufacturer may be fitted.

735.19

Transom brackets may be reinforced.

735.20

The fuel connector in the lower cover may be removed and the fuel hose from the fuel tank connected directly to the fuel pump. If this is done, the opening left after the connector and around the fuel hose must be sealed to prevent extra air entering inside of cover.

735.21

It is permitted to replace lubricating systems using oil tanks and oil pumps with lubrication oil mixed into the fuel. Caps or covers in the motor cover cannot be removed to provide extra air inlet.

735.22

Measurements not quoted in the homologation sheet may not be altered. If a detail cannot be verified with the aid of the pictures in the homologation sheet, the part in question can be compared to a corresponding stock part. In such a comparison it is only possible approximately to use the comparable measurements as the manufacturing tolerances are not available. A component may be machined in order to reach a specific dimension, which is quoted in the engine homologation file. A component for which the weight is quoted in the homologation file may be adjusted down to the weight, by machining or grinding provided that the area where metal is removed is the same as is used by the engine manufacturer.

This is known to be at the top or at the bottom, or both for piston connecting rods. Balancing of one or all piston connecting rods is permitted, as long as this is in accordance with manufacturer's procedures.

Polishing of connecting rods is not permitted. When the volume of the combustion chamber is specified in the homologation sheet, the cylinder head may be faced to achieve the volume allowed.

MODIFICATIONS PERMITTED (EPA/CARB) OUTBOARD ENGINES

735.23

The propeller is free, together with its washer(s) and nut(s).

735.24

The steering bar and brackets may be reinforced as a safety measure. It is also permitted to modify the standard engine cowling in order to facilitate the fitting of steering assemblies, provided that any openings are sealed so the engine can not pull in any additional air.

735.25

The rubber mounts of the engine may be changed or substituted.

735.26

All external (not contained within the powerhead) studs, screws, nuts, bolts and their washers are free as well as the method of locking them, providing there is no performance advantage.

735.27

Where two engines are fitted, the counter rotation of one underwater unit is permitted providing:

- the standard underwater unit casing is retained (subject to 735.31)
- the final propeller to crankshaft ratio is unaltered

735.28

The use of thrust blocks mounted on the lower unit and or on the boat is permitted.

735.29

Power Trim: a properly engineered system for altering engine trim whilst under way is permitted.

735.30

In the process of modification, minor optional alternatives as listed and priced by the manufacturer of the homologated engine, may be permitted provided they do not affect engine performance, but improve convenience and safety.

735.31

The water intake may be repositioned. It must however remain on the gear case torpedo. The width and or height of the gear case torpedo must not be changed. The length fore and aft may be increased. The rear edge of the water intake opening must not be further aft than 20% of the total length of the modified gear case torpedo.

The original intake to the circulation pump may be filled in and faired. The front of the gearcase may have material added for cooling purposes.

The width of the gearcase must not be increased, except where the gearcase meets the lower side of the anti-cavitation plate. The modification shall be for cooling purposes only and shall form a radius. It is also permitted to add material to the centre section exhaust housing for the purpose of reducing noise.

Remote water pick-ups are not allowed.

735.32

Transom brackets may be reinforced.

735.33

The fuel connector in the lower cover may be removed and the fuel hose from the fuel tank connected directly to the fuel pump. The remaining void must be sealed to prevent any extra air entering the cover.

MODIFICATIONS NOT PERMITTED (EPA/CARB) OUTBOARD ENGINES

735.34

The following modifications are expressly NOT permitted.

Blueprinting' of the powerhead.

Alteration or modification of any powerhead components.

Removal of material from the powerhead for any reason other than recognised (by the manufacturer) repair.

736 - INBOARD ENGINES

736.1

The only inboard engines eligible for use in Offshore are those approved (at time of homologation application) by Cominoff. To be eligible for Class 3, marine inboard engines must have a minimum production of 250 engines.

736.2 - NOISE REDUCTION

At time of homologation manufacturers must certify that their motors meet the U.I.M. requirements on noise when fitted to an appropriate

offshore powerboat. The noise level is to be measured by means of a phonometer which conforms to the specifications laid down by IEC and ANSI. A precision sound meter using the "A" weighting and set to slow response. The registering microphone is placed at a distance of 25m from the boat. The boat must pass it at top speed. Measurements must be made on open water free of all obstructions over a minimum distance of 25m in all directions. Two measurements must be carried out, while the boat approaches and while moving away. Neither of the measurements is to exceed 91db (A) with a 4db (A) tolerance.

737 - MODIFICATIONS PERMITTED **INBOARD ENGINES**

737.1

The preparation and modification of inboard engines is completely free, with the following restrictions.

737.2

The cylinder block must be the original one as supplied by the manufacturer, with no modifications permitted except the re-boring of the cylinders, which may be done within the limits indicated by the manufacturer and provided that this does not make the engine exceed the cylinder capacity of its class.

737.3

Modification of the original crankshaft is entirely free, provided that the original stroke is maintained.

737.4

Modification of the cylinder head is entirely free, provided no more valves per cylinder are added.

737.5

- All petrol/gasoline engines must be naturally aspirated. No forced induction, either turbocharged, supercharged or other means is allowed.
- Diesel engines may have double engine capacity.
- All diesel engines are subject to the 1.4 penalty for turbo and supercharging.

737.6

The electric starter and its battery are compulsory as well as a device for recharging the battery.

800 - GENERAL RULES FOR OFFSHORE WORLD AND CONTINENTAL CHAMPIONSHIPS (EXCEPT WPC)

800.1.1 - WORLD CHAMPIONSHIP

There can only be one World Championship per year for each class.

800.1.2 - CONTINENTAL CHAMPIONSHIP

There can only be one Continental Championship per year for each region and for each class.

800.2 - REQUEST TO ORGANISE

The request to organise the Championship event must be made by a National Authority and be submitted to the U.I.M. before the stated deadline date. The request will be considered by Cominoff and after approval, entered on the U.I.M. Offshore Calendar as a titled meeting.

800.3 - MINIMUM PARTICIPATING

For an event to be valid, there must be a minimum of five participating entries from two Nations for each of the Inboard and Outboard categories (unless otherwise agreed by Cominoff). An event for either the Inboard or Outboard category only may also be valid if a minimum of five participating entries from two Nations participate from that category. Entries are to be sent by the National Authority to the organising club, no later than 7 days before the beginning of the Championship.

“Participation” means:

- for the crew, passing administrative control and signing the attendance register at the Driver's Briefing.
- for the boat, passing the Technical Controls.

800.4 - WITHDRAWN ORGANISER

If a National Authority, having been granted a Championship for which it applied, subsequently withdraws or is unable to run the Championship, Cominoff, if time permits, will re-offer the Championship to other National Authorities.

If the Championship cannot be run by another National Authority, the Championship is cancelled for that year.

800.5 - DRIVERS AUTHORISATION

Each National Authority must authorise, in writing, its drivers to participate in any Championship event.

800.6 - UIM COMMISSIONER

A U.I.M. Commissioner is to be appointed by Cominoff to every International Championship event.

800.7.1 - BAD WEATHER COURSE

Should bad weather compel the organisers to have any race run on a shortened course and/or an alternative start time, points are granted provided:

That a minimum of 1 hour notification, prior to the start time, of the amendment to the course or the start time is given to each driver in a Race Bulletin. A register must be signed by every driver confirming receipt of the Race Bulletin.

Should the race be stopped for any reason of 'force majeure', the boats will be classified according to 312.01, providing 50% of the minimum race distance set out in 800.13 has been completed. If this distance has not been completed, no points will be awarded.

800.7.2 - STOPPING AT THE RACE

The organisers may stop the race at some checkpoint of the route before the official finish, and/or if the race has been run more than 30 nautical miles, and/or the drivers have been racing for at least 1 hour, the race will be approved as a Championship race. The curtailment decision must be ratified by the Race Committee immediately after the race.

800.7.3 - CURTAILED RACE

A curtailed race is a race shortened by the Race Committee after the start.

The signal to be used to announce it is the raising of the red flag.

800.8.1 - MINIMUM LAP DISTANCE

Should any part of the course form a lap of less than 6nm for Class 3, or 10nm for Class 2; Such a lap must not be repeated more than twice in a World Championship or a Continental Championship.

Deviation from these rules is allowed only when the race is run on a shorter (storm) course as described in the Race Instructions in which case there is no minimum length.

800.8.2 - WATER DEPTH

The race courses run, including any rough weather course, should have a minimum water depth at all times of racing or pre-race testing. For all classifications, the minimum depth is 4 metres.

800.9.1 - INTERVALL BETWEEN RACES - WC

In a World Championship - If the whole event takes place from a single race site then a scheduled interval of at least 44 hours, but not to exceed 70 hours between each heat is compulsory. Should a race be re-scheduled, the actual interval of at least 20 hours, but not to exceed 60 hours between each heat is compulsory.

800.9.2 - INTERVALL BETWEEN RACES - CC

In a Continental Championship - If the whole event takes place from a single race site then a scheduled interval of at least 20 hours, but not to exceed 60 hours between each heat is compulsory. Should a race be re-scheduled, the actual interval of at least 20 hours, but not to exceed 60 hours between each heat is compulsory.

800.9.3

If a series Championship is based on several race sites the calendar is to be as agreed by Cominoff.

800.9.4 - LAY DAY

There must not be a lay day after the final scheduled race day.

800.10.1 - POINT ACCUMULATION

In a World or Continental Championship, the results will be determined from points accumulated from all the heats in accordance with U.I.M. rule 321. If at one venue there will be two or three heats, if not at one venue the number of heats will be determined by Cominoff when they agree to the calendar. It is not compulsory to participate in all heats. The named driver must drive the same boat throughout the series. A change of boat will mean a new entry into the Championship, freezing points accumulated prior to the change.

The Boat is defined as a Hull/Engine configuration as stated on the Measurement Certificate.

For events with more than 20 entries, the following bonus points should be given, so that all drivers can get points.

21 - 25 boats : 5 bonus points

Position 1) to 20) : should all have 5 extra bonus points

Position 21 : 5 points

Position 22 : 4 points

Position 23 : 3 points

Position 24 : 2 points

Position 25 : 1 point

26 - 30 boats : 10 bonus points

Position 1) to 20) : should all have 10 extra bonus points

Position 21 : 10 points

Position 22 : 9 points

Position 23 : 8 points

Position 24 : 7 points

Position 25 : 6 points

Position 26 : 5 points

Position 27 : 4 points

Position 28 : 3 points

Position 29 : 2 points

Position 30 : 1 point

31 - 35 boats : 15 bonus points

Position 1) to 20) : should all have 15 extra bonus points

Position 21 : 15 points

Position 22 : 14 points

Position 23 : 13 points

Position 24 : 12 points

Position 25 : 11 points

Position 26 : 10 points

Position 27 : 9 points

Position 28 : 8 points

Position 29 : 7 points

Position 30 : 6 points

Position 31 : 5 points

Position 32 : 4 points

Position 33 : 3 points

Position 34 : 2 points

Position 35 : 1 point

36 - 42 boats : 20 bonus points, etc...

800.10.2 - WORLD CHAMPIONSHIP - 3 HEATS

A World Championship will consist of 3 heats at one venue unless otherwise agreed by Cominoff.

800.10.3 - CONTINENTAL CHAMPIONSHIP - 2 HEATS

A Continental Championship will consist of 2 heats at one venue unless Cominoff has agreed to a series.

800.10.4 - CANCELLED RACE

If in the case of a scheduled two race series, only one heat can be run, this will be a valid result. If a three heat or more series, then the result may be decided upon two heats, if the rest are cancelled through 'force majeure'. The Race Committee should ratify the cancellation if through 'force majeure'. If a heat is not run on its scheduled day or its scheduled lay day(s), then it will be cancelled.

800.10.5 - N° 1 DRIVER

The No.1 driver must control the boat in some way, either as driver or throttleman in all heats.

800.11 - POST-RACE INSPECTION

Engines and engine parts may be replaced between the races. The Race Committee must be informed and have given its allowance in advance.

Replaced engines or parts shall be handed over to the Technical Inspectors, or be sealed by them. If an engine is to be inspected after the last race, the replaced parts must be inspected also.

All dimensions must be measured with the engine at ambient temperature.

The title of champion is only granted after a satisfactory post-race inspection of the winning boat's equipment, hull and engine(s).

Should the boat fail and is deemed to be disqualified, the next placed boat is then inspected.

After each heat in a multi venue series or after the final heat of a single venue championship, the first three boats must be inspected. If found illegal, then the 4th boat should be inspected until the Race Committee can announce a 1st, 2nd and 3rd which have been found to be correct.

800.12 - TIE

If a tie exists in the final points accumulation then the winner of the longest heat will win. If this cannot split the two, then the boat with the fastest speed in any heat will win.

800.13 - RACE LENGTH

The races must conform to the following length:

WORLD CHAMPIONSHIPS:

Class 2	1 x 100 - 130 nm	2 x 80 - 110 nm
Class 3 / 6	1 x 70 - 100 nm	2 x 45 - 70 nm
Class 3 / 4	1 x 70 - 100 nm	2 x 45 - 70 nm
Class 3 / 2	1 x 70 - 100 nm	2 x 45 - 70 nm
Class 3 / 1.3	1 x 50 - 70 nm	2 x 30 - 50 nm
Class 3 / 1	1 x 50 - 70 nm	2 x 30 - 50 nm

CONTINENTAL CHAMPIONSHIP - one venue/multi - each race must be:

Class 2	1 x 100 - 130 nm	1 x 80 - 110 nm
Class 3 / 6	1 x 70 - 100 nm	1 x 45 - 70 nm
Class 3 / 4	1 x 70 - 100 nm	1 x 45 - 70 nm
Class 3 / 2	1 x 70 - 100 nm	1 x 45 - 70 nm
Class 3 / 1.3	1 x 50 - 75 nm	1 x 30 - 50 nm
Class 3 / 1	1 x 50 - 75 nm	1 x 30 - 50 nm

There may be a slight variance to these rules to allow for local situation and these must be agreed by Cominoff or at an event by the U.I.M. Commissioner.

800.14 - N° 2 DRIVER

The title will be declared as a Driver No.1/Driver No.2 combination if the No.2 Driver has completed all heats of the championship with the No.1 Driver.

800.15 - N° OF REGISTERED BOATS

There should be at least 25 boats in that class in the previous year and the boats should be actively raced in at least 5 countries before Cominoff will award a World Championship.

There should be at least 15 boats in that class in the previous year and the boats should be actively raced in at least 3 countries before Cominoff will award a Continental Championship.

800.16 - PROTESTS

All Protests will be handled by the normal U.I.M. Jury procedure by the N.A. running the heat from which the protest arises.

800.17 - REPORT TO UIM

Immediately after the completion of the final heat (if one venue) or after each heat, if multi venue series, but not later than 6 hours after the results are out of protest time, the Race Secretary must fax the following to the U.I.M. office:

- a) Entry list with information of: Crew names, Nationality, Boat name, Boat race number, Hull manufacturer and type, Engine manufacturer and type.
- b) Results from all heats if single venue or the heat and the relative standings in the championship.
- c) The results should be typed and show distance run, times speed in knots and must be signed and time dated by the O.O.D.

The above is to be faxed to the U.I.M. office on + 377 92 05 04 60. Proof of transmission should be given to the U.I.M. Commissioner.

800.18 - CONFIRMATION OF UIM

The points allocated will be confirmed by the U.I.M. on receipt of the U.I.M. Commissioner's report.

800.19 - ENTRY FEE - PRIZE MONEY

The National Authority shall guarantee an amount to U.I.M. as agreed with Cominoff, for each race of the U.I.M. Championship. If there is no transportation and prize money made available, no entry fee shall be charged to the visiting nations participating.

855 - ADVISORY REQUIREMENTS FOR U.I.M. **CLASS 1 AND 2 CHAMPIONSHIP EVENTS** **- SECURITY AND LOGISTICS**

Recommended minimum requirements for U.I.M. Offshore Class 1 and 2 Championship events - security and logistics.

COURSE

The milling area and starting chute must comply with the relevant U.I.M. Offshore Rules. It must be ensured that these areas are clearly marked and that all drivers are provided with written briefing describing the exact location of these marks. Immediately before and during the start, the race organisation must provide suitably identified patrol craft to keep these restricted areas clear of all spectator craft. The Starter and the Safety Officer must both receive confirmation that the milling area and the starting chute are clear before the start procedure can begin.

All turning points of the course must be clearly identified and have a control boat in close proximity, but not closer than 50 m from the turning mark. If a permanent navigational mark is not utilised, then the mark will be a temporary clearly identifiable coloured buoy that should not be less than 3 m high X 1 m diameter, anchored in position. A control boat must not be the turn mark. All marks shall be provided with compass co-ordinates in the written instructions.

Where the course has a turning point presenting an angle less than 90 degrees, two orange marker buoys must be utilised, anchored at a minimum distance of 100 m apart.

The first turning mark should be at least 5 nautical miles from the start line, or 3 nautical miles if less than eight boats are starting.

SAFETY ON RACE COURSE

Safety procedures that must be provided during races and official practise periods are :

- 1) The provision of, and organisation of well experienced rescue teams responsible for the recovery of drivers and/or persons involved in a race accident.
- 2) Availability of adequate rescue equipment that has been maintained and tested.
- 3) All emergency units in the vicinity of the event must be notified. These include hospitals, police/coast guard and fire services.

The provision of the rescue facilities applies primarily to persons. It does not take into consideration the recovery of race boats or equipment.

The task of the rescue teams is to maintain a watch of the complete race course or practise area during a race and/or official practise periods. Rescue teams may be placed on boats or in helicopters.

Safety briefings must take place between all persons responsible for safety and key personnel must attend the drivers briefings.

SAFETY DURING TESTS

The course for practise tests must be well defined as follows :

- defined on a nautical chart of area distributed with Race Instructions or at Drivers Briefings;
- defined by marker buoys as utilised for race course, i.e. 3 m high x 1m diameter buoys.
- course area to be no more than 8 km x 2 km (4.3 Nm x 1.1 Nm)
- patrols boats, always moving, must be positioned along the course in order to patrol a surrounding area of 5 Nm x 5 Nm; in any case the area will be the maximum area that can be entirely visible with the naked eye at sea level;
- patrol boats during the whole practise session must be in contact (by VHF) with race control and they must have on board an expert crew including scuba divers;
- an anaesthetist reanimation doctor must always be on a patrol boat;
- during the practise tests, a heli-ambulance must be on alert, in the proximity of the race course, in case of accident, for the transportation of seriously injured person(s) to a specialised hospital.

SAFETY DURING RACE

Patrol and Control Boats

Patrol boats must be positioned around the complete course, which should be divided into a grid pattern. The patrol boats must be each allocated an area of approximately 5 Nm x 5 Nm, which should not exceed naked eye visibility at sea level. All control boats and patrol boats must be clearly identified and should have the authority to keep spectator boats clear of the course.

Control boats must take a safe position, where possible inside the course, avoiding screening the mark buoy(s) from competitors. Control boats must be limited to carrying officials and crew (maximum 7 persons).

Patrol boats must be capable of rescue work. They therefore must be large enough to stay at sea if conditions should deteriorate and be able to receive 2 laid stretchers. At least three of these boats should be equipped with a minimum of six trained crew which shall include two divers and one doctor/paramedic. They should carry rigid stretchers.

These boats must be capable of maintaining constant communication with race control and shall have on board :

- red flags and red flares
- strong towing lines
- boat hook
- fire extinguishers for fuel fires
- first aid kit

AIR ASSISTANCE DURING THE RACE

For Classes 1 and 2 with more than 12 Competitors

A minimum of two rescue helicopters equipped for sea rescue and piloted by experienced pilots shall be available for the full duration of the race. These should both be flying during the start procedure and preferably for the duration of the race. They must each be equipped with the following :

- Two well trained scuba divers, qualified for jumping from helicopters, fully equipped for rescue operation ;
- paramedic ;
- portable reanimation kit ;
- stretcher for injured persons ;
- one floating stretcher.

For all other races, 1 rescue helicopter must be available to fly for the duration of the race.

MAIN TASKS OF RESCUE HELICOPTERS

- 1) Helicopter's crew must patrol the course during the race and alert race control in case of any accident or dangerous situation during the race.
- 2) Radio communications and, if necessary, radio link between local parts of the course and race course to guide the rescue team to the location of the accident.
- 3) Urgent diver availability in case of accidents in order to help drivers escape from cockpits and to provide first aid at sea.

PIT AREA

A security team must be provided 24 hours at the Dry Pits, the Wet Pits and Fuelling area during the event.

An adequate Dry Pit area shall be provided for each boat with sufficient area available for :

- 1) Service centre.
- 2) Parking for trailer/tractor units.
- 3) Organisers must provide an adjacent area for team cars and/or motor caravans.

For Classes 1 and 2 the boat parking area shall be minimum 25 m x 5m; the truck parking area shall be minimum 20m x 4 m.

The Race Organisers must provide an authorised landing area for helicopters. The Organisers must also provide a reanimation centre and two ambulances complete with specialised medical personnel for the duration of the event in the proximity of the landing area and the pit area.

A Wet Pit area suitably protected and with pontoon berthing should be provided for berthing of emergency boats, or other alternative berthing area be provided that is suitable for the transfer of injured persons to an ambulance.

An electrical supply capable of powering power hand tools and for general illumination of the area shall be available for each team.

A high pressure water supply shall be provided for the washing of boats and flushing of engines.

POST RACE SCRUTINEERING AREA

A closed under cover area capable of being closed to unauthorised persons for the purpose of engine inspections by the scrutineers shall be available.

CRANAGE

Cranage must be provided at a suitable location as close as possible to the wet pits. Two cranes of a minimum 30 ton capacity shall be provided complete with operators.

If more than 20 entries are present, there should be a crane and operator for every 10 boats.

At least one crane should operate during test periods.

At the cranage area there should be available a high capacity water suction pump with suitable flexible suction piping attached for the purpose of assisting a swamped boat.

At least two divers should be in attendance.

Access to the cranage area shall be restricted to authorised persons only.

FUELLING AREA

A designated compound should be allocated for fuelling. This compound should be within reasonable proximity to the pit area. An adequate supply of best available commercial grade gasoline and diesel must be available to every competitor. The fuelling facility should be readily available to all competitors as required.

Adequate fire protection equipment must be available with trained operators at all times when fuelling facility is open.

The fuelling area must be barricaded from the public and unauthorised persons with clearly visible warning signs displayed. All local hazardous goods regulations must be met.

All transference of fuel must be done within the fuelling compound.

The Race Organisers must make available a secure area complete with the equipment necessary to empty boat fuel tanks after testing or racing. This is in the interests of safety and also respecting the environmental rules.

To cater for the prevention of pollution at the race site, the following facilities should be provided within the fuelling compound:

- One 200 litre container for gasoline.
- One 200 litre container for oil.
- One 20 litre container for thinners with halogens.
- One 20 litre container for thinners without halogens.

- A bin for empty cans etc.
- A bin for discarded batteries.

RACE CONTROL

Race control should consist of modules. These primarily being:

- 1) **A Central Control Room** restricted to the race organising committee and U.I.M. Commissioner(s). All communication to other authorities should originate from here.
- 2) **A Radio Control Room** restricted to the operating personnel and other officials such as the Safety Officer and the Officer of the Day. These two rooms should be in close proximity and have constant communication between them.
- 3) **Timekeepers.** Their module should preferably have viewing of the course at the start/finish line. The Timekeepers should have direct communication with every control point on a dedicated radio channel. No other personnel should be permitted entry whilst a race is in progress.
- 4) **First Aid Room.** This module should be a base for the medical personnel. During the race, a qualified medic shall be present and an ambulance shall be in attendance or be available for immediate response. The room should have sufficient facilities to enable it to be used for trauma treatment. It must have direct communication with the control room and have telephone facilities.
- 5) **Briefing Room.** This module must provide seating for all crew members and officials required to be present for briefings and drivers meetings. It should have a public address system and if foreign drivers are participating, an interpretation service must be provided. Unauthorised personnel should not be permitted entry during briefing sessions. All briefings should be recorded on video and audio.

- 6) **Private Meeting Facilities** should be available for the purpose of providing private consolation for family members or friends in the event of an accident, or to conduct enquiries.
- 7) **Toilet and Washing Facilities** must be readily available for all crew members in close proximity to the pit areas.
- 8) **Security** must be provided 24 hours in the dry pits, the wet pits and the fuelling area during the event.

PRESS

At all Championship events a Press Centre shall be provided. This centre shall provide telephone, fax facilities, typewriters and a photocopier.

This centre shall include conference facilities for conducting press releases and for interviews with successful teams and/or personalities.

The size of the above facilities shall be in accordance with the number of entries and the importance of the event

The press room should be restricted to authorised press personnel, officials and successful race crews.

It is recommended that adequate hospitality be extended to all bona-fide members of the press.

V.I.P. HOSPITALITY

Organisers should be aware of the presence of and provide hospitality for VIP s including International U.I.M. Officials and their companions. It is recommended that a covered area be provided for such persons to meet, collect current race information and have warm beverage available. Complimentary passes should be issued to these persons.

856 - RECOMMENDED MINIMUM

REQUIREMENTS FOR U.I.M. CLASS 3

CHAMPIONSHIP EVENTS

GENERAL

Class 3 Championship events must be organised and run by a competent Power Boat Club, approved by the National Authority of the country in which the event is to be held. The National Authority shall monitor the preparation and planning of the event including; venues, travel arrangements for participating crews and transportation of their boats, lead-in media coverage, all aspects of race safety and race courses and technical support services.

When planning the race courses the organiser must bear in mind the spirit of Rule 701.01, i.e. offshore powerboats are intended to race in open and unprotected waters. However, the races must also be interesting to the viewing public and consideration must be made to have sections of the course within viewing distance from vantage points on shore.

The courses must be approved in advance by the appointed U.I.M. Commissioner via the U.I.M. office prior to the programme / racing instructions being issued to the drivers.

An accommodation reservation service for competing crews and other visitors must be provided.

COURSE

The race course including the milling area and starting chute must comply with the relevant U.I.M. Offshore Rules. It must be ensured that these areas are clearly marked and that all drivers are provided with written briefing describing the exact location of these marks. Immediately before and during the start, the race organisation must provide suitably identified patrol craft to keep these restricted areas clear of all spectator craft. The starter and the Safety Officer must both

receive confirmation that the milling area and the starting chute are clear before the start procedure can begin.

All turning points of the course must be clearly identified and have a control boat in close proximity, but not closer than 50 m from the turning mark. If a permanent object is not utilised, then the mark will be a temporary clearly identifiable coloured buoy that should not be less than 1.5 m high x 1 m diameter, or equivalent, anchored in position. A control boat must not be the turn mark. All marks shall be provided with compass co-ordinates in the written race instructions.

For further definitions of marks of the course, reference should be made to Rule 303.01.

SAFETY ON RACECOURSE

Safety procedures that must be provided during races and official practise periods are:

- 1) The provision of and organisation of experienced rescue teams responsible for the recovery of drivers and/or persons involved in a race accident.
- 2) Availability of rescue equipment that has been maintained and tested.
- 3) All emergency units in the vicinity of the event must be notified. e.g., hospitals, police, fire services, etc.

The provision of the rescue facilities applies primarily to persons. The recovery of race boats and equipment is secondary.

The task of the rescue teams is to maintain a watch of the complete race course or practice area during a race or official practise period. Rescue teams may be placed on boats or in helicopters.

Safety briefings must take place between all persons responsible for safety and key personnel must attend the drivers briefings.

SAFETY DURING TESTS

The course for practice tests must be well defined on a nautical chart and be identified by marker buoys or by suitable permanent navigational marks.

The practice course area should be no more than 6 km x 1 km (3.2 Nm x .54 Nm).

A minimum of two Rescue Patrol Boats must be utilised during official practise sessions, each boat being positioned in the vicinity of the extremities of the practice area. These boats must have on board an expert crew including scuba divers and medical personnel.

Rescue Patrol Boats must be capable of maintaining constant communication with race control during the entire practise sessions.

During the practise sessions, an ambulance must be on alert, in the vicinity of the practice area, for the transportation of seriously injured persons to a suitable hospital.

SAFETY DURING RACES

Patrol and Control Boats

Rescue Patrol Boats must be positioned around the complete course. The spacing of these rescue boats must no greater distance than naked eye visibility at sea level between each boat. Each Rescue Patrol Boat shall be clearly identified with a suitable flag or identifiable hull.

Rescue Patrol Boats must be capable of rescue work. They must therefore be large enough to stay at sea if conditions should deteriorate and be able to receive two laid stretchers. At least three of these boats should carry two divers and a medical person (doctor or paramedic) with resuscitation equipment and first aid kits. These patrol boats must be capable of maintaining constant communication with race control and shall also have on board:

- strong towing lines
- boat hook

- fire extinguishers

Turn Control Boats must take a safe position, where possible inside the course at a minimum distance of 50 m from the mark buoy or other permanent course marker. They shall carry a red flag and be capable of maintaining constant communication with race control.

AIR ASSISTANCE DURING RACES

It is strongly recommended, but not mandatory, that at least one rescue helicopter be available for the duration of a race. Each helicopter should be equipped with the following:

- well trained scuba diver, qualified for jumping from helicopters, fully equipped for rescue operation
- qualified paramedic prepared to jump from the helicopter to provide patient care at sea with
- advanced life support training and equipment
- portable reanimation kit
- stretcher for injured persons
- one floating stretcher

It is recommended that the rescue helicopter be in the air and patrolling the course during the race and alert race control of any accident or dangerous situation during the race. It is therefore necessary to have effective communication between the helicopter and race control.

In the event of an accident the helicopter may assist direct patrol boats to the accident scene and provide the rescue diver to assist retrieve any trapped or injured drivers. The paramedic may also provide emergency life saving procedures to injured drivers.

PIT AREA

Dry Pits: A security team must be provided 24 hour at the Dry Pits and the fuelling area during the duration of the event, or as agreed with the Commissioner.

An adequate area must be allocated for each boat/trailer and tow vehicle. There must also be an area provided for any team cars and/or motor caravans.

There must be an undercover area provided for the repair and inspection of a minimum of two boats. A suitable work bench, lighting and lifting equipment must be included. This area must be able to be secured for the purposes of after race inspections.

Sufficient fire extinguishers of adequate capacity must be available within the dry pit area. Signs shall be provided to indicate the position of these.

There shall be no smoking or any exposed flames or fires of any type permitted in the pit areas. Smoking may be permitted in a designated smoking area. Refer to Rule No. 205.11. The Race Committee shall penalise any person found not respecting this Rule.

An electrical supply capable of powering power hand tools and for general illumination of the area shall be available for each team.

An adequate fresh water supply and suitable hose shall be provided for the washing of boats and flushing of engines.

Wet Pits: A wet pit area, suitably protected and with berthing available for all race boats after launching and prior to retrieving must be provided. Preferably, the wet pits should be in close proximity to the dry pit area. An area should also be available for the berthing of rescue boats with the ability to easily transfer any injured person to an ambulance.

Launching Facilities: Ramp launching facilities and craneage must be provided at a suitable location as close as possible to the wet pits. There should be at least one crane available for every fifteen boats that require craneage.

At least one crane must be available during test periods.

Divers should be in attendance at the launching area to assist launching and retrieving if necessary.

Access to crane area shall be restricted to authorised persons only.

There must be adequate fire extinguishers available at the launching and wet pit areas. Signs shall be provided to indicate the position of these.

FUELLING AREA

A designated compound must be allocated for all decanting of fuel, including de-fuelling of any damaged boats. This compound should be in reasonable proximity to the pit area.

An adequate supply of best available commercial grade gasoline and diesel (if required) must be available to every competitor. The fuelling facility should be readily available to all competitors as required within the times nominated in the race instructions.

There must be adequate fire extinguisher cover available with trained operators at all times when the fuelling facility is open.

There shall be NO SMOKING or exposed flames of any type in the fuelling area. Adequate signs must be displayed to indicate this.

The fuelling area must be barricaded from the public and unauthorised persons and clearly visible warning signs must be displayed. All local hazardous goods regulations must be met.

All transference of fuel at the race site must be done within the fuelling compound.

Fuelling may be carried out at a public service station if practical. However, all safety precautions must be met and no transference of fuel from the boat can take place at these sites.

To cater for the prevention of pollution at the race site, the following minimum facilities should be provided within the fuelling compound:

- one 200 litre container for fuel
- one 100 litre container for oil
- one 20 litre container for thinners with halogens
- one 20 litre container for thinners without halogens
- a bin for empty cans etc.
- a bin for discarded batteries

RACE CONTROL

Race control should consist of modules. These primarily being:

- 1) A Race Control Room restricted to the Race Officials and U.I.M. Commissioner. All communication to other officials and authorities should originate from here.
- 2) A Radio Control Area, preferably adjacent to the Race Control Room that is restricted to the radio and communication officers and other officers such as the Safety Officer, the O.O.D. and the U.I.M. Commissioner.
- 3) Timekeepers. Their module should preferably have viewing of the course at the start/finish line. The Timekeepers must have direct communication with the Race Control Room and every Control Boat. No other person should be permitted entry whilst a race is in progress.
- 4) First Aid Centre. This centre shall be a base for the medical personnel. It should be located in near to the pit areas. During the race and during practise periods a qualified doctor or first aid person shall be in attendance or be available for immediate response. The centre should have sufficient facilities to enable it to be used for trauma treatment. It must have direct communication with Race Control and have telephone facilities readily available.
- 5) Briefing Room. This module may be a permanent room or a suitable temporary structure that must provide seating for all drivers and crew members as well as all officials required to be present for

briefings and drivers meetings. It must have a public address system if necessary, as well as suitable audio and/or video equipment to record all drivers meetings and/or briefings. The briefing must be in English. An interpretation service may be provided.

- 6) Toilet, Washing Facilities and Changing Rooms must be readily available for all crew members in close proximity to the pit areas.
- 7) A Press Centre shall be provided at the race site. This centre must be equipped with telephone and fax facilities for the use of bonofide press personnel. Also a photocopier should be available.

This centre shall include facilities for interviewing successful teams and/or personalities.

The size of the above facility shall be in accordance with the number of entries and the importance of the event.

The press Centre should be restricted to authorised press personnel, officials and successful race teams.

- 8) Jury Meeting Room. A private meeting room must be made available for meetings of the Jury. This room, or another suitable room should also be made available for the purpose of providing private consolation for family members or friends in the event of an accident, or to conduct enquiries.
- 9) Prizegiving. A suitable room or tent should be provided for all social activities, receptions and after race prizegiving functions. Suitable trophies should be presented to at least the top three drivers/crews and entry recognition plaques or similar trophies to all other competitors.

PROMOTIONAL ACTIVITY

Race Organisers should ensure that their event is adequately advertised to encourage maximum exposure to the general public.

The design of the race courses should be made considering the availability of suitable vantage points for people to view the races.

A public speaker system should be arranged to provide the viewing public with a complete commentary of the race progress and any other associated activities.

V.I.P. HOSPITALITY

Organisers should be aware of the presence of and provide hospitality for VIPs, including International U.I.M. Officials and their companions. It is recommended that a covered area be provided for such persons to meet, collect current race information and have beverage available. Complimentary passes should be issued to these persons.

860 U.I.M. CLASS 1 WORLD POWERBOAT CHAMPIONSHIP 2005

These 860 rules are specifically for the administration of the U.I.M. Class 1 World Powerboat Championship and are the primary rules for this series. For rulings not contained in this section, reference must be made to the general offshore rules contained in this Rulebook

TABLE OF CONTENTS

860.01 TITLE

860.02 INTERPRETATION OF THE RULES

860.03 CALCULATIONS (PERCENTAGE/FRACTION)

CHAMPIONSHIP

860.04 POINTS

860.05 CHAMPIONSHIP

860.06 CHAMPIONSHIP TIES

860.07 CREW/BOAT COMBINATION

860.07.1 Nationality

ADMINISTRATION

860.08 ADVANCE NOTICE

860.09 EVENT INSURANCE

860.10 RACE ADMINISTRATION

860.10.1 Registration

860.10.2 Scrutineering

860.10.3 Boat Representative

860.10.4 Drivers' Representative

860.11 DRIVERS BRIEFING

860.12 LICENCES

860.12.1 Super Licence

860.12.2 Navigator Licence

- 860.12.3 Test Driver Licence
- 860.12.4 National Authority authorisation of all WPC
Licences
- 860.12.5 Restraint Indemnity
- 860.13 PARTICIPATION RULES
- 860.14 RACE PARTICIPATION OF NON WPC TEAMS
- 860.15 CLASS 1 WORLD POWERBOAT CHAMPIONSHIP
COMMITTEE (WPCC)
- 860.16 WPCC FUNCTIONS
- 860.17 PROTEST & APPEALS
- 860.18 JURY
- 860.19 PENALTIES

SAFETY

- 860.20 SEA SAFETY
- 860.21 FUELLING
- 860.22 SMOKING
- 860.23 CLOTHING
 - 860.23.1 Racing vests
- 860.24 BOAT EQUIPMENT
 - 860.24.1 U.I.M. Tracking System
- 860.25 RADIO COMMUNICATION

EVENT - PRACTICE / POLE POSITION / RACE

- 860.26 BOAT PARADE
- 860.27 PRACTICE
- 860.28 POLE POSITION
- 860.29 RACE COURSE
 - 860.29.1 Turn Buoys

- 860.29.2 Right of Way
- 860.29.3 Spectator Areas

860.30 LAUNCHING

860.31 PIT STOPS

- 860.31.1 Definitions
- 860.31.2 Pit Stop Rules

860.32 OUTSIDE ASSISTANCE

860.33 START PROCEDURE

Race Start Procedure

860.34 FINISHING PROCEDURE

- 860.34.1 Stopping the Race
- 860.34.2 Curtailing the Race
- 860.34.3 Official Flags
- 860.34.4 Finishing Procedure
- 860.34.5 70% rule
- 860.34.6 Time Limit
- 860.34.7 No Finishers
- 860.34.9 Stopped / Curtailed Race
- 860.34.10 Returning to the Pits
- 860.34.11 Retirements
- 860.34.12 Post Race
- 860.34.13 Assistance in accidents

860.35 BAD WEATHER

860.36 PRIZE GIVING

860.37 LAY DAYS

860.38 RESCHEDULING OF RACES

BOAT RULES

860.39 REGISTERED BOAT

- 860.39.1 Recovery of damaged / sunken race boats

860.40 LOGOS

860.41 RACE NUMBER DIMENSIONS

- 860.41.1 Similar Boat Identification

860.42 ENVIRONMENT

HULL DIMENSIONS AND MINIMUM WEIGHTS

860.43.1 Reinforced cockpit

860.43.2 Escape hatch

860.44 ENGINES

860.45 ENGINE SPECIFICATION

860.46 ENGINE COMPONENT RESTRICTION

860.47 AIR RESTRICTOR / PLENUM CHAMBER

860.47.1 Air Restrictor Testing

860.47.2 Plenum Chamber Sealing and Vacuum test

860.48 ENGINE VENDOR REGISTRATION

860.49 U.I.M. DATA ACQUISITION

860.50 TRANSMISSION RESTRICTIONS

860.5.1 Propulsion

860.51 PROPELLERS

860.52 TRACTION CONTROL

860.53 CREW CONTROL

860.54 COMPETITOR DATA ACQUISITION (AS USED BY THE COMPETITOR)

860.55 FUEL

WPC = U.I.M. Class 1 World **Powerboat** Championship

WPCC = Class 1 World **Powerboat** Championship Committee

The Rules and/or Regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all **WPC** events and by participating in the events, all U.I.M. members are agreed to abide by these rules.

No expressed or implied warranty of safety shall result from publications of, or compliance with these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to participants, spectators, or others.

860.01 - TITLE

The title of the Championship will be known as the 'U.I.M. Class 1 World **Powerboat** Championship' (**WPC**).

860.02 - INTERPRETATION OF THE RULES

In all cases not foreseen by the international rules or seemingly inaccurately defined, the **WPCC** is to judge bearing in mind the spirit of the rules.

The U.I.M. Rules for series and classes are to be considered as being supplementary to the General Rules.

In the case of conflicting rules, those rules concerning the series are to prevail. U.I.M. Rules are to prevail over National Authority rules, unless it is a safety or insurance requirement of the National Authority or a legal requirement of the community in which the race is being held.

English is the working language.

Official communications of the race will be shown on an Official Notice Board.

860.03 - CALCULATIONS (PERCENTAGE/FRACTION)

Throughout the 860 rules, where a percentage/fraction needs to produce a whole number, the number will be rounded up or down to the nearest whole number. Where the percentage/fraction results in .5 (1/2), the number will be rounded down unless specifically stated in the rule.

For example:

70 % of 7 laps is 4.9, 5 laps would count.

70 % of 5 laps is 3.5, 3 laps would count

CHAMPIONSHIP

860.04 - POINTS

In any of the Class 1 World ***Powerboat*** Championship Races, the points will be awarded to the first and second named drivers only.

The first and second named drivers must be the helmsman, throttleman or navigator. The Navigator cannot be the first named driver.

Points scored in the World Offshore Championship Races and Pole Position are as follows.

Classification	Points
1st	20
2nd	15
3rd	12
4th	9
5th	7
6th	5
7th	4
8th	3
9th	2
10th	1

860.05 - CHAMPIONSHIP

The Class 1 World ***Powerboat*** Championship will be determined from points accrued from all the Class 1 World ***Powerboat*** Championship Races. All races carry points with no discards. Of the planned races at least 50% must be held to award the World Championship title.

If any Class 1 Championship includes some or all of the **WPC** races, then the rules and points awarded will be as per the 860 rules.

860.06 - CHAMPIONSHIP TIES

In the case of a tie in the overall Championship and / or Pole Position Championship, the number of first places shall be considered, then the number of second places etc.

In the case of the Championship still being a tie, the boat with the fastest average speed in any of the Championship races will be deemed the Champion. In the case of the Pole Position Championship still being a tie then the position will be shared.

860.07 - CREW/BOAT COMBINATION

The points for the U.I.M. Class 1 World **Powerboat** Championship will be awarded in principal to the crew/boat combination. The crew is composed of the drivers enrolled to the **WPC**. The driver No. 1 is the person responsible for the boat. The driver No. 1 may be replaced by the driver No. 2 in the case if physical problems of a temporary nature or once in the whole season for any other problems. The **WPCC** must authorise the replacement so that the points are awarded. The driver No. 2 will also be classified and receive awards in the final championship classification together with the driver No 1 provided they have participated together in more than 50% of the races counting for the championship and gained whilst together.

860.07.1 - NATIONALITY

The nationality of the entry is that determined by the Team as declared at the first race. The nationality of Driver No. 1 or Driver No 2 is as declared at the First Race and whose nationality is determined by either the Passport or Racing Licence of that driver.

ADMINISTRATION

860.08 - ADVANCE NOTICE

The Advance Notice will replace the Advance Programme and must include the following:

1. Date of Event
2. Venue
3. Timetable
4. Course Layout
5. Special Conditions
6. Local Contact

In addition, the following Insurance requirements must be included:

1. Terms and conditions of insurance for personal and material damage to third parties, competitors included if available.
2. The minimum limit of insurance cover required.
3. The type of risks to be covered by the insurance.
4. The amount of insurance fees to be paid at the race site.

The Advance Notice must be received by the U.I.M. Office for distribution at least 60 days before the Race having been agreed by the Series Promoter, IOTA and the U.I.M. Permanent Commissioner.

No course, timetable or race site logistics can be published without direct approval of **WPCC**, IOTA and the Series Promoter.

860.09 - EVENT INSURANCE

The Organising Committee must have an Insurance Policy which covers all Race and Non Race activities between Competitor to Organiser liability. It must include event third party insurance which is to cover all non racing activities and in particular any damage resulting from towing

or launching of the boats and full third party cover for all race officials and associated race helpers.

The Organiser's insurance will be available from the Organising Committee during Race Administration.

The competitors must have a racing third party insurance. The maximum amounts must not be inferior to those required by the law of the countries where the races will be held.

See U.I.M. Rule 205.09 for further details.

All insurance must cover the whole period of the practice, Pole Position and races from beginning to end of the event.

860.10 - RACE ADMINISTRATION

860.10.1 - REGISTRATION

Registration will take place at the Race Secretariat

Race Administration times will be as per the Advance Notice. All Race Administration requirements (including payment of insurance) must be completed by the time Race Administration closes. Failure to do so will result in a fine of Euro 600 and/or disqualification.

On the registration form of each World Offshore Championship Race, the names of the first and second drivers together with reserve and test drivers must be specified. Also the name and boat number and a copy of the current valid measurement certificate with registration number must be supplied.

On signing in ***at race administration***, a Scrutineering slip will be issued, which will enable competitors to present their boats for Scrutineering.

All Official Race Information, Bulletins and Results will be posted at the Race Secretariat.

Competitors will be provided with identification tags, if required, which must be worn at all times during practice, Pole Position and racing.

Any person other than a racing crew, wishing to embark on the boat for practice, will only be authorised under the condition that they are a U.I.M. licensed reserve/test driver and have registered as such during Race Administration or as otherwise agreed by the Permanent Commissioner. He must sign up with the Organiser's Insurance Agent and be in possession of all current documents required by the U.I.M. and organising committee.

All race numbers will be between 1 and 99 and will be allocated by the **WPCC** after the entry has been accepted. Number 1 will be allocated to the previous year's World Champion. The replaced number of the World Champions boat will be reserved for this boat to resume when it is no longer the World Champion.

Race Bulletins will be issued and signed for at Race Briefings and / or the Race Secretariat.

860.10.2 - SCRUTINEERING

Technical Scrutineering will take place in the Dry Pits and all competitors are required to have their boats in this area during the times detailed in Event Timetable.

No boats can be scrutineered before production to the scrutineer of the scrutineering slip.

It is the responsibility of the crew to contact a scrutineer.

All boats submitted for scrutineering should be absolutely ready to race with all equipment required, as described in the U.I.M. rules.

Boats may be scrutineered outside the times stated by prior agreement, in writing, with the **WPC Technical Officer** and at his discretion only.

860.10.3 - BOAT REPRESENTATIVE

Each boat must designate its representative at each race venue for contact in case of an accident or Force Majeure who must be present and registered at Race Administration.

860.10.4 - DRIVER'S REPRESENTATIVE

There may be a Driver's representative.

The election of such representative must be held in the presence of an U.I.M. Official. Candidates must be real drivers. To be eligible, they must have obtained the following minimum experience.

Have competed in 4 international races or 40 % of the International races (total rounded up to the next unit) during the previous season.

The Driver's Representative is elected for a 1 year term. He should be available to attend **WPCC** meetings and/or Cominoff meetings at least annually to report and advise on the consensus recommendations and requests from the drivers'.

860.11 - DRIVERS BRIEFING

It is mandatory for both first and second drivers to be present at the main briefing and 1 of the 2 nominated drivers for the weather briefing (if held).

Signing in for any briefing will begin 20 minutes before scheduled drivers briefing time. It is mandatory for all competing crew members to attend and sign in at the briefing. All signing in must be completed prior to the drivers briefing time. Penalty for non-attendance - Euro 1,100 fine per person and / or disqualification.

Drivers who are late for a briefing. Penalty Euro 600.

It is essential that all competitors read and are totally aware of the contents of the Race Instructions and / or any Race Bulletins before attending the Driver's Briefing.

It is the responsibility of all drivers to bring the Event Race Instructions and any bulletins to all Briefings.

860.12 - LICENCES

Every crew member of the boat must be licensed by his National Authority.

The first and second named drivers must hold an appropriate U.I.M. Licence before their entry will be accepted.

A minimum of 1 'Super Licensed' driver who is responsible for the boat and crew during 'practice must be aboard. The event insurance must be complied with.

860.12.1 - SUPER LICENCE

The application for an Offshore Super Licence must be made by the National Authority of the driver concerned and lodged with the U.I.M. before the first event the driver wants to participate in. The price of the Super Licence will be Euro 1,000.

If application is made on the race site, the Super Licence fee may be doubled to Euro 2,000 at the discretion of the **WPCC**. The driver's National Authority is responsible for providing the data to the U.I.M..

The application must be authorised by the driver's National Authority and be accompanied by:

1. The sports career of the driver, including the results obtained in the previous two seasons of offshore racing.
2. Confirmation by the driver's National Authority that the applicant holds a current offshore licence and immersion certificate.
3. A medical certificate providing all the data required by the U.I.M. Rules.
4. 2 photographs (passport size).
5. The fees fixed by and paid to the U.I.M.

The Offshore Super Licence is valid from the date of issue to 31st December of the same year.

The U. I. M. may issue the Offshore Super Licence on the basis of the information supplied by the driver's National Authority and with the approval of the **WPCC** Chairman (or representative). The U.I.M. may withdraw an Offshore Super Licence on the recommendation of the relevant driver's National Authority and the agreement of **WPCC**.

The maximum age for a driver to qualify to hold a U.I.M. Offshore Super Licence, Navigator licence or Test licence and to compete in a **WPC** race is 60 years old unless otherwise agreed by the **WPCC**.

The minimum age for a driver to qualify to hold a U.I.M. Offshore Super Licence, Navigator licence or Test licence and to compete in a **WPC** race is 18 years old unless otherwise agreed by the **WPCC**.

To obtain an Offshore Super Licence in Class 1, each driver or throttleman must have at least one of the following qualifications for an application to be permitted.

1. Raced during the previous 2 years in Class 1 or II or III (6 litre or 4 litre) or equivalent as driver or throttleman in at least 7 races.
2. Other classes may be taken into consideration as agreed by **WPCC**.

The **WPCC** may authorise a provisional Super Licence for the Second Driver who may not have the above-mentioned sports career to participate in the competitions provided that:

1. The First Driver is well-experienced and takes the responsibility of the Second Driver's behaviour in the race and;
2. He has an Offshore Licence issued by his relevant National Authority and he meets the requirements of rule 860.12, 3rd paragraph items 2,3,4 and 5 and;
3. The application must be authorised by his N.A.

The Offshore Super Licence can be issued after having raced a minimum of 3 races in the season and to be agreed by the **WPCC**.

The application and nomination by the driver's National Authority will be deemed to be the National Authority's consent to represent their country for the duration of the Super Licence. Should the NA not wish a Super Licence holder to represent his country at any time, the NA must inform the U.I.M. in writing.

860.12.2 - NAVIGATOR LICENCE

In the case of a boat that is completely controlled by one Driver (throttles and rudder), the first driver must have a “U.I.M. Super Licence” and the second Driver may have a “U.I.M. Navigator Licence”

A “U.I.M. Navigator Licence” will be issued by the U.I.M. with the approval of **WPCC** and authorized by the relevant N.A.

A Driver may be authorised by **WPCC** to use “U.I.M. Navigator Licence” on the condition that the first driver is well experienced and he takes the responsibility of the Navigator’s behaviour in the race and has the complete control of the boat (throttles and rudder).

To qualify for this licence the following conditions must be complied with:

1. A current immersion test certificate
2. A medical certificate providing all the data required by the U.I.M. Rules
3. 2 photographs (passport size)
4. The fees fixed by and paid to U.I.M. of Euro 600
5. The application must be authorised by his N.A.

The ‘Navigator’ licence is valid from the date of issue to December 31st of the same year.

860.12.3 - TEST DRIVER LICENCE

A U.I.M. ‘Test Driver’ licence will be issued by the U.I.M. to use during event ‘official practice times’. To qualify for this licence the following conditions must be complied with:

1. A current immersion test certificate
2. A medical certificate providing all the data required by the U.I.M. rules.
3. 2 photographs (passport size).

4. The sum of Euro 220 paid to the U.I.M.
5. The application must be authorised by his N.A.

The 'Test Driver' licence is valid from the date of issue to December 31st of the same year.

860.12.4 - NATIONAL AUTHORITY AUTHORISATION OF ALL WPC LICENCES

In the case of Non authorisation, the National Authority must have just cause and notify the applicant within 1 week of the application, or it will be deemed to be authorised.

860.12.5 - RESTRAINT INDEMNITY

U.I.M. rules mandate the signing of the National Indemnity form regarding restraints. In view of the International nature of the **WPC** series and the fact all boats use restraint systems, in applying for the licence, the competitor confirms that he has read and understood the U.I.M., his National Authority's and the APBA rules regarding restraint systems and confirms that the boat entered for the event in which he is taking part has been constructed to and conforms with these rules. He further undertakes not to hold the U.I.M., his National Authority, the APBA, the Organising Club, or any of the servants or agents of the foregoing, nor any other person connected with the organisation of an event, responsible for any personal injury incurred by his wearing this restraint system. He will also produce on demand a copy of his National Authority's rules for restrained cockpits.

860.13 - PARTICIPATION RULES

The Team entries for the Championship must be received by the U.I.M. via the **contracted promoter** and notified to the relevant N.A. and must include at least the Owner's name, the names of the First and Second Drivers and the race number of the boat. The name of the two drivers must be stated including any reserve/test drivers. If through Force Majeure the declared drivers are not available to race, the U.I.M.

Permanent Commissioner may allow an agreed suitably qualified driver to race the boat as a substitute.

It is mandatory for each entrant to participate in all races of the World Offshore Championship to accumulate points unless otherwise agreed in writing by the **WPCC**.

WPCC will decide the penalty which may be a fine of min. Euro 11,000 max. Euro 55,000 and/or alternative penalty for contravention of the above.

«Participate» means - boat and drivers must have passed all technical and administrative controls together with attending the race briefing and must be ready and able to race. In case of Force Majeure, dispensation may be obtained in writing from the **WPCC**

In the case of accident damage to a boat, the driver may apply to the **WPCC** for dispensation not to compete in the next races until the damage is repaired. The **WPC Technical Officer** must verify and certificate the damage and report to the **WPCC** If the dispensation is requested for damage outside the races the **WPC Technical Officer** must verify the boat where it is situated. The applicant must pay all the costs.

Insription Fee & Prize Money - as per **contracted promoters'** agreement.

860.14 - RACE PARTICIPATION OF NON WPC TEAMS

The W.O.C.C may authorise non **WPC** teams to participate in any races of the Championship. The entry will only receive awards for the race and will not accrue points for the Championship or obtain prize money.

860.15 - CLASS 1 WORLD POWERBOAT CHAMPIONSHIP COMMITTEE (WPCC)

For the purpose of ensuring professionalism of the World Offshore Championship, the U.I.M. shall institute a Class 1 World **Powerboat** Championship Committee (**WPCC**) made up as follows:

1. Cominoff President – 1 vote
2. Representatives as appointed by Cominoff and agreed by **WPCC** from the relevant N.A. involved in the **WPC** with 1 vote each (**for 2005 Norway, Qatar, UAE, UK**)

To be eligible for representation at **WPCC**, the relevant National Authority must have a team and a race in **WPC**. The representative should not have a vested racing interest, however, it is important that the representative is capable of contributing active input to the Committee.

The representative must be approved and agreed by **WPCC**.

The aim is to have a maximum of 4 representatives.

3. Teams Representative with 2 delegates carrying 1 vote total.
4. Promoter Representative with 2 delegates carrying 1 vote total. (Series Promoter)

Other specialist representatives, at any time, may be nominated by and at the discretion of **WPCC** without vote to attend the meetings.

The Committee Chairman is elected by the **WPCC** by simple majority. All decisions made by the Committee are by simple majority.

860.16 - WPCC FUNCTIONS

The World **Powerboat** Championship Committee shall:

1. Appoint the **WPC** Commissioners. The U.I.M. Permanent Commissioner, **WPC Technical Officers**, Permanent Administrator/**WPC Timekeeper** and Permanent Safety / Medical Co-ordinator.
2. Approve the Officer of the Day and the local Safety Officer who will be chosen from a U.I.M. approved list and selected by the relevant National Authority. If possible, these Officers will be from the nation where the race is held.

3. Cominoff may appoint a U.I.M. Commissioner with the approval of the **WPCC** for each venue.
4. Approve the World Offshore Championship calendar and venues with the agreement of the Cominoff and the relevant National Authority who shall not unreasonably withhold consent.
5. Approve the amount of the entry fee that each entrant to the World Offshore Championship must pay to the **contracted promoter**.
6. Approve the minimum amount of prize money for each World Offshore Championship Race.
7. Approve any rescheduling of a race cancelled for Force Majeure (860.38).
8. Decide all rules for the **WPC**, which must be ratified by Cominoff and Council.
9. Decide on any dispute concerning any question on the rules of the World Offshore Championship.
10. Approve the on and off water events logistics of each venue after liaising with the local organiser and the relevant National Authority including the Courses for Practice, Pole Position and the Race.
11. Review annually and alter as necessary any of the stated amounts of money fines and / or fees as stated in these rules.
12. Approve the Super Licence holder to participate in **WPC**

The committee will meet at least once a year, or at any time if requested by the Chairman of the **WPCC** or 2/3rds of its members.

860.17 - PROTEST & APPEALS

All protests and appeals are to comply with current U.I.M. rules as applicable to **WPC** racing (**§ 403, 404, 405**).

All protests must be accompanied by a fee of Euro 600 which will be returned if the protest is upheld.

The protest must be made in writing by the licensed driver and be signed and handed personally to the Race Secretary at Race Secretariat within the time stipulated in the U.I.M. rules.

The secretary of the Race Jury will inform the protester and all other entitled parties concerned of the time and place of the hearing, which will be at the earliest convenient time. In the event of the protester not attending the protest hearing, the Jury may dismiss the protest.

It is the responsibility of the parties involved in a protest to ensure that witnesses appearing on their behalf are present and the Chairman of the Race Jury at his/her sole discretion may take their availability into account when determining the time of the hearing.

Notwithstanding anything to the contrary herein contained, no protest may be made against any result posted as a consequence of any deliberation made by Race Jury.

No claim for damages arising from an infringement of any of the U.I.M. rules or the Advance Notice or any of the Race Instructions or Race Bulletins shall be adjudicated upon by any Race Committee or appeal authority, but shall be subject to the jurisdiction of the Courts.

The crew of a racing **Powerboat** which acknowledges infringing a rule does not thereby admit liability for damages.

The findings of fact, and the decision of the protest committee shall be relevant only to the purposes of the **Powerboat** Racing Rules and shall not be referred to in any proceedings for damage without the written consent of all parties to the protest.

860.18 - JURY

The U.I.M. Jury will comprise of the following who are entitled to speak and vote

1. one president
2. a delegate from each National Authority of any No 1 or No 2 drivers participating, authorised in writing by their NA and registered before the close of Race Administration.

3. the U.I.M. Permanent Commissioner

The U.I.M. Permanent Administrator may be responsible for the Jury minutes.

If there are less than 3 voting members on the Jury then the U.I.M. Permanent Administrator and / or the U.I.M. **WPC Technical Officer** may become members of the Jury.

A list of Jury members will be posted on the Official Notice board after the close of registration.

860.19 - PENALTIES

Penalties for infringements where the offence and / or penalty are not specifically stated will be given by the Officer of the Day or the Race Committee or the Permanent Commissioner as appropriate and should be relevant to the offence and the danger and / or advantage gained by the offence.

In the event of a protest, penalties may be agreed, rejected or changed by the Jury. In addition, as the result of the findings of a protest, the Jury may impose a penalty on the offending party.

If an infringement is being investigated during a race, this information may be communicated to the relevant Team Manager.

Penalties that can be given

- Reprimand (**see art 406.04 Offshore rules**)
- Yellow Card / **Warning** (**see art 406.05 Offshore rules**)
- Time penalty
- Lap penalty (**Long lap – see procedure below**)
- Fines
- Deduction of Points
- Disqualification (**see art 406.06 Offshore rules**)
- Temporary Suspension (**see art 406.07 Offshore rules**)
- **Prolonged suspension** (**see art 406.08 Offshore rules**)
- **Exclusion** (**see art 406.09 Offshore rules**)

Long Lap Penalty procedure

The team manager will be informed by radio of the Long Lap Penalty. When the race boat passes the “finish boat” for the first time after the Team Manager being informed, a Class1 white flag will flown together with displaying the boat number.

This penalty must be carried out within TWO laps of the communication to the Team Manager and /or the displaying of the number. If the above is not carried out within the two laps the penalty is in immediate DQ. It is the Team Managers responsibility to inform their boat.

The race boat must do the penalty Long Lap in addition to the one mandatory Long Lap in addition to the one mandatory Long Lap for all competing boats.

No protest is permitted with reference to this penalty.

New penalties may be implemented during the season and will be announced at the main drivers briefing.

Two Yellow Cards in the same racing season, will result in a Long Lap Penalty during next race (in the current or further racing season).

Three Yellow Cards in the same season, will result in disqualification for that race.

The Race Committee should take care not to substitute monetary fines for infringements of a technical nature or infringements that would normally result in disqualification or suspension.

The financial fines will be collected by the race secretary and will be checked by the promoter. They will be added to the general prize funds which will be distributed at the end of the season.

SAFETY

860.20 - SEA SAFETY

Drivers must have the full Safety Equipment for all Practice, Pole Position and Races.

Communications via VHF radio are allowed excluding Official Race Safety Channels and Channel 16.

Competitors must be fully conversant with, and at all times, must comply with, the International Regulations for the Prevention of Collisions at Sea and such other safety measures promulgated in connection with this event.

It shall be the driver's sole and ultimate responsibility to ensure the fitness of his boat and to decide whether or not to start or to continue in the race once the boat has passed scrutineering.

The Organisers wish to inform competitors that all safety and patrol boats will be instructed that their first priority at any incident is saving human life. If possible, although no guarantee can be given, assistance may be given to boats.

In case of an incident or accident on the racecourse, a yellow flag will be flown or waved by any "Official" boat. Race boats should keep a distance of at least 50 metres away from any boat displaying this flag and the accident.

Harbour restrictions. All boats must respect the local rules with reference to the maximum speed in the harbour. A Euro 600 penalty for infringement of this rule may be enforced.

It is the Team's sole and ultimate responsibility to recover damaged and / or sunken participating race boats.

860.21 - FUELLING

Fuelling or decanting of fuel may be made until 2400 hours (or as announced in the relevant Racing Instructions) on the day prior to the

race. In the case of Force Majeure, an Official Bulletin may authorise the refuelling on the race day. Penalty – Disqualification.

Any team who dispenses fuel either in or out of the boat other than in the designated fuelling area or without prior approval of the U.I.M. Permanent Commissioner will be subject to a fine of:

- First time of the season: Penalty - Euro 1,200
- Any future offence: Penalty - a much heavier fine and/or disqualification.

Fuelling times will be notified in the Race Instructions or by Bulletin and posted on the Official Notice Board.

860.22 - SMOKING

“All smoking of any type is prohibited in pit areas”. The deemed area for non -smoking is in the dry pits (excluding inside workshops), crange area and in the wet pits.

Competitors are reminded that it is their responsibility to ensure that the rule is strictly adhered to by all their team members. Any contravention of this rule shall incur a penalty of Euro 600.

860.23 - CLOTHING

Any person aboard any boat partaking in races, Pole Position and practice must always wear an adequate race vest as per U.I.M. rules. Racing vests must be worn during all races, Pole Position and practice. Racing vests may only be removed when effecting repairs and with the boat at very slow speed (less than 10 knots). The choice and efficiency of a race vest is the sole responsibility of the wearer.

Any person aboard any boat partaking in races, Pole Positions and practice must always wear an adequate helmet of predominantly orange colour. Helmets must be worn during all races, Pole Position and Practice. Helmets may only be removed when effecting repairs and the boat is at very slow speed (less than 10 knots). The wearer is entirely responsible for the choice and the efficiency of his helmet.

At all times during practice, Pole Position and the race, competitors or test drivers must wear suitable protective clothing that covers the torso and all limbs to the wrists and ankles. Protective clothing used must be durable enough to provide bodily protection and it is recommended to be fire retardant. (U.I.M. rule 205.13).

This protective clothing must have the name of the competitor clearly visible on the front chest section or waist band or the competitor must wear clothing with no person's name and apply to Race Control for a wristband.

Contravention of any of the above clothing rules will result in a minimum of a Euro 600 fine.

It is strongly recommended that neck collars are worn.

860.23.1 - RACING VESTS

During all official times, all assistance / safety / rescue boat teams and all teams support boats must wear racing vests.

860.24 - BOAT EQUIPMENT

The following items are not mandatory for the World Offshore Championship Class 1 (*Ref. § 508.28 & 715*)

- Orange tip on the bows and number of occupants - unless the number of occupants exceeds 2
- All Flares
- Flag
- Torch
- Fog Horn
- Navigation Lights
- Sea Anchor
- Normal Compass and Deviation card provided an electronic compass is used
- Waterproof Radio

- Compress & whistle
- Anchor
- Official Charts, provided Race instructions charts are on board

860.24.1 - U.I.M. TRACKING SYSTEM

It will be mandatory for each race boat to carry, fit and operate a U.I.M. tracking system when requested to do so by the U.I.M. Permanent Commissioner. The data will be used by the U.I.M. solely.

860.25 - RADIO COMMUNICATION

Radio communications with the Race Boats will be maintained on the designated VHF channel as detailed in the Event Race Instructions, Bulletins or Briefings.

Competitors must use at all times the Race Number of their boat as their call sign.

Competitors are required to communicate with Race Control before leaving and returning to their berth in the Wet Pits at all times except when proceeding to the Pole Position and / or Race.

No boat will be allowed outside of the harbour before a satisfactory radio communication has been established with Race Control.

Penalty for non-compliance – Penalty Euro 600

EVENT - PRACTICE / POLE POSITION / RACE

860.26 - BOAT PARADE

All boats must take part in the Boat Parade unless previously agreed by the Permanent Commissioner 1 hour before the official start time.

860.27 - PRACTICE

Any boats wishing to practice during the official practice period, must communicate with Race Control on the designated event channel for approval before proceeding out on the course.

All boats must also call Race Control on safe arrival back in the harbour. Penalty Euro 600.

Practice times will be detailed in the Event Race Instructions and/or a Race Bulletin. Any practising outside of these times must be agreed by the OOD, U.I.M. Permanent Commissioner and Teams Representative.

Practice will not be allowed outside of the official practice times and / or the official practice course. Boats must not cut across the practice course except in the case of Force Majeure and with extreme caution. Penalty for non-compliance - Euro 600 per boat for 1st offence, Euro 1,200 for subsequent offences in the event and/or the championship.

Official practice will only take place after Administration and Scrutineering has been completed satisfactorily. Penalty for non-compliance will result in a penalty of Euro **1.200**.

All boats practising must return to and be alongside in the wet pits by the practice finish time. Failure to do so will result in a penalty of Euro 600 per boat for the 1st offence and Euro 1,200 for subsequent offences during the event and / or the Championship.

860.28 - POLE POSITION

1. A Pole Position will be held over a timed course the day before the race, except for Force Majeure.
2. If a boat crew who due to technical problems cannot participate in the Pole Position, the crew must give notice to the U.I.M. Permanent Commissioner at least two hours before the start time of the Pole Position.
3. The **WPCC** will approve all rules for the official Pole Position.

4. All communication to the Race Boats will be on the event official VHF channel.
5. Boats may run ***in accordance with the official race instructions*** within the hour duration
6. The Pole Position course will be run on the full Race Lap unless otherwise agreed by **WPCC** and announced in the Advance Notice, Race Instructions and / or Bulletin.
7. Boats must be at very slow speed (max 10knots) in the harbour at all times. Failure to comply will result in a penalty of Euro 600 and/or disqualification from the Pole Position.
8. Boats may leave the harbour when the green flag is raised and proceed to the Milling Area area.
9. Boats must proceed from the harbour directly to the **Milling** area. While in the **Milling** area, all boats must mill around the Milling Area buoy in an anticlockwise direction and at a very slow speed (max 10knots)
10. During Pole Position, boats can remain in the wet pits and go out to the **Milling** area at any time.
11. Speeding (max 10 knots) in the **Milling** area will result in a penalty of Euro 600 and/or disqualification from the Pole Position.
12. The green flag as flown by the Official Observer Boat is the definitive signal for Pole Position to commence and will be flown throughout the Pole Position when the course is open.
13. The red flag as flown by the Official Observer Boat is the definitive signal for the race boat to stop and return to the **Milling** area and the Pole Position course is closed.
14. The race boat may proceeds from the **Milling** area around the Pole Position course as a "warm up". Boats on a timed lap have priority and room must be given.
15. On passing the lap timing line, timing will commence. On passing the timing line for the second time, boats should reduce speed

without impeding boats which are commencing their timed lap, and return to either the Milling Area or the pits. Multiple timed laps in one run will not be permitted unless by prior arrangement with the Permanent Commissioner. No multiple timed laps will be permitted after 55 mins of the Pole Position.

16. Once a boat has passed the Official Observer Boat, it is deemed to have commenced on a timed lap, even if it does not reach the Start Timing Line or does not complete the lap.
17. The Pole Position duration will be 60 minutes. The start time of the Pole Position is when ***the U.I.M. Commissioner opens the circuit and the green flag is raised***. Any boat that has crossed the timing line within the 60-minutes limit is allowed to complete their timed lap providing that lap is completed within 10 minutes maximum.

The length of any stoppages is calculated from when the red flag is raised from the Official Observer Boat until the green flag is raised again.

18. ***Unless by prior arrangement with the Permanent Commissioner***, boats must return to the ***Milling Area*** before commencing any timed lap.
19. Boats are allowed a total of three timed laps. No dispensation for any reason will be given to re run a timed lap.
20. Boats completing a lap and wishing to return to the Pits must not interfere with boats wishing to continue around the course
21. After the start time of the Pole Position, the priority use of the cranes is for pit stops. It is the sole discretion of the ***WPC Technical Officer*** to decide when any other usage, including launching and recovery of race boats will take place. Any requirement for "Late launching"

will be under the sole discretion of the ***WPC Technical Officer***.

22. No refuelling will be allowed during the Pole Position.
23. The result of the Pole Position will be determined by the fastest timed lap.

24. Boats compete in the Pole Position for the Pole Position Championship. Pole Position trophies will be awarded to the 1st, 2nd and 3rd placed boats for the overall Pole Position Championship.
- 25. *In the case of a tie in the Pole Position race, the points will be shared. The boat that gets the best time first in the Pole Position will have the better start position in the race.***
26. No part of the Pole Position is subject to protest.
27. In the case of Force Majeure or accident the Pole Position will be stopped by the waving of a red flag from the Official Observer Boat and / or official boats. All boats must stop their timed lap and proceed back to the Milling Area area.
28. Boats disqualified from the race for technical reasons will automatically be disqualified from the event including the Pole Position.
29. Under no circumstance during the Pole Position may any boat run outside of the Pole Position course area. Failure to comply will result in a fine of Euro 1,200 and/or disqualification.

860.29 - RACE COURSE

The race must be programmed to be multi-lap with a minimum of **12** and a maximum of 20 laps unless otherwise agreed by the **WPCC** Race Laps must be repeated. The length of any World **Powerboat** Championship Race shall be a minimum of **90** nautical miles unless otherwise agreed by the **WPCC** and a maximum of 1 hour.

The Time schedule, all Practice, Pole Position and Race courses must be approved by the U.I.M. Permanent Commissioner, Promoter Representative, OOD and Teams Representative who reserve the right to change any of the course marks for any reasons that they may be considered necessary. All race boats must pass through the lap timing/finish line, which is a mark of the course.

Under no circumstance during the event may any boats practice, Pole Position or race outside of the designated area. Failure to comply will result in a fine of **up to** Euro 1,200 .

Failure to complete the course correctly as described in the Race Instructions and/or at Race Briefing and/or Race Bulletins, will result in disqualification.

Long Lap

It is mandatory for all racing boats to do one or more Long Laps, as specified in the race instructions, at any time commencing from the beginning of lap 3 and to be completed by the end of the 70% lap. Failure to do the Long Lap will result in Disqualification.

860.29.1 - TURN BUOYS

All marks are laid in the approximate positions referred to in the Event Race Instructions. It is the competitor's responsibility to drive close enough to the course marks to ensure that he has been seen to pass correctly and to satisfy himself to this effect.

Unless otherwise specified in the Event Race Instructions the colour of the Course Buoys must be as follows:

Milling Area Buoy :	YELLOW
All Outside Course Marks	YELLOW
All Inside Course Marks	ORANGE

Should a buoy go 'missing' the driver must pass through the approximate geographical position referred to in the event race instructions. Alternatively, the buoy may be replaced by a boat waving a yellow flag – this must be rounded in a safe manner ***at a minimum distance of 50 m.*** and in the same direction as the original buoy.

Penalty for not passing through the geographical position or around the waved yellow flag – disqualification. Penalty for rounding the boat / yellow flag in an unsafe manner – **Yellow card.**

If the buoy is attached to, or next to a Permanent Navigation point, then the Permanent Navigation point will also become a mark of the course if the buoy becomes missing.

If a boat hits a buoy on the correct side, no action will be taken. If a boat goes over a buoy or demolishes a buoy, he will be deemed not to have gone around it and must take appropriate correcting action. If it is demolished, the team will be fined Euro 1,800 **as refund of the buoy cost**.

Any single buoy passed on the wrong side will result in one Long Lap penalty. No repetition of any buoy is permitted. To take the missed buoy again will result in Disqualification. Failure to make all the Long Lap Penalty/s by the end of the race will result in Disqualification.

860.29.2 - RIGHT OF WAY

In addition to Rule 313.11 when passing a mark, and an overlap has been established, both boats must maintain their distance from the mark until the mark has been rounded.

Boats which are being lapped are encouraged to take a wide line around turn buoys in order not to impede the faster boats.

If a driver does not comply, he will be warned with a yellow card.

860.29.3 - SPECTATOR AREAS

Organisers should designate spectator areas. If a race boat finds itself in a spectator area, it must reduce speed to less than 12 knots immediately and proceed at less than 12 knots with caution until back on the racecourse. Penalty Euro 600 fine and Yellow Card.

860.30 - LAUNCHING

Launching and recovery facilities are available to competitors during official races, Pole Positions and practice. The operational hours are indicated in the Race Timetable. No facilities will be available until

signed in and having completed Administration and Technical Scrutineering.

No liability will be accepted by the Organisers for any damage caused directly or indirectly whilst moving, launching and recovering boats, trailers or any other equipment.

All boats movements, launching and recovery instructions will be under the control of the Pits/Launch Official.

Once signed in, no boat may leave the compound for any reason unless agreed by the Permanent Commissioner. The penalty is at the discretion of the Permanent Commissioner.

In all cases when a crane is used it is forbidden for any person to be on or in the race boat while the boat is being lifted. The penalty for non-compliance will be a Euro 1,200 penalty and/or disqualification from the event.

The order of launching for the race and the choice of the crane and launch time will be decided by the classification order of the Pole Position.

If the race boat, is not ready to be launched at its designated time

1. The team will be fined Euro 600
2. The boat will be launched at a time to be decided solely by the **WPC Technical Officer**.

No boat will be recovered from the water once the launching has commenced unless with the permission of the **WPC Technical Officer**.

If a race boat, having been launched wishes to use the crane, they may only do so after all race boats have been launched and with the permission of the **WPC Technical Officer**,

No propeller or ratio changes will be permitted until the start has taken place and the change will be deemed a pit stop.

860.31 - PIT STOPS

860.31.1 - DEFINITIONS

- Pit Stops – visit to the designated Pit Stop area for assistance during the Race and / or Pole Position i.e. from the time the Start of the race to the time the last boat crosses the finish line. If the Pit Stop area is not defined in the Race Instructions, then the Pit Stop area will be the inside of the harbour.
- Cranage Time – the time under the crane during a Pit stop.

The cranage time starts from the time the boat is in the cranage area, which will be defined and boats can only be lifted from this area, with the strops on the hook and as the Pit Crew board the boat. If the boat is not ready to be lifted immediately the boat must not go under the crane.

The cranage time stops when the pit crew have released the strops and vacated the boat with the pilots ready to leave the cranage area, or when the boat is pulled clear of the cranage area to allow another boat in its place. Boats exceeding the cranage time will not be permitted to continue in the Pole Position or race.

860.31.2 - PIT STOP RULES

1. Boats returning to the harbour must turn on their strobe light.
2. If several boats are returning at the same time, use of the cranes will be designated by the **WPC Technical Officer** based on the order of the boats entering the harbour.
3. Boats must be at very slow speed at all times while in the harbour (no wake) Penalty for infringement Disqualification and/or fine.
4. Refuelling is not permitted.
5. During the Race and Pole Positions, a boat may have unlimited Pit Stops but is limited to 10mins total Cranage Time for both Pole Position and Race at each event.

6. No one is permitted in the water during pit stops.
7. Only the official cranes may be used during pit stops unless agreed by the **WPC Technical Officer**.
8. No boats will be recovered during the duration of Pole Positions except for damaged boats and at the discretion of the **WPC Technical Officer**.
9. Cranes may be used to recover boats during the race period but priority will be given to pit stops, except for damaged boats, all at the discretion of the **WPC Technical Officer**.

860.32 - OUTSIDE ASSISTANCE

Refuelling is not authorised during any of the Race or Pole Position.

Outside assistance may only be provided in the officially designated Pit Stop Area.

The Race Committee has the right to decide if there was illegal outside assistance, which will result in disqualification.

860.33 - START PROCEDURE

Race Start Procedure

All boats will proceed directly to the Milling Buoy and circulate in an anticlockwise direction at very low speed (max 10 knots)

The first five boats of the Pole Position will take their position, in the Pole position order, behind the Starter Boat

In the Milling area after the orange smoke has been given, the yellow flag will be raised along with the 'Class 1' white flag, meaning that the boats must follow the alignment procedure.

When the start boat leaves the milling area towards the Start Line at a controlled speed with all boats in line and the first five boats in the correct order, the white flag will be lowered.

The first 5 boats of the Pole Position will line up, next to and in Pole Position order, adjacent to and 30 metres astern of the Start Boat.

All other competitors must line alongside these 5 boats, but not in any specific order.

The Start Boat will proceed towards the Start Line at a controlled speed (30/35 Knots) with all boats in line and the first 5 boats in the correct order. If any of the first five boats is out of order by the Start Line , a yellow card and or a fine may be applied.

The Start Line will be marked by two buoys and will referred to as the starting gate. All boats must pass through this gate.

The start of the race is denoted when the Start Boat will cross the Start Line and raises the green flag and lowers the yellow flag.

If a red flag is raised from the Start Boat at any time during the start procedure (including the run up to the start line), after the orange smoke flare has been fired, the start will be deemed to have been aborted. All boats must return, at slow speed, to the Milling Area area for a restart or to follow specific instructions of the Start Boat

All race boats must line up and remain at a constant speed – No ‘runs’ through the line up will be permitted. Penalty – Long Lap

The starter will use VHF radio to control the start. All crews must pay attention.

If a race boat starts before the yellow flag is raised, the penalty is Disqualification

If a race boat cross the Start Line before the Start Boat, the penalty is one Long Lap.

Penalties for infraction of the Starting Procedure are:

- a. Turning wrong direction ***in milling area – Long Lap Penalty***
- b. Interference with starting procedure – ***Long Lap Penalty and if the driver had caused danger, a Yellow Card***

- c. Excess speed in the milling area, or faster than 10 knots: ***Long Lap Penalty and if the driver had caused danger, Yellow Card***
- d. Should the bow of any boat be in front of the transom of the start boat when the green flag is raised – ***Long Lap Penalty***
- e. Starting on the incorrect side of the Start Boat - ***Long Lap Penalty and if the driver had caused danger, a Yellow Card***
- f. The penalty for any boat that does not start in its correct position will be as follows:
 - 1st offence during the season: ***Yellow Card***
 - Penalty for 2nd & subsequent offence – ***Long Lap Penalty***

Late Start Procedure

See relevant diagram issued with race instructions.

860.34 - FINISHING PROCEDURE

Throughout this rule, reference to winners, finishing procedure, finishing flags etc. must be read with reference to the classes racing, and will apply only to the Class in which the boat is racing. The Class 1 finishing flag will be the Chequered Flag.

860.34.1 - STOPPING THE RACE

In case of Force Majeure or accident, the OOD may stop the race by the waving of the Red flag at the finishing line and the turn mark boats. Additionally this signal may be given by other Official boats. Boats will be classified considering their position when crossing the previous lap timing line

860.34.2 - CURTAILING THE RACE

In case of Force Majeure or accident, the OOD may curtail (shorten) the race for some or all classes by the waving the Finishing Flag. Boats will be classified on completion of their current lap, considering their position at the Finishing line.

860.34.3 - OFFICIAL FLAGS

In the event of a Stopped or Curtailed race, boats must have completed a minimum of 70% of the laps completed by the 'winning boat' to be classified a finisher.

The flags for the End of the Race will be as follows:

- Stopping of the Race: Red Flag from the Finishing line and the Turn Mark Official Observer Boats
- Curtailing of the Race: The Chequered Flag at the Finish Line
- Race Finish: The Chequered Flag

860.34.4 - FINISHING PROCEDURE

When the leading boat completes the course and crosses the finishing line, the appropriate finishing flag will be waived. It designates the 'winning boat' for the Class and the start of the finish procedure.

The Winner, 2nd and 3rd placed boats, will be lead to the Wet Pits by the Start Boat

860.34.5 - 70% RULE

The minimum number of laps for each Class, to qualify as a finisher will be announced at the relevant drivers briefing and/or bulletin. This should equate to 70% of the laps of the race not including the Start run.

Any boat crossing the finishing line after the 'winning boat', will be classified considering the number of laps completed and its time, provided it has covered the minimum number of laps.

Any boat that fails to cross the finishing line after the winning boat will be classified considering the number of laps completed and its time, provided it has covered the minimum number of laps.

The Course will close 15 mins after the 'winning boat' has finished unless otherwise stated in the Event Race Instructions, Race Briefing and/or Bulletin. Any Lap completed after this time will not be included in the final results.

860.34.6 - TIME LIMIT

All races will have a maximum time limit of 1 hour. The chequered flag will be waived when the lead boat crosses the line after the 1 hour maximum time limit, and all boats will finish as they complete that lap. The number of laps completed by the 'Winning boat' will determine the number of laps for the 70% rule.

860.34.7 - NO FINISHERS

Should no boats finish, boats will be classified considering the number of laps completed and its time, provided it has covered the minimum number of laps of the 70% rule, as announced at the relevant drivers briefing and/or bulletin.

860.34.9 - STOPPED / CURTAILED RACE

In the event of a Stopped or Curtailed Race

If the 'winning boat' has completed:-

- A minimum of 50 % of the actual programmed number of laps: Full points will be awarded.
- 2 laps but less than 50 % of the actual programmed number of laps: Half points will be awarded
- Less than 2 laps:

The race may be restarted using normal procedures provided:

- A minimum of 1 hour is allowed for all competitors to return to port.
- The drivers are notified of the new start time for which a minimum of 1-hour notification must be given.
- The course will be shortened so that refuelling will not be necessary.
- The minimum length of the revised course will be at least 50% of the original programmed length.

Note: "Actual programmed number of laps" is deemed to be the number of laps announced in the race instructions or subsequent race bulletin.

A boat is timed when the bow crosses the finishing line.

A boat that finishes a race must withdraw from the course and keep clear of the finishing line, without hindering the boats that are still in the race. This may constitute an exception to the Rule of 'Right of Way.' Special regulations prescribed by the Race Committee must be observed. Failure to comply may result in disqualification.

860.34.10 - RETURNING TO THE PITS

When a race boat retires, the strobe light must be on until in port to indicate that no assistance is required. A Euro 600 penalty for infringement of this rule may be enforced.

860.34.11 - RETIREMENTS

Boats when retired, must report to Race Control on the VHF channel detailed in the Event Race Instructions.

In the event of retiring, but subsequently repairing the boat, it is forbidden to proceed on the course.

860.34.12 - POST RACE

To be classified as a finisher, competitors must not receive any outside assistance, go alongside, collect other persons aboard etc., before reporting to the official Post-Race Scrutineering area and being cleared by the **WPC Technical Officer**. Disregard for this rule will result in disqualification.

Post Race Scrutineering will comprise of any checks deemed necessary by the **WPC Technical Officers** for them to be satisfied the boat, engines and equipment fully comply with the U.I.M. Rules.

All competitors must remain available and the Race Secretariat must be advised of their whereabouts for contact after the race during the protest period.

All boats must be available for post-race scrutineering up to one hour after the race finish.

The Race Secretariat will close one hour after the posting of the race results.

Random fuel samples may be taken at some events for laboratory test on conformity.

860.34.13 - ASSISTANCE IN ACCIDENTS

U.I.M. Rule 206 Assistance in Accidents does not apply to **WPC**.

1. It is not mandatory for competing boats to stop and the drivers to provide assistance at the scene of an accident.
2. The driver of a boat should only stop if he believes:
 1. He is sure he can really be of assistance
 2. He is the first race boat to stop on the scene
 3. He is sure there is no official assistance
3. Except in certain circumstances, as decided by the **WPCC** representatives, the boat / driver that stops must, when relieved of his duties, continue in the race until the completion.

A time amendment will be awarded as follows:

- i) He will be awarded his time from the previous lap
- ii) If he is on the start lap or the 2nd lap when he stops:
 - a. He will be awarded the average time of the boats in front of him and behind him on that lap.
 - b. If there is no boat in front of him, he will be awarded the time of the boat behind him.
 - c. If there is no boat behind him, he will be awarded the time of the boat in front of him.

Teams must be aware that the Time bonus may not be defined until after the race finish.

4. The Team Manager must inform the official **WPC Timekeeper** within 5 minutes of the boat restarting that their boat had stopped to provide assistance.

The **WPC Timekeeper** will post the following notice on the Team Manager's screens "Investigation - Boat ?? Assistance in Accidents" so that the Team Manager's may notify their drivers.

The amount of the Time amendment may not be defined until after the race.

If the boat is deemed to have stopped in accordance with the rule, the **WPC Timekeeper** notified, and the team managers informed, then the boat's position after the time amendment will be definitive, and will be placed in the appropriate position with other boats moving down a position as necessary. There will be no joint positions unless the times for the end of the race are the same.

860.35 - BAD WEATHER

If it is necessary to run a Bad Weather course and / or bring the Race start time forward, full points will be given provided:

- The length must be a minimum of 50% of the original programmed number of laps.
- A minimum of 1-hour notification must be given prior to the start time.
- The Teams Representative, U.I.M. Permanent Commissioner, Promoters Representative and OOD must agree the bad weather course and the number of laps.

860.36 - PRIZE GIVING

1st, 2nd, 3rd place will be awarded for the Pole Position and Grand Prix.

Additional prizes may be awarded at the discretion of the Organisers.

The No. 1 and No.2 driver's of the first three classified boats must appear at the prize-giving and at the following press conference at each World Offshore Championship heat unless agreed in writing by the

U.I.M. Permanent Commissioner. The prize giving and the press conference will be held within one hour from the end of the race. The drivers must wear their own race overalls. Penalty for either of the above Euro 600 fine.

860.37 - LAY DAYS

Lay days are allowed if previously agreed with the **WPCC** and announced in the Advance Notice.

860.38 - RESCHEDULING OF RACES

Should it be impossible to hold a scheduled race due to Force Majeure, the **WPCC** may reschedule to race at a different time and place, the commitment of the teams to participate remaining unaffected, provided a minimum of 28 days advance notice is given prior to the date of the original event that is being cancelled.

If insufficient notice is given, the race may not be rescheduled and may be lost.

Reserve dates/venues will be fixed by the promoter in agreement with **WPCC**.

BOAT RULES

860.39 - REGISTERED BOAT

Drivers may run different boats but points may not be accumulated or transferred from one boat to another boat, except in the following circumstances.

For point allocation in the final classification the registered boat is deemed to be the hull and engines used for the first race of the season. Points can be transferred in the following cases:

- Should the registered hull become totally lost or totally destroyed, it may be replaced by like kind, (monohull or multihull) for completion

of the racing season, or if seriously damaged, until restoration of the original hull. (This replacement hull then becomes the registered hull during its period of use). This replacement hull may have engines with a different homologation certificate.

- A driver may also change his hull once during the season, provided this new hull is used for all the remaining races. In this case it is not possible for this designated driver/hull combination to use the previous hull any more in that racing season unless the replacement hull becomes seriously damaged. The new hull must have the hull similar to the previous hull (monohull or multihull). This replacement hull then becomes the registered hull.
- A driver may also change to engines with a different homologation certificate once during the season.
- Any hull or engine changes must be agreed in writing by the **WPCC**
- In the event of exceptional circumstances, the **WPCC** may authorise further changes in hull or homologated engines without penalty.

860.39.1 - RECOVERY OF DAMAGED / SUNKEN RACE BOATS

It is the Team / driver's sole and ultimate responsibility to recover damaged / sunken race boats.

860.40 - LOGOS

Every team shall be obliged to affix upon its own craft logos as agreed between the U.I.M., and the promoter. Two U.I.M. stickers will be affixed to each race boat, minimum size 70cm x 20cm on the left and right of the top deck towards the stern of the boat. Position to be agreed with the **WPC Technical Officer**.

860.41 - RACE NUMBER DIMENSIONS

The Race number has to be contained in a white square of 60 x 50cm for numbers 1 to 9, a white square of 70 x 50cm for numbers 10 to 19

and two digit numbers containing a 1, and a white square of 80 x 50cm for numbers 20 to 90 except two digit numbers containing a 1.

Race numbers must be black in colour, plain and upright and use the Arial font or similar. The number 1 in any number will not be Arial font but a plain upright single line

The white background must be surrounded by a 2cm thick black line with 12.5cm radius corners.

They shall conform to the following minimum dimensions:

Height: 40cm Width: 30cm Thickness: 7.5cm Spacing: 10cm (see diagrams)

The numbers shall be positioned as far forward as possible (see example)

The distance between the Race Boat number area and any advertising will be 150mm.

Diagram numbers 1 to 9
(White cm 60x50)

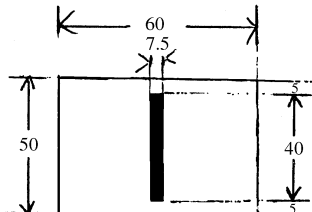


Diagram numbers 10 to 19
and 21 (White cm 70x50)

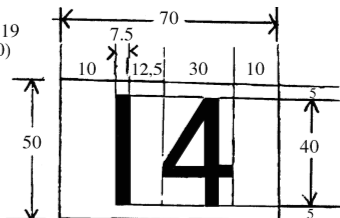


Diagram numbers 20 to 30
except 21 (White cm 80x50)



860.41.1 - SIMILAR BOAT IDENTIFICATION

If a number of boats are competing in the World **Powerboat** Championship of similar colour, it is requested the team provide a differing feature to each which is easily recognisable from a distance in liaison with the **contracted promoter**.

860.42 - ENVIRONMENT

The engine must not produce visible exhaust emission under race conditions.

1. The **WPC Technical Officer** will be the "Sole Judge" of any "Visible Exhaust Emission" and his decision is final and will not be liable to any protest as a result of his decision.
2. If any emission is seen, a "BLACK FLAG" will be shown from the committee boat together with the offending race boat number denoting disqualification of the race boat and must stop racing.

If the **WPC Technical Officer** subsequently certifies that the emission was as a result of a mechanical failure, then the boat will be classified at the lap prior to the display of the black flag.

3. The offending boat must then return immediately to the pit area.

860.43 - HULL DIMENSIONS AND MINIMUM WEIGHTS

To be eligible to compete in **WPC**, hulls must comply with one of the following criteria:

1. Any existing hull built prior to 1st January 2001, measuring 12.0/14.63m (39ft.4in/48ft.0in) and weighing a minimum of 4650kg, will be grand fathered indefinitely.
2. Any hull built after 1st January 2001, measuring 12.19/13.41m (40/44 feet) and weighing a minimum of 4650kg.
3. Other hull/engine combinations may be permitted to compete in **WPC** through specific dispensation from **WPCC**.

The boat weight may be checked at the end of the Pole Positions and or race. The weight will be an all-up weight including engines, residual fuel, drives and safety equipment as stated in the racing rules.

It is mandatory that a fully dimensioned drawing of all liquid containers and or tanks are given by each team before the first race to the U.I.M. **WPC Technical Officer**. This drawing shall include measurements from the nearest reference point e.g. a bulkhead to show its location. Methods of filling and emptying must also be shown and the tanks must be able to be checked if empty or not – Penalty Disqualification

Compression Flaps are not permitted. To stop any extra compression being built into the hull, the top area of the tunnel shall only follow a continuous flat plane, with no stepping down or sloping of the trailing edge of the tunnel. Any boats measured before 17th October 2002 may retain the compression flap but must not be able to adjust it during the race and must have a visible stop so as it cannot go lower than a continuous “flat plane”.

Compression flaps are not permitted on any boats measured after 17th October 2002.

860.43.1 - REINFORCED COCKPIT

All boats with restraints must have a reinforced cockpit area with canopy, which should be constructed entirely of materials equal to the strength of the running surface of the boat/or stronger. This area must be the sides, floor, decking and bulk heads, fore and aft. It is mandatory to close the canopy with a hatch, and the hatch remain closed during all racing and practice.

It is strongly recommended that a floor escape hatch is the secondary means of escape.

Any floor escape hatch fitted must remain operable during the entirety of the race and practice.

860.43.2 - ESCAPE HATCH

All new Class 1 catamarans measured after May 1999 must have a secondary escape hatch in the tunnel.

860.44 - ENGINES

All inboard reciprocating engines, fuelled by gasoline and used in the **WPC** series boats, must be built using a basic cylinder block and cylinder head (original or manufactured as spare parts) of a marine, automobile or industrial engine.

All engines must be homologated by the U.I.M. and approved by **WPCC** for use in the **WPC**.

The minimum quantity of engines built prior to homologation will be 30.

Should the minimum quantity of engines not be reached before this date, a written certificate, witnessed by a practicing lawyer of the firm intention to manufacture the specified numbers is required from the manufacturing company, and endorsed by the National Authority.

This certificate must illustrate the production schedule, and a forecasted date of completion of the minimum number.

The required number of engines is to be verified by the manufacturer and the National Authority of the country of manufacture.

These engines must be in the same format as the homologation file specifies.

(Vendors using existing homologated engines must submit to the U.I.M the revised specification of the `2003 Restrictor Engine`.)

The homologation file must contain a comprehensive technical specification of the engine in its `WPC 2003` state of tune. This file will be used as the definitive document (regarding materials) to which the engine will be compared at time of inspection.

The maximum number of fitted engines (per boat) is two.

Random inspections including dyno testing may be made to ensure the legality of the boat / engine(s) at any time during the season / event at the request of the **WPC Technical Officer** and should be completed before the next race in the series. Any costs incurred will be paid by the Team

860.45 - ENGINE SPECIFICATION

Only gasoline fuelled engines are permitted. Maximum capacity per engine of 8.2 litre (500 cu in). Engines must be normally aspirated. No forced induction is permitted, other than that provided by the air restrictor of the engine which must be positioned above the deck in the air stream of the boat. With the exception of a single orifice air restrictor situated in the mandatory sealed plenum chamber, and a component material/ method restriction (listed below), engines are free in specification, however the finite material specification must be as recorded in the homologation file.

Other engines (i.e. larger capacities, forced induction) can be permitted after U.I.M. Homologation and a specific decision taken by WPCC.

WPCC shall also decide on any restriction that should be applied to new engines to meet with the agreed configuration.

860.46 - ENGINE COMPONENT RESTRICTION

The following materials, coatings and methods are not permitted;

- Titanium components or coatings. (Titanium valves are permitted)
- Ceramic components or coatings. (Excluding spark plugs)
- Pneumatic valve operation.
- Variable length inlet tracts.
- Variable length exhaust tracts.
- Max 12 cylinder configuration
- Engine block to be manufactured from cast iron or aluminium alloy only.
- Head castings Iron or Aluminium only.
- Maximum 4 valves per cylinder.
- Camshafts are to remain in the Original Engine Manufacturers location.
- Four camshafts per engine maximum.
- One spark plug per cylinder.
- Compression ratio limited to 12:1 maximum.
- Two injectors per cylinder maximum.
- Engine vendors must provide the **WPC** technical Committee with all technical equipment and methods to verify the legality of engines.

860.47 - AIR RESTRICTOR / PLENUM CHAMBER

A single orifice air restrictor is to be situated at the first point of air entry into the mandatory sealed plenum chamber.

The bore of the air restrictor orifice must be as follows:

- Aspirated engines V12 – 4 valves : 62.00 mm

- Aspirated engines V8 – 2 valves : 70.00 mm .

The V8 engines under consideration are those already homologated by U.I.M. and, since nobody is currently using these

engines, this setting should be adjusted if necessary once three races had taken place after the first application of these engines.

The Air Restrictor will be supplied by U.I.M. **WPCC** and is a mandatory requirement per engine. No duplicated or otherwise interpreted designs are permitted. The only permitted Air Restrictor is that supplied by U.I.M. **WPCC**

Each engine must be equipped with its own dedicated single plenum chamber.

It is mandatory that a drawing (a sketch) of the Air Box / Plenum chamber is given by each team before the first race to the U.I.M. **WPC Technical Officer** including showing the location of any cables etc running through the air box. It is the responsibility of the team that any changes during the season must be documented and agreed by the U.I.M. **WPC Technical Officer**. – Penalty Disqualification –

The Air Restrictor must be fitted to the plenum in such a way that it can be removed and substituted for another U.I.M. official Air restrictor at any time.

The U.I.M. reserve the right to substitute either or both of the Air Restrictors before or during an event. The U.I.M. official serial number of all restrictors and their allocation will be recorded by the **WPC Technical Officer**. These serial numbered restrictors may not be substituted for any other serial numbered restrictor at any time without the permission of / or at the request of the **WPC Technical Officer**.

It is permissible to burnish / polish any blemishes from the restrictor providing this is carried out with the agreement of the U.I.M. **WPC Technical Officer** and does not alter the diameter of the air restrictor orifice or its profile or provide any performance enhancement.

The maximum dimensions of the Plenum Chamber must be contained within an imaginary straight-sided square edged box.

The volume of this box must be no more than 1 cubic metre.

The vertical measurement of the Plenum chamber will be taken from a horizontal line through the centre of the Air inlet port at the cylinder head face.

This measurement and / or a reference point from which a measurement can be easily taken must be recorded on the Engine Homologation papers.

Each engine must make its induction through one air restrictor only.

It is not permitted for two engines to share a common plenum chamber.

This air restrictor will be issued by **WPCC** (at a cost of Euro 600 each) and will be engraved with the U.I.M. - W.O.C motif and size and serial number. The air restrictor will further be anodised to provide optical assurance to the technical inspectors that re-sizing has not occurred.

The plenum chamber is free in design, providing the following is complied with:

From the point of entry through the air restrictor, the plenum chamber must be sealed to prevent any extra air being induced other than through one single air restrictor.

100% of the air feeding the engine is to be controlled through the air restrictor orifice.

The aim of the air restrictor is to limit engine horsepower to circa 850hp.

N.B. The U.I.M. **WPCC** RESERVES THE RIGHT TO CHANGE THE SIZE OF THE AIR RESTRICTOR HAVING GIVEN REASONABLE NOTICE AND IMPLEMENTATION DATE.

No change will be authorized within the same racing season.

A minimum of six months notification must be given before any change is authorized for the following season.

Random inspections will be made confirming the legality and power output of the engines. After the engine(s) have been satisfactorily sealed at the race venue (at a time agreeable to all parties) they may be dyno tested under the scrutiny of U.I.M. (**WPC**) Technical personnel. The dyno used to verify the legality of the engine(s) shall be currently certified, a copy of which shall be available to the U.I.M. (**WPC**) Technical personnel.

At the discretion of the U.I.M. (**WPC**) Technical personnel, disassembly of the engine may still be requested after the engine has been demonstrated on the dyno.

860.47.1 - AIR RESTRICTOR TESTING

Conformity of Air Restrictor size will be checked with the official U.I.M. “go – no go” gauge.

One end of the gauge will be a parallel section; the diameter of this section will be an interference fit with the air restrictor at ambient temperature. This end of the gauge must not fit through the restrictor.

If this end of the gauge does fit through, the restrictor will be considered illegal.

860.47.2 - PLENUM CHAMBER SEALING AND VACUUM TEST

Conformity of the plenum chamber seal will be checked using the official U.I.M. “vacuum gauge”.

The check will be performed at post race Scrutineering whilst the boats are still in the water.

The engine speed during the check is permitted to vary between idle speed and 2500rpm max. A minimum reading of –100 millibars must be reached to confirm 100% seal.

Should the reading fail to reach –100 millibars the plenum chamber seal will be considered to be illegal.

860.48 - ENGINE VENDOR REGISTRATION

Engine vendors must register with the U.I.M. at the beginning of each season, declaring the following;

1. That the vendor is a bonafide engine manufacturer or engine builder supplying **WPC** competitor(s). The declaration must be on a

company letter heading clearly showing the name, address, and company registration details

Only genuine vendors will be accepted.

2. That the vendor assures the U.I.M. the engine(s) as supplied to participating teams in the **WPC** series comply with the relevant U.I.M. rules.
3. That the vendor accepts random inspections will be made confirming the legality of the engines and further accepts that after the engine(s) have been satisfactorily sealed at the race venue, will then (at a time agreeable to both parties) be dyno tested under the scrutiny of U.I.M. (**WPC**) Technical personnel, at the vendors facility. The dyno used to verify the legality of the engine(s) shall be currently certified, a copy of which shall be available to the U.I.M. (**WPC**) Technical personnel.

At the discretion of the U.I.M. (**WPC**) Technical personnel, disassembly of the engine may still be requested after the engine has been demonstrated on the dyno.

860.49 - U.I.M. DATA ACQUISITION

A U.I.M. data acquisition system (as specified by the U.I.M.) may be attached to and operational on each fitted engine. Each engine must have it's own dedicated data acquisition system which must remain with the engine for the entirety of the season. This system is not to be interfaced with any part of the existing engine management system, however it is permitted for sensors to be shared by both the existing management system and the data acquisition system where necessary. The **WPC Technical Officer** will have exclusive access to the data acquisition at time of Post-Race Inspection. Data will not be disclosed to the respective competitor, unless circumstances exist where the engine has been deemed to be illegal. Data will then only be disclosed to another party with the exclusive permission of the respective competitor.

The full operating parameters of the engine must be declared and demonstrated at time of inspection. These elected parameters will be

recorded by data acquisition and will be compared to that of the data recorded during racing and or Pole Position.

All expenses incurred by the **WPC Technical Officer** in carrying out any of the above inspections, will be covered by the vendor before any verification is announced.

860.50 - TRANSMISSION RESTRICTIONS

No method of shiftable gear ratio selection is permitted in the drive train (other than neutral facility) between the engine crankshaft and the propeller i.e. no shiftable gearboxes or shiftable dropboxes are allowed.

No form of variable speed transmission or torque biasing devices are permitted.

During the event, all ratio changes (if made) must only take place in the pit area and by mechanical replacement only and under the strict control and supervision of the Technical Commissioner.

860.50.1 - PROPULSION

Class 1 boats competing in the **WPC** series may dispense with reverse gear capability requirement.

860.51 - PROPELLERS

Propellers will be as specified by WPC.

860.52 - TRACTION CONTROL

Any form of traction control is not permitted.

860.53 - CREW CONTROL

The trimming of the boat must be conducted by the crew. It is not permitted to interface the operation of the flaps, ballast tanks, fuel tanks, drives, or throttles with any electronically assisting devices. The

operation of the aforementioned is to be exclusively executed by the boat crew.

860.54 - COMPETITOR DATA ACQUISITION (AS USED BY THE COMPETITOR)

It is permitted to down-load data during the Pole Position and or Race.

It is not permitted to up-load data to the boat during Pole Position and or Race that will be used to adjust on-board engine/drive management systems.

860.55 - FUEL

The only fuel approved for use in **WPC** from 2003, will be Control Fuel as supplied by the appointed contractor.

No modifications or additives are permitted to the fuel.

Fuel samples showing any characteristics other than those of the fuel issued before the event, will be deemed to be illegal.

Personnel from the fuel manufactures will be present at each race, to take samples from boats to ensure compliance with the above.

Requests for fuel samples will be made by the **WPC** Technical personnel at their discretion.

Specification of the fuel will be available from the appointed fuel contractor and will be a minimum of 95 octane.

SELECTED U.I.M. OFFSHORE GENERAL RULES APPERTAINING TO CLASS 1 WPC

	Pages
203.06 - PUBLICITY ON BOATS	45
205.02 - MEDICAL EXAMINATION.	48

205.02.01 - CONSUMPTION OF ALCOHOL.....	49
205.02.02 - ANTI-DOPING REGULATIONS	49
205.05 - INJURED DRIVERS	52
205.07 - PROTECTING HELMET.....	53
205.10 - TECHNICAL PROTECTION.....	55
313 - RULES OF THE ROAD	67
313.07 - OVERLAP	68
313.08 - PASSING OBSTACLES	68
313.09 - PASSING MARKS	69
313.10 - ALTERING COURSE	69
313.11	69
400 - JURISDICTION.....	76
401.01 - DEFINITIONS	76
402 - JUDICIAL BODIES	76
402.01 - THE INTERNATIONAL JURY.....	76
402.02 - INTERESTED PARTIES.....	77
402.03 - THE U.I.M. APPEALS BOARD.....	77
403 - PROTEST PROCEDURES.....	77
403.01 - VALIDITY OF PROTESTS	77
403.02 - PROTEST FEE.....	78
403.03 - RIGHT OF PROTEST.....	78
403.04 - TIME OF LODGING A PROTEST/ PROTEST PERIOD	79
403.05 - JUDGEMENT	79
403.06 - NOTIFICATION OF DECISION	79

403.07 -	AVAILABILITY OF DOCUMENTS.....	79
404 -	HEARING OF THE PROTEST.....	80
404.01 -	RIGHT TO A HEARING	80
404.02 -	WITNESSES.....	80
404.03 -	THE HEARING PROCEDURE.....	80
405 -	THE APPEALS PROCEDURE.....	81
405.01 -	APPEAL BOARD	81
405.02 -	INTERESTED PARTIES.....	81
405.03 -	RIGHT OF APPEAL	81
405.04 -	NOTICE OF INTENTION TO APPEAL.....	82
405.05 -	TIME LIMIT.....	82
405.06 -	LODGING OF AN APPEAL.....	82
405.07 -	NOTIFICATION OF THE PARTIES OF THE APPEAL..	82
405.08 -	TIME LIMIT FOR DECISIONS ON APPEALS	83
405.09 -	APPEAL BOARD DECISIONS.....	83
405.10 -	COSTS	83
407 -	PRIORITY OF THESE RULES.....	87
502 -	INSPECTIONS	99
502.01.01	.....	99
502.01.02	.....	99
502.01.03	.....	99
502.01.04	.....	99
502.01.05	.....	99
503 -	EQUIPMENT.....	104

503.01	.....	104
503.02	.....	105
503.03	.....	105
508 -	CREW SAFETY	118
508.01 -	COCKPIT EVACUATION / IMMERSION TESTING ...	118
508.03 -	REINFORCED COCKPIT AREA AND CANOPY	119
508.17	.....	122
508.18	.....	123
508.19	.....	123
508.20	.....	123
508.21	.....	124
508.22	.....	124
508.23	.....	125
508.24	.....	126
508.25	.....	126
508.27	.....	126

861 - RESERVE NUMBER

900 - OFFSHORE CIRCUIT RACING (O.C.R.)

Rules suspended - pending new rules to be studied.

950 - U.I.M. V24 CLASS

951 - GENERAL

951.1

The general U.I.M. offshore racing rules are to be applied. In cases of conflicts between general rules and class rules (and specifications), the class rules have precedence.

951.2 - SPIRIT OF THE RULES

The aim is to provide an offshore racing class in which driving skill is more important than the opportunities to take advantage of the latest advances in the use of exotic building materials, the art of boatbuilding and in the art of extracting still more power from highly stressed machinery. Boats are to be alike, no modifications (tuning) are allowed except the few points mentioned below.

The V24 Class is modern, has unique safety features, has a reliable serial produced power unit and is intended to provide good sport with a minimum of maintenance and repairs.

951.3 - MEASUREMENT CERTIFICATE

V24 Class boats are to have a measurement certificate according to rule 501.

The complete boat, hull, equipment, as well as engine, transmission and propellers must during races remain as delivered from the boat builder and as specified in the U.I.M. homologation. Sole modifications are described below.

The measuring procedure consists of checking that the boat and machinery comply with the specifications in the homologation file.

Instructions for how this is done is to be found in the file. It is obligatory for a boat owner to:

- Have the homologation file at hand both before the measuring procedure and at the measuring procedure.
- Lend the homologation file (or a copy of it) to the measurer, at least one week in advance, in order to give the measurer an opportunity to acquaint himself with what he has to check and how to do it.

951.3 - ADMINISTRATION

The V24 Class ***Working Group*** consists of ***a minimum of one representative*** from U.I.M. the designer ***and the V24 licenseholder***.

951.4 - U.I.M. V24 CLASS PLAQUE

In order to obtain the measurement certificate necessary for a V24 Class boat, it must be built by an U.I.M. certified boat builder and carry the U.I.M. plaque fixed to it by the builder.

Boats not having a U.I.M. V24 Class plaque are not to be measured, and are not to have a measurement certificate.

The U.I.M. V24 Class plaque is available to the boat builders licensed by the ***V24 Class Working Group***. The plaque is available from the ***V24 Working Group***.

For boats which do not comply with the V24 Class rules, U.I.M. general rule 502.01.04 on eligibility is applicable.

951.5 - HOMOLOGATION FILE

The homologation file is to be distributed by U.I.M. and National Authorities in the same way as other homologation files.

To be valid, a homologation file must have an original U.I.M. or N.A. (not photocopied) stamp on every page.

951.6 - CHANGES OF THE HOMOLOGATED SPECIFICATIONS

Changes of the homologated specifications must be approved by the V **24** Class **Working Group** and are not allowed until one month after they are published in a U.I.M. bulletin.

Changes in spare parts are allowed as long as the driver, at races, can show that there is change from the manufacturer (a part number being replaced by another, etc.).

Changes of class rules must be approved by the U.I.M. General Assembly and can not be valid until published in a U.I.M. offshore rule book or in a widely distributed supplement to a rule book. Even so, a rule change is not to be implemented until the calendar year after the year in which it was approved by the U.I.M. General Assembly.

951.7 - OWNERSHIP OF BOATS

All boats entitled to race must be owned by a physical person, a group of persons or a legal entity. The owner is stated in the measurement certificate.

951.8 - COURSES

V24 Class boats are intended to race in open and unprotected waters as U.I.M. Class 3 boats and courses of ***similar but not necessarily the same to*** the OCR type according to rule 900.16.

951.9 - ECM BOX

Technical inspectors are allowed to switch (by lottery) the engine ECM box between boats.

The V24 race committee reserve the right to switch ECU's and or propellers from any boat it chooses.

952 - MODIFICATIONS ALLOWED

952.1

The Volvo 5,7 GXI engine may not be modified in any way, including the ECU. The Volvo Penta DPX outdrive must have a 1:1,59 gear ratio in it. No modification is allowed to the out drive including the exterior of the drive. The height of the out drive may not be altered from manufactured.

Only the following modifications are allowed.

952.2

Sparkplugs may be changed for any other.

952.3

The propellers may not be hidden from competitors at the race venue when fitted to the out drive. The propellers to be only Volvo Penta E2 duo props, ***these may be modified. The Volvo Penta E2 markings must be clearly visible on each propeller.***

952.4

It is allowed to fit flat anchor plates for bolting the dash to the deck and the fitting of aerals are allowed.

952.5

Internal cockpit arrangements are free as long as both crew members have free and unobstructed exit to both doors. No sharp edges or protrusions are allowed anywhere.

Cockpit seats, steering wheel, instruments, choice of power trim button, placement of dashboard, throttle and footrests are free. Cockpit seats as per U.I.M. rule 508.21.

952.6

On-board cameras are free as long as they do not interfere with visibility.

952.7

An air-conditioning system may be fitted.

952.8

The replacement of nuts, or the addition of lock nuts, drilling and wiring, split pinning or keying is allowed, provided the screws and pins are the original ones.

952.9

It is allowed **to fit** water drain off's from the engine exhaust manifolds – no performance gained – **only available from the V24 builder.**

952.10

It is allowed **to fit a** water pickup to spray water onto the top of gear case only – no performance gain – **only available from the V24 builder.**

952.11

It is allowed to fit hard engine mountings ***only available from the V24 builder.***

952.12

It is allowed to add rubber hose to the end of the exhausts.

953 - FUEL

Fuel regulations according to U.I.M. rule 504.

954 - IDENTIFICATION

954.1

Class identification letter is V. Number is same as the U.I.M. V24 Class plaque number. However, if the race organiser thinks it is necessary, he has the right to order a number to be changed.

Size and positions of the race number according to U.I.M. rule 710.

955 - SAFETY

955.1

The U.I.M. V24 Class boat is an approved one-design class, consequently rules 508 to 508.07 do not apply.

Boats must always comply with the homologation sheet.

Each boat will be weighed at the end of each race. Every boat must not weigh less than 1490 kg, including the crew, boat, safety gear and balance of fuel, but not water ballast and excluding strops. The teams

needing to add weight must either permanently fix it under the seats in the cockpit, or under the **cockpit** liner or must carry extra fuel.

955.2

All V24 Class boats must have the detachable nose painted red/**orange** with the number 2 on each side of the lower running surface. The number must be written in white and be at least 0.07 m high.

955.3

Canopy hatch handle and release mechanism must be painted fluorescent orange to identify them, both inside and outside the cockpit. The text “ OTHER SIDE ”, written in red must be clearly visible on both doors from the outside.

955.4 - ENGINE CUT-OUT

The engine cut-out must work on engine circuits only as a complement to the ignition key switch. It must be possible to operate the switch from the outside. The text “ Engine Cut-Off ” written in red/**orange** must be clearly visible from the outside.

955.5 - RESTRAINT SYSTEM

The restraint system shall be according to U.I.M. rules 508.22 to 508.23. Attachment points shall be as shown in the homologation specification.

955.6 - REAR VIEW MIRRORS

All V24 Class boats must have two rear view mirrors fitted. Mirrors must have a minimum area of 60 cm² ***These may be of any design.***

955.7 - RACING VESTS, HELMETS AND AIR SUPPLY

Racing Vests, helmets and air supply shall be according to U.I.M. rules 205.06, 713 and 508.17.

955.8

Flares (excluding personal flares), anchor and warps must all be carried in the safety box in the bow.

955.9

It is a U.I.M. requirement that the overtaking boat will give way, and this rule will be firmly enforced.

Any contact between boats during a race will mean that the boats involved will be penalised, and this may include disqualification. This includes the claimed innocent party.

956 - EQUIPMENT

956.1

Safety equipment according to U.I.M. rules 715.01 through to 715.16.

956.2

For U.I.M. V24 Class boats, it is permitted to use electronic positioning fixing systems. It is forbidden to interface any of this equipment to the steering system.

957 - CREW

957.1

The minimum age of the **Driver** is 18 years old. The minimum age of the navigator is 17 years old. Number of crew members is 2.

958 - ENGINE SEALS

The engines are factory sealed. A broken seal is considered as a violation of the rules and is to be penalised according to rule 502.01.04. If a seal is broken in connection with an engine repair, the engine must be checked against the rule requirements by a N.A. Technical Scrutineer when the engine is re-assembled and re-sealed by him. The kind of seal used must be noted in the measurement certificate.

959 - CHAMPIONSHIP

The European Championship races will be run in three (3) (4 for 2005) U.I.M. Offshore countries.

The U.I.M. published calendar announces the dates and venues.

One Championship heat only is allocated to each country.

The Championship points system will include all boats who cross over the start line.

Boats that do not finish a heat will get points similar to last position, excluding DQ boats.

The point system for each race will be 400 points for 1st place, reducing by 10 % for each subsequent position.

Boats disqualified receive no points.

A U.I.M. Commissioner will be appointed to each Championship heat.

V 24 boats must be positioned together in the pits area.

Race suits or smart team clothing must be worn to drivers briefing or suffer 100 point team deduction.

Race suits must be worn to the podium presentation for podium teams or suffer 100 point team deduction.

V24 Championship sponsors must be displayed on all boats. This may be on top of the canopy.

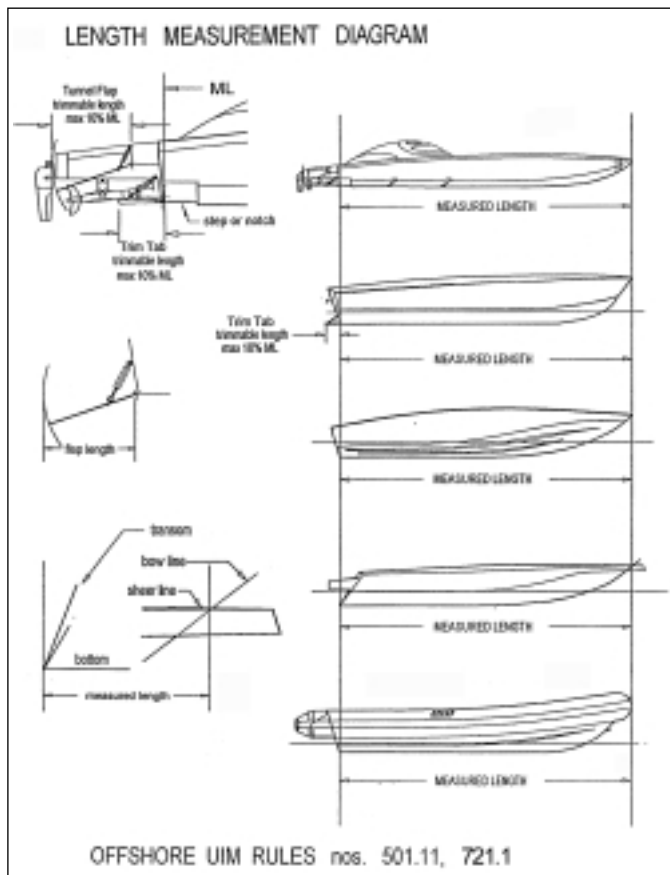
When a boat stops to assist an overturned competitor, refer to rule 206.

The 70% rule does not apply to this class.

960 - RULES FOR U.I.M. PRO VEE CLASS

Guide lines for this experimental class are available from the U.I.M. office upon request. They may also be viewed on the U.I.M. Web site.

Appendix to Rules 501.11



A

Advance programme	202 - 202.00 - 202.01
After the event	326
Age limit	203.04 - 702.1
Altering course	313.10
Anchoring (during a race)	317
Appeal (board)	402.03 - 405.01
Appeals (protests and)	405
Approaches to the starting line	305
Assistance (outside)	319
Assistance (accidents)	206
Authority (change of National)	120.02
Automatic penalties	406
Automatic throttle cut-off device	706
Awarding of prizes	204.01.16

B

Base (course)	300 - 604
Boat (ownership)	203.05
Boats (equipment)	503
Boats (safety)	205.08

C

Calculation of speed	605
Calendar	101
Cancellation of licence	120.06
Cancelled (race)	101.04 - 309
Canopies	508.03 - 703.8.1
Carbon-monoxide alarm	508.25
Cards (yellow)	406.03
Certificate (measurement)	501
Certificates (performance)	614
Championship (World)	800
Championship (Continental)	800
Championship (World Class 1)	860
Change of National Authority	120.02
Circulation of the result (record)	615.02
Circulation of the result (championship)	800.17
Class, hull and engine rules (Class 3)	730 - 737
Classes (International)	500
Classes eligible for records	601
Cockpits	508 - 703.9
Commissioner (U.I.M.)	201.08
Committee (organising)	201.01

Committee (powers of the race)	201.01
Committee of appeal	402.03
Communication officials	201.02.10
Consumption of alcohol	205.02.01
Control (boats by race committee)	701.09
Cost of measurement	501.01/09/10
Course Officials	201.02.5
Course (minimum depth)	800.13
Cranage	209 - 703.7.1/2
Crew safety	508
Crew	702
Cut-off device	706.1

D

Deadline dates	101.03
Deck	703.1 - 703.2
Deputy Officer of the Day	201.02.2
Diesel (definition of)	504.03
Disqualification	406.04
Distance record	611
Doping test	205.02.02
Drivers meetings	204.2
Drivers representative	120.08

Duties of the Course Officials	201.02.5
Duties of the Safety Officer	201.02.2
Duties of the Technical Officers	201.02.4
Duties of the Timekeepers	201.02.7

E

Engines	502.04 - 704 - 705
Engine compartments - well	704.3 - 707
Engines class 2	720
Engines class 3 inboards	736 - 737
Engines class 3 outboards	734 - 735
Engine cut-off	706
Entries	202.02.11 - 203
Equipment of boats	503
Event (after the)	326
Exclusion	406.07
Exhausts	704.1

F

Fee (calendar)	101.01
Fee (entry fee)	203.03
Finishing a race	320 - 800.7.1/2/3
Finishing line	320

Fire extinguishers	508.25 – 704.4
Floatation	501.12 - 733
Fouling competing boats	315
Fuel	504.04/...17
Fuel test procedure	504.06
Fuelling	205.14

G

Guard rails	703.4
Gyros	709

H

Hand signals	206.02
Helmet	205.07
Homologation of engines	505
Homologation of records	615
Homologation procedure	506
Hour records	612
Hull measurement	501.11
Hull specifications	703
Hulls (inspection of)	324 - 502 - 607.04
Hulls (dimensions and minimum weights - class 2)	721
Hulls (dimensions and minimum weights - class 3)	730 - 731 - 732

I

Immersion testing	508.01
Inboard engines	704 - 705
Injured drivers	205.05
Inspection of engines and hulls	324 - 502 - 607
Insurance	205.09
Interested parties	405.02
International offshore rules	700
International races recognised by the U.I.M.	103 - 104 - 105
Interpretation of the rules	200.02

J

Jurisdiction	400
--------------	-----

L

Length (minimum/maximum class 3)	730.1 - 732
Length (minimum/maximum class 2)	721.1
Licences (list of)	120.02
Licences (rules for racing licences)	120
Licences (validity of)	120.03

M

Margin required (records)	606
Mark Lavin Guidelines (appendix to)	500
Marks (rounding and fouling)	313.09 - 314
Marks of nationality	711
Marks of the course (definition)	303
Measurement certificate	501
Medical and rescue officials	201.02.9
Medical examination	205.02 - 205.03 - 205.04
Medical Officer	201.02.9
Minimum age	203.04 - 702.1
Minimum requirements (venues)	855 -856
Modifications allowed (class 3)	734 -735 - 736 - 737
Monetary penalties	200.03
Monohull definition	502.03
Mooring cleat	703.5

N

National Authority (change of)	120.02
National flag	711
Nationality (foreign)	203.02
Nationality of the drivers	203.05

Noise reduction	736.2
Non approved races	104
Number (race)	710

O

Obstacle (definition)	300.01 - 313.08
Officer of the Day	201.02.1
Offshore circuit racing (OCR)	900
Organisation of races	200
Organisation of records	600 - 608
Organising body	201
Outside assistance	319
Overlap	313.07
Ownership of boats	203.05

P

Penalties	406
Petrol (definition of)	504.21
Performance certificates	614
Posting of the results	323
Postponed races	310
Procedure (protest)	403
Programme (race)	204

Propulsion	318 - 708
Protecting helmet	205.07
Protection (technical)	205.10
Protective clothing	205.13
Protest fee	403.02
Protest form (appendix to)	400
Protest (and appeals)	403
Protests (timing)	403.04
Pseudonym	120.07
Publicity (on boats)	203.06
Pit officials	201.02.8

Q

Qualification	306
---------------	-----

R

Race (finishing a)	320
Race (stopping of a)	312 - 320.02
Race cancelled	309
Race curtailed	312.01
Race number	710
Race postponed	310
Race programme	204

Race Secretary	201.02.3
Race shortened	311
Race start	305
Races recognised by the U.I.M.	103 - 104 - 105
Racing instructions	204
Racing licences (rules)	120
Racing rules	300
Racing vest	205.06 - 508.26
Record Officials	609
Record permit	608
Record results (circulation)	615.07
Records (rules)	600
Reinforced cockpit area and Canopy	508.03
Reports (after the event)	326
Restraints (class 3)	508.50 - 508.60
Results (posting of)	323
Rounding marks	314
Rules (interpretation of the)	200.02
Rules of the road	313
Running aground	316
Running production changes	507.01

S

Safety boats	205.08
Safety equipment	205.12 - 715
Safety of the crew	508
Safety Officer	201.02.2
Safety rules	205
Scoring system	321 - 800.10.1
Shaft guards	704.2
Smoking (prohibited)	205.11
Spectator areas	303.02
Speed records (calculation)	605
Speed records	610
Start	305
Starting (boat starting before time)	305.16
Starting officials	201.02.6
Steering position	703.3
Stopping of a race	312 - 320.02
Super licence	860.12
Suspension (prolonged)	406.06
Suspension (temporary)	406.05

T

Team Representative	203.07
Technical Officials	201.02.4
Technical protection	205.10
Technical rules, classes	500
Time (of protest)	403.04
Timekeepers and timing devices	201.02.7 - 301
Tow hooks (class 2)	703.6
Transmission (class 3)	708.04

V

V24 Class Rules	950
-----------------	-----

W

Water pick-up (class 3)	734.6 - 734.12 - 735.17 - 735.31
Weight (minimum class 3)	730.1 - 731
Weight (minimum class 2)	721.2
Windshield	703.8.2
Wings	502.02
World championship	102 - 800 - 855 - 856 - 860

Y

Yellow cards

406.03

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**UNION INTERNATIONALE
MOTONAUTIQUE**

**AQUABIKE
RULES**

2005

2004 AQUABIKE CLASS PRO WORLD CHAMPIONS

Ski Division



LAIGLE Kevin - France

Runabout 1200 CC



NAVARRO Didier - France

Freestyle



LENZI Alessander - Brazil

2004 AQUABIKE CLASS PRO EUROPEAN CHAMPIONS

Ski Division



PUJOL Narcis - Spain

Runabout 1200 Stock



SCHINTU Alessio - Italy

Ski Division Super Stock



LAIGLE Kevin - France

Runabout 1200 Super Stock



NAVARRO Didier - France

Freestyle



DELIS Dimitros - Austria

2005 U.I.M. CLASS PRO CHAMPIONSHIP RULES

INDEX

ABBREVIATIONS / GLOSSARY OF TERMS

200 GENERAL RACING RULES

200.01 INTERPRETATION OF THE RULES

201 RACE COMMITTEE

201.01 INTERNATIONAL COMMITTEE AT THE RACE SITE

201.02 DAILY MANAGEMENT

202 APPROVAL FOR U.I.M. CHAMPIONSHIPS

203 ENTRIES

203.01 AGE REQUIREMENTS

203.02 ENTRIES AND SUPERLICENCE

203.03 PILOTS MEETING

203.04 ADVERTISING

204 SAFETY RULES

204.02 RESCUE BOAT

204.03 DOPING TEST

204.04 ANTI-DOPING REGULATIONS

206 RACE NUMBERS

300 INFRINGEMENTS

301 RACE SCHEDULE

301.01 POSTPONEMENTS

302 RACING FORMAT

302.1 CIRCUIT

302.1.1 WORLD CHAMPIONSHIP CLASSES

- 302.1.2 CONTINENTAL CHAMPIONSHIP CLASSES
- 302.1.3 WORLD CHAMPIONSHIP DURATION
- 302.1.4 CONTINENTAL CHAMPIONSHIP DURATION
- 302.1.5 WORLD CHAMPIONSHIP PILOTS
- 302.1.6 CONTINENTAL CHAMPIONSHIP PILOTS
- 302.1.7 QUALIFICATION
- 302.1.8 GREEN CARD
- 302.1.9 POLE POSITION
- 302.2 RALLYE JET
 - 302.2.1 RALLYE JET CLASSES
 - 302.2.2 RALLYE JET DURATION
 - 302.2.3 RALLYE JET PILOTS
- 303 COURSE, BUOYS AND ROUNDING BUOY**
 - 303.01 BUOY MISSING
 - 303.02 RACE INTERRUPTION
 - 303.03 COURSE MARKER BUOYS
- 304 FLAG SIGNALS**
 - 304.01 GENERAL
 - 304.02 GREEN FLAG
 - 304.03 YELLOW FLAG
 - 304.04 RED FLAG
 - 304.05 BLACK FLAG
 - 304.06 WHITE FLAG
 - 304.07 BLUE FLAG
 - 304.08 CHEQUERED FLAG
- 305 START / FINISH**
- 306 MODIFICATIONS OF THE RACE INSTRUCTIONS AND NOTIFICATIONS**
- 307 CLOSED PIT**
- 308 RIDING RULES**
 - 308.01 BLOCKING OR RECKLESS / DANGEROUS RIDING
 - 308.02 CONTROL OF MACHINE DURING RACE

- 308.03 OBSTRUCTION
- 308.04 OVERTAKING
- 308.05 ACCIDENTS
- 308.06 INJURED PILOTS / DAMAGED
- 308.07 REPORT

309 HEAT / RACE FINISH

310 CLASSIFICATION AND POINTS SYSTEM

- 310.01 CLASSIFICATION
- 310.02 U.I.M. POINTS

311 PRIZES

- 311.01 WORLD CHAMPIONSHIP PRIZE MONEY
- 311.02 WORLD CHAMPIONSHIP TRAVEL MONEY
- 311.03 WORLD CHAMPIONSHIP FINAL PRIZE MONEY
- 311.04 CONTINENTAL CHAMPIONSHIP PRIZE MONEY

312 PENALTIES AND DISQUALIFICATION

- 312.01 PENALTIES
- 312.02 DISQUALIFICATION

400 JURISDICTION

- 401.01 DEFINITIONS

402 JUDICIAL BODIES

- 402.01 THE INTERNATIONAL JURY
- 402.02 INTERESTED PARTIES
- 402.03 THE U.I.M. APPEALS BOARD

403 PROTEST PROCEDURES

- 403.01 VALIDITY OF PROTESTS
- 403.02 PROTEST FEE
- 403.03 RIGHT OF PROTEST
- 403.04 TIME OF LODGING A PROTEST
- 403.05 JUDGEMENT
- 403.06 NOTIFICATION OF DECISION
- 403.07 AVAILABILITY OF DOCUMENTS
- 403.08 RETURN OF FEES

404 HEARING OF THE PROTEST

- 404.01 RIGHT TO A HEARING
- 404.02 WITNESSES
- 404.03 THE HEARING PROCEDURE

405 THE APPEALS PROCEDURE

- 405.01 APPEAL BOARD
- 405.02 INTERESTED PARTIES
- 405.03 RIGHT OF APPEAL
- 405.04 NOTICE OF INTENTION TO APPEAL
- 405.05 TIME LIMIT
- 405.06 LODGING OF AN APPEAL
- 405.07 NOTIFICATION OF THE PARTIES OF THE APPEAL
- 405.08 TIME LIMIT FOR DECISIONS ON APPEALS
- 405.09 APPEAL BOARD DECISIONS
- 405.10 COSTS

406 PENALTIES

- 406.01 GENERAL
- 406.02 DEADLINES FOR PENALIZING
- 406.03 UNACCEPTABLE BEHAVIOUR
- 406.04 REPRIMAND
- 406.05 YELLOW CARD
- 406.06 DISQUALIFICATION
- 406.07 TEMPORARY SUSPENSION
- 406.08 PROLONGED SUSPENSION
- 406.09 EXCLUSION

407 PRIORITY OF THESE RULES**408 POWERBOAT PROTEST FORM****500 FREESTYLE****501 TECHNICAL RULES**

- 501.1 PERSONAL WATERCRAFT
- 501.2 SKI DIVISION
- 501.3 RUNABOUT DIVISION

501.5 FOUR-STROKE RUNABOUT

502 STOCK

502.1 STOCK CLASS

502.2 HULL

502.3 ENGINE TWO-STROKE

502.4 ENGINE FOUR-STROKE

502.5 AIR/FUEL DELIVERY TWO-STROKE

502.6 AIR/FUEL DELIVERY FOUR-STROKE

502.7 IGNITION AND ELECTRONICS TWO-STROKE

502.8 IGNITION AND ELECTRONICS FOUR-STROKE

502.9 TURBOCHARGER / SUPERCHARGER

502.10 DRIVELINE

503 SUPERSTOCK

503.1 SUPERSTOCK CLASS

503.2 HULL

503.3 ENGINE TWO-STROKE

503.4 ENGINE FOUR-STROKE

503.5 AIR/FUEL DELIVERY TWO-STROKE

503.6 AIR/FUEL DELIVERY FOUR-STROKE

503.7 IGNITION AND ELECTRONICS TWO/FOUR-STROKE

503.8 TURBOCHARGER/SUPERCHARGER

503.9 DRIVELINE

504 FUEL TESTS

505 CHANGE AQUABIKE

506 PERSONAL EQUIPMENT

600 RECORDS AND PERFORMANCES

601 CLASSES ELIGIBLE FOR RECORDS

602 DEFINITION OF UNITS

604 BASES (THE COURSE)

605 CALCULATION OF SPEED

- 606 MARGIN REQUIRED**
- 607 MOTORS**
- 608 REQUEST TO ORGANISE**
- 609 OFFICIALS**
- 610 SPEED RECORDS**
- 611 DISTANCE RECORDS**
- 612 HOUR RECORDS**
- 613 WORLD COMPETITION RECORDS**
- 614 PERFORMANCE CERTIFICATES**
- 615 HOMOLOGATION OF RECORDS**
- 616 PROTESTS**
- 700 ENVIRONMENTAL REGULATIONS**
- 701 RE-FUELLING**
- 702 ENVIRONMENTAL CARE IN RACE AREAS**
- 800 RECOMMENDATIONS FOR ORGANISING AN AQUABIKE
WORLD CHAMPIONSHIP**
 - 800.01 INFRASTRUCTURE**
 - 800.02 RACE HEAD QUARTERS**
 - 800.03 CONTROL TOWER**
 - 800.04 PADDOCK**
 - 800.05 NECESSARY ON SITE PERSONNEL**
 - 800.05.01 RACE CONTROL**
 - 800.05.02 SECRETARIAT**
 - 800.05.03 RESULTATS**
 - 800.05.04 BUOY CONTROL**
 - 800.05.05 PADDOCK**
 - 800.05.06 CONSTRUCTION**
 - 800.06 REQUIRED INFRASTRUCTURE**

- 800.06.01 RACE CONTROL
- 800.06.02 SECRETARIAT
- 800.06.03 CONSTRUCTION
- 800.07 EMERGENCY REQUIREMENT
 - 800.07.01 SECURITY
 - 800.07.02 MEDICAL
 - 800.07.03 TRANSMISSION
 - 800.07.04 ENVIRONMENT

ABBREVIATIONS / GLOSSARY OF TERMS

U.I.M.	Union Internationale Motonautique
A.B.	Aquabike
Aftermarket	A replacement or replica part replacing or used in addition to the OEM part. Aftermarket parts must provide the same function as their OEM counterparts.
Bond Flange	The bond flange is the overlapping/mating section where the deck (upper) and the hull (lower) portions are joined.
Course Marshall	Helps the Race Director control the race and assists stopped pilots on the course. Most often is positioned on the course perimeter riding an Aquabike.
Deck	The upper structural body of the Aquabike located above (and including) the upper bond flange.
Fuel Injection	A means of forcing gasoline into a cylinder other than a carburettor. Any unit that does not depend on the engines vacuum to draw fuel into the engine will be considered a fuel injection unit.
HIN	Hull Identification Number. A unique serial number generated by the manufacturer and affixed to each Aquabike.
Hull	The lower structural body of the Aquabike located below (and including) the lower bond flange.
I.R.C.	International Race Committee
Moto	When the number of entries is less than or equal to the maximum number of Aquabikes allowed on the course at one time, there is no qualifying and the moto system is in effect. The moto system uses results of two or more separate races to

mathematically determine overall results.

N.A.	National Authority (recognised by the U.I.M. as the country's sole authority for Aquabike racing).
OEM	Original Equipment Manufacturer. OEM parts are defined as those that were installed on each model of Aquabike at the time of manufacture.
O.O.D.	Officer Of the Day
PFD	Personal Flotation Device. A device used to keep pilots afloat.
R.C.	Race Committee
Pilot	Competitor
Sponson	A special planing surface which may be attached to the hull sides or transom.
Starging Area	The place near the starting line where Aquabikes and pilots wait for their scheduled race.
Starting Line	A straight boundary, either real or imaginary, that denotes the beginning point of the race.

200 - GENERAL RACING RULES

GENERAL

200.01 - INTERPRETATION OF THE RULES

In all cases unforeseen by the international rules, the National Authority is to judge bearing in mind the spirit of the rules.

In case of conflicting rules, those rules concerning the series are to prevail.

The rules contained herein shall be used and strictly adhered to at all N.A. and U.I.M. sanctioned events. All members and racing personnel, including mechanics, staging area personnel and promoters are deemed to be fully aware of all rules and will be expected to abide by them at all times. All entries are subject to technical and safety inspection prior to an event.

A pilot in a N.A. sanctioned event shall have no claim for damages or recompense of any kind for any advertising, exploiting his name, picture or the picture of his A.B. or exploiting the achievements of any product used by the pilot relating to the pilots participating in the event.

A N.A. sanctioned event is a competitive event, the conduct of which is governed by N.A. officials in accordance with the U.I.M. rules, as it may be amended from time to time, and Special Rules that may be published by N.A. specifically for the event, and any applicable agreement to which N.A. is a party. By submitting his entry application and taking part in any activity relating to the event, a pilot agrees to abide by the decisions of those officials relating to the event, except as provided in the U.I.M. By-Laws and Rules.

Such pilot further agrees that it is his obligation to inspect the racing area to determine that it is in a safe and raceable condition and that he voluntarily assumes the risks of and has no claim for damages against N.A. or U.I.M., its promoters or their officers, officials or employees by reason of damage to either the A.B., the pilot and/or pit crew. All such pilots assume full responsibility for any injuries sustained, including death and property damage, any time they are in the racing area, practice course or pits, etc. to themselves or any other pilot.

While on the water, a pilot will be governed by all rules and from the time of arriving at the race area until leaving the race area.

201 - RACE COMMITTEE

The pilot or his crew will not be allowed on the premises used and occupied as Operating Headquarters of the Race Control by the Race Committee. If asked to come to the headquarters of Race Control, the pilot or his crew must do so but shall immediately depart when the matter has been attended to.

All local races and all A.B's taking part shall be under the control and direction of a Race Committee, consisting of Officer of the Day (O.O.D.), Safety Officer, the Sports and Race Commissioner and the Technical Inspector. All matters shall be subject to its approval and control.

The Race Committee shall work together with the International Committee.

201.01 - INTERNATIONAL COMMITTEE AT THE RACE SITE

A group of U.I.M. commissioners will be set in order to guarantee permanent commissioners at each round of the Championships.

Reasonable cost payable by organiser for the U.I.M. Commissioner to attend meeting and assist organiser (APEX fare, accommodation, etc.)

The commissioners will observe that all Championships rounds are run according to the U.I.M. rules and based on the same criteria. The International Committee represents the U.I.M.

Members of the International Committee :

U.I.M. Promoter and General Coordinator

U.I.M. Race and Sports Commissioner

U.I.M. Technical Commissioner

U.I.M. Safety Commissioners (two)

U.I.M. Commissioner assistant to the Race and Sports Commissioner

201.02 - DAILY MANAGEMENT

The Daily Management Committee is composed of the U.I.M. Promoter and the U.I.M. Race and Sports Commissioner. It will take all necessary arrangements to assure the promotion and the good progress of the competition.

202 - APPROVAL FOR U.I.M. CHAMPIONSHIPS

The U.I.M. Aquabike Commission approves all championships. After this approval, the U.I.M. office will send an invoice to the N.A. to be paid for the calendar fee.

203 - ENTRIES

203.01 - AGE REQUIREMENTS

Minimum age limit for racing is 15 years old (except derogation by U.I.M.). Pilots between the age of 15 are required to have parent or guardian signature on minor release form. Minor release forms are available on request at the race registration or at N.A. Only one form per race season is necessary. Birth certificates may be required for an age verification.

203.02 - ENTRIES - SUPER LICENSE

203.02.01

THE WORLD CHAMPIONSHIP CLASS PRO U.I.M. IS OPEN.

The U.I.M. will take the responsibility to issue superlicenses for any drivers whose National Authority are members of IJSBA, run the IJSBA

series and do not follow or do not recognise the U.I.M. activities in Aquabike racing.

Alle entrants must have a valid superlicense delivered by the U.I.M.

Superlicense criteria :

- Four national races or international ordinary
- Sports CV with official results
- To provide a valid attestation of Civil Responsibility insurance in race and out race with the exception of coverage of the pilots and their aquabike between them for a minimum amount of 1.000.000 Euro by case
- To be requested 4 weeks prior to the first event with payment of fee of 150 Euro and a photograph.
- Daily license : 50 Euro ; remainder to be paid if participating to more races.
- U.I.M. Pass for Continental Championship : 25 Euro for all races.

203.02.02

A pilot who wants to participate all races of the World Championship Class Pro U.I.M. and be reimbursed with travel monies, must have a contractual agreement with the U.I.M. Contracted promoter, Spes. The conditions of the contract are in the U.I.M./SPES format rules and are part of the present rules.

203.02.03

The annual U.I.M. superlicence, daily superlicence and U.I.M. Pass gives to the pilots the following rights :

- To cash the prize if he is between the first 6 drivers for World Championship
- To cash the prize if he is between the first 3 drivers for Continental Championship

- To get a trophy if he is between the first 3 drivers
- To get the U.I.M. points if he is between the 15th first drivers
- Solely contracted pilots participating to all the World Championship will receive the travel money.

203.02.04

The daily U.I.M. superlicence gives to the pilots the following rights:

- To cash the prize if he is between the first 6 drivers
- To get a trophy if he is between the first 3 drivers
- To get the U.I.M. points if he is between the 15th first drivers

203.02.05

Commission of selection

A commission has been named to select the contracted pilots according to the criterias “ Title - previous Involvements - Nations and fidelity “.

203.03 - PILOTS BRIEFING

203.03.01

It is mandatory that a pilots briefing is held before the start of the event. The drivers which are absent from this meeting will not be allowed to participate to the event. The U.I.M. Commissioner will sanction the drivers that arrive too late to the pilots briefing with a penalty of 100 Euro. This penalty must be paid before the event starts. All penalties cashed during the season will be deposited in a year-end prize fund, which will be distributed to all drivers, or for one special trophy. The drivers must sign up at the beginning of the pilots briefing and will receive the final race instruction. Drivers and team members should wear team uniform.

203.03.02

In sanctioned races when on account of local conditions, special racing rules not provided by these rules, or changes in these rules are deemed necessary by the International Committee for a particular race, these changes must be announced at the pilots briefing.

The pilots briefing shall include the following items, after the pilots roll call :

1. Thank you to all the people who have made the event possible (sponsors, Town Mayor, Officers, Venue Owner, etc..).
2. Short presentation of the main organising Officers and their duties.
3. Short presentation of the Commentator, U.I.M. Commissioners and the Race Jury Members.
4. Programme timing for all races.
5. Start and Finish procedures.
6. Information regarding the elimination heats.
7. Flag positions and descriptions.
8. Race area, riding direction, riding to the start area, riding after the chequered flag, number of laps or duration, etc..
9. Security items : fire fighting points, ambulance and personnel, rescue boats, rules of conduct in the pits, etc..
10. Location of information board, results, etc..
11. Time and place of Prize Giving Ceremony.
12. Questions.
13. Circuit description.
14. Prize fund.

203.04 - ADVERTISING

According to the rules of the National Authority.

An advertising space of 25 x 10 cm to be located at the front of the AB will be reserved for the sponsor(s) of the event.

204 - SAFETY RULES

According to the legislation and the rules and regulations enforced in the country where the race is being organised.

204.01

The Officer of the Day of a U.I.M. event must require that :

All Officials must be present and must be active during the event.

Adequate facilities are available for the efficient conduct of the race before the sanctioned testing or racing begins.

An ambulance, rescue and transport that meets the requirements of the local authority shall be present at all times during testing or the racing.

A minimum of 4 patrol A.B.'s and more if conditions warrant, are available for duty on the course at all times. Only authorised personnel shall attend to safety and/or patrol A.B.'s.

204.02 - RESCUE BOAT

Signals and safety on the water circuit must be done by Aquabikes or by turbine propelled boats (no underwater propeller).

Following points are MANDATORY :

1. The aquabike must have a BOW STRAP attached to the front or bow eye opening. This will enable the Course Marshall to quickly attach the tow line to the race boat when it has ceased to function. The tow straps must be strong enough to be pulled under a load
2. The Course Marshall will tow the aquabike back to the pits area or the main event beach and release the boat over there. This will only be done when all hazardous traffic is clear and there is an open pathway.
3. The pilot of an aquabike must at any moment of the event have a helmet and a lifejacket when he is on the water.

4. When a pilot needs to leave the race course, he must go outside the circuit and return to the pits area at slow speed and without crossing the circuit.
5. In case of injury and assistance is required, remain sit down on your aquabike with one arm straight up over your head (for ski class drivers, stay close to your aquabike waiving the same arm manoeuvre)
6. When the Course Marshall points his arm and finger towards one pilot, this is the signal that one pilot is being overtaken and lapped by another pilot . The slower pilot(s) must make way for the overtaking pilot(s) to ensure safe overtaking (blue flag).
7. In case the pilot gets separated from his aquabike, the Course Marshall will guarantee safe entry into a zone where assistance can be given. The Course Marshall will go near by the pilot asking for his left hand and will assist the pilot toward the back of his aquabike as soon as the conditions are safe.
8. If your aquabike is idling or not functioning properly and one would become a hazard to the other competitors, the Course Marshall has the right to tow any driver and /or his boat out of the race course
9. If any aquabike parts or goggles are discovered and retrieved during the race or on the race course, they will be transported and placed at the technical inspection tent or along the main event beach nearby the starting line.

The rescue craft shall have radio communication with the O.O.D. or his assistant.

The rescue crew shall lift all accident victims from the water in as nearly a horizontal position as possible on a Stokes, or backboard type lifter.

The rescue boat staff shall consist of a minimum of two persons.

Race Committee shall never place a man in the water or a man in a stopped A.B. as a course marker.

No event shall be started if there is a warning of other boats in the area in which the race course is situated with the exception of a protected course, which may be accepted at the discretion of the Race Committee.

No RED articles, such as clothing, umbrellas or placards shall be allowed in the vicinity of the starters or judges stand and the patrol A.B. during the running of any event.

No pilot shall participate in a race nor shall an official serve in an official capacity after having consumed alcohol or controlled substances or while intoxicated.

The O.O.D. shall have the authority to stop any event as he deems necessary to ensure the safety to participants, spectators and/or officials because of technical problems.

204.03 - DOPING TEST

CONSUMPTION OF ALCOHOL

Competitors shall not be under the influence of alcohol while competing and practising

Testing when carried out shall be by using an alcometer

A competitor shall, if requested by an authorised official, submit to an alcohol test. Written notification of selection for testing will be handed to a competitor by an authorised official. Refusal or failure to do so may be taken as if a positive test had been ordered and dealt with accordingly.

For international races, any competitor found to have more than **BAK 0,15 0/00 (0,15 g/L blood // AAK 0,075 mg/L air) 1 hour before and during testing or race** shall be immediately suspended and disqualified from the whole event. A format report shall be forwarded to the NA of the competitor.

If in an organising country, the legal requirements of the above numbers are lower, then these numbers have to be announced in the advance regulations and prevail.

204.04 - ANTI-DOPING REGULATIONS

Anti-doping regulations based upon the world anti-doping code.

As per separate publication.

206 - RACE NUMBERS

Each pilot is designated a Race Number at the beginning of the season. A pilot's Race Number remains the same for the entirety of the Championship. As of the first race, a set of self-adhesive numbers and a dossard will be given to each pilot. These must be used as instructed and kept in good condition throughout the season.

The size of numbers will be 25cm x 25 cm. Numbers will be printed black on white for all categories. Numbers will be provided by the organiser. The pilot will stick it on his Aquabike in a visible way at the front of his machine, on the right and on the left hand side.

Experience has shown that the pilot should wear a number on his back (as is done in skiing) to be more easily identified by the timer. Only numbers provided by the promoter will be allowed.

300 - INFRINGEMENTS

In every country there are areas where it is forbidden to navigate or where the speed is limited. These areas must be strictly mentioned in the race instructions.

301 - RACE SCHEDULE

301.01 - POSTPONEMENTS

The Race Committee shall adhere to the racing schedule as advertised in the advance programme unless some real emergency, such as adverse weather or shifting buoys, necessitates a postponement. The Race Committee should publicly announce all postponements as far in advance as possible.

The Race Committee may postpone an event, race or heat.

Should unfavourable weather or circumstances make a postponement advisable, an event, race or heat may be re-run beyond the sanction. The Race Committee, at their discretion, shall decide whether the event, race or heat postponement shall be considered a new race, and hence open to all entrants or restricted to the valid entries at the time registration was closed.

Should it be necessary to postpone a race during an event programme, the black flag will be permanently displayed at the official starting line and at such points on the race course as are authorised by these rules.

If a race is cancelled by the O.O.D. for any reason he deems warranted, points will be awarded on the basis of heats completed.

302 - RACING FORMAT

302.1 CIRCUIT

The course is a closed circuit. The competitors will have several laps on

a course where speed is combined with ability through a series of buoys with left and right hand turns. The participating machinery must be conform to the defined U.I.M. classes.

302.1.1 WORLD CHAMPIONSHIP

CLASSES	Ski division	0 - 800 cc	Super stock
	Freestyle	Free	
	Runabout 1200	0 - 1200 cc	Super stock*

302.1.2 CONTINENTAL CHAMPIONSHIP

CLASSES	Ski division	0 - 800 cc	Stock
		0 - 800 cc	Super stock
	Freestyle	Free	
	Runabout 1200	0 - 1200 cc	Stock*
		0 - 1200 cc	Super Stock*

302.1.3 WORLD CHAMPIONSHIP

DURATION	Ski division	Super Stock	20 minutes + 1 Lap
	Runabout 1200	Super Stock	30 minutes + 1 Lap
	Freestyle		3 minutes / Pilot

302.1.4 CONTINENTAL CHAMPIONSHIP

DURATION	Ski division	Stock	12 minutes + 1 Lap
	Ski division	Super Stock	15 minutes + 1 Lap
	Runabout 1200	Stock	15 minutes + 1 Lap
	Runabout 1200	Super Stock	20 minutes + 1 Lap
	Freestyle		3 minutes / Pilot

302.1.5 WORLD CHAMPIONSHIP

PILOTS	Ski division	Super Stock	Maximum 25 pilots
	Runabout 1200	Super Stock	Maximum 30 pilots
		Freestyle	Maximum 10 pilotes

302.1.6 CONTINENTAL CHAMPIONSHIP

PILOTS	Ski division	Stock	Maximum 20 pilots
	Ski division	Super Stock	Maximum 20 pilots
	Runabout 1200	Stock	Maximum 25 pilots
	Runabout 1200	Super Stock	Maximum 25 pilots
	Freestyle		Maximum 10 pilots

* normally aspirated and turbocharged/supercharged four-stroke watercraft (1600 cc) may compete in the current 1200 Superstock classes

302.1.7 - QUALIFICATION

The aim of qualifying heats is to discover and encourage new talent and also to enable local pilots to participate in events. Qualifying heats determine the pilots that participate in the Pole Position competition and consequently in the Grand Prix event. The qualifications will take place Saturday afternoon, prior to Pole Position.

The duration of qualifications will be half the time of the competitions in the relevant classes.

302.1.8 - GREEN CARD

The International Committee can assign a green card to a pilot who has not qualified itself. This qualification can not be used to the detriment of another pilot.

302.1.9 - POLE POSITION

The Pole Position determines the order of the departure grid for the Grand Prix of Sunday. The Pole Position will take place on a timed lap. Participating to pole position is mandatory for all pilots except in case of force majeure.

Order of departure:

1st Line Pilots classified from 1 to 3

2nd Line Pilots classified from 4 to 10

- 3rd Line All pilots who participated in the Pole Position
- 4rd Line All pilots who could not participate to the Pole Position
(e.g. A pilot who broke down and could not participate to the Pole Position will leave in 4rd Line.)

The drivers attention should be drawn to the fact that it is up to each pilot, under his/her sole responsibility to decide whether he/she starts a race or continues a race. Therefore, the competitors are personally responsible for any material or bodily damage that can occur to their Aquabikes or to themselves. They must assess the performance of their Aquabikes or their personal ability and they must decide to start or to continue a race, taking into account the weather forecasts and only if they consider they can do it.

302.2 - RALLYE JET

The World Championship World can be run over one or more rounds. Each round can be run over one or more heats, over one or more days, with refuelling. The final classification will be done by adding the points of all rounds. For each round the advance programme will define the circuit, the duration of each heat, the zones of refuelling and the zones of neutralisation.

An additional fuel tank may be authorized according to the circuit of the race. The conditions will be announced in the advance programme.

302.2.1 - CLASS

Ski division	0 – 800 cc Stock
	0 – 800 cc Super Stock
Runabout 1200	0 – 1200 cc Stock
	0 – 1200 cc Super Stock

302.2.2 - DURATION

The number of stages, the duration of each stage and the conditions of refuelling will be described in the particular rules of the advance programme.

302.2.3 - PILOTS

<i>Ski division</i>	<i>Stock</i>	<i>Maximum 25 pilots</i>
	<i>Super Stock</i>	<i>Maximum 25 pilots</i>
<i>Runabout 1200</i>	<i>Stock</i>	<i>Maximum 30 pilots</i>
	<i>Super Stock</i>	<i>Maximum 30 pilots</i>

303 - COURSE, BUOYS AND ROUNDING BUOY

303.01

In the event that a buoy is missing or is displaced during a race, the Race Committee will replace it whenever it is possible. If the buoy cannot be replaced, the pilots will take the next buoy by the shortest path while ignoring the missing or out of place buoy.

303.02 - RACE INTERRUPTION

In case of interruption before the 2/3 of the race has been run, a new departure will be given. In case of interruption after the 2/3 of the race has been run, the classification will be according to the positions of the lap preceding the stoppage of race.

A new departure is considered like a new race

303.03 - COURSE MARKER BUOYS

All pilots must negotiate the A.B. completely around the course marker buoys. Any pilot riding an A.B. over the course marker buoy will be judged as missing the buoy no matter what side of the A.B. the buoy reappears. Pilots will be required to renegotiate the ran-over buoy in the proper direction of the course and will be required to do so in a safe manner so as not to create hazard or danger to other pilots on the course, in case of missing the buoy, the driver will be penalised adding a penalty of 1 lap. In case of missing intentionally the buoy, the driver will be disqualified.

All buoys of the Championship will be round, inflatable and with minimum dimension of 80 x 80 cm.

Red buoy	Left-hand turn
Yellow buoy	Right-hand turn
Chequered buoy	Arrival.

304 - FLAG SIGNALS

304.01 - GENERAL

The use of flags is the primary communication between Pilots and Officials.

Flags will be displayed by the Commissioners, both on start/stop line and in peripheral A.B.'s. It is of primary concern to be aware of flags displayed. Pilots not adhering to flag signals may be disqualified or have other penalties.

304.02 - GREEN FLAG

Signifies the circuit is free and start of the race.

304.03 - YELLOW FLAG

Warns of safety hazard on the course. Pilots are allowed to continue racing in a safe manner (considerable decrease in speed, all pilots holding position). A pilot will be disqualified if he continues to race in an unsafe manner (not slowing down or trying to overtake another pilot).

304.04 - RED FLAG

Signifies the event will stop immediately regardless of position of machines on the course. Pilots are to return to the starting line unless instructed differently at the pilots briefing. In the event of single injury being the reason for the red flag, that pilot could be disqualified from the day events.

304.05 - BLACK FLAG

It must be used with a number board indicating the racing number of the competitor who has to stop racing immediately and report immediately to the O.O.D.

Non -respecting of the black flag might lead to disqualification.

304.06 - WHITE FLAG

When the white flag is displayed, pilots have entered their last lap.

304.07 - BLUE FLAG

The flag Marshall shall show the blue flag to signal that a pilot is being overtaken and lapped by a faster pilot. The slower pilot must make way for the overtaking pilot to pass safely. This blue flag will have to be exposed by the O.O.D. and it will be responsibility of the driver to watch the flag ; if not, the driver will be disqualified.

304.08 - CHEQUERED FLAG

Race completed.

305 - START / FINISH

305.01

Due to varying shore line and water conditions, the type of start will be explained at the pilots briefing. During events pilots are to be ready and on stand-by for Race Control 30 minutes prior to scheduled start times.

305.02

At the green flag signal : departure towards the first buoy as indicated in race instructions. A driver who jumps the start, will receive the black flag with his race number. He will be penalised adding a penalty of 1 lap.

If a pilot abandons the race, he has no right for a second start in case that a re-start is given.

A pilot and his A.B. shall be considered a unit in order to finish. The pilot must pass the chequered flag driving his Aquabike. Pilots not doing this will not be classified in the heat.

306 - MODIFICATIONS OF THE RACE

INSTRUCTIONS AND NOTIFICATIONS

The modifications of the race instructions as well as all the other modifications including the Race Committee notifications and decisions will be displayed on the special information board (see race instructions). This posting will take place on a written official notification so that each pilot can consult this board before and after each event.

307 - CLOSED PIT

307.01

In the evening, at the end of the last heat of the race, the Aquabikes must go to a closed pit, which is guarded. No pilot will be allowed to intervene on his/her Aquabike without being accompanied by a Race Official

Any pilot who is caught in the closed pit will be excluded from the event.

307.02

Refuelling, mechanical intervention, work on the hull are strictly forbidden in the closed pit.

308 - RIDING RULES

308.01 - BLOCKING OR RECKLESS / DANGEROUS RIDING

The deliberate blocking of a faster machine is cause for disqualification or a one lap penalty at the discretion of the O.O.D. Any reckless/dangerous riding, unnecessary bumping, crowding, chopping, blocking, deliberate striking, breaking a course marker buoy or unsportsmanslike conduct on the course or off may subject the pilot to disqualification and other sanctions and if in the case of a team effort, the complete team may be penalised.

308.02 - CONTROL OF MACHINE DURING RACE

It is forbidden to ride an A.B. in a direction opposite to which the event is being run. A pilot who has spun out is permitted to turn the A.B. around and to continue the event provided such action is taken only when the course is clear and must give the right of way to other pilots on the course. Pilots running in the wrong direction must be blackflagged and disqualified.

308.03 - OBSTRUCTION

If for any reason, a pilot is forced to stop on or near the course during a heat, it is the pilot's first duty to remove the A.B. from the course so not to endanger or obstruct other pilots.

308.04 - OVERTAKING

A pilot must be prepared for another A.B. to overtake and must always be aware of other A.B.'s approaching from behind. The overtaking pilot must consider the safest route to pass and must do so without forcing the other pilot to suddenly alter the course.

308.05 - ACCIDENTS

Flipped pilots, if uninjured should clasp hands above head as an "OK" signal.

No pilot or representative thereof shall hold any other pilot or representative liable for any personal injuries or damage resulting from an accident or racing occurring in a sanctioned race.

308.06 - INJURED PILOTS / DAMAGED

An injured or otherwise incapacitated pilot shall be prohibited from racing unless, in the O.O.D's judgement following an examination by qualified medical personnel, the pilot is determined not to be a danger to himself or any other competitor. It is up to the decision of the O.O.D. to deem a damaged A.B. as safe to continue competition. The O.O.D's decision is final.

308.07 - REPORT

The O.O.D. shall report all accidents requiring medical attention to the N.A. Race Officials will assist local authorities in completing their reports where necessary. reports will also be sent to the U.I.M. through the Commissioner.

309 - HEAT / RACE FINISH

After the winner passes the chequered flag, the race is finished. Within the next five minutes the first three pilots should go to the podium. Immediately after the race, the first six classified pilots should bring their AB to the closed pit area for the technical inspection by the U.I.M. commissioner. It is not allowed to do any change or work on the AB before technical inspection. Any pilots that will not follow this rule, will be disqualified.

310 - CLASSIFICATION AND POINTS SYSTEM

310.01 - CLASSIFICATION

Pilots classified according to U.I.M. points, retain their points whatever the form of their engagement.

Pilots classified in Prize Money Ratings receive their Prize whatever the form of their engagement.

Pilots classified in Trophy Ratings receive their Trophy whatever the form of their engagement.

The Final World Championship Classification will be the addition of all of a pilot's points accumulated during the season.

310.02 - U.I.M. POINTS

The assignment of the points for every race will be according to following scale :

1 st	25 pts	6 rd	10 pts	11 rd	5 pts
2 nd	20 pts	7 rd	9 pts	12 rd	4 pts
3 rd	16 pts	8 rd	8 pts	13 rd	3 pts
4 rd	13 pts	9 rd	7 pts	14 rd	2 pts
5 rd	11 pts	10 rd	6 pts	15 rd	1 pts

To be classified in a race of the World Championship Class Pro U.I.M., the pilot has to pass the checkered finish flag.

In case of a tie, the number of first places in these championship races will be considered, then the number of second places etc...

311 - RACE TRAVEL MONEY

311.01 - WORLD CHAMPIONSHIP PRIZE MONEY 10'000 €/ \$ PER RACE

Ski division		Freestyle		Runabout 1200	
1st	1'000 €/ \$	1st	700 €/ \$	1st	1'500 €/ \$
2nd	700 €/ \$	2nd	500 €/ \$	2nd	1'200 €/ \$
3rd	500 €/ \$	3rd	300 €/ \$	3rd	1'000 €/ \$
4th	400 €/ \$			4th	700 €/ \$
5th	300 €/ \$			5th	600 €/ \$
<u>6th</u>	<u>200 €/ \$</u>			<u>6th</u>	<u>400 €/ \$</u>
TOT	3'100 €/ \$	TOT	1'500 €/ \$	TOT	5'400 €/ \$

311.02 - TRAVEL MONEY

At every race the travel money of 300 € will be attributed to every pilot under contract with SPES for the full World Championship and present at the race (obtained result not being considered).

311.03 - FINAL TRAVEL MONEY FOR THE WORLD CHAMPIONSHIP

The total amount of the Final Prize Money is of 5'000 €/ \$ per race ; allocation according to following scale (based on 6 races per season) :

Ski division		Freestyle		Runabout 1200	
1st	3'000 €/ \$	1st	2'500 €/ \$	1st	5'000 €/ \$
2nd	2'000 €/ \$	2nd	1'200 €/ \$	2nd	3'000 €/ \$
3rd	1'500 €/ \$	3rd	800 €/ \$	3rd	2'000 €/ \$
4th	1'100 €/ \$	4th	300 €/ \$	4th	1'500 €/ \$

5th	700 €//\$	5th	200 €//\$	5th	1'000 €//\$
6th	500 €//\$			6th	700 €//\$
7th	300 €//\$			7th	600 €//\$
8 th	300 €//\$			8 th	500 €//\$
9 th	300 €//\$			9 th	400 €//\$
<u>10 th</u>	<u>300 €//\$</u>			<u>10 th</u>	<u>300 €//\$</u>
TOT 10'000 €//\$		TOT 5'000 €//\$		TOT 15'000 €//\$	

Solely contracted pilots are to be assigned "Final prize money" on condition that they take part in all of the events of the World Championship. An exception may be made in case of force majeure.

311.04 CONTINENTAL CHAMPIONSHIP RACE

TRAVEL MONEY 4'500 €//\$ PER RACE

Ski StockSki Super StockRun StockRun Super StockFreestyle

1 st	250 €//\$	1 st	600 €//\$	1 st	400 €//\$	1 st	700 €//\$	1 st	300 €//\$
2 nd	150 €//\$	2 nd	400 €//\$	2 nd	200 €//\$	2 nd	500 €//\$	2 nd	200 €//\$
<u>3rd</u>	<u>100 €//\$</u>	<u>3rd</u>	<u>200 €//\$</u>	<u>3rd</u>	<u>100 €//\$</u>	<u>3rd</u>	<u>300 €//\$</u>	<u>3rd</u>	<u>100 €//\$</u>
TOT	500 €//\$	TOT	1'200 €//\$	TOT	700 €//\$	TOT	1'500 €//\$	TOT	600 €//\$

For races within Europe, travel and prize monies are paid in Euro.

For races outside Europe, travel and prize monies are paid in US dollars.

312 - PENALTIES AND DISQUALIFICATION

312.01 - PENALTIES

The following Penalties may be applied during competitions counting for World Championships :

Late arrival at briefing	100 € of penalty
Late at start line	100 € of penalty
Jumped start	<i>30 second or 1 lap of penalty</i>
Missing a buoy	<i>30 second or 1 lap of penalty</i>
Non conformity of race number	Disqualified
Not present at briefing	Disqualified
Disobeying Race Control orders	Disqualified
Non participation in Pole Position	Disqualified
Loosing race number (dossard)	Disqualified
Unsporting driving	Disqualified
Dangerous driving	Disqualified
Ignoring signals	Disqualified
Craft not conform	Disqualified

The penalty of a buoy can only be applied by a commissioner to the buoys or an official of the race.

A report of penalty mentioning the time and number of the missed buoy will be handed to the race committee.

312.02 - DISQUALIFICATION

During the final race, a driver cannot receive any technical assistance.

A pilot who receives outside help will be disqualified. The help of a safety commissioner is not considered as outside help.

A pilot's disqualification, for whatever reasons may only be pronounced by the Officer of the Day or a commissioner U.I.M.

In the event of disqualification, and according to the gravity of the fault, the case may be brought before the U.I.M. Aquabike Commission. The Commission is empowered to effect further sanctions, up to and including, the withdrawal of the superlicence.

400 - JURISDICTION

401.01 - DEFINITIONS

Jurisdiction applies to :

The U.I.M. Offshore, Circuit, Aquabike Rules.

402 - JUDICIAL BODIES

402.01 - THE INTERNATIONAL JURY

The Jury hears any protest raised by a driver.

1. Composition

The Jury must be composed of at least three members, the Chairman included. The Organisers of the event appoint the Chairman. Each competing country, including the hosting country, may appoint one member delegate who can represent only one country. These appointments must be made in writing, authorised and signed by a National Authority Official and delivered to the event Secretary.

If there are not enough NA appointed members to form a Jury of three persons, the Organisers must appoint additional members, but only enough to reach three persons.

The Jury composition will be announced at the first pilots briefing. The jury must be present at the first pilots briefing.

Race Officials can not be Jury members.

The U.I.M. Commissioner appointed to the event is a full member of the Jury.

In an event comprising races of several classes there may be a different Jury for each class.

2. Authority and Duties

The Jury sits as long as the event lasts, and hears all protests relating to that event.

The jury must decide as soon as possible on all protests presented to it during the event.

402.02 - INTERESTED PARTIES

No member of a Jury who has taken part as a competitor or Official in the race concerning which a judgement is to be given, or knows himself to have directly or indirectly an interest in one of the parties involved, may judge the protest.

However, a member of an International Jury shall not be disqualified even if one of the parties is from his NA.

402.03 - THE U.I.M. APPEALS BOARD

The U.I.M. will decide on Appeals arising from International events according to the provisions laid out in Rule 405.1.

In each case, the U.I.M. Executive Committee, or in case of an appeal lodged against an Executive Committee's decision, the appointed neutral Council member will nominate three members, one to be Chairman after consultation of the relevant commission. The members must not be from the National Authorities of the parties to the case.

One member will act as Clerk to the Board.

403 - PROTEST PROCEDURES

403.01 - VALIDITY OF PROTESTS

Only a driver may protest against facts pertaining only to the races in which he takes part. Joint protests signed by several drivers will not be considered.

All protests must be in writing - type written or hand written in printed characters in English.

It must state the reason for the protest and be accompanied by any relevant documents that shall provide evidence within the given time

and any protest fee as stipulated by the Race Organiser.

All protests must be signed by the protester.

The protest must be handed to the Race Secretariat who must, in the presence of the protester, record the time of receipt.

Any costs involved in verification are to be borne by the losing party.

A written protest can be withdrawn by the protester ; he then loses the protest fee.

403.02 - PROTEST FEE

The amount of the Protest fee must be decided by the National Authorities and it must be published in the advance programme. The maximum amount for a Protest fee will be decided by the U.I.M. General Assembly. It will be 80 €, or an equivalent sum in the national currency.

The Jury must determine whether the protest presented to it, has been lodged according to the rules. If found to be invalid, the reasons must be given to the protester.

If a protest is upheld by the Jury, the fee must be returned to the protester.

403.03 - RIGHT OF PROTEST

A protest can be lodged against the posted provisional results or against any decision made by the Race Committee or against one or several competitors.

A protest can only be lodged on a subject which directly concerns the protester himself. A protest which complies with these rules can not be refused by the Jury.

403.04 - TIME OF LODGING A PROTEST/ PROTEST PERIOD

A protest regarding the qualification of a boat, a motor, or a driver must

be made before the drivers briefing. Starting in a race is considered as acceptance of the conditions and the eligibility of the other competitors. The only exception to the above is when the protester can prove the facts were not given to him before the drivers briefing, then a protest can be lodged up to one hour after the posting of the results.

Any other protest must be lodged within one hour of the first results being posted with the following exceptions:

A protest against new results can be lodged within one hour of these new results being posted, but only concerning the decision, which lead to the changing of the results.

A protest against any other decision, posted after the posting of the first results can be lodged within one hour after the posting of this decision.

403.05 - JUDGEMENT

Decisions by the Jury shall be reached by simple majority of votes. In the case of equal votes, the Chairman's vote will be the casting vote.

Any judgement pronounced by the Jury stays in effect until altered by the Appeal Board.

403.06 - NOTIFICATION OF DECISION

All decisions by any jury must be notified to the concerned parties in writing, including their right of appeal.

403.07 - AVAILABILITY OF DOCUMENTS

All documents relevant to any decision by the Jury must be filed with the organising N.A. and held available for a higher authority until the time limit for an appeal has expired.

403.08 - RETURN OF FEES

If a protest is upheld by the Jury, the fee must be returned to the protester.

404 - HEARING OF THE PROTEST

404.01 - RIGHT TO A HEARING

Any person being party to a protest, or being charged for an offence against the rules shall have the right to be heard before the Jury in order to defend himself.

The Jury must give notice to all parties of where and when the hearing will take place.

If any party duly notified, fails to appear without giving an acceptable reason, judgement can be rendered by default.

404.02 - WITNESSES

Each party may bring witnesses to the hearing.

Any costs incurred by the appearance of witnesses shall be borne by the respective parties unless decided otherwise by the Jury.

404.03 - THE HEARING PROCEDURE

The following procedure must be followed in all hearings unless otherwise stated elsewhere in these rules :

- Written minutes must be taken.
- All parties to the case are entitled to be present and to hear and question all evidence at the hearing up to the time the Jury makes its decision. The protest, shall be read out to the parties.
- The parties shall be invited to state their cases. The parties may call witnesses. Each witness, after having given his account of the case, may be questioned by all parties and by the members of the Jury.
- Any member of the Jury who is familiar with the case may give his evidence
- The parties shall be entitled to question each other and any member of the Jury who may have given evidence.

- The witnesses shall withdraw and the parties shall be invited to make a final statement of their cases.
- The Jury may recall any party, previous witness or new witness and call on other evidence.
- The parties must be present during the whole of the recall and must be given the opportunity to question any new evidence after which they may re-make their final statements.

405 - THE APPEALS PROCEDURE

405.01 - APPEAL BOARD

The members of the Appeal Board may hold other sports functions and consist of a Chairman and two members.

The Appeal Board sits whenever the Chairman calls a meeting.

405.02 - INTERESTED PARTIES

No member of an Appeal Board who has taken part as a competitor or Official in the race concerning which a judgement is to be given, or knows himself to have directly or indirectly an interest in one of the parties involved, may judge the appeal.

405.03 - RIGHT OF APPEAL

A decision by a Jury may be appealed by the parties involved when they consider that an injustice has been made against them and/or fresh evidence has been made available that may alter the decision of the Jury.

A driver can also appeal against :

- a penalty, imposed by the U.I.M. Executive Committee, or imposed by the OOD for the reason of post race technical scrutinizing
- new results, but only concerning the decisions, which lead to the changing of the results,

without the prior lodging of a protest, but only if this was not possible, because the race jury has had dissolved itself before or within the protest period after the finish of the event.

405.04 - NOTICE OF INTENTION TO APPEAL

The intention of appeal must be notified in writing to the Secretariat of the U.I.M. by the interested party within four days of the day following the Jury decision or in the case of a decision against which a driver can appeal without the prior lodging of a protest, within four days of the day following the notification of this decision to the NA of the driver or if earlier to the driver himself.

405.05 - TIME LIMIT

All appeal documents and fees must be received within ten days from the date when the notice to appeal was received by the U.I.M. Secretariat.

405.06 - LODGING OF AN APPEAL

The appeal must be sent by telefax, letter or E-mail and it must be signed by the appellant. The reasons for the appeal must be stated.

Address, telephone, e-mail and/or fax should be clearly mentioned on the appeal, so that the U.I.M. can send any correspondence directly to the driver with copy to relevant N.A.

405.07 - NOTIFICATION OF THE PARTIES OF THE APPEAL

The U.I.M. Secretariat must within two days from receiving the appeal, notify by telefax or letter the other parties that an appeal has been lodged and that they may send a rejoinder.

Eventual rejoinders must be received within ten days from receiving this notification.

405.08 - TIME LIMIT FOR DECISIONS ON APPEALS

All appeals must be determined no later than forty-five days after the date when the appeal documents and fees were received by the U.I.M. Secretariat.

405.09 - APPEAL BOARD DECISIONS

The Appeal Board takes its own decision based on every information available. The decision of the jury may be upheld, changed or not upheld. The decision of the Appeal Board is final.

405.10 - COSTS

The appeal fee is 1.525 €. In addition the driver has to pay 2.290 € to the U.I.M. for possible costs of the appeal committee (meetings, telephone or fax costs). The appeal fee for a single championship/invitation race (not a Formula race, or a WOC race or similar) is 760 €, in addition the driver has to pay 760 €, to the U.I.M. for possible costs of the appeal committee.

The driver who appeals has to pay for the meeting costs anyway if the appeal is upheld or not. If the costs of the appeal committee is less than 2.290 € then the difference will be paid back to the driver.

The appeal fee of 1.525 € will be refunded if the appeal is upheld. If the original decision is changed by the appeal board (405.09), it can decide to refund a percentage of the appeal fee if appropriate.

The driver may agree to forego an Appeal Committee meeting and for the appeal to be handled through telephone, E-mail and fax, if possible, in order to minimise the cost for the appeal.

406 - PENALTIES

406.01 - GENERAL

Any proven breach of applicable Rules may be penalised.

If the rules do not determine a special penalty for the relevant breach, the penalty to be given has to be proportional to the seriousness of the breach. The proportionality is at the discretion of the decision maker.

The U.I.M. Executive Committee or the relevant National Authority shall only impose penalties, when they deem the relevant infringement to be so serious, that it has to be penalized although the deadlines for penalizing or lodging a protest have already expired. After the expiry of the aforementioned deadlines, a penalty for an ordinary breach of the rules shall in general not be imposed.

The Race Jury or the Appeal Board following a protest or an appeal can (only) impose penalties, which can be imposed by the OOD.

406.02 - DEADLINES FOR PENALIZING

The OOD and the U.I.M. Commissioner can only impose Penalties until the moment, the first results of the race are posted.

The National Authority and/or the U.I.M. Executive Committee can only impose penalties within 3 month from the day, the infringement occurred.

These deadlines are not valid for penalties imposed by the OOD for the reason of post race technical scrutinizing. In this case, the penalty has to be imposed as soon as possible after the receipt of the scrutinizing results.

406.03 - UNACCEPTABLE BEHAVIOUR

In order to protect the interests of the sporting community, the following actions may also be penalised by the OOD, by the U.I.M. Executive Committee or by the NA of the Licence Holder.

- any deliberate act taken to gain unfair advantage.
- any false act made or statement given with the intention of suppressing facts required for the proper conduct of the race.
- any attempt to bribe or the taking of a bribe.
- any abusive or unsportsmanlike behaviour.

406.04 - REPRIMAND

A Reprimand is a notice of disapproval of an unacceptable action. It must be recorded by the Race Secretariat. A reprimand must be witnessed. A reprimand automatically constitutes warning that if the offence recurs, a heavier penalty will be given. A Reprimand is valid for 12 months.

A reprimand can be given by the OOD, or the National Authority of the Licence Holder.

406.05 - YELLOW CARD

A yellow card may be given by the OOD or U.I.M. Commissioner for behaviour on the water.

A yellow card will be confirmed on a form which will be posted with the results, the pilot has 60 minutes to protest to the Jury if the protest is not accepted, he can make an appeal. Protests and appeals can be lodged according to U.I.M. rules.

All yellow cards as well as red cards given to a driver in any event counts to his C Yellow cards can also be given for the following reason :

- Up to two yellow cards at a time for dangerous driving
- Red card for extremely dangerous driving.

A driver who has a red card or three valid yellow cards will lose the right to take part in the next titled UIM race in his classes/formulae. All existing cards are removed. All cards will remain valid for the four events in which he competes following the date on which the last card was delivered.

Yellow cards must be notified to the U.I.M. and National Authority of the driver concerned within three days.

The U.I.M. will notify all National Authorities if a driver obtains three yellow cards and is suspended from international competition.

All yellow cards including reason for the card given must be mentioned in the commissioner's report.

406.06 - DISQUALIFICATION

Disqualification deletes a competitor from the results of the heat or race where the offence occurred.

Disqualification is done by the OOD

406.07 - TEMPORARY SUSPENSION

Temporary suspension suspends a competitor, a competitor together with crew, a crew member, or an Official from all or part of an event. Temporary Suspension may be given for serious indiscipline.

Temporary Suspension will be imposed by the OOD.

406.08 - PROLONGED SUSPENSION

Prolonged suspension can be imposed on a competitor, an Official or an organisation for deliberate fraud, repeated or very serious indiscipline or very serious misconduct.

Prolonged Suspension can be imposed only by the NA of the licence holder or organisation, or by the U.I.M. Executive Committee.

A Prolonged Suspension is always international. The NA must immediately inform the U.I.M. of a Prolonged Suspension and the U.I.M. must inform all other NA's.

406.09 - EXCLUSION

Exclusion means a permanent loss of all rights to take part in any activities falling under the U.I.M. and its affiliated organisations. A person or an organisation who has committed a moral or sporting offence of extreme gravity is liable to be excluded.

A sentence of exclusion can be pronounced only by the NA of the licence holder or organisation, or by the U.I.M. Executive Committee.

Exclusion is always International. The NA must immediately inform the U.I.M. when it has imposed a sentence of Exclusion and the U.I.M. must inform all other NA's.

In the case of suspension, prolonged suspension or exclusion, all National Authorities and therefore their affiliated clubs bind themselves to respect the decision and apply it in their own country.

407 - PRIORITY OF THESE RULES

The course of the U.I.M. has to be exhausted, before the dispute can be brought before civil court.

408 - POWERBOAT PROTEST FORM

PART A

Is for you, the driver, to complete and hand in within the specified time limit.

PART B

Is for you, the driver, to complete and give to the Protest Committee Chairman at the beginning of the Protest Meeting.

PART C

is for the Protest Committee to complete.

DRIVERS PROTEST FORM

PART A

SECTION 1

NAME OF EVENT: _____ DATE OF RACE OR HEAT: ____

HEAT NUMBER: _____ CLASS: _____

SECTION 2

YOUR NAME (PROTESTOR) _____

YOUR BOAT NUMBER: _____ BOAT NAME: _____

SECTION 3

I (the Protestor) am protesting against: (tick ✓ as appropriate)

- ☐ To seek redress from the Race Organising Committee for actions or omissions.
- ☐ Another driver, name: _____ Boat No: _____
- ☐ The Results as posted

SECTION 4

Which rule has been infringed _____

Time of incident: _____

On which lap was the incident _____

Where was the incident _____

SECTION 5

Your signature _____

Time of signature _____

SECTION 6

You must see this section completed by the official receiving this Protest:

Name of Official: _____

Time Protest received: _____

PART B**SECTION 7**

Either, explain incident with another driver; in writing and by drawing;

Or, explain your reason for Protesting against the Race Committee:

[illegible]

500 - FREESTYLE

500.01

The Grand Prix of Freestyle will take place in one heat. Every competitor will have 3 minutes for his exhibition. The provisional overall classification of the World Championship will determine the order for departure.

500.02

The departure is given by blowing a horn and the presentation of the green flag. The pilot indicates to the jury that he is ready to start his exhibition by raising an arm. To signal to the pilot that there remain 30 seconds, a brief horn blow will be given and the green flag will be withdrawn. The end of the 3 minutes exhibition will be indicated by a long horn blow and the presentation of the red flag. A tolerance of 10 seconds is admitted.

500.03

The jury, composed of minimum 4 members plus a chairman (one of the U.I.M. commissioners), will allocate the points individually. The number of figures, the quality of execution, the pilot's imagination, the use of the water plan and the sequence between the figures will be the determinant criteria for the assignment of the points. In case that the same figure is presented several times, the number of points assigned will be divided by two for every new presentation.

A Freestyle figure of greater difficulty with minimal mistakes will be scored higher than a figure of lesser difficulty with fewer or no mistakes.

In case that a figure is not finished, is not executed correctly or if the pilot falls into to water there will be no allocation of points.

500.04

The points for every figure will be assigned on the basis of a scale of 1 to 20 according to the quality of the figure itself (start of figure, figure itself and finishing the figure).

The final classification will be the addition of the points of all jury members.

500.05

If during the exhibition, a competitor enters in the jury zone, puts in danger the public or the officials, or damages the facilities, he will be classified last of the Grand Prix.

501 - TECHNICAL RULES

501.1 - PERSONAL WATERCRAFT

501.1.1

Personal Watercraft (PWC) shall mean a vessel which uses an inboard engine powering a water-jet pump as its primary source of motive power. It is designed to be operated by a person sitting, standing or kneeling on the vessel rather than inside it. They are steered from the front directing a rear jet or fully enclosed prop drive system. They are defined as Class Ainboard boats by the United States Coast Guard.

501.1.2

Two divisions of race used are Ski division and Runabout

501.1.3

For two-stroke watercraft to be homologated for competition in the 800cc class, the watercraft must not exceed 785cc in stock configuration when furnished by the manufacturer.

501.1.4

Unless defined as a four-stroke only class, all class listings refer to two-stroke engine displacements.

501.2 - SKI DIVISION

501.2.1

The Ski Division includes PWC which are designed for one person to stand on and are controlled from a pivoting handlepole.

501.2.2

Watercraft competing in the Ski Division must conform to the following criteria:

Dry weight must be greater than 114kg (260 lb)

2) Hull length cannot exceed 304.8cm (120")

3) Hull width must be between 50,8cm (20") and 76,2cm (30")

501.3 - RUNABOUT DIVISION

501.3.1

The Runabout Division includes PWC which are designed for one or more people and have a seat.

501.3.2

Watercraft competing in the Runabout Division must conform to the following criteria:

1) Dry weight must be greater than 216kg (475 lb.) for 1200cc class

2) Hull length cannot exceed 342.9cm (135")

Hull width must be between 96.5cm (38") and 127.0cm (50")

501.3.3

For Endurance only:

1) Hull length cannot exceed 394.0cm (156")

Hull width must be greater than 96.5cm (38")

501.3.4

The following minimum weight criteria applies to Ski division et Runabout 1200 homologated prior to September 1, 1998:

- 1) Dry weight must be greater than 168kg (370 lb.) for Ski division*
- 2) Dry weight must be greater than 191kg (420 lb.) for Runabout 1200*

501.5 FOUR-STROKE RUNABOUT

501.5.1

The Four-Stroke Runabout includes PWC, which are designed for one or more people and have a seat. Four-stroke Runabout modification levels are broken down into two levels: Stock and Superstock.

501.5.2

For the purposes of this rule book, normally aspirated four-stroke watercraft only utilize atmospheric pressure for induction.

501.5.3

Turbocharged and supercharged four-stroke engines utilize boosted induction systems that increase pressure in the intake tract for combustion.

501.5.3

In addition to four-stroke-specific classes, four-stroke PWC may compete in two-stroke classes, provided the following:

1) Normally aspirated

1a) Normally aspirated Stock-class four-stroke watercraft may compete in Stock and Superstock two-stroke classes.

1b) Normally aspirated Superstock-class four-stroke PWC may compete in Superstock twostroke classes.

2) Turbocharged/supercharged

2a) Stock-class turbocharged/supercharged four-stroke PWC may compete in Superstock two-stroke classes

2b) Superstock-class turbocharged/supercharged four-stroke PWC may compete in Superstock two-stroke classes.

3) Four-strokes up to 1600cc

Four-strokes up to 1600cc may compete in 1200cc two-stroke classes.

501.5.4

PWC competing in Four-Stroke Runabout must conform to the following criteria:

1) Dry weight must be greater than 272kg (600 lb.)

2) Hull length cannot exceed 342.9cm (135")

3) Hull width must be between 96.5cm (38") and 127.0cm (50")

501.5.5 FOR ENDURANCE ONLY:

1) Hull length cannot exceed 394.0cm (156 in.)

2) Hull width must be greater than 96.5cm (38 in.)

502 - STOCK**502.1 - STOCK CLASS**

Intended to promote interest in stock personal watercraft competition and to enable individuals to become active competitors with relatively modest investment and maintenance costs. Watercraft competing in these classes must conform to the specifications which follow.

502.1.1

All watercraft must remain strictly stock, except where rules allow or require substitutions or modifications. Changes or modifications not listed here are not permitted.

Some original equipment components may not comply with rules. Hull Identification Numbers must be displayed as furnished by the manufacturer.

502.1.2

When rules permit or require equipment to be installed, replaced, altered or fabricated, it is the sole responsibility of the rider to select components, materials and/or fabricate the same so that the watercraft operates safely in competition.

502.1.3

Original equipment parts may be updated to newer original equipment parts of the same model. The part must be a bolt-on requiring no modifications to that part or any other parts except where rules allow substitutions or modifications

502.1.4

Sound level shall not exceed 86 dBA at 22.86m (75 ft.).

502.1.5

Fuel must consist of gasoline meeting the criteria defined in Art. 504

502.2 - HULL

502.2.1

All watercraft must have a flexible tow loop attached to the bow. The tow loop should be made of a flexible material (e.g., nylon

strap, rope, etc.) so as not to create a hazard. Tow hooks which protrude beyond the plane of the hull must be removed.

502.2.2

Hull and deck repairs may be made. However, these repairs must not alter the original configuration by more than 2.00mm (0.08 in.). Handles, drop-in type storage buckets, bolt-on type mirrors and gauges may be modified, aftermarket or removed provided a hazard is not created

502.2.3

All watercraft may be equipped with a maximum of two sponsons. Original equipment sponsons may be modified, aftermarket, repositioned or removed. Overall length of each sponson shall not exceed 91.45cm (36.00 in.). Sponsons shall not protrude from the side of the hull by more than 100.00mm (3.94 in.) when measured in a level horizontal plane. The vertical channel created by the underside of the sponson shall not exceed 38.00mm (1.50 in.). No part of the sponson shall extend downward below the point at which the side of the hull intersects the bottom surface of the hull by more than 38.00mm (1.50 in.). Aftermarket or modified sponsons must exceed 6mm (0.24 in.) in thickness. All leading edges must be radiused so as not to create a hazard. Sponsons may not be attached to the planing surfaces of the hull. Fins, rudders, skegs and other appendages that may create a hazard will not be allowed.

502.2.4

For the Ski Division only, sponsons may be attached to the inside of the bond flange, but no part of the sponson may extend more than 38.00mm (1.500 in.) below the lower part of the bond flange (bumper removed). Sponsons attached to the inside of the bond flange shall not protrude outside the bond flange (bumper removed) when measured in a level horizontal plane.

502.2.5

Intake grate may be modified or aftermarket. Intake grate is required and must be the full-length type with at least one bar

running parallel to the drive shaft. Grates may not extend more than 12.00mm (0.47 in.) below the flat plane of the pump intake area. All leading edges must be radiused so as not to create a hazard.

502.2.6

Pump cover plate may be modified or aftermarket. An extension may be added to the rear of the pump cover plate but shall not exceed the width of the original equipment plate. Modified and aftermarket plates must not extend more than 100mm (3.94in) beyond the end of the original equipment plate for Ski Div. or 177.8mm (7.00in) for Runabout 1200. The sides of the extension must be connected to the radiused portion of the pump plate so as not to create a hazard. Fins, rudders, skegs and other appendages that may create a hazard will not be allowed.

502.2.7

Replacement trim plates may be used. Only replica parts that offer handling characteristics the same as stock are allowed. Material shall not be restricted to original equipment provided a hazard is not created (i.e., aluminum in place of plastic)

502.2.8

Replacement bumpers may be used provided a hazard is not created.

502.2.9

A soft, flexible water-spray deflector may be attached to the hull sides or to the bond flange provided a hazard is not created. No part of the deflector may extend beyond the perimeter of the original equipment bumper or side moldings as measured by a plumb line.

502.2.10

Handlebar, throttle, throttle cable, and grips may be modified or aftermarket. Handlebar cover may be modified or removed.

Aftermarket switches and switch housings may be used. Steering shaft, steering shaft holder and handlebar holder may be aftermarket. The handlebar must be padded at the mounting bracket or, if it has a crossbar, the crossbar must be padded. Quick-turn steering modifications to alter steering ratio are allowed. Aftermarket steering cables will be allowed.

502.2.11

Ski Division Only: Handlepole (and mounting bracket) may be modified or aftermarket provided it functions as originally designed. Handlepole attaching point may be reinforced.

502.2.12

Runabout 1200: Original equipment seat base and height must be used. Seat cover may be changed.

502.2.13

Padding and/or mat kits may be added and custom painting is allowed. The surface finish of any metal component outside the hull area above the bond flange may be polished, shot peened or painted.

502.2.14

Original bilge pump may be modified or disconnected. Aftermarket bilge draining systems that do not create a hazard are allowed.

502.2.15

Engine compartment ventilation tubes may be modified, aftermarket, or removed. Inlet and outlet openings may not be enlarged (i.e., when the tube is removed, the opening may not be larger than stock). Vents may be shielded or plugged. No other modifications to the hood will be allowed.

502.3 - ENGINE - TWO-STROKE

502.3.1

Engines may be bored. Replacement piston assemblies may be used provided the original port timing, compression ratio, dome profile, skirt length and shape and type of material are not changed. Replacement piston assemblies must weigh within $\pm 25\%$ of original equipment. Oversize pistons must not exceed 1mm (0.04 in.) larger than the original piston size. Engine displacement must not exceed class designation (e.g. 800cc in 800 Stock, etc.). Chamfering of cylinder ports must not exceed 1mm (0.04 in.) at a 30 degree maximum angle. (See diagram in Appendix.) No additional holes and/or fuel passages of any kind in replacement pistons are allowed.

502.3.2

Crankshaft may be rebuilt using replacement counterweights, crank pins, bearings and connecting rods. Counterweights, crank pins and connecting rods made of non-ferrous metals are not allowed. Stroke and rod length may not be changed. Counterweights on non-rebuildable style crankshafts may be machined to accept a press-through crank pin. Replacement bearings must maintain their original type and dimensions. Replacement counterweights must resemble the original part (i.e., holes and/or pockets not existing on the original part may not be on the replacement part). Total weight of the crankshaft assembly must be within $\pm 5.00\%$ of original equipment. Crankpins may be welded and/or keyed to the counterweights.

502.3.3

Repairs to cracked or punctured crankcases may be made provided only one damaged area affecting one cylinder bank has been repaired. No other modifications or repairs are allowed.

502.3.4

External modifications to the engine finish (e.g., plating, polishing and/or painting) are allowed for cosmetic purposes only.

502.3.5

No internal modifications of any kind, including grinding, surfacing, polishing, machining, shot peening, etc., will be allowed on any engine components.

502.3.6

Exhaust system must remain stock as supplied by the manufacturer.

502.3.7

Cooling system flush kits that do not create a hazard will be allowed. No other additions or alterations to the cooling system will be allowed.

502.3.8

Replacement starter motor and bendix may be used.

502.3.9

Replacement engine mounts may be used.

502.3.10

Oil-injection system may be disconnected or removed.

502.3.11

Replacement of general maintenance parts (e.g., gaskets, seals, spark plugs, spark plug wires, spark plug caps, wiring, water hoses, fuel lines, clamps and fasteners) shall not be restricted to original equipment providing the following:

- 1) Replacement gaskets may be used but must be of the same type (e.g., sheet, o-ring, etc.) and thickness as their OEM counterparts.*
- 2) Stripped threads must be repaired to the original size.*
- 3) Replacement hoses must maintain their original inside diameter*
- 4) Fasteners (e.g., bolts, nuts and washers) may not be substituted with titanium pieces unless originally equipped. Fasteners may integrate locking mechanisms.*

502.4 - ENGINE - FOUR-STROKE

502.4.1

Cylinder head combustion chambers may be cleaned by bead blasting with valves seated in place. Intake and exhaust ports may not be bead blasted or cleaned with abrasive material such as steel wool or Scotch-Brite®. Repairs to the cylinder head affecting one cylinder bank are allowed.

502.4.2

Crankshaft must remain stock. Replacement bearings or bearing shells are allowed, providing they maintain their original type and dimensions.

502.4.3

Camshaft(s) must remain stock. Replacement bearings or bearing shells are allowed, providing they maintain their original type and dimensions.

502.4.4

Intake and exhaust valves may be shimmed with OEM or aftermarket shims.

502.4.5

Original cooling system thermostat may be removed, modified or aftermarket. Cooling system flush kits are allowed. No other additions or alterations to the cooling system will be allowed.

502.4.6

Valve cover may be modified or replaced for cosmetic purposes and/or weight reduction only.

502.4.7

Replacement of general maintenance parts (e.g., gaskets, seals, spark plugs, spark plug wires, spark plug caps, wiring, water hoses, fuel lines, fuel filters, oil filters, clamps and fasteners) shall not be restricted to original equipment provided the following:

- 1) Replacement gaskets may be used but must be of the same type (e.g., sheet, metal, o-ring, etc.) and thickness as their OEM counterparts.*
- 2) Stripped threads must be repaired to the original size.*
- 3) Replacement hoses must maintain their original inside diameter.*
- 4) Fasteners (e.g., bolts, nuts and washers) may not be substituted with titanium pieces unless originally equipped. Fasteners may integrate locking mechanisms.*

502.5 - AIR/FUEL DELIVERY - TWO-STROKE**502.5.1**

Aftermarket flame arresters that meet USCG, UL-1111 or SAE J-1928 Marine standards may be used. Carburetor jets (replaceable type), needle valves and needle valve springs may be changed. Choke may be removed provided additional air intake for the engine is not created. Aftermarket primer system may be installed. No other carburetor modifications will be allowed.

502.5.2

The entire fuel system is a closed system. The watercraft must not vent or spill fuel at any attitude with or without the engine running. Original equipment fuel tank, fuel pickup, fuel filler, fuel filter, fuel tap assembly and relief valve must be used and cannot be modified. Fuel petcock may be bypassed. Additional fuel filters may be used. Fuel tank filler cap may be modified or aftermarket provided a hazard is not created.

502.6 - AIR/FUEL DELIVERY - FOUR-STROKE

502.6.1

Electronic fuel-injection systems: Flame arresters that meet USCG, UL-1111 or SAE J-1928 Marine backfire flame arrester test standards must be installed. If not equipped with an airflow sensor, the ducting between the flame arrester and throttle body may be modified or aftermarket. If originally equipped with an airflow sensor, the ducting may be modified or aftermarket between the flame arrester and airflow sensor. Modifications to the airflow downstream of the airflow sensor are not allowed. No modifications to the turbocharger and supercharger system, if applicable, are allowed.

502.6.2

Carbureted induction systems: Flame arrestors that meet USCG, UL-1111 or SAE J-1928 Marine backfire flame arrester test standards must be installed. Carburetor jets (replaceable type), needle valves and needle valve springs may be changed. Choke may be removed provided additional air intake for the engine is not created. Aftermarket primer system may be installed. No other carburetor modifications will be allowed.

502.6.3

Fuel injectors and fuel pump must remain stock. Fuel pressure regulator may be modified to change fuel pressure.

502.7 - IGNITION AND ELECTRONICS - TWO-STROKE

502.7.1

Replacement batteries are allowed but must fit into the original equipment battery box and be securely fastened.

502.7.2

The original electronic control unit may be modified or aftermarket so long as it does not offer any additional inputs or outputs than the original unit, and it must connect with the original connections. No additional sensors may be added (e.g., exhaust gas temperature, detonation sensors, etc.). Engine temperature sensors may be disabled.

502.7.3

Ignition timing may be altered by slotting ignition trigger mounting plate. An adapter plate may be used for the sole purpose of relocating the ignition trigger.

502.7.4

Aftermarket spark plugs with a different heat rating may be used.

502.8 - IGNITION AND ELECTRONICS - FOUR-STROKE

502.8.1

Replacement batteries are allowed but must fit into the original equipment battery box and be securely fastened.

502.8.2

The original electronic control unit may be modified or aftermarket so long as it does not offer any additional inputs or outputs than

the original unit, and it must connect with the original connections. No additional sensors may be added (e.g., exhaust gas temperature, detonation sensors, etc.). Engine temperature sensors may be disabled.

502.8.3

Ignition timing may be altered by slotting ignition trigger mounting plate. An adapter plate may be used for the sole purpose of relocating the ignition trigger.

502.8.4

Aftermarket spark plugs with a different heat rating may be used.

502.9 - TURBOCHARGER/SUPERCHARGER

502.9.1

Modifications to any part of the turbocharger or supercharger system (i.e., housing, turbines, rotors, sensors, ducting) are not allowed.

502.10 - DRIVELINE

502.10.1

Impeller may be modified or aftermarket, providing that the original diameter is maintained. Replacement wear rings that are within OEM internal diameter specifications may be used. Silicone adhesive sealant may be used in addition to original equipment seal to seal pump inlet. Visibility spout must be removed or plugged.

502.10.2

No internal modifications of any kind, including grinding, surfacing, polishing, machining, shot peening, etc., will be allowed on any driveline components (e.g., pump stator, reduction nozzle, etc).

503 - SUPERSTOCK

503.1 - SUPERSTOCK CLASS

Intended to promote interest in personal watercraft competition with a higher degree of modification. Watercraft competing in this class must conform to the specifications which follow.

503.1.1

All watercraft must remain strictly stock, except where rules allow or require substitutions or modifications. Changes or modifications not listed here are not permitted. Some original equipment components may not comply with rules. Hull Identification Numbers must be displayed as furnished by the manufacturer.

503.1.2

When rules permit or require equipment to be installed, replaced, altered or fabricated, it is the sole responsibility of the rider to select components, materials and/or fabricate the same so that the watercraft operates safely in competition.

503.1.3

Original equipment parts may be updated or backdated to original equipment parts of the same model. The part must be a bolt-on requiring no modifications to that part or any other parts except where rules allow substitutions or modifications

503.1.4

Pre-1996 Yamaha SuperJet may update to 1996 and newer engine components. Kawasaki JS750SX and SXi may update to SXi Pro engine components.

503.1.5

Sound level shall not exceed 86 dB(a) at 22.86m (75 ft)

503.1.6

Fuel must consist of gasoline meeting, criteria defined in Art 504

503.2 - HULL**503.2.1**

All watercraft must have a flexible tow loop attached to the bow. The tow loop should be made of a flexible material (e.g., nylon strap, rope, etc.) so as not to create a hazard. Tow hooks, which protrude beyond the plane of the hull, must be removed.

503.2.2

Hull and deck repairs may be made. However, these repairs must not alter the standard configuration by more than 2.00mm (0.08 in.). Hull, bulkhead and deck may be internally reinforced. Fasteners may be installed through the hull, bulkhead and deck for the purposes of securing components to interior surfaces, provided a hazard is not created. Other than for the use of fasteners, the bulkhead may not be modified.

503.2.3

All watercraft may be equipped with a maximum of two sponsons. Original equipment sponsons may be modified, aftermarket, repositioned or removed. Overall length of each sponson shall not exceed 91.45cm (36 in.). Sponsons shall not protrude from the side of the hull by more than 100mm (3.94 in.) when measured in a level horizontal plane. The vertical channel created by the underside of the sponson shall not exceed 38mm (1.50 in.). No part of the sponson shall extend downward below the point at which the side of the hull intersects the bottom surface of the hull by more than 38mm (1.50 in.). Aftermarket or modified sponsons must exceed

6mm (0.24 in.) in thickness. All leading edges must be radiused so as not to create a hazard. Sponsons may not be attached to the planing surfaces of the hull. Fins, rudders, skegs and other appendages that may create a hazard will not be allowed.

503.2.4

Ski Division Only: Sponsons may be attached to the inside of the bond flange, but no part of the sponson may extend more than 38.00mm (1.50 in.) below the lower part of the bond flange (bumper removed). Sponsons attached to the inside of the bond flange shall not protrude outside the bond flange (bumper removed) when measured in a level horizontal plane.

503.2.5

Intake grate may be modified or aftermarket. Intake grate is required and must be the full-length type with at least one bar running parallel to the drive shaft. Grates may not extend more than 12.00mm (0.47 in.) below the flat plane of the pump intake area. All leading edges must be radiused so as not to create a hazard.

503.2.6

Pump cover plate may be modified or aftermarket. An extension may be added to the rear of the pump cover plate but shall not exceed the width of the original equipment plate. Modified and aftermarket plates must not extend more than 100.00mm (3.94 in.) beyond the end of the original equipment plate for Ski and Sport Division or 177.80mm (7.00 in.) for Runabout Divisions. The sides of the extension must be connected to the radiused portion of the pump plate so as not to create a hazard. (See diagram in Appendix.) Fins, rudders, skegs and other appendages that may create a hazard will not be allowed.

503.2.7

Aftermarket trim tabs, either fixed, automatic and/or rider controlled, may be used. Original equipment trim plates that are

detachable from the hull may be removed or replaced when installing aftermarket trim tabs. Trim tabs cannot exceed the width of the planing surface or extend rearward more than 100mm (3.94 in.) beyond the end of the original planing surface. All hull extensions mounted on the hull's transom will be considered as a trim tab. All edges must be radiused so as not to create a hazard. Fins, skegs, rudders and other appendages that may create a hazard are not allowed.

503.2.8

Replacement bumpers may be used provided a hazard is not created.

503.2.9

A soft, flexible water-spray deflector may be attached to the hull sides or to the bond flange provided a hazard is not created. No part of the deflector may extend beyond the perimeter of the original equipment bumper or side moldings as measured by a plumb line.

503.2.10

Handlebar, throttle, throttle cable, and grips may be modified or aftermarket. Handlebar cover may be modified or removed. Aftermarket switches and switch housings may be used. Steering shaft, steering shaft holder and handlebar holder may be aftermarket. The handlebar must be padded at the mounting bracket or, if it has a crossbar, the crossbar must be padded. Aftermarket steering cables are allowed.

503.2.11

Ski Division Only: Handlepole (and mounting bracket) may be modified or aftermarket provided it functions as originally designed. Handlepole attaching point may be reinforced.

503.2.12

Runabout Division Only: Seat assembly may be modified or aftermarket. Seat height may be changed.

503.2.13

Padding and/or mat kits may be added and custom painting is allowed. The surface finish of any metal component outside the hull area above the bond flange may be polished, shot peened or painted.

503.2.14

Original bilge pump may be modified or disconnected. Aftermarket bilge draining systems that do not create a hazard are allowed.

503.2.15

Engine compartment foam may be removed, modified or aftermarket. Only floatation foam within the engine compartment may be removed. Only foam that can be removed without modification to any other part or parts, except where rules allow the parts to be modified, is allowed. Parts may not be relocated based on the removal of the foam. The hull's inner liner or deck may not be cut or modified to remove foam. Removal of foam between layers of the hull and/or deck is not allowed.

503.2.16

Storage covers, hatches, instrument cowlings and engine covers may be modified/aftermarket provided a hazard is not created and the OEM appearance is maintained. Additional engine compartment ventilation is allowed. Original equipment vents may be shielded or plugged. Handles, drop-in type storage buckets and bolt-on type mirrors may be modified, aftermarket/removed provided a hazard is not created.

503.2.17

Ballast weight may be added within the normally exposed areas of the hull to alter the handling of the watercraft provided a hazard is not created. Only weight consisting of constant mass (i.e., water or other fluid is not allowed) that does not require the modification or relocation of any parts will be allowed unless such modification or relocation is specified by other rules.

503.3 - ENGINE - TWO-STROKE

503.3.1

Engines may be bored. Aftermarket piston assemblies are allowed. Engine displacement must not exceed class designation (e.g., 800cc in 800 Superstock, 1200cc in 1200 Superstock, etc.). The number, type, and placement of rings on piston may be changed.

503.3.2

Original equipment crankcase must be used. Internal modifications to the fuel, oil and/or water-exposed surfaces are allowed. Filler material may be added to hollow pockets in the base gasket areas. Base gasket and intake surfaces may be machined. Additional carburetor pulse line fittings may be installed. Bearing and seal surfaces may not be modified. Crankcase drain system may be removed and plugged. Repairs to cracked or punctured crankcases may be made provided only one damaged area affecting one cylinder bank has been repaired. No other external modifications or external repairs are allowed.

503.3.3

Crankshaft may be rebuilt using replacement counterweights, crank pins, bearings and connecting rods. Counterweights, crank pins and connecting rods made of non-ferrous metals are not allowed. Stroke and rod length may not be changed. Counterweights on non-rebuildable style crankshafts may be machined to accept a press-through crank pin. Replacement

bearings must maintain their original type and dimensions. Replacement counterweights must resemble the original part (i.e., holes and/or pockets not existing on the original part may not be on the replacement part). Total weight of the crankshaft assembly must be within $\pm 5.00\%$ of original equipment. Crankpins may be welded and/or keyed to the counterweights.

503.3.4

Original equipment cylinder must be used. Base gasket, head gasket and exhaust manifold gasket surfaces may be machined. Port heights, widths and shapes may be changed. Ports may not be added or deleted from cylinder. Cylinders may be machined to accept aftermarket cylinder liners. Filler material may be added to hollow pockets in the base gasket areas. No other material may be added. Cylinders may be machined to accept girdle system cylinder heads. Water-cooling fittings may be added to cylinder. Exhaust power valve components and means of actuation may be modified or aftermarket.

503.3.5

External modifications to the engine finish (e.g., plating, polishing and/or painting) are allowed for cosmetic purposes only.

503.3.6

Cylinder head may be modified or aftermarket.

503.3.7

Engine gaskets may be modified or aftermarket.

503.3.8

Exhaust system (i.e., manifold, head pipe, expansion chamber, waterbox, muffler(s), etc.) may be modified or aftermarket. Through-hull exhaust may be modified or aftermarket, providing a hazard is not created. Exit location of the exhaust gases may be

relocated to the transom below the bond flange. No tuned portion of the exhaust system shall protrude outside the hull.

503.3.9

Cooling system may be modified or aftermarket. Aftermarket cooling lines and water bypass systems may be used. Bypass fittings may be modified, aftermarket and/or relocated but must be directed downward and/or rearward so as not to create a hazard for other riders. Any valves used within the entire cooling system must be of the fixed type or automatic (e.g., thermostats, pressure regulators, solenoids, etc.). Manually controlled devices (by any means of actuation) that alter the flow of cooling water during operation are not allowed. Cooling system flush kits are allowed.

503.3.10

Replacement starter motor and bendix may be used.

503.3.11

Replacement engine mounts may be used.

503.3.12

Oil-injection system may be disconnected or removed.

503.3.13

Replacement of general maintenance parts (e.g., spark plugs, spark plug wires, spark plug caps, wiring, water hoses, fuel lines, clamps and fasteners) shall not be restricted to original equipment. Stripped threads can be repaired.

503.4 - ENGINE - FOUR-STROKE

503.4.1

Original engine block must be used. Internal modifications to the

oil and/or water-exposed surfaces will be allowed. The head gasket surface of the cylinder block may be machined.

503.4.2

The original cylinder head casting must be used. Intake and exhaust runners may be modified. Material may be added to the runners. Intake and exhaust ports may be modified. Port diameters and shapes may be changed. Combustion chambers may be modified. Material may be added to the combustion chamber. The original number of intake and exhaust valves must be the same as original. Repairs to the cylinder head affecting one cylinder bank are allowed. The head gasket surface may be machined.

503.4.3

Aftermarket valvetrain components are allowed, providing the original method of activation is maintained (e.g., if originally activated by a camshaft, they may not converted to solenoid activation). Valves may be shimmed with OEM or aftermarket shims. Valve springs may be modified or aftermarket. Camshaft(s) may be aftermarket. The number of camshafts must be the same as original. Original bearing type and dimensions must be used. Cam timing may be changed. Cam gears, tensioners, chain or belt may be modified or aftermarket.

503.4.4

Engines may be bored. Aftermarket piston assemblies are allowed. Engine displacement must not exceed class designation (e.g., 1100cc in Runabout 1100 Superstock, 1600cc in Runabout Superstock Turbo, etc.).

503.4.5

Crankshaft may be modified or aftermarket. Stroke must remain the same as original. Total weight of the crankshaft must be $\pm 5\%$ of original equipment. Replacement bearings or bearing shells are allowed, providing they maintain their original type and dimensions.

503.4.6

Aftermarket connecting rods made of ferrous materials are allowed. Rod length must be the same as original.

503.4.7

Exhaust system (i.e., manifold, connecting pipes, hoses, muffler(s), etc.) may be modified or aftermarket. Through-hull exhaust may be modified or aftermarket, providing a hazard is not created. No tuned portion of the exhaust system may protrude outside of the hull. Exit location of the exhaust gases may be relocated to the transom below the bond flange

503.4.8

Cooling system may be modified or aftermarket. Additional cooling lines may be added. Aftermarket water bypass systems may be used. Cooling system bypass fittings may be modified or aftermarket and/or relocated but must be directed downward and/or rearward so as not to create a hazard for other riders. Any valves used within the entire cooling system must be of the fixed type or automatic (e.g., thermostats, pressure regulators, solenoids, etc.). Manually controlled devices (by means of actuation) that alter the flow of cooling water during operation are not allowed. Original cooling system thermostat may be removed, modified or aftermarket. Cooling system flush kits are allowed.

503.4.9

Baffles in oil reservoir may be modified. The addition of baffles in oil reservoir is allowed. Oil pump may be modified or aftermarket.

503.4.10

Valve cover may be replaced for cosmetic purposes and/or weight reduction only.

503.4.11

Replacement starter motor and bendix may be used.

503.4.12

Replacement engine mounts may be used.

503.4.13

External modifications to the engine finish (e.g., plating, polishing and/or painting) are allowed for cosmetic purposes only.

503.4.14

Replacement of general maintenance parts (e.g., gaskets, seals, spark plug wires, spark plug caps, wiring, water hoses, fuel lines, fuel filters, oil filters, clamps and fasteners) shall not be restricted to original equipment. Stripped threads may be repaired. Fasteners may integrate locking mechanisms.

503.5 - AIR/FUEL DELIVERY - TWO-STROKE**503.5.1**

Carburetor(s) may be modified or aftermarket provided they do not vent or spill fuel at any attitude with or without the engine running. The number of venturis cannot exceed the number of cylinders. No slide-type carburetors. Aftermarket primer may be used. Intake manifold assembly may be modified or aftermarket. Aftermarket crankcase pressure operated fuel pumps may be used.

503.5.2

Modified or aftermarket vapor/air separators must not exceed 2 in. x 6 in., and must have a return line to the fuel tank open at all times. Additional fuel reservoirs may not be used. Aftermarket or modified electric fuel pumps, not exceeding 4 psi, may be used. When the engine is shut off or stops, the fuel pump must automatically stop. No manually operated on/off-type fuel pumps are allowed.

503.5.3

Aftermarket fuel-injection systems and components are allowed provided the following regulations are adhered to: High-pressure fuel hose meeting SAE J30R9 must be used; A.N. threaded-type fittings or equivalent and nonremovable, crimped- type clamps must be used on the high-pressure portion of the system (i.e., hose clamps, tie wraps, etc. are not allowed); only metaltype fuel filters may be used on the high-pressure portion of the system; all other in-line filters must be installed on the low-pressure portion of the system. When the engine is shut off or stops, the fuel pump must automatically stop. No manually operated on/off-type fuel pumps are allowed.

503.5.4

The entire fuel system is a closed system. The watercraft must not vent or spill fuel at any attitude with or without the engine running. Original equipment fuel tank, fuel filler and relief valve must be used and cannot be modified. The fuel pickup, fuel filter and fuel petcock may be removed and/or aftermarket parts may be used. Additional fuel filters may be used and fuel cell foam may be added to the original equipment fuel tank. Fuel tank filler cap may be modified or aftermarket provided a hazard is not created.

503.5.5

Flame arrester(s) which satisfy United States Coast Guard, SAE-J1928 Marine or UL-1111 Marine backfire flame arrester test standards must be installed. Aftermarket flame arresters satisfying one of these test standards are allowed. Intake silencer may be removed.

503.5.6

Reed valve assemblies may be modified or aftermarket. Rotary valve may be modified or aftermarket.

503.6 - AIR/FUEL DELIVERY - FOUR-STROKE

503.6.1

The original fuel injectors may be modified to increase fuel-flow rate. Aftermarket fuel injectors that increase fuel flow are allowed provided they must not increase airflow into the combustion chamber. Fuel rail and fuel regulator may be modified or aftermarket. Additional fuel injectors may be added. Aftermarket fuel pumps are allowed provided that when the engine is shut off or stops, the fuel pump must automatically stop. No manually operated on/off fuel pumps are allowed. High-pressure fuel hose meeting SAE J30R9 must be used; only metal-type fuel filters may be used on the high-pressure portion of the system; all other in-line filters must be installed on the low-pressure portion of the system

503.6.2

Flame arresters that meet USCG, UL-1111 or SAE J-1928 Marine standards must be used. Airflow sensor may be modified, aftermarket or removed. Ducting between the flame arrestor and throttle body may be modified or aftermarket.

503.6.3

Throttle body may be modified or aftermarket. The number of butterflies may be increased but may not exceed the number of cylinders. Intake manifold assembly may be modified or aftermarket.

503.6.4

Carburetor(s) may be modified or aftermarket provided they do not vent or spill fuel at any attitude with or without the engine running. Carburetors may be used in addition to or in place of the fuel-injection system. The number of venturis cannot exceed the number of cylinders. No slide-type carburetors. Aftermarket primer may be used. Intake manifold assembly may be modified or aftermarket. Aftermarket air-pulse-pressure operated fuel pumps may be used.

503.7 - IGNITION AND ELECTRONICS - TWO/FOUR-STROKE

503.7.1

Ignition system, electrical box, flywheel and flywheel cover may be modified or aftermarket. Battery charging circuit may be disabled and/or removed.

503.7.2

An additional battery and battery box may be used. Batteries must fit into a proper battery box and be securely fastened. Batteries may be relocated.

503.7.3

Engine temperature sensor assembly may be disconnected and/or removed.

503.8 - TURBOCHARGER/SUPERCHARGER

503.8.1

Turbocharger housing must be of the full circulating, water-jacket type at all times when the engine is running. Aftermarket turbochargers and superchargers may be used provided a hazard is not created. Original turbocharger or supercharger may be modified. Aftermarket turbochargers and superchargers may be added to originally normally aspirated watercraft. All hoses and pipes may be modified or aftermarket. Turbocharger four-stroke engines are not allowed to race in Class Runabout Stock 0-1200 cc. They will be allowed in Class Super Stock and the cubic capacity must not exceed 1600 cc.

503.8.2

Intercooler may be modified or aftermarket.

503.8.3

Boost pressure-relief valve may be modified or aftermarket

503.8.4

Boost sensor may be modified or aftermarket.

503.9 - DRIVELINE**503.9.1**

Impeller, impeller housing, stator vane assembly, pump mounting plate and/or pump shoe may be modified or aftermarket. Pump nozzle and directional nozzle may be modified or aftermarket. Overall length of the complete pump and nozzle assembly may be no more than 50.00mm (1.97 in.) longer than original equipment. Aftermarket nozzle trim systems may be used. Additional cooling fittings may be installed. Visibility spout must be removed or plugged. Silicone adhesive sealant may be used in addition to original equipment seal to seal pump inlet.

503.9.2

Couplers, bearing housing and driveshaft may be modified or aftermarket provided they maintain a 1:1 drive ratio between the engine and the pump.

504 - FUEL TESTS**504.1**

A rider's statements as to the contents of his/her personal watercraft's fuel system will be binding. Gasoline is tested and certified at events through the application of various chemical analyzes as con-sidered appropriate by Fuel Check personnel. Gasoline may be checked before and/or after use in competition.

504.2

Aerosol cans of ether will be allowed for starting purposes.

504.3

Engine fuel must consist of unleaded gasoline only. Gasoline for the pur-pose of this Rule Book, is defined as a mixture of hydrocarbons and oxygen bearing compounds with the following clarifications:

- 1) Oxygen content must not increase the specific energy of the gasoline.*
- 2) Oxygen content must not exceed 3.7% by weight.*
- 3) Oxygen content must have been blended in by the refiner or the fuel man-ufacturer.*
- 4) Specific gravity must be between: 0.715 and 0.770 at 60°F (15°C).*
- 5) The only allowable oxygenates are ethers and alcohols. Epoxides (e.g., propylene oxide) will not be considered ethers. Nitrogen-bearing com-pounds are not allowed.*

NOTE: Most commercially-produced unleaded fuels and oils will meet these criteria. However some may contain additional additives which do not to meet these criteria.

505 – CHANGE AQUABIKE

A pilot can change Aquabike before the departure of the race with the agreement of the UIM technical Commissioner.

506 - PERSONAL EQUIPMENT

506.01

A properly fitting helmet that meets the current N.A. standards or current Snell standard is required to be worn by all pilots in competition. Mountain bike helmets are not allowed. An helmet is mandatory, Freestyle excepted. It is further recommended that any helmet involved in an accident be returned to the appropriate manufacturer for inspection.

506.02

A life jacket will be worn by all pilots and at all times when on the water. It is required that the life jacket shall be designed for racing. There must be a re-inforced back protector worn separately. It is recommended to use back-protector and leg-protector.

506.03

Every pilot shall verify his flotation equipment to function properly when requested.

506.04

It is required that all jackets have a minimum of two buckle-type straps across closures.

506.05

All participants must wear closed footwear when riding A.B.

506.06

Wetsuits are required to be worn at all times during competition, Freestyle excepted. Jeans, sweats or shorts are not acceptable.

506.07

The O.O.D. of an event shall have the authority to prohibit the use

of any helmet, life jacket or other equipment which they may consider unsafe, insufficient protection or inadequate.

506.08

No pilot shall participate in an event with any type of splint, including but no limited to, a cast or brace applied to his body without showing written authorisation from an approved medical doctor and approval by the O.O.D. at the event.

506.09

It is the responsibility of the pilot to submit his equipment to the Safety Officer of the day for his inspection. If, in the judgement of the inspector, an A.B. is unseaworthy unsafe or unmanageable, he must refuse to allow the A.B. to participate in the race. The O.O.D. at his discretion may require any A.B. involved in an accident to be re-inspected before entering another heat.

506.10

All A.B.'s will be required to pass a safety inspection before being allowed to compete. The Technical inspector will remove any A.B. from competition that does not meet safety requirements. Damaged or broken safety equipment not detected before or during a race, is no grounds for disqualification after completion of that race, unless pilot is black-flagged.

506.11

The question of how many A.B.'s in each class shall be inspected is left to the discretion of the O.O.D. and the Technical inspector, except in those cases where the procedure is set forth in the Technical Rules for any given class. During the course of the Pilots Meeting, it shall be the duty of the Technical Inspector to announce how many of the lead A.B.'s in each class shall report to the Inspector for inspection immediately after the running of their event.

600 - RECORDS AND PERFORMANCES

600.01 - GENERAL

All record trials and their control are submitted to the following rules.

The record belongs personally to the driver who has established or beaten it. The record is entered under the name and nationality of the driver.

Any race against the clock, and called kilometre or mile trials or any similar name is prohibited if not conducted in accordance with these rules.

Combining speed records with distance records is not allowed.

No direct or indirect aid is to be provided to the driver who attempts a record.

600.02

World speed records, hour records, distance records and competition records are all records open to all boats and Aquabikes of international classes adopted by U.I.M.

600.03

National records recognised by U.I.M. are the records established by boats of national classes duly recognised by the national authority (the rules of which have been approved by U.I.M.). The Certificate differs from those issued for World Records.

600.04

The World's Unrestricted Water Speed Record is awarded to the fastest boat in the world piloted by the driver, irrespective of the class of boat; this record is established in conformity with U.I.M. rules.

601 - CLASSES ELIGIBLE FOR RECORDS

601.01

World records may be attempted with the following craft :

- All U.I.M. series and classes
- American Powerboat Association classes
- Prototypes

601.02

A boat can establish a record in its own class only.

601.03

When a class is modified so as to reduce the performance of the class, for example by :

- reducing the cylinder capacity,
- restricting the tuning permitted,
- restricting fuel permitted,
- introducing a minimum weight,
- increasing the minimum weight,
- or any other means, the records established before the modification comes into force will be frozen and a new series of records will be started.

All proposals for new rules, rule changes and U.I.M. General Assembly minutes shall state :

- whether or not records will be frozen,
- the classes to be frozen,
- the date(s) when the classes are to be frozen.

601.04 - QUALIFICATION

For offshore records a boat must have completed the course, taken the chequered flag and finished first, second or third, in an U.I.M. World or U.I.M. Continental Championship race in the class to be attempted and after the date when the class was last frozen.

602 - DEFINITION OF UNITS

602.01 - TIME, DISTANCE AND SPEEDS

The hour is the twenty-fourth part of a terrestrial day. The symbol is letter "h".

The minute of time is the sixtieth part of one hour. The symbol is letters "mn";

The second of time is the sixtieth part of one minute. The symbol is the letter "s".

nautical mile	=	nm
statute mile	=	st.m
kilometre	=	km
nautical mile per hour	=	knot
statute mile per hour	=	mph
kilometre per hour	=	kmh

602.02 - STATUTE MILE, NAUTICAL MILE AND KILOMETRE

The statute mile is an Anglo-Saxon measure.

The nautical mile is used at sea.

The knot is not a unit of length; it expresses the speed of one nautical

mile per hour.

One statute mile (st.m) = 5280 feet = 0.8690 nm = 1,609.3 km

One nautical mile (nm) = 6080 feet = 1,852 km = 1.1508 st.m

One kilometre = 3280.844 feet = 1000 metres

604 - BASES (THE COURSE)

604.01

Records may only be established on bases recognised by National Authorities and notified to the U.I.M. in accordance with these rules.

604.02

The base is measured and certified by an official surveyor holding a certificate of competence from a recognised institute, in the presence of a delegate of the National Authority.

The base must be measured by triangulation or electronic distance measuring equipment (EDM), which must have a first class certificate delivered by an Observatory or similar authority. This certificate must not be older than two years.

The official surveyor provides a certificate in duplicate in which the strict adherence to the above requirements is certified.

The certificate is signed by the surveyor and countersigned by the delegate of the National Authority. One of the copies is for retention by the National Authority for its special national records files and the other is sent to U.I.M. for its special world record files.

The base for a record must be a fixed base, as laid out for a national authority or must be measured for a special occasion in waters designated by the driver. It must be defined by fixed marks ashore.

604.03

No distinction is made between records established on the sea, rivers or lakes. Records must not be attempted during the period between half an hour before and half an hour after a change in direction in flow of water over the course (change of tidal flow).

604.04

Any expenses incurred in the laying of a temporary course are defrayed by the entrant attempting the record.

604.05

For speed records there shall be only one attempt on the course at any one time.

An attempt must not start until the director of the trial is satisfied that effective rescue boats are on the course.

605 - CALCULATION OF SPEED

605.01

The National Authority calculates the speed from the data provided by timekeepers. The speed is to be shown in kilometres per hour (kph) and statute miles per hour (mph), even when the attempt takes place on a course of one nautical mile;

The duration of each run is obtained by taking the arithmetical mean of the times recorded by two sets of timing devices the mean time being given to one tenth of a second.

Should one of the timing devices stop or one of the signals fail and only one duration be registered, this duration will be accepted.

605.02

The only duration so obtained serve to calculate the speed of each run. Only two decimal places are to be used.

605.03

The speed of the trial is the arithmetical mean of the speeds, two decimal places being used.

605.04

For speed records established on a distance of one nautical mile (1 nm = 1.852 km = 1.1508 st.m) the following constants are to be used :

Where t is the time in seconds :

the speed in mph = $4142.9 / t$

the speed in kph = $6667.2 / t$

606 - MARGIN REQUIRED

A record will be valid only if its speed at least equals the speed of the previous record multiplied by 1.003.

607 - MOTORS**607.01**

All accessories and/or elements required by the racing rules for each class of boat attempting to establish or improve a record must be on board during said attempt.

607.02

A boat which holds two certificates for two different classes is entitled to

establish world records in those two classes, but shall make two distinct trials, one for each class.

607.03

Immediately after the record trial, the motor(s) or engine(s) must be sealed with a distinctive seal in the presence of the officials who have witnessed the trial.

607.04

Within 48 hours of the trial, two measurers must check that the hulls, motors and equipment are in conformity with the specifications of their class, and this inspection must be carried out in the presence of the Officer of the Day or an Official specifically appointed by the National Authority.

607.05

When attempting to break a U.I.M. class record, the noise level of the motor shall not exceed the authorised level (rule 504).

608 - REQUEST TO ORGANISE

608.01

All applications for records trials must be made in writing to the National Authority at least one week before the trial and must be accompanied by the fee stipulated by the National Authority.

The National Authority applies to the U.I.M. for homologation of records.

608.02

Only records established under the direct control of a National Authority affiliated to U.I.M. can be recognised.

608.03

Each National Authority decides on the application for records to be established on its territory. The National Authority may refuse to consider a record application but must then explain the reasons in writing.

The National Authority indicates the validity of the record permit.

608.04

It is the duty of the driver to accept responsibility for any safety measures deemed necessary for this attempt.

The officials will not accept any responsibility, they will only record the results and ensure that the rules are complied with.

609 - OFFICIALS

609.01

The National Authority shall appoint an Official Observer and other duly qualified persons to hold the official functions.

An official so nominated shall not hold any other office than that to which he was nominated. An official shall not himself establish a record.

609.02

The official timekeepers and officials may receive payment for their services at rates fixed by the National Authority.

609.03

Time keeping may only be carried out by timekeepers and officials who have been recognised and authorised by the appropriate National Authority.

The stop-watches or any other apparatus employed must have a first-class certificate delivered by an observatory or similar authority. This certificate must not be older than two years.

Each timekeeper hands to the National Authority a signed report of the record attempt, countersigned by the official observer.

609.04

Timing from aboard anchored boats or aboard competing boats is not allowed.

610 - SPEED RECORDS

610.01

There is only one world record per class. This record can be established over the nautical mile, land mile or the kilometre, for Aquabikes the distance may be 500 metres.

610.02

The course is covered once in each direction consecutively.

The course is to be marked at either end by two posts in solid material, put up on shore with approved distinguishing marks affixed to them. The two lines uniting the two pairs of posts are parallel to each other and at right angles to the line of the measured course. Steering marks may be placed wherever needed.

1 mile 90°

1 kilometre

1 land mile

610.03

In wide rivers and lakes, the width of the course may be marked off at each end by two buoys, moored at a distance of one hundred metres from each other. The boats must run between these buoys.

610.04

Timing is to be one tenth of a second up to 200 kph. Above 200 kph timing is to be by photoelectric or similar apparatus to one hundredth of a second.

There must be a timekeeper and a timekeeper observer at each position.

Individual timers may be used at each position or a single timer electrically activated from each position.

Where two timers are used at a position the time is the average of the two timers.

The times of day of each attempt must be recorded.

The time elapsing between the two runs must not exceed 20 minutes except for A.B. attempts when the permitted interval is one hour.

610.05

A driver may re-start an attempt because of breakdown or other reason and may stop the engine or boat between runs.

If the attempt has not been completed within four hours of a starting time fixed by the National Authority, the officer in charge may stop the trial.

611 - DISTANCE RECORDS

611.01

Distance records are established on a total course of twelve nautical

miles for the outboard series and twenty-four nautical miles for other series, and six nautical miles for Aquabikes.

611.02

In distance records, the base is covered up and down successively in even numbers and the turns are not included in the distance.

The course for distance records may be :

- a course established for speed records;
- a base of three nautical miles maximum, in straight lines with buoys placed at the corners if necessary, no corner to be less than 135 degrees.

The buoys placed at the corners must always be rounded.

611.03

Any base must be defined by two posts set in concrete at either end.

611.04

Timing is to be to one tenth of a second.

The timing of distance records is done by a timekeeper with two timing devices at the starting line. He is assisted by two officials, one with him and one at the other end.

611.05

Stopping the motor or the boat during a distance record attempt is prohibited.

612 - HOUR RECORDS

612.01

The following international and national records are recognised by U.I.M.: 1, 2, 3, 4, 6, 9, 12 and 24 hours duration records for all the series recognised by U.I.M.

The homologation of a record of lesser duration than that applied for may be granted, but only within such limits as provided for by the above rule.

612.02

The base must not exceed six nautical mile from station to station, that is to say twelve nautical miles up and down, for Aquabikes only, the distance may be 3 nautical miles from station to station, that is to say 6 nautical miles up and down).

612.03

The base is defined by two posts set in concrete at either end.

612.04

The boat may be driven by more than one driver, but their names must be filed in the record application.

612.05

Repairs and adjustments which can be carried out on board may be effected on the course.

The same motor or engine and hull must be used throughout the trial.

On the other hand, repairs that cannot be effected on board and refuelling and changing of drivers must be effected at the control station and are to be supervised by an official.

612.06

Any time during which a boat stops during an attempt is included in the elapsed time for the attempt.

Turns are included in the course and are consequently not excluded from the timing.

612.07

Timing is carried out by one or several timekeepers with two timing devices stationed at the start. The timekeeper is assisted by two officials, one stationed with him and one stationed at the other end.

612.08

A driver can claim several different hour records during one trial by selecting the best series of consecutive laps.

612.09

The boat must cross the finishing line at the end of the lap (the last lap) during which the record time has elapsed.

The average speed of the last lap is calculated and the distance which corresponds to the time still to be run is added to the distance covered at the end of the penultimate lap.

The result is shown as the distance covered in the record time and the average speed for that distance.

612.10

Example for a two hour record on a base of six nautical miles (nm), twelve nautical miles per lap :

12 nautical miles (nm) \times 1.1508 = 13.81 statute miles (st.m)

boat completes 8 laps in 1h 54mn 40s = 110.48 st.m

ninth lap completed in 13mn 50s = 830 s

time balance 5 mn 20s = 320 s

distance balance = 13,81 st.m : $830 \times 320 = 5.32$ st.m

Total distance in 2 h = 115.80 st.m

Average speed = $115.80 / 2 = 57.90$ mph

613 - WORLD COMPETITION RECORDS

613.01

The U.I.M. recognises world records of 5, 10 and 15 statute miles in races for all U.I.M. circuit classes.

No other records may be established during a race.

Unlimited hydroplanes only are entitled to statute mile records established on :

- a single lap course (two laps for APBA Gold Cup races) while qualifying or racing,
- total heat average speed while racing.

614 - PERFORMANCE CERTIFICATES

614.01

U.I.M. may issue Performance Certificates for the best heats made during races for the principal international trophies.

614.02

Application for these is to be made by the National Authority of the

country where the race took place and the accuracy of the results, timing course and class is to be certified by them.

614.03

U.I.M. only recognise the results which beat previous performances.

614.04

The amount of the fee for Performance Certificate is decided upon annually by the General Assembly.

614.05

U.I.M. may issue a performance Certificate for speed trials made by boats not included in U.I.M. classes. These trials must, however, be made in accordance with U.I.M. rules.

U.I.M. award Performance certificates for boats of the Production boats Series having established or broken speed, duration, distance or competition records according to the Record Rules.

614.06

For these performances, it is not permissible to remove any of the boat's accessories, or items of comfort, provided by the builder and these are compulsory for racing.

U.I.M. may award Performance certificates for any of the offshore classes, international and national, established at sea on properly measured runs.

615 - HOMOLOGATION OF RECORDS

615.01

Only records homologated by U.I.M. are considered as World Records.

Only records established under the direct control of a National Authority affiliated to U.I.M. can be recognised.

615.02

If a record has been established on the territory of two National Authorities, it will be entered as a record by the National Authority on whose territory the start was made.

As soon as a record trial has taken place, the National Authority communicate the results to the Secretary General of U.I.M. by telegram and confirm it by letter.

The information has no official character and is only transmitted to such National Authorities as apply for it, in order to avoid useless trials.

615.04

The timekeepers and measurers must complete the official U.I.M. record application form which must be signed as correct by OOD and be sent to the National Authority within three days of the trial, seven days for APBA controlled classes.

The National Authority transmits the application for homologation, together with other necessary papers, to the U.I.M. no later than four weeks after the trial, sixteen weeks for APBA controlled classes.

615.05

For a request for homologation to be considered by the U.I.M., the National Authority must send:

- A plan of the course if not already in possession of the U.I.M.,
- The completed official U.I.M. application form which must include any National Authority calculations,
- A copy of the boat's measurement certificate,
- A photograph of the boat under way with the colours, inscriptions, etc.

noted on the reverse ; also to be noted on the reverse, the driver's name, class and speed achieved,

- A list of the full names of the officials with, at least, the names of the Timekeepers,
- Measurers, Rescue Officer, OOD and Observer,
- For offshore boats, a copy of the results in which the boat qualified, as per U.I.M. rule 601.04,
- For attempts on tidal waters, the calculation stating the times of the changes of direction of water along the course (changes in tidal flow) and the source of the information,
- A cheque for the amount due

615.06

The recognition fee is fixed annually by the General Assembly.

615.07

Pending the official confirmation by U.I.M., neither the driver, builder nor any other shall publish or cause to be published, circulate or cause to be circulated, the results of the trial, until the delegate of the National Authority has given the actual results in writing, after checking the calculations in the presence of timekeepers.

Even when such authorisation has been received, results may only be published with the addition of the words "Subject to official certification by U.I.M." in as large and prominent type as that in which any part of the notification itself is printed.

Neglect to comply with the above regulations automatically entails the refusal to recognise the record. The National Authority have the right to penalise whoever is responsible.

615.08

U.I.M. issues annually a list of world records, changes being made known, when they occur, by means of newsletters.

U.I.M. keeps a registered list of all word records and delivers a certificate for each new record.

616 - PROTESTS

The right to lodge a protest against a record is admitted under same rules as for racing. See Racing Rules.

700 - ENVIRONMENTAL REGULATIONS

All organisers and pilots in U.I.M. Aquabike Racing events shall conform to the following regulations :

701 - RE-FUELLING

All re-fuelling must be completed away from the water. Every care must be taken not to spill fuel or oil. It is recommended to use an absorbent carpet to avoid any spillage on to the ground.

702 - ENVIRONMENTAL CARE IN RACE AREAS

Every organiser must appoint a responsible person or persons to make necessary arrangements to control the environment of the whole race area.

There must be large containers in the pits where waste, such as cans, containers, oil waste can be placed. One person must be responsible for this and to authorise the proper removal after the race meeting with the Local Authorities.

It is mandatory to inform the pilots of all these environmental matters at the pilots meeting.

All fuel tanks which are transported must be :

1. Tight
2. Mention the racing number -7 cm height
3. Placed in a protected area and at a minimum distance of 5 m of any flames

A special designated area may be reserved by the OOD to assemble all fuel tanks.

800 - RECOMMENDATIONS FOR ORGANISING AN AQUABIKE WORLD CHAMPIONSHIP

800.01 - INFRASTRUCTURE

It is important that organisers thoroughly understand that they are holding an event which is open from the Friday at midday until after the prize-giving of the Sunday evening is over. The overall infrastructure and Race Head Quarters must therefore be effectively operational for the duration of that period.

800.02 - RACE HEAD QUARTERS

Imperatively the premises or shelter must be closed and weatherproof. Mains electricity (220V - 10A), a photocopier supplied with 1500 sheets of A4 paper, and a sufficient number of chairs and tables must also be provided. If the shelter is exterior and of a provisional nature, effective means of closing the area both by day and by night is essential. Night surveillance is also a necessity.

800.03 - CONTROL TOWER

It must be of robust construction, with a minimum surface of 5m x 5m, and weatherproof to the extent of being suitable for housing a computer timing system. Mains electricity of 220V - 10A must be laid on and tables and chairs for eight people must be present. If Race Headquarters is not in the immediate vicinity a further photocopier equipped with 500 sheets of A4 paper must be supplied.

800.04 - PADDOCK

In order for the Paddock to be well managed, it is important that there be a Paddock Director, present on site right from the Friday morning. The Director will see to it that the various teams are allocated their designated areas. The Paddock Director will also oversee the setting up and correct functioning of water, electricity, showers and WC facilities.

Those facilities should be operational from the Friday midday until the following Monday morning.

800.05 - NECESSARY ON SITE PERSONNEL

- | | |
|-------------------------------|--|
| 800.05.01 RACE CONTROL | 1 Officer of the Day (OOD)
1 Assistant Officer of the Day |
| 800.05.02 SECRETARIAT | 1 Race Secretary "French - English" |
| 800.05.03 RESULTATS | 2 Line Marshals
2 Lap Marshals
1 Voice Order Announcer |
| 800.05.04 BUOY CONTROL | 5 Land-based Judges |
| 800.05.05 PADDOCK | 1 Paddock Director
1 Assistant Paddock Director |
| 800.05.06 CONSTRUCTION | 4 persons for circuit set-up |

800.06 - REQUIRED INFRASTRUCTURE

- | | |
|-------------------------------|---|
| 800.06.01 RACE CONTROL | 1 Timing Podium (min 5m x 3m)
2 Tables et 8 Chairs
1 Weatherproof Shelter
Electricity : 220 Volts 10 Ampères
1 Official Results & Notices Board (1m x 1.5m) |
| 800.06.02 SECRETARIAT | 1 local (minimum 5m x 5m)
1 Photocopier + 1500 sheets A4
1 Telephone et 1 Fax (Secretariat)
4 tables et 8 chairs
Electricity 220 Volts 10 Amps |
| 800.06.03 CONSTRUCTION | 1 circuit set-up boat
Closure barriers for Race Control
Closure barriers for Competitors
Paddock |

800.07 - EMERGENCY REQUIREMENTS

- | | |
|-------------------------------|--|
| 800.07.01 SECURITY | 3 Security Jets + towing & signals equipment
3 Pilots with Safety Officer Qualifications
2 Rescue Craft
2 Pilot & 4 Rescue Officers |
| 800.07.02 MEDICAL | 2 On-site Medical Doctors (Minimum)
2 Equipped Ambulances
4 Qualified Ambulance Personnel |
| 800.07.03 TRANSMISSION | 1 Radio Transmission HQ:Frequency 1-2
5 Race Control (Freq. 1)
7 Security 3 Jets,2 Craft,
2 Doctors : (Freq. 2) |
| 800.07.04 ENVIRONMENT | 1 Dedicated competitors' fuel stocking area
1 Fuelling point equipped with adequate protection
A Sufficient Quantity of Fire Extinguishers
A Sufficient Quantity of Refuse Collection |

The Class Pro Aquabike U.I.M. / SPES format 2005 is added as recommendation and for any contradiction with the U.I.M. rules, the U.I.M. rules will prevail.

Any correspondence regarding the U.I.M. Class Pro Series must be sent to the following address :

UNION INTERNATIONALE MOTONAUTIQUE

STADE LOUIS II - ENTRÉE H

MC 98000 MONACO

Fax (377)92.05.04.60

E-mail : uim@uimpowerboating.com

Website : www.uimpowerboating.com

[illegible]

Achévé d'imprimer
sur les presses de
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en mars 2005



UNION INTERNATIONALE MOTONAUTIQUE

**PLEASURE
NAVIGATION
RULES**

**SPORTS
TECHNICAL**

AS FROM 2003

2002 WORLD CHAMPIONS

ENDURANCE - SPORT CLASS



R. BUONUOMO



T. DE SIMONE
(ITALY / UK)



J. ISMAEL



J. HAMPTON

ENDURANCE - TOURING CLASS



G. ZANONI



M. SABBIONI
(ITALY)



L. PREVISANI



V. TOTA

ENDURANCE PROTOTYPE CLASS



RIZZARDI (ITALY)



FATARELLA

ENDURANCE - GROUP B - TURISM



MICHELI (ITALY)



PANZERI

ENDURANCE GROUP B - SPORT



FOCCHI (ITALY)

ENDURANCE - GROUP B UNIQUE



L. MANTOVANI (ITALY)

2002 WORLD CHAMPIONS

ENDURANCE - GROUP A & GROUP B - SPORT



P. DREDGE



I. SANDERSON
(GREAT BRITAIN)



A. BISSETT

ENDURANCE - GROUP A & GROUP B - TOURING



G. ZANONI



M. SABBIONI



L. PREVISANI



V. TOTA

(ITALY)

ENDURANCE - GROUP A & GROUP B - PROTOTYPE



RIZZARDI

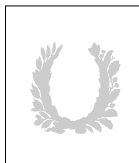


FATARELLA

(ITALY)

2002 EUROPEAN CHAMPIONS

PAVILLON D'OR

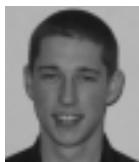


E. DE WILDE
(BELGIUM)

REGULARITY CONSTANT SPEED - CLASS 1



J. FENCL



J. M.L. FENCL

(SLOVAKIA)

REGULARITY CONSTANT SPEED - CLASS 2



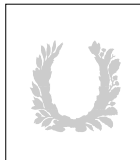
J. MEDVECKÝ



I. KOCOVA

(SLOVAKIA)

REGULARITY DECLARED TIME



P. BOER

(ITALY)



S. BOER

ENDURANCE GROUP B - OVERALL



F. BUZZI (ITALY)

GROUP 100

U.I.M. ADMINISTRATION

- 101 SPORT CALENDAR**
- 102 U.I.M. COMMISSIONER**
- 104 NON APPROVED EVENTS**
- 105 TITLED INTERNATIONAL EVENTS**
- 106 INTERNATIONAL CHAMPIONSHIPS**
- 107 WORLD CHAMPIONSHIP**
- 108 CONTINENTAL CHAMPIONSHIP**
- 111 RACING LICENCE**

GROUP 200

COMPETITION ORGANISATION

- 201 ORGANISING BODIES**
- 202 ADVANCE-PROGRAMME**
- 203 ENTRIES**
- 204 EVENT INSTRUCTIONS**
- 205 SAFETY RULES**
- 206 INSURANCE**
- 207 MEDICAL EXAMINATION**
- 208 DISTINCTIVE MARKS**
- 209 ENVIRONMENTAL CARE**

GROUP 300

SPORT RULES

300.01 EVENTS

300.02 DEFINITIONS

300.03 TIMING

300.04 COURSE

300.05 COMPETITION CANCELLED

300.06 COMPETITION POSTPONED

300.07 COMPETITION SHORTENED

300.08 COMPETITION STOPPED

300.09 DISTINCTIVE MARKS

300.10 INSURANCE

300.11 SAFETY EQUIPMENT

300.12 FIRST AID STATION

300.13 TECHNICAL PROTECTIONS

- 301 REGULARITY COMPETITIONS**
- 301.01 DECLARED SPEED COMPETITION**
- 303 PURSUIT COMPETITION**
- 304 PARALLEL SLALOM COMPETITION**
- 305 HISTORICAL BOATS COMPETITION**
- 306 RALLY COMPETITION**
- 307 PROMOTIONAL MEETINGS**
- 308 ELECTRIC AND SOLAR PLEASURE UNITS**
- 309 WORLD AND CONTINENTAL ENDURANCE CHAMPIONSHIPS**

GROUP 400

JURISDICTION

- 401 DEFINITIONS**
- 402 JUDICIAL BODIES**
- 403 PROTEST PROCEDURES**
- 404 HEARING OF THE PROTEST**
- 405 THE APPEALS PROCEDURE**
- 406 PENALTIES**
- 407 PRIORITY OF THESE RULES**
- 408 POWERBOAT PROTEST FORM**

GROUP 500

TECHNICAL RULES

500.01 CLASSIFICATION OF PLEASURE BOATS

501 REGULARITY COMPETITION

501.01 DECLARED SPEED COMPETITION

502 ENDURANCE COMPETITION

502.01 GENERAL

502.02 GROUP A

502.03 GROUP B

503 PURSUIT COMPETITION

504 PARALLEL SLALOM COMPETITION

505 HISTORICAL BOATS COMPETITION

GROUP 600

RECORDS AND PERFORMANCES

- 600 RECORDS AND PERFORMANCES**
- 601 CLASSES ELIGIBLE FOR RECORDS**
- 602 DEFINITION OF UNITS**
- 604 THE COURSE**
- 605 CALCULATION OF SPEED**
- 606 MARGIN REQUIRED**
- 607 MOTORS**
- 608 REQUEST TO ORGANISE**
- 609 OFFICIALS**
- 610 SPEED RECORDS**
- 611 DISTANCE RECORDS**
- 612 HOUR RECORDS**
- 613 WORLD COMPETITION RECORDS**
- 614 PERFORMANCE CERTIFICATES**

615 HOMOLOGATION OF RECORDS

**616 LONG DISTANCE OFFSHORE WORLD
SPEED RECORDS**

GROUP 700

CUPS AND TROPHIES

- 700 INTERNATIONAL CUPS**
- 701 PAVILLON D'OR CRUISE**
- 702 TROPHY MARIO AUGUSTA**
- 703 TROPHY SANZ PINAL**

GROUP 100

U.I.M. ADMINISTRATION

101 - SPORTS CALENDAR

101.01 - INSCRIPTION FEE

Registration of international events in the annual calendar of the U.I.M. is compulsory and subject to the payment of a fee, the amount of which is fixed annually by the General Assembly.

Payment of these fees must be made within three months after the fixing of the international calendar.

Permission to organise these meetings is only delivered by the U.I.M. upon payment of these fees.

No permission is to be granted to National Authorities in arrears of payment.

All requests to organise an international championship must be sent to the U.I.M. Secretariat before the time set by the deadline in the rules.

The organisation of an event cannot be granted if the title, the classes, the locality and the date of the event are not given.

When compiling the Sports Calendar due attention is to be paid to the reports of the U.I.M. Commissioner before granting the request to organise titled meetings.

The titled meetings have priority over all other meetings before the closing of the registrations for the sports calendar. However, when the sports calendar is finalised, all the motor boating meetings are on an equal footing.

In order to promote new series and classes, new hulls, motors or engines, new ideas regarding propulsion and new concepts of competition, the Council may, - at its sole discretion, provided that the

request to organise is accompanied by an amount equal to the double of the fee of an ordinary meeting, approve the organisation of international contests for such boats, motors or engines, type of propulsion and races of a kind not yet known to the U.I.M. provided such events do not clash with other U.I.M. titled meetings.

101.02 - OTHER EVENTS

It is forbidden to organise National championships for series and classes which are in the programme of a titled event taking place the same day.

The calendar of National or local events is not to be submitted to the U.I.M. The interested National Authorities take responsibility for their own calendar(s).

101.03 - DEADLINE DATES

1 March Forwarding of requests for international titled events

1 April Allotting of titled events to N.A.

September 10 Forwarding of dates and venues of titled events by the N.A.'s
Forwarding of request for ordinary events, complete with dates and venues

Calendar meeting

October Drawing up of the international calendar

November 15 Publishing of the international calendar

101.04 - LATE REQUEST, CHANGE OF DATE AND CANCELLATION

Titled events

Late request: A late request will be considered only if the title is still vacant or if the title or the event has been cancelled by the NATIONAL

AUTHORITY to whom it was granted or by the U.I.M. If the title is granted, the organiser will pay the normal inscription fee.

Late forwarding of date or venue : the inscription fee will be raised by 50%.

Change of date : The inscription fee will be raised by 25 %.

The U.I.M. give its assent to the new date. The request for a change of date must be received by the U.I.M. Secretariat not less than 90 days before the calendar date. The new date will be fixed not less than 60 days after the date on which the request was received.

At less than 90 days before the calendar date, the date cannot be changed. The event will be cancelled or run as an ordinary event, but no part of the inscription fee will be refunded.

Cancellation : If the title or the event is cancelled 90 days or more before the calendar date, 50 % of the inscription fee will be refunded.

In a later cancellation there will be no refunding. A cancelled title will be treated like a title not yet requested.

Non titled events

Late request : The inscription fee will be raised by 25%.

Change of date : The inscription fee will be raised by 25%.

Cancellation : If the event is cancelled 30 days or more before the calendar date, 50% of the inscription fee will be refunded. In later cancellations there will be no refunding.

102 - U.I.M. COMMISSIONER

102.01 - GENERAL

The presence of a U.I.M. Commissioner is required at any international titled meeting.

The function of Commissioner must be fulfilled most carefully.

As soon as he gets the Commissioner's agreement, the Administrative Secretary of the U.I.M. will send a letter to the National Authority and the organiser of the event informing them that the Commissioner has been designed for the events referred in the letter.

National Authority Authorities are requested to send yearly to the U.I.M. Secretariat a list of candidate Commissioners with the following data :

- Name, address, telephone and telefax ;
- languages spoken ;
- languages read and understood;
- the period of availability of the candidate ;
- knowledge in sports and technical matters ;

The National Authority must remind the organisers that the Commissioner must be accommodated in a good hotel for the whole duration of the event and as long as required by the accomplishment of his mission. The costs of such accommodation are borne by the organisers.

102.02 - FUNCTIONS

The U.I.M. Commissioner:

- Assists the local organiser and more particularly the Officer Of the Day (O.O.D.);
- Is a voting member of the international jury;
- Approves last minute amendments to the advance-programme;
- Supervises the signing of drivers' declarations;
- Takes part in important decision-making during the competition with the Officer Of the Day ;
- Takes part in urgent decision-making with the Officer Of the Day and the organising promoter, in matters related to the overall organisation;

The U.I.M. Commissioner is entitled to take action with the organising committee if safety conditions are not met. He will be entitled to request that the event is suspended if safety conditions are not satisfactory.

102.03 - AFTER THE EVENT

After the event, he sends his report to the U.I.M. Secretariat by using the official form, within ten days. The official results of the event must be enclosed with the report.

The reports from Commissioners are to be considered as confidential matter and cannot be used for aims other than U.I.M.'s, may not be communicated to the press or otherwise disclosed for any purpose whatsoever.

Should any Commissioner fail to obtain all necessary data and results for transmission to U.I.M., he will note it in his report and will urge the organisers to send all required documents by themselves to the Secretariat of U.I.M., as soon as possible.

A copy the Commissioner's report will be sent, as soon as possible, by the U.I.M. office to the President and all members of Pleasure Navigation Commission and to the NATIONAL AUTHORITY of the organiser.

102.04 - TRAVEL EXPENSES

The National Authority should also make it clear that travel expenses for the Commissioner, from his town of residence to the place where the meetings are taking place, calculated upon the basis of a return air ticket, economy class, are also borne by the organisers.

The Commissioner is to be accommodated in a good hotel for the duration of the event and as long as is necessary for the accomplishment of his mission at the expense of the organisers.

The expenses referred to above must be refunded to the Commissioner during his stay by the organisers or to the U.I.M. upon receipt of the relevant invoice.

104 - NON APPROVED EVENTS

Events not organised according to U.I.M. rules are not to be recognised and all officials and drivers who take any part in such races are suspended.

The same applies to motor boating events not approved by National Authorities.

Any driver having competed in a competition not approved by the National Authority of that Country or forbidden by the National Authority may be suspended for a period to be fixed by the National Authority.

In the case of an international meeting being organised by a non-affiliated club which has not asked permission to organise such a meeting, the National Authority must :

- Notify the organising club that the drivers can be suspended ;
- Notify the drivers that they will be suspended in their own Country if they take part in international competitions not approved by the National Authority; the latter may request U.I.M. to extend this suspension to other Countries ;

A National competition, or a competition by "invitation" in which a foreign driver is invited to take part must be entered on the U.I.M. Calendar otherwise it becomes an unauthorised event. Invited drivers entering such unauthorised events, may lose their international licences. Remember that drivers having international licences, are only allowed to enter a meeting outside their Country provided their National Authority gives them permission to do so.

105 - TITLED INTERNATIONAL EVENTS

There are five categories of titled event(s):

- World Championship ;
- World Cup ;
- Continental Championship ;
- Continental Cup ;
- Grand Prix ;

There can be one World Cup and Continental Cup event per class and per year, provided that a World or Continental Championship is not organised for the class in question.

There can be only one Grand Prix event per Country and per year, except when a World Championship series takes place in that Country

No meeting is allowed a title unless authorised to do so by the U.I.M. who draw up the annual list in the official calendar.

No organiser can run more than 2 titled events (e.g. WC or CC) at any one meeting.

The U.I.M. aims to protect titled meetings, the Council may refuse to sanction any meeting which takes place at the same time as a previously approved event. Such refusal is only notified at the specific request of the National Authority lodging a protest.

106 - INTERNATIONAL CHAMPIONSHIPS **WORLD AND CONTINENTAL CUPS**

106.01- GENERAL RULES

World Championships are open to all U.I.M. and American Power Boat Association (APBA) classes. There can be only one World Championship per class, per year.

The requests to organise these championship must be sent in due time.

The Pleasure Navigation Commission will allocate the Championships.

No exception is to be allowed to the preceding rule.

The U.I.M. delivers a "champion's certificate", following requests made by the champions to their National Authorities and transmitted by that National Authority to the U.I.M. Secretariat.

The cost of such certificate is fixed annually by the General Assembly and is to be paid by the National Authority to the U.I.M. when the request is made.

A list of champions is issued annually.

The U.I.M. rules and those of the National Authority are applicable for any point not foreseen by these rules.

No person shall be proclaimed:

- “a World Champion” unless at least five competitors from different Nations in two different continents have competed for the title;
- “a Continental or European Champion” unless at least three competitors from at least three different Nations have competed for the title.

A driver of non-European Nationality may be proclaimed European Champion provided he has had a European licence for more than two years. The same applies for other continents.

Immediately after the Championship, the National name of the Champion must be transmitted by fax to the U.I.M. secretariat.

106.02 - WATER REGISTRATION

Permission to organise an International Meeting is granted **ONLY** if the race is run on a course recognised by the National Authority and documented as follows :

A plan to scale (1:1250...1:2500) showing accurately:

- a) The depth within a 5M distance either side of racing line ;
- b) Position of ALL race buoys ;
- c) Position of finish line ;
- d) Position of jetty/beach (if applicable) or start line ;
- e) Direction of competition ;
- f) Other obstructions duly marked ;
- g) Any other buoys not applicable to the competition ;
- h) Position of control ;
- i) Entry to water.. crane/slipway ;
- j) Area for pits ;
- k) Spectator barriers etc. ;
- l) First aid facilities ;
- m) An **ALTERNATIVE** competition course (if possible) only for use under "Force Majeure" conditions ;
- n) Altitude of venues ;

107 - WORLD CHAMPIONSHIP

World Championships, allocated by the U.I.M. Pleasure Navigation Commission are raced under the following rules as the Continental Championships.

108 - CONTINENTAL CHAMPIONSHIPS

108.01 - NUMBER OF CHAMPIONSHIPS

There can be one championship per year, per class, per continent (except where there is a series) only in classes approved by the Pleasure Navigation commission at the previous General Assembly of the U.I.M.

A championship for any class can only be organised when a minimum of three participate.

108.02 - CONTINENTAL CHAMPIONSHIP EVENTS IN PLEASURE NAVIGATION

Continental Championships are open to all U.I.M. Pleasure Navigation classes.

There can be only one Continental Championship per class, per year.

The Pleasure Navigation Commission will allocate the championships subject to any contract between the U.I.M. and any promoter.

International classification is to apply for the teams of the National Authorities whose drivers take part in the championship. For the classification of the National Authority teams the result of those three drivers of each team having the highest number of points will be taken into consideration.

If fewer drivers are entered by National Authority teams or if only two or less drivers are classified, the points of that drivers are considered as those of the whole team. In case of a dead-heat, the classification will be made according to the best result of the team in question.

108.04 - PARTICIPATION

Drivers must be able to show their international licence, measurement certificate, and the homologation sheet for the boat and the motor(s).

The driver's entry is to be sent through his National Authority to the organising National Authority and to the organiser of the event at least 21 days before the beginning of the championship.

The composition of the National teams may be altered up to one hour before the start of the first heat.

108.05 - SELECTION HEATS

If the number of drivers entered is too large for all of them to start together the organising committee has to split the group of drivers into several groups.

Care should be taken to ensure that the drivers of each Nationality are evenly split into separate groups as far as possible. The committee will have each group run selection heats.

When heats are run, the classification is done as follows :

1 - 400 pts	11 - 22 pts
2 - 300 pts	12 - 17 pts
3 - 225 pts	13 - 13 pts
4 - 169 pts	14 - 9 pts
5 - 127 pts	15 - 7 pts
6 - 95 pts	16 - 5 pts
7 - 71 pts	17 - 4 pts
8 - 53 pts	18 - 3 pts
9 - 40 pts	19 - 2 pts
10 - 30 pts	20 - 1 pts

In case of a dead-heat, classification is decided by the best heat run by the drivers in question.

All classes must race separately even if they belong to the same series.

108.06 - GENERAL SPORT RULES

A separate classification is made for each class.

The champion receives a trophy given by the organising club.

Additional prizes may be awarded.

After the competition the boats of the first three classified drivers are inspected by the official measurer.

The title of champion is only granted after examination of the aforesaid boats.

111 - RACING LICENCES

111.01 - COMPULSORY LICENCE

No one can take part in a U.I.M. event, establish or improve on a U.I.M. record in international class boats, without being in possession of an international licence from their National Authority.

111.02 - REGISTRATION

The National Authority establishes each year a list of people to whom licences have been delivered and this is sent to the U.I.M. Secretariat.

The list must include:

- The name and surname, the address, the class(es) and whether National or International Licence holder.

These lists may be sent to other National Authorities.

Licence numbers are given annually by the National Authority to the owners and drivers entered on the list.

Any person wishing to obtain a licence must apply to the National Authority through their club.

A National Authority may refuse to give a licence without having to state the reason. Notification of this is sent to all affiliated clubs and to the U.I.M.

The licence is granted by the National Authority.

Any National Authority has the right to deliver licences to :

- Its own nationals,
- Nationals of another Country affiliated to the U.I.M. but with the agreement of the National Authority of that nation.

With the approval of the U.I.M. a National Authority may also deliver licences to persons belonging to a Country which is not yet affiliated to the Union.

Drivers are only permitted to hold licences from one National Authority.

111.03 - VALIDITY OF LICENCES

Licences are valid from the 1st of January until the 31st of December each year, unless local circumstances require other date. In any case validity should always be shown on the licence itself, and must be for one year.

A licence is valid in any Country affiliated to the U.I.M. and entitles the holder to enter or drive in a boat in all competitions organised in any affiliated Country, provided they comply with what is stated in the U.I.M. general rules.

When entering any event held under special rules, the licence holder must abide by the rules in question.

111.04 - COST OF LICENCE

The cost of the yearly licence is to be fixed by the National Authority.

111.05 - EXAMINATION OF LICENCES

At any meeting, the holder must produce his licence at the request of any official of the meeting.

111.06 - CANCELLATION OF LICENCE

Anybody who takes part in an unauthorised meeting may lose their licence.

However, if the unauthorised meeting is held in waters of a Country other than the one which has delivered the licence, both National Authorities must agree as to the duration of the suspension.

The U.I.M. decides finally in case of dispute.

111.07 - PSEUDONYM

The use of a pseudonym must be requested from the National Authority and the National Authority marks the pseudonym on the licence.

A licence holder, as long as he is listed under a pseudonym, may not take part in any sports event under any other Name.

Changing a pseudonym is subject to the same formalities as the adoption of it.

Anyone who has chosen a pseudonym may not return to his actual name until the National Authority has agreed and has delivered a new licence.

GROUP 200

COMPETITION ORGANISATION

200.01 – GENERALITIES

The following bodies have the right to organise any Pleasure Navigation motor-boating event: National Authorities and their Affiliated Clubs, Corresponding members ***of the U.I.M., U.I.M. contracted promoters (with the agreement of the National Authority).***

All events have to be sanctioned by the National Authority.

Affiliation to the National Authority implies their acceptance of the National rules and those of the U.I.M.

A club, member or driver who is suspended, is not allowed to take part in any sporting event in his Country or in any other Country neither as a driver nor as an official, as long as the suspension lasts.

A member or a driver who takes part in any event in a foreign Country affiliated to the U.I.M. is, ipso facto, under the jurisdiction of the National Authority of that Country.

A member or a driver belonging to a Country which is not affiliated, are to be considered as nationals of the Country where they take part in an event.

All clubs, organisers, officials and drivers are obliged to know the general rules of the U.I.M. as well as the rules related to competitions or series (local, national or international).

Two updated copies of the present rule-book must be available at every race.

All events organised by the National Authorities or their clubs are subject to the U.I.M. rules. All rules and programmes must mention this.

The general rules and the rules related to records are applicable to all the international series.

200.02 - INTERPRETATION OF THE RULES

In all cases not foreseen by the International rules, the National Authority, a race committee, race jury or an appeal board, is to judge bearing in mind the intention of the disputed rules.

The English text is the official one. In case of disagreement on the interpretation of these rules, the English text prevails.

201 - ORGANISING BODIES

201.01 - COMPETITION COMMITTEE

All competitions are under control of the organising body who is to nominate:

- An Organising Committee which organises the general and administrative aspects of the meeting;
- A Competition Committee which organises and supervises the actual competition and
- Decides whether competitors are eligible or not. The Competition Committee is composed as follows : the Officer Of the Day, the Deputy Officer Of the day (D.O.O.D.), the Medical Officer, the event Secretary, the Technical Officers, Buoy & Course Marshals, Boat Park Marshal, Rescue and Starter.

- A Jury,

The jury shall consist of two nominees of the organising body and one nominee from each nation participating, with a minimum of three nationalities.

The jury decides on any dispute submitted to them. The names of the members are to be printed in the official programme.

The decisions of the Competition Committee and the Jury must be based upon the U.I.M. rules, but as no rules can be devised capable of covering every case, the Committee should discourage all attempts to win by means other than fair racing, superior speed and skill.

The Organising Committee appoints a Committee that verifies all the documents required i.e. Measurement Certificates, Driving Licence, etc.

This Committee also verifies whether or not the boats and motors conform to the rules.

No officer, nor member of the Jury, nor member of a National or International Committee or Commission, is allowed to intervene in the judgement of a problem in which he is involved himself or is an interested party.

The following are the Competition Officials :

1. The Officer Of the Day;
2. The Deputy Officer Of the day;
3. The Medical Officer;
4. The Secretary of the Event Committee;
5. The Technical Officers;
6. The Course and Buoy Marshals;
7. The Timekeepers;
8. The Lap Scorers;
9. The members of the Jury.

201.02 - INTERNATIONAL LISTS

- Lists of International Officers Of the Day (I.O.O.D.) and of International Jury Chairman (I.J.C.) are deposited with the Secretariat of the U.I.M.
- Every year, each National Authority has to transmit to the Secretariat its new list of I.O.O.D. and I.J.C. or the confirmation of the previous year's one not later than September 30th.
- Each National Authority may propose for enrolment in the list of International Officer Of the Day and I.J.C. only those Officers that have been regularly qualified by the said National Authority.
- Requirements for the enrolment of a candidate :
 - a) a personal record (containing personal data, address, telephone numbers) ;

- b) knowledge of French or English ;
- c) not less than a five year experience in the position applied for ;
- Only people enrolled in the U.I.M. lists are allowed to be appointed to the positions mentioned above and therefore to be appointed by their National Authority to perform such duties in international events valid for U.I.M. titles.

201.03 - DUTIES OF THE COMPETITION OFFICIALS

The Officials shall perform only the duties with which they are entrusted.

201.04 - THE OFFICER OF THE DAY

The Officer Of the Day must supervise the organisation of the competitions on behalf of the Organising Club. He must maintain order on the course, ascertain that all services work properly, ensure that all correspond to the rules and observe the Racing Rules according to the Programme, gather written reports from the Officers and Timekeepers and any other documents that will enable the results to be compiled. He cannot be a member of the Jury.

201.05 - THE DEPUTY OFFICER OF THE DAY (SAFETY OFFICER)

The Deputy Officer Of the day co-ordinates (together with the Officer Of the day) the safety services on shore (fire) as well as on the water (boats, people on board, material). When necessary he deputises for the Officer Of the day.

201.06 - THE MEDICAL OFFICER

The Medical Officer is responsible for the medical organisation in the competition area.

In agreement with the Officer Of the Day and his deputy, he controls the relevant emergency responses.

201.07 - THE EVENT SECRETARY

The Event Secretary is responsible on behalf of the Officer Of the Day, for the practical side of the meeting and organisation and must satisfy himself that the various officials know their respective duties and that they have all the necessary documents and equipment. He must collect all documents.

201.08 - THE TECHNICAL OFFICERS

The Technical Officers ensure that all drivers are in possession of homologation sheets for the engines or hulls used.

They check and control safety devices for boats and drivers as provided for in the rules.

Special attention must be paid to the automatic engine shut-off and that the competition number conform with the rules.

They verify the synchronisation of the clock and the camera if used.

After the competition, they must do all necessary checks ; all infringements must be communicated in writing to the O.O.D.

201.09 - THE BUOY AND COURSE MARSHALLS

They must ensure that the drivers conform to the competition rules. At the end of the meeting, they draw up and sign a statement certifying that the drivers conduct during the competition and at the turn buoys was according to the rules.

This statement is handed to the Officer Of the Day who will enforce the rules when needed. Buoy officials are allowed to be in boats. If they cannot be in boats inside the circuit, there will be two of them : one inside and one outside of each turn.

201.10 - THE TIMEKEEPERS, LAP SCORERS AND STARTER

The Timekeepers will :

- Calculate on completion the elapsed time of each driver;
- Establish the finishing order after having made all necessary calculations and establish the average speeds;
- For international events there must be at least two timekeepers. Their results must only be handed to the Officer Of the Day;
- The Starter signals the start when ordered to do so by the Officer Of the Day.

202 - ADVANCE-PROGRAMME

202.01 - FORWARDING DATE

45 days before any international event the Organising Committee must forward at least one Advance Programme to the U.I.M. Secretariat, to the National Authorities and to the invited Clubs and drivers. The advance-program will also be forwarded to the commissioners for recommendations.

Any such recommendation should be accomplished at least two weeks prior to the event.

The U.I.M. representative will ensure on-site that the circuit matches the plan.

Should rule 202.01 not be complied with, the Council has the right to request the Pleasure Navigation Commission to apply a suitable penalty against international events during the following year.

Should the Advance Programme fail to be forwarded within the terms provided for, the championship will be granted to another organiser from the same National Authority.

Should the 45 days delay not be complied with, the calendar fee will be double.

The above advance-programme must be written in the language of the organising Country and in English and must contain the following information :

1. The name and the address of the organiser;
2. The series and classes of boats catered for;
3. The competition or competitions which will be run;
4. The venue and the date of the competitions;
5. The course of each competition and plan drawn to scale showing all details as per rule "106.02 WATER REGISTRATION".
6. The competitions are held under the U.I.M. rules;
7. The special conditions which in accordance with the U.I.M. rules may be added or modified;
8. Time limit and place to which entries must be sent, and whether written, by fax or by hand;
9. The amount of the entrance fee, except for Continental or World Championships for which there is no fee;
10. All information regarding the prizes for each race;
11. Where and when the entry forms, programmes and competition instructions can be obtained from the Organising Committee;
12. Transport facilities and concessions;
13. Terms and conditions of a mandatory insurance for personal and material damage to third parties and competitors, with details about:
 - The minimum limit of coverage's required;
 - Kind of risks to be covered by the insurance;
 - Whether insurance's from other National Authorities are authorised and if they are, in which language they must be drawn up;
 - The amount of insurance fees to be paid on the spot (80 Euro maximum).
14. Minimum age of competitors;

15. Times for practice per class and the interval between the heats;
16. Full instructions regarding noise regulations;
17. Date and times for scrutineering;
18. Should no publicity be allowed during a competition, this is to be mentioned in the Advance Programme;
19. The time limits for late starters and finishers;
20. No modifications are to be made to any particular rule after the Advance Programme is posted except if decided by the Event Committee and approved by the Jury and for cases of force majeure only. The modifications must be communicated, by written means, to the officials and participants, 1 hour before the first start at the latest;
21. Place and time of the drivers' meeting.

203 - ENTRIES

203.01 - RATIFICATION

The National Authorities must ratify in writing any entry for drivers wishing to compete abroad.

Direct correspondence between the Organising Committee and foreign drivers is permitted but no entry is to be accepted unless approved by the driver's National Authority.

For international Championship, and cup events the correspondence is held between the National Authorities.

The Organising Committee will keep blank entry forms at the disposal of the drivers.

An entry is not valid unless it reaches the organising committee within the time prescribed in the advance programme.

On submitting entries, competitors shall present :

- National Authority membership ;
- A medical certificate qualifying them to participate or equivalent documents issued by their National Authority ;
- Nautical driving licence which conforms with the documented type of craft and the standard of event.

203.02 - ADMISSION

The Organising Committee has the right to decide whether the entrant is acceptable.

No National Authority, organising an international competition has the right to refuse the entry of a foreign driver duly commissioned by his own National Authority provided they conform to U.I.M. rules.

The closing date for inscriptions may be set 14 days before the event.

203.03 - ENTRY FEE

Each Organising Committee fixes the amount of entry fee for the competition.

Entry is valid only after payment of the entry fee.

The National Authorities have the right to establish a rule whereby higher fees are imposed on late entries.

203.04 - MINIMUM AGE AND IDENTITY

The minimum age for all drivers racing international races is 16 years.

Each National Authority has the right to fix a minimum age for the drivers allowed to race on its waters, such age restriction is to be shown in the advance programme.

The driver may only race under his name or the pseudonym written on the licence.

203.05 - NATIONALITY

The nationality of the driver only must be taken into consideration in the classification.

203.06 - PUBLICITY ON THE BOATS

Advertisements may be freely applied on the entire surface of the boat, but there must be a clear space of no less than 0,15 meter around the racing number.

No crew can be compelled to carry any advertising whatsoever on their person, clothes or upon the boat that is competing and their refusal to do so cannot in any way be an obstacle to their participation in any competition.

Penalties may be applied to any driver who, in the course of any sporting event, has made any advertisement contrary to the truth or distorting the facts.

Political publicity or immoral advertisements are not allowed.

Any National Authority has the right to fix its own rules regarding publicity for its own boats and courses.

Should no publicity be admitted for an event, this has to be announced in the Advance Programme.

204 - EVENT INSTRUCTIONS

The Organising Committee must clearly state the following points in their competition instructions :

1. Places and dates of the race;
2. Layout of course drawn to scale :
 - Length of the course;
 - Direction of the course;
 - Starting line and its marks;

- Finishing line and its marks;
 - Full details of buoys;
 - Full details of obstacles.
3. Starting time of each competition;
 4. Signals for each competition;
 5. Signals of postponement, cancellation, re-start and shortening of the competition.
 6. Time when the control closes for each competition;
 7. Full particulars of prizes for each competition;
 8. Time limit and place for handing over the protests;
 9. Time and place of the distribution of prizes;
 10. Appeal procedure;
 11. Composition of the Committee, of the Jury and of the Technical Commission;
 12. List of boats entered for each competition and if possible, names and Clubs of the drivers;
 13. Place and time at which written instructions will be issued. Where and when the official time is to be communicated and place and time of driver's meeting. A brief driver's meeting must be held before practice takes place;
 14. Only drivers whose entries have been accepted by the organising committee have their names printed in the programme;

205 - SAFETY RULES

The safety rules of any competent National or International body in force for the area of the event shall be complied with.

The Organising Committee shall comply with National Authorities' regulations.

Additional safety equipments to be carried on board of the competing boats and not mentioned or set out in these rules may be specifically required by the Organising Committee as it feels to be necessary .

Such equipment shall be specified in the Advance Programme.

205.04 - NAVIGATION RULES

Being the competitions organised in free waters open to public navigation, all Competitors shall always conform with:

- The International Rules for Prevention of Collision at Sea. (COLREGS)
- The rules of any competent authority governing the use of pleasure craft in the hosting Country.

These provisions, if not known by foreign participants, must be communicated to them in writing. English will be the official language for any such notices.

206 - INSURANCE

All boats must be properly insured by themselves against any claims for damage to other boats and persons.

If in the Country where the competition takes place a special competition insurance is required by law, it shall be underlined in the Advance Programme, and the relevant fee recovered from the competitor.

The minimum insurance coverage required from the competitor must be specified in the Advance Programme.

207 - MEDICAL EXAMINATION

The Medical Officer who is present at the motor boating event has the right to require any driver entered to submit to a psycho-physical

examination and /or an alcohol or drug test at any time before during or within 24 hours of the termination of an event.

The result of such examination will be immediately communicated to the Race Committee, who basing themselves on the report of the Medical Officer, may exclude the driver from the competition.

208 - DISTINCTIVE MARKS

208 01 - NATIONALITY

The national flag of the craft, painted in a rectangle 25 cm x 40 cm on the deck, the superstructure, or the vertical part of the sides, it must be visible on both sides of the hull.

208.02 - RACE NUMBERS

A competition number is allocated to every driver by his National Authority.

Should two drivers of an international competition have the same number :

- The driver of the hosting Country will change his number according to the organiser's instructions;
- If none of the drivers is from the hosting Country, the driver which entered last will change his number according to the organiser's instructions;

209 - ENVIRONMENTAL CARE IN RACE AREAS

209.01 - NOISE LEVEL

Every driver is responsible for demonstrating that his engine(s) does not exceed noise levels established.

209.02 - EXHAUST EMISSIONS

To reduce emissions as much as possible, all drivers are recommended to use lead free petrol and biodegradable lubrication oil.

209.03 - RE-FUELLING

Every care must be taken not to spill fuel or oil.

An absorbent carpet to avoid any spillage on to the ground must be used.

Suitable containers shall be provided in the pits area, to properly collect waste such as cans, containers, etc.

One person must be appointed by the organiser to be responsible for proper waste pick up and removal after the race, according to the procedures set by the Local Authorities .

GROUP 300 - SPORT RULES

300.01 - EVENTS

Pleasure Navigation events are created specifically to foster development of motor boating .

They may be divided in two main groups.

Pleasure Navigation activities :

- Competitions ;
- Promotional events ;

Competitions are run for pleasure craft where each crew starts individually following a course, a route and a time previously established. Different rules are set for the following specialities :

- Regularity competitions ;
- Endurance competitions;
- U.I.M. Trophies;
- Parallel slalom competition;
- American pursuit competition;
- Rally competition;
- Championships.

Promotional meetings are all the initiatives concerning pleasure craft and that can be summarised as follows :

- Rallies;
- Meetings;
- Treasure-hunt;
- Pleasure Navigation training;
- Navigation demonstration;
- Sea skill tests;
- Gymkhana : The rules governing these events are usually prepared each time by the organisers;

The rules governing these events are usually prepared each time by the organisers.

SOLAR or other alternative energies powered boats with in-water propulsion;

Radio Controlled Competitions of Radio Controlled powerboats are regulated by specific technical and sport rulebook;

Special and/or Experimental boats (Units not included in previous points).

Type of events

- Regularity : declared speed competition - pursuit - slalom - social rally - competition rally;
- Endurance : - speed - time declared - distance declared - index of performance;
- Promotion : - social rally - competition rally - meetings - skill tests;
- U.I.M. Trophies Pavillon d'or ;
- Historical boats : age of boats (min. 25 years) - presentation:- history - concours d'elegance - restoration;
- Solar or alternative energy powered boats with in-water propulsion;
- Radio-controlled : as per U.I.M. international rulebook issued by the R.C. sub-commission;
- Special Experimental boats.

300.02 - DEFINITIONS

Declared speed competition : the speed must be declared by the competitors. The average speed expressed in km/h (according to EC regulations) on the basis of which they will cover the course. Fractions of km/h are not permitted. Visible and secret controls will be done.

Pursuit : course definition - position of buoys.

Slalom : defined start and finish (length of course) - buoy lay-out diagram - distance between buoys - parallax referred to the course axis.

Competition-rally

- route to the meeting;
- special trials - meeting (theme event);
- seamanship test : navigation skill test, safety test, boat handling, docking etc.

300.03 - TIMING

In all international competitions and National championships, the timing is carried out by officially appointed timekeepers using certified timing devices graduated to one tenth of a second.

The devices must have a split second system, one part of which must be capable of being stopped and restarted, or any electronic of the same or better accuracy.

300.04 - COURSE

All courses may be measured on the spot by official surveyors or may also be measured on an official map or chart of not less than 1:25,000, depending on the type of competition. If the length of the course cannot be certified, the marks having drifted or for any other reason, the speed is not to be declared in the results. The organisers must ensure that the position of the buoys does not present any danger to the drivers. It is recommended that the pits be sited where possible away from the course on the outside.

Competition control :

- Competition control should be situated so that the circuit can be easily seen and controlled ;
- The competition should be observed by at least two persons :
 - the Officer Of the Day; or his assistant ;
 - one or two Commissioners or one or two members of the Jury ;

- The Officer Of the Day is the general observer, being in radio or telephone contact with the Deputy Officer Of the Day, the Safety Officer and the rescue team ;
- The other two supervisors share the circuit in observation ;
- The competition can only be stopped by order of the Officer Of the Day.

Marks of the course

- The Marks of the Course are any objects (boat, buoy...) that are specifically designated as such in the instructions.
- The objects that are not designated specifically as such are to be considered as obstacles.
- All floating marks (buoys) must carry an orange identification of 70x70 cm (28x28 ins.) except the mark on the starting/finishing line which carries a black and white chequered identification or a white and black striped identification.
- The safety zone (100 m) is defined by at least one buoy which carries a white and red chequered identification or a white and red striped identification.
- The buoys are to be made of rubber or similar material.

An Official is posted near the turning marks. He must observe the turning of the marks and report to the Race Committee. Should any mark be removed from its proper position either by accident or otherwise, the Officer Of the day shall, if possible, have it replaced. Should it be impossible to replace the mark in time for the boats to round it, the Officer Of the day shall decide whether the race is to be restarted or not.

Signals from the shore :

- Series call letter :

Signals are passed by means of "letters". These signals are rigid boards measuring 60 cm x 80cm painted on both sides with black letters on a white background :

Cancelled = N (no race);

Postponed = R (Retarded);

Shortened = D (Diminished);

Stopped = S (Stopped).

■ Flag signals:

The flags used for signalling have the following meaning :

RED : to stop the competition and mark serious danger on the circuit ;

YELLOW : danger on the circuit ;

BLACK : the driver to whom the flag is shown must stop ; this flag must be accompanied by the number of the boat which is to be stopped ;

WHITE AND BLACK : Chequered :finish ;

WHITE AND RED : Chequered : course mark showing the safety zone ;

ORANGE : course turning marks.

The distance between the starting line and the first turning buoy must be at least 500 metres.

300.05 - COMPETITION CANCELLED

The Committee has the power to cancel any competition should unfavourable weather or other serious circumstances render such action necessary. The letter "N" hoisted over the class or competition signal, indicates that such competition has been cancelled.

300.06 - COMPETITION POSTPONED

The Committee has the power to postpone a competition, in which case the letter "R" and the class letter are hoisted. Competitions thus postponed are subject to written notice on the notice board. The Officer Of the day may postpone the start, even after the first signals have been given, but only in cases of force majeure or if an error in the starting signals has been made.

In both cases, signalling is to be recommenced. When the need for postponement has passed, the Committee signals the start according to the instructions contained in the programme. New entries are not to be accepted for postponed races. Only drivers who duly entered and were present when the competition was postponed are admitted.

A race or heat must in no case be postponed more than 24 hours. If it is impossible to compete within this time, the other heats held will be used as a basis for classification.

300.07 - COMPETITION SHORTENED

A shortened competition is one run over a course which has been shortened by the Committee before the start. The decision with instructions about the shortened course must be handed in writing to the drivers and the timekeepers. The shortening is shown by the letter "D" (Diminution) being hoisted with the class letter.

300.08 - COMPETITION STOPPED

A stopped competition is one which has been interrupted by the Officer Of the day after the start. Stopping the competition is decided by the Officer Of the day for reasons of which he is the sole judge. A competition must be stopped when anybody is in the water as a result of an accident (blow over, roll, submarining, collision, etc.) as the continuation of the competition constitutes a danger to those involved. Restarts are given as soon as the course is cleared. A boat disqualified during a stopped competition is not allowed to restart. No new entry is accepted for a restarted competition/heat. Any penalty incurred in a competition which has been stopped is taken forward into any restart of that race.

300.09 - DISTINCTIVE MARKS

Distinctive marks and the competition number must remain visible during the whole race.

The absence or loss of the competition number or part of that number leads to the disqualification of the boat.

300.10 - INSURANCE

Evidence of the insurance coverage shall be produced before the start.

300.11 - SAFETY EQUIPMENT

Every competing boat shall have on board during the whole duration of the event (including practice) all the following:

- The efficient safety equipment required by the Country whose flag is flown by the craft;
- Any safety equipment prescribed by these rules or the competition rules;
- During race and practice each person aboard a boat must wear a life jacket duly homologated and coloured orange;
- If required by the competition rules, any person aboard any boat taking part in competitions must wear an adequate helmet of orange colour during the competition and during the practice runs and also after stopping on the course (e.g. in case of technical failure). If not otherwise specified, such helmet shall be of the type prescribed by the National Authority who have issued the driver's licence of the wearer. The efficiency of the helmet is the sole responsibility of the wearer.

300.12 - FIRST AID STATION

A first aid station with qualified medical staff must be available in the vicinity of the competition area.

The Organising Committee must alert it before the competition and a procedure for urgent calls must be agreed between the race Medical Officer and the coordinator of the First Aid Station.

300.13 - TECHNICAL PROTECTIONS

- Pleasure craft participating in competitions shall be equipped with a security cut-off device except for:
craft competing in regularity competitions;
craft driven solely from the cabin.

- At any stage of the event it is forbidden to start the engine of a boat while its propeller is out of the water.

301 - REGULARITY COMPETITIONS

301.01 - DECLARED SPEED COMPETITIONS

1. *The speed must be declared by the competitors, after the first training for the announcement at the first driver briefing following the first training. The average speed expressed in km/h (according to EC regulations) on the basis of which they will cover the course. Fractions of km/h are not permitted.*
2. *Such speed shall be constantly kept from the start in the termination of the competition, passing the visible and secret controls set along the course.*
3. *All designated waypoints shall be passed, or rounded, within a distance equivalent to one length overall of the competitor boat, and a straight seaman-like course should be followed at all times.*

Every boat shall carry a crew made up of a driver and a navigator:

- a) *other members of the crew can be carried up to the number specified in the homologation documents.*
- b) *if the boat is not homologated, in addition the driver and the navigator, it may carry additional crew as follows:*

Overall length	Additional Crew
<i>from 2 to 3,5 m.</i>	<i>none</i>
<i>from 3,5 to 4,5 m.</i>	<i>none</i>
<i>from 4,51 to 5 m.</i>	<i>one</i>
<i>over 5 m.</i>	<i>three</i>

All people on board must be members of a National Authority.

4. *In accordance with local Authorities regulations, the Organising Committee can provide for speed limits for certain*

sections of the course or establish neutral sections. This must be clearly specified in the competition rules and such sections must be shown on the nautical chart of the course.

5. The Organising Committee fixes speed values.

The speed values must be included in the advance-programme.

6. Irrespective of whether the competition course is in confined river waters or the open sea, the International Regulation for the Prevention of Collision at Sea (COE, REGS) shall apply, unless local by-laws are in force. In the latter case these prevail.

7. Start

Starts are made according to the speed values declared by the competitors, beginning from the lowest speed declared.

Start must be given with at least one minute interval between competitions.

8. Course

The course will not be shorter than 50 km. for additional competitions.

The course will not be shorter than 100 km. for W.C and C.C.

If this overall distance is achieved by a number of laps, no lap will be less than 8 km.

9. Visible controls

At least 2 (two) controls for national competitions and 3 (three) controls for W.C. and C.C. shall be placed on the course, other than the starting line. The control is ashore with a clear reference in the water (orange Buoy) placed at right angles to the track.

The visible control can also be a pre-fixed point on the course, whose coordinates are specified; alternatively a boat may be

on the pre-fixed point acting as the Control with coordinates ashore. The boat shall carry a Timekeeper and a Buoy Marshall.

10. Secret controls

At least 1 (one) secret control for national competitions and 2 (two) secret controls for W.C. and C.C. shall be placed on the course. It must be sited where it is possible to record the competitor's passage having coordinates at right angles to the track.

11. Competition track

On submitting an entry, the competitor will receive a nautical chart relating to the area of the competition, scale of at least 100,000 where the course is shown.

This chart must show the minimum distance in km. between the starting line and the first visible control point; between this control and the following one and so on to the finishing line.

The sum of such distances must be equal to the total length of the course.

12. Timing and passage recording

Timing and passage recording at all the control points is carried out by the Official Timekeepers.

Times are recorded to the nearest tenth of second as the bow of the boat breaks the control line.

This is compared with the theoretical passing time consequent to the declared speed, causing a positive or negative difference (expressed in seconds and tenths of second).

13. Race results

a) Results are to be expressed in terms of percentage error against the predicted speed.

The winner will be the boat with the lowest percent error.

b) Before calculating results, possible protest or communications must be examined for which decisions

must be taken by the Officer of the Day in conformity with the Rules.

- c) Any protest must be submitted in writing to the Officer of the Day, or his nominated Deputy within one hour of crossing the finishing line.***
- d) Timekeepers makes the sum of all the differences of time recorded at the visible controls. The resulting sum to be expressed in seconds and lenght of second.***
- e) The differences of times recorded at the secret controls will be calculated in a similar manner.***
- f) The total time differences resulting from the visible controls sums, plus those of secret controls, are then used to calculate the error against the theoretical course time related to the predicted speeds.***
- g) in the unlikely event of a tie, the slower craft shall be declared the winner having spent the longer time on the water.***
- h) In case of a tie the following progressive parameters shall be considered:***
 - The slower boat shall be declared the winner***
 - Time gap in excess (late passage) at the visible controls***
 - Time gap in excess (late passage) at the secret controls***

13. Penalties

- The competitor who clearly modifies his speed in the proximity of the visible controls by zigzagging or stopping, will be penalised with 10" equal to 10 penalties.***
- The competitor who, next to the finishing line, clearly modifies his speed by zigzagging or stopping will be penalised with 10" equal to 10 penalties.***

14. Disqualification

Reversing entails disqualification.

303 - AMERICAN PURSUIT COMPETITION

This event is open only to pleasure craft duly homologated.

303.01 - RACE

- a) The race is run with boats of different categories confronting themselves on a circuit delimited by buoys.
- b) The competitors race against the time two by two, and each of them starting from the middle of the opposite straight of the course determine the pursuit formula.
- c) The winner is the competitor who, among all categories, gets the best score built on times to which a compensation coefficient is applied.
- d) Each participating boat must have on board the driver only.
- e) Overtaking is allowed only from the outside and on the leg of the longest straight course.
- f) Overtaking at turning buoys is forbidden.
- g) The reached competitor must facilitate the overtaking.

303.02 - RACE COURSE

The course is rectangular, with a total perimeter of 700 m., two sides of 50 m. and two sides of 300 m., marked at the four angles by buoys. In the middle of each of the two straight courses of 300 m. two other buoys are positioned to determine the starting and the finishing lines.

The starting and the finishing lines must be positioned so that correct timekeeping by the timekeepers is possible.

The race is run anticlockwise.

303.03 - CATEGORIES AND CLASSES

To pursuit competitions the following pleasure navigation categories

may participate, each of them divided into classes according to their engine capacities :

- Inflatable with outboard - Inflatable with out/inboard;
- Boats with outboard;
- Boats with outboard and inboard;

303.04 - COMPENSATION COEFFICIENT

For the determination of a single final classification, a compensation coefficient is attributed to each class, as indicated in the scheme attached.

303.05 - RACEFORMAT

The starting order of the competitors is communicated by the O.O.D. before each heat.

Start is given by an acoustic signal.

303.06 - HEATS

- a) The race is run in three heats:

First qualification heat;

Second heat;

Third heat;

- b) First qualification heat – 3 laps

Each competitor, individually, makes three laps of the course. For his qualification to the second heat the best time of the three laps is taken into consideration.

- c) Second heat – 5 laps

Considering all the times recorded in the first qualification heat, irrespective of the categories, a progressive classification is made starting from the lowest time.

On the basis this classification, the two by two coupling for entering the second heat is made. Therefore the first two by two result to be consisting of the first and second time, the second two by two by the third and fourth time and so on.

If the number of competitors is odd the last will race alone and makes his own heat.

d) Third heat – 5 laps

Considering all the times recorded in the second heat, again the time classification is made and also the new two by two is determined. The first two by two is made by the first and second time, the second two by the third and fourth time and so on. If the number of competitors is odd the last will race alone and makes his own heat.

303.07 - STARTING ORDER

The starting order of the two by two, in the first heat is determined by ballot, while in the second and third it is determined by the highest time.

303.08 - FINATIONAL AUTHORITYL CLASSIFICATION

For each competitor, the best lap time on the first heat and the total time of the second and third heat are added together. This total time, expressed in seconds, is then multiplied by the pertinent compensation coefficient ; this calculated time will determine the final classification.

303.09 - DISQUALIFICATION

Failing to respect rule “303.01.e” determine the disqualification of the competitor.

303.10 - PURSUIT RACES - COMPENSATION COEFFICIENT SCHEME

Compensation Formula: $\text{Time (expressed in seconds)} \times \text{Coeff. Kt} = \text{Score}$

BOATS WITH OUTBOARD

4 Stroke	2 stroke	Class	Coeff. Kt
up to 550cc	up to 550cc	A	0.93
551cc - 1,000cc	551cc - 750cc	B	1.18
	751cc - 1,100cc	C	1.22
	1,101cc - 1,500cc	D	1.28
	1,501cc - 2,000cc	E	1.37
	2,001cc - 2,600cc	F	1.41
	2,601cc - 3,000cc	G	1.46
	3,001cc - 4,000cc	H	1.50

INFLATABLE WITH OUTBOARD

4 Stroke	2 stroke	Class	Coeff. Kt
up to 550cc	up to 550cc	A	1.00
551cc - 1,000cc	551cc - 750cc	B	1.24
	751cc - 1,100cc	C	1.29
	1,101cc - 1,500cc	D	1.36
	1,501cc - 2,000cc	E	1.47
	2,001cc - 2,600cc	F	1.51
	2,601cc - 3,000cc	G	1.56
	3,001cc - 4,000cc	H	1.58

BOATS WITH I.O.B. or I.B.

4 Stroke	2 stroke	Class	Coeff. Kt
	up to 1,350cc	A	1.26
	1,351cc - 5,700cc	B	1.38
	5,701cc - 7,450cc	C	1.47
	7,451cc - 8,200cc	D	1.55
	8,201cc - 10,000cc	E	1.65

INFLATABLE WITH I.O.B.

4 Stroke	2 stroke	Class	Coeff. Kt
	up to 1,350cc	A	1.29
	1,351cc - 5,700cc	B	1.40
	5,701cc - 7,450cc	C	1.46
	7,451cc - 8,200cc	D	1.59
	8,201cc - 10,000cc	E	1.71

304 – PARALLEL SLALOM COMPETITION**304.01 - CATEGORIES AND CLASSES**

This event is open only to pleasure craft duly homologated.

To slalom competitions the following pleasure navigation categories may participate, each of them divided into classes according to their motorisation :

- Inflatable with outboard;
- Inflatable with out/inboard;
- Boats with outboard;
- Boats with out/inboard and inboard;

304.02 - CLASSES

Each category is divided into classes as specified in Group 500 rules.

304.03 - COURSE

Two parallel slalom courses (course A and Course B) of equal length, They shall have a minimum of 150 m in length and be at least 50m apart from each other.

The courses shall be marked with a large buoy at each end, and eight smaller buoys spaced at equal-distance along the course.

The actual course dimensions shall be specified in the Advance Programme.

304.04 - CREW

Only one driver shall be on board of each boat.

304.05 - HEATS

Heats are drawn before the start according to the relative category and class. Two competitors participate in each heat and draw to start in course A or B.

Boats of different categories / classes may run together.

304.06 - START

Boats will be aligned at the start line of the course with power on.

The starting signal will be given by the Officer Of the Day.

304.07 - RACE RESULTS

Competitors will be timed, to complete the course alternatively slaloming the intermediate buoys.

Competitors then exchange the course and again will be timed to complete the course in the reverse direction A combined time is obtained by adding together the time of the two runs.

For each competitor a final time is then calculated: $((\text{time 1} + \text{time 2}) \times (\text{Kt coefficient})) + \text{time penalties}$

The pertinent compensation coefficient "Kt" is found in the "Kt" table, which is part of these rules.

304.08 - PENALTIES

A competitor receives a 20 second penalty for each missed buoy. A missed buoy shall not be rounded.

304.09 - FINAL CLASSIFICATION

The winner is the driver who, among all categories, establishes the best calculated time.

304.10 - PARALLEL SLALOM - COMPENSATION COEFFICIENT SCHEME

Compensation Formula: Time(expressed in seconds) x Coeff. Kt = Score

BOATS WITH OUTBOARD

4 Stroke	2 stroke	Class	Coeff. Kt
up to 550cc	up to 550cc	A	0.93
551cc - 1,000cc	551cc - 750cc	B	1.18
	751cc - 1,100cc	C	1.22
	1,101cc - 1,500cc	D	1.28
	1,501cc - 2,000cc	E	1.37
	2,001cc - 2,600cc	F	1.41
	2,601cc - 3,000cc	G	1.46
	3,001cc - 4,000cc	H	1.50

INFLATABLE WITH OUTBOARD

4 Stroke	2 stroke	Class	Coeff. Kt
up to 550cc	up to 550cc	A	1.00
551cc - 1,000cc	551cc - 750cc	B	1.24
	751cc - 1,100cc	C	1.29
	1,101cc - 1,500cc	D	1.36
	1,501cc - 2,000cc	E	1.47
	2,001cc - 2,600cc	F	1.51
	2,601cc - 3,000cc	G	1.56
	3,001cc - 4,000cc	H	1.58

BOATS WITH I.O.B. or I.B.

4 Stroke	2 stroke	Class	Coeff. Kt
	up to 1,350cc	A	1.26
	1,351cc - 5,700cc	B	1.38
	5,701cc - 7,450cc	C	1.47
	7,451cc - 8,200cc	D	1.55
	8,201cc - 10,000cc	E	1.65

INFLATABLE WITH I.O.B.

4 Stroke	2 stroke	Class	Coeff. Kt
	up to 1,350cc	A	1.29
	1,351cc - 5,700cc	B	1.40
	5,701cc - 7,450cc	C	1.46
	7,451cc - 8,200cc	D	1.59
	8,201cc - 10,000cc	E	1.71

305 - HISTORICAL BOATS**305.01**

Historical boats are those which meet the minimum requirements of the U.I.M.-ASDEC certification.

305.02

Historical boats which have obtained the U.I.M.-ASDEC certification with a total score of at least 53,5/210 and with none of the evaluation parameter having scored zero points, meet the minimum requirements of points 317.00.01 and have the right, upon request, to be listed on the National Authority - ASDEC Nautical Historical Register.

305.03

All the historical boats as defined in are entitled to enter U.I.M. - ASDEC events and competitions organised solely for historical boats according to ASDEC sport rules.

305.04

The above mentioned boats may also participate in U.I.M. regularity competitions, where if required, special reduced speed and length of course may be used. Such rules modifications must be specified in the advance programme of the event.

305.05 - EVENTS FOR "HISTORICAL" BOATS

There are four different types of event :

- STATIC SHOWS;
- "RENDEZ - VOUS";
- MEETINGS;
- RAID ON HISTORICAL AND CULTURAL COURSES;

305.06 - LICENCES

In order to participate in "Rendez-vous", Meetings and Raids, drivers and boats shall have the documents and legal insurance.

That is to say : Navigation Permit, Nautical Driving Licence, Insurance R.C. Licence for radio devices, if on board. Or the documents corresponding to the Navigation Permit and to the Nautical Driving Licence issued by the relative National Authority for calendar events, that is to say : Certificate of U.I.M./ASDEC Classification Competitor Licence, issued by the relative National Authority.

In case of events abroad, the rules of the Organising Country shall be respected.

305.07 - U.I.M. CHAMPIONSHIP

Competitors who participate, even with different boats, in at least three calendar events with final results, can compete for the title.

The title will be awarded according to the sum of the points obtained with the three best positions reported in the general classifications of three events.

In case of a tie the winner will be the driver who obtained the best sum of points, as for the calendar events considered, in U.I.M./ASDEC certification.

Jury, Race Officers, Timekeepers

- The jury of the event is composed by three people, notably the Officer Of the Day who is the president.
- The Officer Of the Day is designed by the National Authority, the other two components, the Race Secretary and the third judge are appointed by the Organising Committee.
- The U.I.M./ASDEC certification is issued by three Certifiers appointed by National Authority.
- At least one of the three Certifiers shall be registered in the Certifiers' Register.
- The timed events, where the time is the evaluation priority, shall be timed exclusively by Official Timekeepers.
- The general classification will be obtained by summing the points assigned in the different events, U.I.M./ASDEC classification included.

Logbook

The log-book follows the naval activity and the social life of boats registered in the Historical Naval Registry. The log-book is therefore linked to the boat and to the possible transfers of property. It cannot be transferred to other boats, even if they are owned by the same partner.

The Organising Committees of U.I.M. events record the participation of the boat and the result obtained, in the log-book; moreover, in case the boat participates in raids, stamp controls are recorded.

The ship owner can record in the log-book : single or group cruises or raids of a particular importance (because of the subject, the difficulties, the extraordinary events, the distances or the unusual places etc.). In such cases, it is suggested to previously inform the National Authority.

The cost of the Log-Book is decided annually the National Authority.

305.08 - STATIC SHOWS

Definition :

Static Shows are parades of "Antique" or Classic boats on the ground.

They can be either single events or coupled to Elegance Contests or Barter-Shows. They can also be complementary to "Rendez Vous", Meetings and Raids.

Rules :

The special rules and the Program shall be drawn up by the Organising Body of the event and previously approved by U.I.M.

They must specify and provide for :the venue, dates and modes of registration, times for any control and the distribution of the registration number, the allocation of the exposition site and the delivery of the Rules. They shall also specify the procedure for the prize giving.

The Classification of U.I.M./ASDEC Certification is not requested.

Prizes are awarded, by irrevocable decision of the Jury, according to the peculiarities of the boats.

305.09 - "RENDEZ - VOUS"

Definition :

"Rendez - Vous" are non-competitive events, aiming to gather amateurs in fixed venues and dates, with a recreational, evocative, tourist and cultural goal.

An event is a exhibition of boats in the water, with a demonstrative parade and possible common routes in typical places.

In addition, Elegance Contests, Static Shows and Barter-Shows can be organised.

Rules

The special Rules and the Program shall be drawn up by the Organising Body and previously approved by U.I.M.

The Rules, in addition to what is established for the Static Shows, shall provide information about clothing for the teams. .

The Classification of U.I.M./ASDEC Certification is not requested.. 014 Prizes are awarded, by irrevocable decision of the Jury, according to the peculiarities of boats and teams.

305.10 - MEETINGS

Definition

Meetings are events aiming to gather amateurs in fixed venues and dates, with the same goal as the "Rendez-Vous" and, like these last ones, with the possibility to have in addition Elegance Contests, Static Shows and Barter-Shows.

Unlike "Rendez-Vous", meetings are characterised by the presence of competitive races, enhancing the sporting aim, with a general classification possibly valid for the U.I.M. Championship.

Rules

The special Rules and the Program shall be approved by the National Authority and registered in the Calendar.

The program shall specify dates and venues for any activity of the event. A meeting shall include a U.I.M./ASDEC Certification test and at least two basic competitive races. Such races shall respect the provisions of the Sport Rules.

Basic Tests are the following : -U.I.M./ASDEC Certification Test-Regularity Race-Sea Skill Tests (Rescue of a man at sea and manoeuvre, mooring test and manoeuvre).

Subsidiary Tests are the following:-Slalom -Parallel Slalom -Treasure-hunt-Water Gymkhana -Knots Test and nautical techniques-Knowledge of security and prevention rules and of the pleasure navigation rules-Skill test in nautical games-Naval Etiquette Test.

For subsidiary tests which do not contribute to the creation of the general classification, there are no set rules. The pertaining special rules will be drawn up by the Organising Committee.

Final Results shall be foreseen:

Final results for any race A general classification resulting from the points obtained in the three basic tests, that is to say in the ASDEC certification test and in the two competitive races.

Prizes : A prize to the first in any test (basic and subsidiary), and eventually to a second and a third must be foreseen. A prize to the first three in the general classification. Possible additional prizes shall be specified in the program.

305.11 - HISTORICAL AND CULTURAL RAID

Definition

Raids are tourist – sport - cultural events which take place on routes recalling facts of power boating history or on cultural and naturalistic routes.

They consist of a navigation, also in stages, taking place according to a fixed schedule, in compliance with rules, limits and places.

Along the course transit and stamp controls shall be established.

Rules

As for the other events with a General classification, Rules and Program shall be approved by the National Authority and registered in the calendar, according to the specified provisions.

Particularly

Every driver receives a navigation schedule where control stamps can be affixed, if the competitor does not possess the Log-Book; a detailed

travel map, with the relative progressive mileage, the position of the transit controls, the theoretic transit time, the site, the maps of the venues and the characteristics of the basic tests foreseen; any other information on the program.

Starts

Starts are given as specified in the programme as for the transfer stages to the stamp control positions, at the time foreseen on the navigation schedule.

If the driver does not appear on the starting line at the set starting time, he is penalised by one point for every second of delay and he is excluded from the final results after 15 minutes from the starting time.

Controls

Along the course stamp controls are placed in the sites shown in the map and on the finishing line.

A delay at the controls, compared to the schedule, implies a penalty of one point for any second of delay or advance.

Competitors arriving at to the control points in groups will obtain the same time.

The absence of a control stamp implies the exclusion from the final results.

At any control and at the stage arrival a maximum delay of 60 minutes is permitted, compared to the time foreseen in the schedule, always with a penalty of one point per second, over which the competitor is excluded from the final results.

Penalties

In addition to penalties due to the times to respect, as described above, penalties due to behaviour during navigation are foreseen:

Any violation of the Naval Etiquette and an improper use of fenders implies a penalty of 5 points per infringement.

The non-respect of one rule of behaviour for the Prevention of Collisions, if directly reported by the Jury or following proved claims, implies 15 points of penalty.

Additional basic tests

The tests specified in the program can be performed during the course, (at the arrival or starting of a stage) or at the final arrival.

For rules and penalties, see what is specified for basic tests.

Scoring System

The scoring system for the Raid is drawn up starting from the basic score of 140 points and deducting the points of the different penalties.

The General Classification is the sum of the points of the Raid Final Results with the points of the final results of the basic competitive race and the points of the Classification of U.I.M./ASDEC Certification.

305.12 - RESCUE OF A MAN AT SEA AND MANOEUVRE

Definition

This kind of test is a sea skill test. Accuracy and behaviour determine penalties for the evaluation of the test.

Rules

In a designated area equipped with buoys and easily controllable by timekeepers and jury, competitors must: cast the moorings off, navigate in a set course, in the fixed direction, launch a life-belt in the water, make a 180° evolution and recover the life-belt with any means.

Repeat the set course in the fixed direction and take the mooring back.

Competitors will receive the map of the venue when they submit for their entry.

Accuracy and behaviour

The performance of the test is evaluated with penalties charging the basic score, equal to 140 points.

A maximum time must be specified, over which disqualification is foreseen.

The time spent is the time calculated between the starting sign and the termination of the mooring manoeuvre, when the test is concluded.

305.13 - SCORING SYSTEM (SUMMARY)

Events

Theoretical score that can be obtained in the general classification of any event is a maximum of 450 points. It is the sum of the highest points obtained in the basic tests. Particularly :

- Theoretical maximum score in U.I.M./ASDEC Certification test: 210 points;
- Theoretical maximum score in sea skill test and RAID: 140 points;
- Theoretical maximum score Regularity race: 100 points;

Regularity test

Max. score : 100 / A penalty of 1 point for each sec.

Name of the boat	N° :	Competitor :
Declared Speed :	N.M :	= km/h :
Departure time :	Arrival time :	
Effective used time :	Ideal time :	
Effective secret time :	Ideal time :	(at invisible check points)
Total penalties:		

Manoeuvring competition

The classification is determined by reducing the penalty points from the 140 points . For each penalty a rating from 0 to 10 will be given ; the perfect score, being conventionally "0".

Name of the boat	N°	Competitor :
1. Dressing Penalty;		

2. Hoisting and position of the flags;
3. Use of fenders;
4. Hitting or risk to hit the pontoon;
5. Hitting or damaging buoys during competition;
6. Error of the course;
7. Touching of moving boats or bad manoeuvring whilst rescuing a man at sea (life-belt);
8. Docking lines (right diameter and length);
9. Docking knots (ability to execute);
10. Number of persons on board during manoeuvring in function of the length of the boat;
11. Smoke and noise pollution during the manoeuvring;
12. Professionalism of the team;
13. Boats characteristics (single or multiple engines, beam, length and outer keel dimensions / flying bridge etc.);
14. Maximum allowed time :Effective time.....;

TOTAL POINTS Note :

Exceeding the maximum time allowed or not completing the competition will result in disqualification. Boats characteristics will influence its manoeuvrability. Number of engines, kind of outer keel, structure of flying bridge will determine number of handicap points (0 - 10).

306 - RALLY COMPETITION

A Rally is a concentration of pleasure boats at a time and place established by the Organising Committee.

The venue must be reached following a specified course, in one or more stages.

Along the course and/or at the venue regularity competitions will take place.

Such competitions will be organised in accordance with the relevant U.I.M. rules and will score points for each participant ; the total of all points awarded will establish the final result.

The winner is the boat with the lowest number of penalties.

The points for each stage are scored according to the U.I.M. scoring system (400, 300, 225, etc.).

Difficult sections may be introduced into the course : in this case the event is called RALLY MARATHON and points are scored to be included in the final results.

All stages must be specified in the competition programme.

The Organising Committee will draw up the Rules to conform with the features and facilities of the venue.

The event may take place on one or more days.

During the event at least two regularity competitions must take place, one of which shall be a parallel slalom or American pursuit.

During parallel slalom and/or American pursuit, all the relevant U.I.M. rules shall apply.

The boat will only carry the number of passengers established by the homologation sheet of the craft. In the absence of this document, the relevant "regularity competition" rule applies.

All routes, averages, passage controls, regularity competition, parallel slalom, American pursuit etc. will be kept secret until revealed to competitors when the documents are distributed by the Organising Committee at the start.

Such documents shall specify the length of the course, type and number of the stages, maximum times allowed.

At the start every competitor will be given a schedule, for which the driver is solely responsible.

The schedule shall be presented at each check point.

Absence at any control, loss of the schedule or falsification of the document shall incur disqualification.

On-board Equipment shall comply with the regulations of the boat's flag Country.

Procedures for stage starts will be established by the organisers.

Penalties will be established by the organisers.

The Organising Committee is empowered to establish rules for any event not covered by the U.I.M. rules.

Such rules must be specified in the Advance Programme.

The points in each individual competition are added together to provide the total points for the event.

307 - PROMOTIONAL MEETINGS

Activities

Promotional meetings are all non-competitive events, used to promote pleasure power boating and may include one or more of the following activities:

1. Passages;
2. Meetings;
3. Treasure-hunt;
4. Pleasure Navigation training;
5. Navigation demonstration;
6. Sea skill race;
7. Gymkhana;

A Meeting is a concentration of boats at a time and place specified by the Organising Committee, with the aim of exchanging new experiences in navigation, and techniques concerning power boating in general.

Meetings may be one-design, organised by boat/engine manufacturers or other companies involved in power boating, in co-operation with an Association affiliated to the National Authority.

All these events must be authorised by the National Authority, to which the request must be submitted within set deadlines, in order to obtain the permission of the relevant Authorities.

Sporting Pleasure Navigation Events may be organised within meetings, in accordance with the U.I.M. rules.

The General Rules for Pleasure Navigation and the relevant regulations must always be observed by organisers and participants.

The rules for treasure-hunts or sea skill events must be deposited with the National Authority with the request to organise.

The events of Pleasure Navigation training do not need specific rules.

Only a timetable is necessary for events of Navigation, demonstration and sea skill race.

Gymkhana consists of a series of different skill competitions, as for example the recovering of a man at sea, mooring, landing and starting from the shore, anchoring to a buoy etc. The course, competitions, penalties and points shall be specified in the programme.

A Passage is a course exceeding 500 km, both at sea and on inland waters or both, without minimum and maximum times and special heats.

Drivers wishing to organise a passage must apply to the relevant National Authority, enclosing details of the course they wish to follow and a log book. The National Authority will authenticate the pages of the log book with an official stamp.

At the end of the Passage, the log-book will be submitted, together with a photocopy to the National Authority for its homologation procedure.

The course shall be authenticated by local Authorities certifying the log-book at appropriate points.

The competitor(s) may extend the established course by means of certifications in the log-book.

Should bad weather stop the passage before the minimum programmed length specified above, it may be re-run by notification to the National Authority and using the same log-book. The National Authority will note the rerun of the Passage, specifying the new date of the event.

308 - ELECTRIC AND SOLAR PLEASURE CRAFT

Solar or other alternative energy powered boats with in-water propulsion.

They may participate in all the competitions specified in these rules, with independent events relevant to their particular characteristics.

When the Organising Committee submits rules to their National Authority, these must include courses, speed, classes, and all other relevant details.

309 - WORLD & CONTINENTAL ENDURANCE CHAMPIONSHIPS

A. Introduction

The Title of the Championship shall be known as the “Powerboat Production 1 - U.I.M. World Championship Endurance Racing “ (hereinafter “Powerboat Production 1”). A similar title is applicable to any Continental Championship to which these rules apply.

Powerboat Production 1 is the U.I.M. Pleasure Navigation endurance classification comprising Group A monohull craft in a racing series including the following categories: Sport, Super Sport, and Evolution (formerly Tourism, Sport, Prototype and Super Yacht). The purpose of the rules set forth herein is (1) to ensure safe and competitive racing in the race series at reasonable cost to the participants, (2) to expand and broaden the general base of participation and (3) for the marine industry to develop and demonstrate the performance of its products.

This rule 309 concerns only the U.I.M. World and Continental Endurance Championship. In the case of conflicting rules, these rules (Rule 309) shall prevail over the other relevant U.I.M. rules applicable to Endurance Racing.

In cases seemingly not foreseen by the rules decisions shall be taken bearing in mind the spirit of the rules.

B. Championship and Race Format

The U.I.M. Powerboat Production 1 World Championship is composed of a minimum of five races per season. The Title "World Champion" shall be awarded to the highest points winner over the course of the Championship for each category. To be eligible for the Championship, the boat and team combination must be present and participate in all Championship events subject to exceptional circumstances agreed by the Production 1 Management Committee

Championship points shall be awarded to the top six (6) finishers in each category with points awarded as follows: Winner 10 points, 2nd place 7 points, 3rd place 5 points, 4th place 3 points, 5th place 2 points and 6th place 1 point.

Each Powerboat Production 1 event shall be composed of two separate races, a Saturday race and a Sunday race. Championship points shall be awarded for both the Saturday and Sunday races.

1. Saturday (Rally) Race

This race shall be minimum 50 nautical miles in distance.

2. Sunday (Endurance) Race

This race shall be minimum 80 nautical miles in distance.

The courses are subject to change or modification upon appropriate notice to teams due to safety reasons, bad weather, local authority restrictions, etc...

C. Boat Specifications

These boat specifications (taken together with the race format described above) seek to create a broad and fair competitive balance within each race category.

1. General Rules applicable to all categories

(a) All race boats shall be monohull craft.

(b) All race boats shall be limited to two identical inboard engines while racing. Teams are limited to three engine blocks per race

weekend; in other words, teams may only change one engine during the course of the race weekend. Spare parts are unlimited except that in Sport and Super sport after market replacement and/or modified parts that seek or have the effect of an increase in power are prohibited.

- (c) There shall be no performance index.*
- (d) Boat canopies are not permitted, all boats shall be open top.*
- (e) There shall be no technical outside assistance during the race. Teams may carry a mechanic during the race and are allowed any assistance that the team in the boat may provide.*
- (f) Each team may carry up to 4 crew in the boat during the race.*
- (g) boats may run on fuel pursuant to rule 302.10. No performance increasing additives are permitted*
- (h) all participating boats are subject to manufacturer certification and certification of a technical body as per U.I.M. rules*
- (i) boat race numbers will be provided by the Promoter upon inscription for the Championship.*
- (j) Boat name shall be that of primary sponsor or team owner.*
- (k) each boat shall carry the appropriate U.I.M. logo identification*
- (l) Attendance at all official race briefings is mandatory. Non-attendance may be punished by disqualification.*

2. Category Sport (formerly Tourism)

Eligible boats shall:

- (a) be Group A boats with minimum length from 9.00m having standard production model hulls and standard production model engines;*
- (b) have no modifications to hulls and engines, changes to accommodation fittings, etc... ;*

- (c) *have a minimum commercial hull production of 25 units and have sold and titled them to pleasure boat owners;*
- (c) *be engines listed in the manufacturer catalogue and have having a commercial production of 100 units;*
- (d) *have the same props and drives as sold by manufacturer;*
- (e) *have minimum cabin clearance and cabin size determined with reference to length of boat as follows:*

<i>Length</i>	<i>Min. Clearance</i>	<i>Min. cabin surface</i>
<i>9.00m - 11.99m</i>	<i>1.75m</i>	<i>2m²</i>
<i>12.00m - 14.99m</i>	<i>1.90m</i>	<i>4m²</i>
<i>15.00m - 19.99m</i>	<i>1.90m</i>	<i>6m²</i>
<i>20.00m - open</i>	<i>1.90m</i>	<i>8m²</i>

- (f) *have a maximum power to weight ratio of 1hp per 10kg (5% tolerance). There shall be no reference to maximum capacity of engine;*
- (g) *have all other matters consistent with existing Group A rules (502.02).*

3. Category Supersport (formerly Sport)

Eligible boats shall:

- (a) *be Group A boats with minimum length of 9.00m and a maximum length of 15.00m having standard production model hulls and standard production model engines that would otherwise qualify as Sport category but fall outside because they do not comply with one or more category Sport requirements;*
- (b) *have a maximum power to weight ratio of 1hp per 5kg (5% tolerance). There shall be no reference to maximum capacity of engine;*
- (c) *have all other matters consistent with existing Group A rules (502.02).*

4. Category Evolution (formerly Prototype)

Eligible boats shall:

- (a) consist of boats not meeting the requirements of the Sport or Super sport categories;**
- (b) have a power to weight ratio of 1hp per 4kg. There shall be no reference to maximum capacity of engine.**

D. Interpretation of Rules / Changes to Rules

The race format and the boat specifications found herein seek to create a broad competitive balance within race category. The use of the term "standard" in these rules is meant to describe components or parts that are both manufactured and intended for sale to the public at commercially reasonable prices. For purposes of competitive balance, the Production 1 Management Committee is entitled to determine into which Powerboat Production 1 category a particular boat qualifies. It may require adjustments to boats, engines, drives, propellers, etc... to ensure a competitive balance within category.

It is the intent of these rules to limit top speeds to 90mph for safety reasons.

E. Enforcement

Consistent with the purpose and intent of the rules, the Production 1 Management Committee is entitled to take whatever action it deems necessary to enforce these rules. Any parts or components which give a boat or team an unfair competitive advantage may be deemed illegal and result in disqualification. The Production 1 Management Committee, in its sole and absolute discretion, may decide if any part, component, or equipment change is in an effort to defeat or beat the rules, and it may disqualify an entry in violation of the spirit and intent of these rules.

The Production 1 Management Committee is composed by

- * The U.I.M. President**
- * The President of Pleasure Navigation Commission**
- * The promoter**

These rules stand for the 2003 season only and are subject to revision for the future.

400 - JURISDICTION

401.01 - DEFINITIONS

Jurisdiction applies to :

The U.I.M. Offshore, Circuit, Aquabike Rules.

402 - JUDICIAL BODIES

402.01 - THE INTERNATIONAL JURY

The Jury hears any protest raised by a driver.

1. Composition

The Jury must be composed of at least three members, the Chairman included. The Organisers of the event appoint the Chairman. Each competing country, including the hosting country, may appoint one member delegate who can represent only one country. These appointments must be made in writing, authorised and signed by a National Authority Official and delivered to the event Secretary.

If there are not enough NA appointed members to form a Jury of three persons, the Organisers must appoint additional members, but only enough to reach three persons.

The Jury composition will be announced at the first drivers meeting. The jury must be present at the first drivers meeting.

Race Officials can not be Jury members.

The U.I.M. Commissioner appointed to the event is a full member of the Jury.

In an event comprising races of several classes there may be a different Jury for each class.

2. Authority and Duties

The Jury sits as long as the event lasts, and hears all protests relating to that event.

The jury must decide as soon as possible on all protests presented to it during the event.

402.02 - INTERESTED PARTIES

No member of a Jury who has taken part as a competitor or Official in the race concerning which a judgement is to be given, or knows himself to have directly or indirectly an interest in one of the parties involved, may judge the protest.

However, a member of an International Jury shall not be disqualified even if one of the parties is from his NA.

402.03 - THE U.I.M. APPEALS BOARD

The U.I.M. will decide on Appeals arising from International events according to the provisions laid out in Rule 405.1.

In each case, the U.I.M. Executive Committee, or in case of an appeal lodged against an Executive Committee's decision, the appointed neutral Council member will nominate three members, one to be Chairman after consultation of the relevant commission. The members must not be from the National Authorities of the parties to the case.

One member will act as Clerk to the Board.

403 - PROTEST PROCEDURES

403.01 - VALIDITY OF PROTESTS

Only a driver may protest against facts pertaining only to the races in which he takes part.

Joint protests signed by several drivers will not be considered.

All protests must be in writing - type written or hand written in printed characters in English.

It must state the reason for the protest and be accompanied by any relevant documents that shall provide evidence within the given time and any protest fee as stipulated by the Race Organiser.

All protests must be signed by the protester.

The protest must be handed to the Race Secretariat who must, in the presence of the protester, record the time of receipt.

Any costs involved in verification are to be borne by the losing party.

A written protest can be withdrawn by the protester ; he then loses the protest fee.

403.02 - PROTEST FEE

The amount of the Protest fee must be decided by the **National Authorities** and it must be published in the advance programme. The maximum amount for a Protest fee will be decided by the U.I.M. General Assembly. It will be 80 € **or an equivalent sum in the local currency.**

The Jury must determine whether the protest presented to it, has been lodged according to the rules. If found to be invalid, the reasons must be given to the protester.

If a protest is upheld by the Jury, the fee must be returned to the protester.

403.03 - RIGHT OF PROTEST

A protest can be lodged against the posted provisional results or against any decision made by the Race Committee or against one or several competitors.

A protest can only be lodged on a subject which directly concerns the protester himself. A protest which complies with these rules can not be refused by the Jury.

403.04 - TIME OF LODGING A PROTEST/ PROTEST PERIOD

A protest regarding the qualification of a boat, a motor, or a driver must be made before the drivers briefing. Starting in a race is considered as acceptance of the conditions and the eligibility of the other competitors. The only exception to the above is when the protester can prove the facts were not given to him before the drivers briefing, then a protest can be lodged up to one hour after the posting of the results.

Any other protest must be lodged within one hour of the first results being posted with the following exceptions:

A protest against new results can be lodged within one hour of these new results being posted, but only concerning the decision, which lead to the changing of the results.

A protest against any other decision, posted after the posting of the first results can be lodged within one hour after the posting of this decision.

403.05 - JUDGEMENT

Decisions by the Jury shall be reached by simple majority of votes. In the case of equal votes, the Chairman's vote will be the casting vote.

Any judgement pronounced by the Jury stays in effect until altered by the Appeal Board.

403.06 - NOTIFICATION OF DECISION

All decisions by any jury must be notified to the concerned parties in writing, including their right of appeal.

403.07 - AVAILABILITY OF DOCUMENTS

All documents relevant to any decision by the Jury must be filed with the organising N.A. and held available for a higher authority until the time limit for an appeal has expired.

404 - HEARING OF THE PROTEST

404.01 - RIGHT TO A HEARING

Any person being party to a protest, or being charged for an offence against the rules shall have the right to be heard before the Jury in order to defend himself.

The Jury must give notice to all parties of where and when the hearing will take place.

If any party duly notified, fails to appear without giving an acceptable reason, judgement can be rendered by default.

404.02 - WITNESSES

Each party may bring witnesses to the hearing.

Any costs incurred by the appearance of witnesses shall be borne by the respective parties unless decided otherwise by the Jury.

404.03 - THE HEARING PROCEDURE

The following procedure must be followed in all hearings unless otherwise stated elsewhere in these rules :

- Written minutes must be taken.
- All parties to the case are entitled to be present and to hear and question all evidence at the hearing up to the time the Jury makes its decision. The protest, shall be read out to the parties.
- The parties shall be invited to state their cases. The parties may call witnesses. Each witness, after having given his account of the case, may be questioned by all parties and by the members of the Jury.
- Any member of the Jury who is familiar with the case may give his evidence
- The parties shall be entitled to question each other and any member of the Jury who may have given evidence.
- The witnesses shall withdraw and the parties shall be invited to make a final statement of their cases.

- The Jury may recall any party, previous witness or new witness and call on other evidence.
- The parties must be present during the whole of the recall and must be given the opportunity to question any new evidence after which they may re-make their final statements.

405 - THE APPEALS PROCEDURE

405.01 - APPEAL BOARD

The members of the Appeal Board may hold other sports functions and consist of a Chairman and two members.

The Appeal Board sits whenever the Chairman calls a meeting.

405.02 - INTERESTED PARTIES

No member of an Appeal Board who has taken part as a competitor or Official in the race concerning which a judgement is to be given, or knows himself to have directly or indirectly an interest in one of the parties involved, may judge the appeal.

405.03 - RIGHT OF APPEAL

A decision by a Jury may be appealed by the parties involved when they consider that an injustice has been made against them and/or fresh evidence has been made available that may alter the decision of the Jury.

A driver can also appeal against a penalty, imposed by the U.I.M. Executive Committee, or imposed by the OOD for the reason of post race technical scrutinizing without the prior lodging of a protest, but only if this was not possible, because the race jury has had dissolved itself before or within the protest period after the finish of the event.

405.04 - NOTICE OF INTENTION TO APPEAL

The intention to appeal must be notified in writing to the Secretariat of the U.I.M. by the interested party within four days of the ***day following the*** Jury decision or the ***day following the*** decision against which he can appeal without the prior lodging of a protest.

405.05 - TIME LIMIT

All appeal documents and fees must be received within ten days from the date when the notice to appeal was received by the U.I.M. Secretariat.

405.06 - LODGING OF AN APPEAL

The appeal must be sent by telefax, letter ***or Email and it must be signed by the appellant.*** The reasons for the appeal must be stated.

Address, telephone, e-mail and/or fax should be clearly mentioned on the appeal, so that the U.I.M. can send any correspondence directly to the driver with copy to relevant N.A.

405.07 - NOTIFICATION OF THE PARTIES OF THE APPEAL

The U.I.M. Secretariat must within two days from receiving the appeal, notify by facsimile or letter the other parties that an appeal has been lodged and that they may send a rejoinder.

Eventual rejoinders must be received within ten days from receiving this notification.

405.08 - TIME LIMIT FOR DECISIONS ON APPEALS

All appeals must be determined no later than forty-five days after the date when the appeal documents and fees were received by the U.I.M. Secretariat.

405.09 - APPEAL BOARD DECISIONS

The Appeal Board has the right to change the decision of the Jury. The decision of the Appeal Board is final.

405.10 - COSTS

The appeal fee is 1525 €. In addition the driver has to pay 2290 € to the U.I.M. for possible costs of the appeal committee (meetings, telephone or fax costs). The appeal fee for a single championship/invitation race (not a Formula race, or a WOC race or similar) is 760 €, in addition the driver has to pay 760 €, to the U.I.M. for possible costs of the appeal committee.

The driver who appeals has to pay for the meeting costs anyway if the appeal is upheld or not. If the costs of the appeal committee is less than 2290 € then the difference will be paid back to the driver.

The appeal fee of 1525 € will be refunded if the appeal is upheld.

The driver may agree to forego an Appeal Committee meeting and for the appeal to be handled through telephone, e-mail and fax, if possible, in order to minimise the cost for the appeal.

406 - PENALTIES

406.01 - GENERAL

Any proven breach of applicable Rules may be penalised.

If the rules do not determine a special penalty for the relevant breach, the penalty to be given has to be proportional to the seriousness of the breach. The proportionality is at the discretion of the decision maker.

The U.I.M. Executive Committee or the relevant National Authority shall only impose penalties, when they deem the relevant infringement to be so serious, that it has to be penalized although the deadlines for penalizing or lodging a protest have already expired. After the expiry of the aforementioned deadlines, a penalty for an ordinary breach of the rules shall in general not be imposed.

The Race Jury or the Appeal Board following a protest or an appeal can (only) impose penalties, which can be imposed by the OOD.

406.02 - DEADLINES FOR PENALIZING

The OOD and the U.I.M. Commissioner can only impose Penalties until the moment, the first results of the race are posted.

The National Authority and/or the U.I.M. Executive Committee can only impose penalties within 3 months from the day, the infringement occurred.

These deadlines are not valid for penalties imposed by the OOD for the reason of post race technical scrutinizing. In this case, the penalty has to be imposed as soon as possible after the receipt of the scrutinizing results.

406.03 - UNACCEPTABLE BEHAVIOUR

In order to protect the interests of the sporting community, the following actions may also be penalised by the OOD, by the U.I.M. Executive Committee or by the NA of the Licence Holder.

- any deliberate act taken to gain unfair advantage.
- any false act made or statement given with the intention of suppressing facts required for the proper conduct of the race.
- any attempt to bribe or the taking of a bribe.
- any abusive or unsportsmanlike behaviour.

406.04 - REPRIMAND

A Reprimand is a notice of disapproval of an unacceptable action. It must be recorded by the Race Secretariat. A reprimand must be witnessed. A reprimand automatically constitutes warning that if the offence recurs, a heavier penalty will be given. A Reprimand is valid for 12 months.

A reprimand can be given by the OOD, or the National Authority of the Licence Holder.

406.05 - YELLOW CARD

A yellow card may be given by the OOD or U.I.M. Commissioner for behaviour on the water.

A yellow card will be confirmed on a form which will be posted with the results, the pilot has 60 minutes to protest to the Jury if the protest is not accepted, he can make an appeal. Protests and appeals can be lodged according to U.I.M. rules.

A card is valid for 12 months. A driver who has been given three yellow cards is excluded from all races run under U.I.M. rules for a period of two calendared racing months in the current or following season.

Yellow cards must be notified to the U.I.M. and National Authority of the driver concerned within three days.

The U.I.M. will notify all National Authorities if a driver obtains three yellow cards and is suspended from international competition.

All yellow cards including reason for the card given must be mentioned in the commissioner's report.

406.06 - DISQUALIFICATION

Disqualification deletes a competitor from the results of the heat or race where the offence occurred.

Disqualification is done by the OOD.

406.07 - TEMPORARY SUSPENSION

Temporary suspension suspends a competitor, a competitor together with crew, a crew member, or an Official from all or part of an event. Temporary Suspension may be given for serious indiscipline.

Temporary Suspension will be imposed by the OOD.

406.08 - PROLONGED SUSPENSION

Prolonged suspension can be imposed on a competitor, an Official or an organisation for deliberate fraud, repeated or very serious indiscipline or very serious misconduct.

Prolonged Suspension can be imposed only by the NA of the licence holder or organisation, or by the U.I.M. Executive Committee.

A Prolonged Suspension is always international. The NA must immediately inform the U.I.M. of a Prolonged Suspension and the U.I.M. must inform all other NA's.

406.09 - EXCLUSION

Exclusion means a permanent loss of all rights to take part in any activities falling under the U.I.M. and its affiliated organisations. A person or an organisation who has committed a moral or sporting offence of extreme gravity is liable to be excluded.

A sentence of exclusion can be pronounced only by the NA of the licence holder or organisation, or by the U.I.M. Executive Committee.

Exclusion is always International. The NA must immediately inform the U.I.M. when it has imposed a sentence of Exclusion and the U.I.M. must inform all other NA's.

In the case of suspension, prolonged suspension or exclusion, all National Authorities and therefore their affiliated clubs bind themselves to respect the decision and apply it in their own country.

407 - PRIORITY OF THESE RULES

The course of the U.I.M. has to be exhausted, before the dispute can be brought before civil court.

408 - POWERBOAT PROTEST FORM

- PART A is for you, the driver, to complete and hand in within the specified time limit.
- PART B is for you, the driver, to complete and give to the Protest Committee Chairman at the beginning of the Protest Meeting.
- PART C is for the Protest Committee to complete.

DRIVERS PROTEST FORM

PART A SECTION 1

NAME OF EVENT: _____ DATE OF RACE OR HEAT: _____

HEAT _____ NUMBER: _____

CLASS: _____

SECTION 2

YOUR NAME (PROTESTOR): _____

YOUR BOAT NUMBER: _____ BOAT NAME: _____

SECTION 3

I (the Protestor) am protesting against: (tick ✓ as appropriate)

To seek redress from the Race Organising Committee for actions or omissions.

☐ Another driver, name: _____ Boat No: _____

☐ The Results as posted

SECTION 4

Which rule has been infringed _____

Time of incident: _____

On which lap was the incident _____

Where was the incident _____

SECTION 5

Your signature _____

Time of signature _____

SECTION 6

You must see this section completed by the official receiving this Protest:

Name of official: _____.

Time Protest received: _____

PART B

SECTION 7

Either, explain incident with another driver; in writing and by drawing;

Or, explain your reason for Protesting against the Race Committee:

Part B cont/d.....

GROUP 500 - TECHNICAL RULES

500.01 - CLASSIFICATION OF PLEASURE CRAFT

The competitions are open only to pleasure craft, boats and vessels, as defined by the National Technical Institutions qualified for such classifications.

500.02

Only the craft whose technical characteristics are homologated and registered by one of the following Technical Institutions should be considered pleasure craft:

- RINA (Italy);
- C.N.S.N.P. (France) ;
- AMERICAN BUREAU OF SHIPPING (USA);
- DET NORSKE VERITAS (Norway);
- LLOYD'S REGISTER OF SHIPPING (United Kingdom);
- NIPPON KYOKAI JAPAN (Japan) ;.
- GERMANISCHER LLOYD (Germany);

500.03 - CRAFT HOMOLOGATION

All pleasure craft shall bear a certificate issued by official Technical Bodies according to the relevant Laws concerning the pleasure navigation, and a lead seal or special plaque attached to the hull containing the builders serial number relevant to the boat.

In competitions taking place at sea over six miles from the shore only pleasure boats registered and certified for this type of navigation are permitted.

500.04 - CRAFT DIMENSIONS

The identification of the dimensions of the hull and of the motorization characteristic shall be determined exclusively by the details contained in

the official documents of the boats issued by the concerned National Bodies or certified by the boat builder.

500.05 - ENGINES

All engines installed on pleasure craft must be homologated and specified in the craft certificate issued by a certified Technical Body.

An inspection of the engines can be made after the competition is finished; if the engines are not in accordance with those stated in the official documentation the competitor will be disqualified.

500.06 - RACE NUMBERS

The competition number may be painted or fixed as follows :

On a surface corresponding to the following measures and characteristics :

- the surface measures 25 x 40 cm;
- the numbers must be painted in black on a white surface on both sites of the hull in digits of 24 x 4 cm;
- the space between the rectangular with the digits must be 8 cm;
- the white surface must surround the digits with 4 cm;
- in case of a 3 digits number, the length of the surface must be modified consequently;

The competition numbers may also be painted or glued to the slightly curved surfaces and on one vertical surfaces provided that the whole number is within 30° from the vertical position.

It is advisable to use two digits only, whenever possible.

For outboards competition, it is recommended that the number be painted twice on the foredeck.

This general rule may change according to what is specified in category / class rules.

Exceptions must be approved by the U.I.M. and mentioned in the advance-programme; the Officer Of the Day may accept exceptions, his

final decision being based on effectiveness and readability of the number.

501 - REGULARITY COMPETITIONS

501.01 - DECLARED SPEED COMPETITIONS

Maximum speeds are established by the Organiser according to the engine power and/or cubic capacity.

The maximum speed must be declared in the advance-programme.

502 - ENDURANCE COMPETITIONS

502.01 - GENERAL

In order to clarify the regulations of Endurance Championship it is pointed out that all boats certified CE entering the over mentioned Championship must conform to ISO-CE regulations currently in force, with particular attention to :

- ISO 10088 – 7840 (tanks installation)
- ISO/DIS 11592 : 1997 (determination max power for FB engines) relevant to boats
- ISO/DIS 6185 – 3 : 1997 (determination max power for FB engines) relevant to inflatables

Engines

The maximum engine capacity for the various categories and classes is referred to:

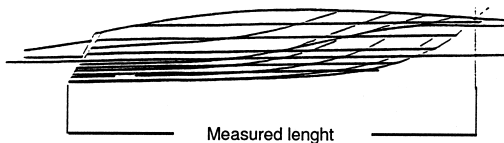
- the total of the engines installed in case of petrol aspirated engines;
- the allowed capacity must be the same used for petrol turbocharged engines and diesel turbocharged engines, while it is legitimate to allow the capacity doubling exclusively for diesel aspirated engines;
- for petrol and diesel engines the presence of supercharging causes a reduction of the maximum allowed capacity that shall be divided per coefficient 1,4.

- No tolerance is allowed to cubatures resulting from the computation.

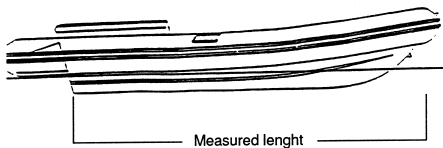
Length Measurement

The criteria of length measurements for the purpose of craft classification and breadth calculation are illustrated below :

Length of a boat



Length of a RIB (Rigid Inflatable Boat)



Beam of a craft

The beam is to be measured on the deck, excluding any side appendices. For RIB (Rigid Inflatable Boat) the measurement shall include the overall size of the tubes; a tolerance of $\pm 3\%$ is allowed.

502.02 - GROUP "A"

RACE NUMBER

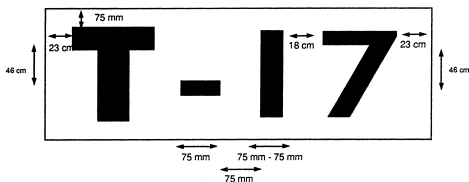
The race number shall be displayed on the topsides (port and starboard) of the hull and on the deck, within a dedicated area:

- The numbers on the topside (port and starboard) shall be displayed on the forward half of the boat.

- The number on the deck shall be correctly read from the driver seat position.
- Numbers and letter must be highlighted in black on a white background
- The letter and number which identify the class shall be put in front of the race number.

The race number and its dedicated area shall have minimum dimensions as indicated below

- Numbers and letter must have a thickness of no less than 75mm.



CATEGORIES AND CLASSES

Tourism category

It includes cruising vessels regularly homologated for such a purpose including internal fittings complying with the specifications of the boat builders; these fittings must be suitable

for normal life on board for the whole crew and provide accommodation overnight for at least two persons.

When accommodation fittings are removed, the boat is automatically reclassified into the Sport category.

Each craft must be fitted with two identical and independent motors capable of developing a speed of at least

30 knots, and have a minimum cruising range of 250 nautical miles.

No modification is allowed in respect of the standard production of the model used that must correspond to the one reported on the approval certificate.

Driving seats can be replaced.

The width of the craft must not be less than the product of the length measurement multiplied by a coefficient of 0.26 .

All engine models coming from the standard engine which have been modified in order to increase the performances for sporting activity, even though included in the boat builders catalogue, are not permitted.

For turbocharged engines the crankcase of the turbocharger must be original as supplied by the boat builder originally.

The gears-box must allow the following manoeuvres: forward, reverse and idle with the engines running.

Class T1:

- length (LOA): from 7.51m. to 8.99 m.
- minimum inside cabin ceiling: 1.40m. high, measured throughout a continuous surface of 0.50 m² at least.
- maximum capacity of the engine(s): 4,750cc Outboard / 8,600cc Inboard

Class T 2

- length (LOA): from 9.00 m. to 11.99 m.
- minimum inside cabin ceiling: 1.60m. high, measured throughout a continuous surface of 0.75 m² at least.
- maximum capacity of the engine(s) : 12,000cc.

Class T 3

- length (LOA): from 12.00 m. to 15.00 m.
- minimum inside cabin ceiling: 1.80m. high, measured throughout a continuous surface of 1.5 m² at least.
- maximum capacity of the engine(s) : 18,000cc.

Sport category

It includes boats with all the general characteristics provided for Tourism category except for what specified below:

The beam width of the hull is not restricted.

Sport Category boats may also not be provided with cabin.

All engine models coming from the standard engine which have been modified in order to increase the performances for sporting activity, if included in the boat builder catalogue and reported in the craft approval certificate, are allowed in the sport category.

Class S 1

- length (LOA): from 7.51 m. to 8.99 m.
- maximum capacity of the engine(s) : Outboard 9,000cc. / Inboard 15,000cc.

Class S 2

- length (LOA): from 9.00 m. to 11.99 m.
- maximum capacity of the engine(s) : 21,000cc.

Class S 3

- length (LOA): from 12.00 m. to 15.00 m.
- maximum capacity of the engine(s) : 28,000cc.

Prototypes category

It includes all cruising vessels regularly homologated for such a purpose as a prototype with all the general characteristics provided for Sport category except for what is specified below:

The certified speed of the craft shall be of 50 knots minimum.

Craft can be fitted with a maximum of three engines.

Class P 1

- length (LOA) from 7.51 m. to 8 m.
- maximum capacity of the engine(s) : Outboard 11,000 cc. / Inboard 18,000cc.

Class P 2

- length (LOA) from 9.00 m. to 11.99 m.
- maximum capacity of the engine(s) : 25,000cc.

Class P 3

- length (LOA) from 12.00 m. to 15.00 m.
- maximum capacity of the engine(s) : 33,000cc.

Super Yacht Category

It includes cruising vessels regularly homologated for such a purpose including internal fittings complying with the specifications of the boat builders and with all the general characteristics provided for Tourism category except for what specified is below:

Belonging to Super Yacht category are all craft whose length is more than 15 m. and whose characteristics correspond to those provided by Tourism Category.

The number of engines is not restricted

When accommodation fittings are removed, the boat cannot be classified in this category.

Class Y

- length (LOA) : 15.01m. minimum
- minimum inside cabin ceiling: 1.80m. high, measured throughout a continuous surface of 1.50 m² at least.
- maximum capacity of the engine(s) : minimum 2 engines / maximum 4 engines
- The width of the craft must not be less than the product of the length measurement multiplied by a coefficient of 0.26

502.03 - GROUP “B”

RACE NUMBER

The race number shall be displayed on the topsides (port and starboard) of the hull within a dedicated area:

- The numbers on the topsides (port and starboard) shall be displayed on the forward half of the boat;
- Numbers and letter must be highlighted in black on a white background;

- The letter and number which identify the class shall be put in front of the race number;

The race number and its dedicated area shall have minimum dimensions as indicated below:

- Numbers and letter must have a thickness of no less than 50mm.

CATEGORIES AND CLASSES

4 Meters (Unique) Category

All pleasure boats with LOA from 4.00m. to 7.50m. fully conforming to the Boat Certification issued by an appropriate Technical Body, stating the dimensions of the craft and the installed engine capacities, may belong to this category according to the following:

Class U

- length (Lao): from 4.00m. to 7.50m.
- maximum engine capacity: 1,450cc. Outboard / 3,000cc. Inboard

Pleasure craft with engine capacities not less than 500cc. may participate in Endurance competitions of Group B.

If reported in the craft's certification, the powering can be made of two identical and independent engines.

Tourism Category

Production pleasure craft, including fittings, produced in series and in compliance with the specifications of the boat builders, belong to this category.

No modification is allowed in respect of the standard production of the model used that must correspond to the one reported on the approval certificate

Driving seats can be replaced

The beam width of the craft must not be less than the product of the length measurement multiplied by a coefficient of 0.26

Craft may be fitted with two engines if allowed by approval certificate.

All engine models coming from the standard engine which have been modified in order to increase the performances for sporting activity, even though included in the boat builders catalogue, are not permitted.

The gears-box must allow the following manoeuvres: forward, reverse and idle with the engine running.

Class T1

- length (LOA): up to 6.00 m.
- minimum weight: 700 Kg.
- maximum engine capacity: 2,100cc. and 4 cylinders - Outboard
4,300cc. and 6 cylinders - Inboard

Class T 2

- length (LOA): up to 6.85 m.
- minimum weight: 950 Kg.
- maximum engine capacity: 2,700cc. and 6 cylinders - Outboard
5,800cc. and 8 cylinders - Inboard

Class T 3

- length (LOA): up to 7.50 m.
- minimum weight: 1,300 Kg.
- maximum engine capacity: 4,000cc. and 8 cylinders - Outboard
7,500cc. and 8 cylinders - Inboard

Sport Category

It includes all the general characteristics provided for Tourism category except for what specified below:

The beam width of the hull is not restricted.

The units produced in series which have been modified in at least one of the following respects, belong to this category:

- in the deck structures through removal of parts, or by creation of a partial rigid bridge on prow;
- by removal of fittings (furniture);

All engine models coming from the standard engine which have been modified in order to increase the performances for sporting activity, if included in the boat builder catalogue and reported in the craft approval certificate, are allowed in the Sport category.

Tourism craft units equipped with engines exceeding the allowed capacity in their class, automatically enter Sport category.

Class S1

- length (LOA): up to 6.00 m.
- maximum engine capacity: 2,100cc. and 6 cylinders - Outboard
5,800cc. and 8 cylinders - Inboard

Class S2

- length (LOA): up to 6.85 m.
- maximum engine capacity: 2,700cc. and 6 cylinders - Outboard
7,500cc. and 8 cylinders - Inboard

Class S3

- length (LOA): up to 7.50 m.
- maximum engine capacity: 8,000cc. and 8 cylinders - Outboard
8,900cc. and 8 cylinders - Inboard

503 - AMERICAN PURSUIT COMPETITIONS

503.01

The American Pursuit competition is open only to pleasure craft duly homologated.

Craft competing shall be equipped with strictly standard engines and propellers. Craft shall be equipped with safety cut-off device. To pursuit competitions the following pleasure navigation categories may participate, each of them divided into classes according to their engine capacities.

503.02 - CATEGORIES AND CLASSES

- Inflatable with outboard;
- Inflatable with out/inboard;
- Boats with outboard;
- Boats with out/inboard and inboard;

Each of these categories is divided into classes as follows:

Inflatable with outboard

4 Stroke	2 stroke	Class
up to 550cc	up to 550cc	A
551cc - 1,000cc	551cc - 750cc	B
	751cc - 1,100cc	C
	1,101cc - 1,500cc	D
	1,501cc - 2,000cc	E
	2,001cc - 2,600cc	F
	2,601cc - 3,000cc	G
	3,001cc - 4,000cc	H

Inflatable with I.O.B.

4 Stroke	2 stroke	Class
	up to 1,350cc	A
	1,351cc - 5,700cc	B
	5,701cc - 7,450cc	C
	7,451cc - 8,200cc	D
	8,201cc - 10,000cc	E

Boats with outboard

4 Stroke	2 stroke	Class
up to 550cc	up to 550cc	A
551cc - 1,000cc	551cc - 750cc	B
	751cc - 1,100cc	C
	1,101cc - 1,500cc	D
	1,501cc - 2,000cc	E
	2,001cc - 2,600cc	F
	2,601cc - 3,000cc	G
	3,001cc - 4,000cc	H

Boats with I.O.B. or I.B.

4 Stroke	2 stroke	Class
	up to 1,350cc	A
	1,351cc - 5,700cc	B
	5,701cc - 7,450cc	C
	7,451cc - 8,200cc	D
	8,201cc - 10,000cc	E

504 - PARALLEL SLALOM COMPETITIONS**504.01**

The Parallel Slalom competition is open only to pleasure craft duly homologated.

Craft competing shall be equipped with strictly standard engines and propellers. Craft shall be equipped with safety cut-off device. For slalom competitions the following pleasure navigation categories may participate, each of them divided into classes according to their engine capacities :

504.02 - CATEGORIES AND CLASSES

- Inflatable with outboard;
- Inflatable with out/inboard;
- Boats with outboard;
- Boats with out/inboard and inboard;

Each of these categories is divided in classes as follows:

Inflatable with outboard

4 Stroke	2 stroke	Class
up to 550cc	up to 550cc	A
551cc - 1,000cc	551cc - 750cc	B
	751cc - 1,100cc	C
	1,101cc - 1,500cc	D

1,501cc - 2,000cc	E
2,001cc - 2,600cc	F
2,601cc - 3,000cc	G
3,001cc - 4,000cc	H

Inflatable with I.O.B.

4 Stroke	2 stroke	Class
	up to 1,350cc	A
	1,351cc - 5,700cc	B
	5,701cc - 7,450cc	C
	7,451cc - 8,200cc	D
	8,201cc - 10,000cc	E

Boats with outboard

4 Stroke	2 stroke	Class
up to 550cc	up to 550cc	A
551cc - 1,000cc	551cc - 750cc	B
	751cc - 1,100cc	C
	1,101cc - 1,500cc	D
	1,501cc - 2,000cc	E
	2,001cc - 2,600cc	F
	2,601cc - 3,000cc	G
	3,001cc - 4,000cc	H

Boats with I.O.B. or I.B.

4 Stroke	2 stroke	Class
	up to 1,350cc	A
	1,351cc - 5,700cc	B
	5,701cc - 7,450cc	C
	7,451cc - 8,200cc	D
	8,201cc - 10,000cc	E

505 - HISTORICAL BOATS

505.01- EVALUATION FORM FOR U.I.M.-ASDEC CERTIFICATION.

Points for the evaluation of Historical boats for award of the U.I.M./ASDEC Certificate or for inclusion in the Historical Naval Register shall be awarded on the following basis:

BOAT'S NATIONAL AUTHORITY		YEAR OF CONSTRUCTION	
KEEL NUMBER			
BOATBUILDER	MODEL	ENGINE MANUFACTURER	MODEL
SHIPOWNER		ENTRY NUMBER	

Maximum score 70 points. (the maximum score of the three Scrutineers is 210 points).

The boats that do not get the minimum score in every category, obtained by adding the points assigned by the three Scrutineers (minimum of: 12 points for Age, 6 points for History, 3 points for Importance, 18 points for Originality, 18 points for Maintenance, 3 points for Aesthetics) are considered Historical Aspirant.

505.01 - AGE

The score will be given in proportion to the documented age of the boat

Guide Index from 1 to 10 points, with a maximum increase to 20 points.

Up to 25 years old (classic boats)	points 1
<ul style="list-style-type: none"> ■ Boats less than 25 years old but on designs of more than 25 years old 	
(Total rebuild or copy)	points 2
<ul style="list-style-type: none"> ■ Boats less than 25 years old, belonging to a series with more than 25 years of production 	
<ul style="list-style-type: none"> ■ Boats more than 25 years old, built after 1st January 1970 	points 4
<ul style="list-style-type: none"> ■ Built between 1960 and 1969 	points 5

- Built between 1950 and 1959 points 6
- Built between 1940 and 1949 points 7
- Built between 1930 and 1939 points 8
- Built between 1920 and 1929 points 9
- Built before 1920 points 10

Total scored points by :

- Scrutineer 1 :
- Scrutineer 2 :
- Scrutineer 3 :

If reliable age: scored points x 1.5

If documented age : scored points x 2.0

Total Points :

When the established age of the build is more than 25 years, the score is doubled. Nevertheless if there are substantial external modifications (proportional to the size of the boat) effected in the last 25 years, the score may be increased by 50% with a rounding off to the greater whole point. In the absence of official documents or photos in dated publications, the age may be estimated from objective elements (engine, design of the boat, structural particulars etc...) without going back to the precise year, and the score based on the estimated age may be increased by a maximum of 50%.

502.02 - HISTORY

Judged on the production of the documents showing the boat's history and the nautical competitions in which it has competed. The authenticity and the rarity of the documents produced shall also be taken into consideration.

Guide Index Maximum 10 points

- Official documents (Lloyd's Registry, navigation's license Measurement certificate etc...) ; points 1
- Books in which are reproduced the boat and its history; points 1
(copy of the pages)

- Books concerning the boatbuilder's history (copy of the summary); points 1
- Press articles concerning the boat, the mode or the Boatbuilder; points 1
- Unpublished news, written and documented on the history of the boat or of the yard; points 1
- Photographic or filmed documentation of the launch or the building in the yard; points 1
- Photographic documentation of the boat engaged with activity and meetings; points 1
- Photographic documentation of restoration or maintenance; points 1
- structural plans, specific of building, and unpublished documents; points 1
- Operating and maintenance booklets and documents of the Period and contemporary journalism writing on current affairs; points 1

Total scored points by

- Scrutineer 1 :
- Scrutineer 2 :
- Scrutineer 3 :

Total Points :

505.03 - IMPORTANCE

Judged on the uniqueness, rarity and peculiarity of the history, of technology, of success, etc. of the boat.

GUIDE INDEX Maximum Points 10

- Same family or owner points 1
- Options:
 - Of international famous owners points 2
 - Of well-known owners at National level points 1

- For having participated to competitions, meetings raids, war events points 1
- Famous designer points 1
- Famous Boat builder points 1
- Monotype or rare example of craftmade Boat builder unknown or not much known, or exclusive model of famous Boat builder the first or last current model of famous Boat builder, or rare example of imitated series of a famous Boat builder points 1
- Important for technique or aesthetics and constructional innovations points 1
- Important for success of sale points 1
- Important for success of collection points 1

Total scored points by

- Scrutineer 1 :
- Scrutineer 2 :
- Scrutineer 3 :

Total points :

505.04 - ORIGINALITY

Judged on the authenticity of the boat and its constituent elements.

Guide Index Maximum points 10.

Open small boats with mixed propulsion, inboard and oars or engine sail

- Options:
 - Original plan not modified; points 2
 - Original plan modified or updated more than 25 years ago by the same designer or yard; points 1
- Options:
 - Boat's materials and designed superstructures never replaced (excluding the replacement of the bottom planking, for safety reasons); points 2

- Boat's materials and of designed superstructures with invisible partial replacement; points 1
- Options:
 - Original engine; points 3
 - Engine of similar type; points 1
 - System of restoration and painting;
 - Original windscreen, porthole and glasses centring, awnings, benches and dunnages;. points 1
 - Original mast and rudder, tank and battery close to the original.
 - Sails, halyards, shrouds, sheets, rigging, blocks, oars & rowlocks; points 1
 - Tiller, winch, dashboard. Boats internal visible upholstery; points 1
 - Deck equipment, navigation and metallic fittings; points 1

Total scored points by

- Scrutineer 1 :
- Scrutineer 2 :
- Scrutineer 3 :

Total points :

Runabouts and day cruisers(with deck) inboard and outboard.

- Options:
 - Original plan not modified; points 2
 - Original plan modified or updated more than 25 years ago by the same designer or yard; points 1
- Options:
 - Boat's materials and designed superstructures never replaced (excluding the replacement of the bottom planking, for safety reasons); points 2
 - Boat's materials and of designed superstructures with invisible partial replacement; points 1

- Options:
Original engine and transmission. Rudder, tank, battery, tubes and original plants close to the original; points 2
Engine of similar type; points 1
- System of restoration and painting. (materials and colour); points 1
- Original windscreen, porthole, skylights, dashboard, instruments, wheel, hand drives, and deck equipment and glasses centring, awnings, benches and dunnages;. points 1
- Awnings, hoods, original seat or similar to original; points 2
- Options: Aped as the boatbuilder updatings; points 1

Total scored points by

- Scrutineer 1 :
- Scrutineer 2 :
- Scrutineer 3 :

Total Points:

Cabin cruiser and motor yacht

- Options:
Original plan not modified; points 2
Original plan modified or updated more than 25 years ago by the same designer or yard; points 1
- Boat's materials and designed superstructures never replaced or imperceptible partial replacement; points 1
- System of restoration and painting. (materials and colour); points 1
- Original windscreen, porthole, skylights ,dashboard, instruments, wheel, hand drives; points 1
- Original mast and deck equipment or like the original; points 1
- Furnishing and tapestry suit to the period; points 1
- Original internal division into compartments (possible limited Modifications functional to internal division inside are tolerated); points 1

- Wind glasses, wind sleeves, gangways, and accessories of the period; points 1

Total scored points by

- Scrutineer 1 :
- Scrutineer 2 :
- Scrutineer 3 :

Total Points:

Workboats and military craft

Guide Index Maximum 10 points:

- Options:
 - Without modifications to the original plan and with no external structural or behavioural modifications; points 2
 - Limited internal modifications; points 1
- Boat, deck and deckhouse with matching or original materials; points 1
- System of restoration, painting and matching colours; points 1
- Mast, tackles, heating appliances and original instruments; points 1
- Options:

Engine and original mechanical system; points 2

Close to the original; points 1

- Original Machine room, bridge and bathrooms; points 1
- Furnishing and upholstery of cabins and wardroom

in the style of the period; points 1

Total scored points by

- Scrutineer 1 :
- Scrutineer 2 :
- Scrutineer 3 :

Total Points:

Racing Boats

- Boat without aesthetic modifications; points 1
- Boat with original materials (excluding minimal restoration); points 1
- Original deck, windscreen, fairing and cockpit; points 1
- Original System of restoration, painting and matching colours; points 1
- Options:

Original engine; points 3

Similar engine; points 1

- Tank, wheel, rudder, dashboard, hand drives and instruments original and matching; points 1
- Battery, tubes, deck's equipping and carpentry close to the original; points 1
- Original seats and ceiling; points 1

Total scored points by

- Scrutineer 1 :
- Scrutineer 2 :
- Scrutineer 3 :

Total Points:

505.05 - MAINTENANCE

Judged on efficiency, general condition of hull and deck materials and paintings, of upholstery and furniture, operation and appearance of engines and fittings independently of their originality.

Guide Index Maximum Points 10

- Options
Materials of construction as original or painted without serious flaws or disfiguring spots (small imperfections reflecting the age of the material are allowed); points 3

- Options
 - as above but with partial restoration; points 2
 - as above but with partial replacements; points 1
- Painting or antifouling of the hull, painting of the upperworks in very good condition; points 1
- Options:
 - Paintings (or polishing) of hull and superstructures in very good condition; points 2
 - in good or fairly good condition; points 1
- Paintings and cleaning of bilges in very good conditions, paintings and upholstery of the interior well preserved; points 1
- Chroming or cleaning of deck fittings; points 1
- Maintenance of engines, fittings and engine room; points 1
- Patina of general use, internal and external, enchanting (charm due to the level of maintenance and not to recent restoration); points 1

Total scored points by

- Scrutineer 1 :
- Scrutineer 2 :
- Scrutineer 3 :

Total Points:

505.06 - BEAUTY

Judge on beauty and charm, proportions, elegance in shape, materials and colours; (changes provided over time do not affect beauty if well designed and built).

Guide Index Maximum 10 Points

- Options:
 - Harmonious external lines, refined and charming; points 4
 - Harmonious and pleasant external lines; points 3
 - Essential and balanced external lines; points 2
 - External lines without homogeneous design, but acceptable ; points 1

- Exterior colours and look of elegant materials; points 1
- Options:
 - Inside with organic and refined design of furniture in the style of the boat; points 2
 - Spartan but sailor-like inside; points 1
- Inside: colours and finishing materials with charming look; points 1
- set of period accessories, if contributing to the charm ; points 1
- design level, realisation and finishing of the inside and outside; points 1

Total scored points by

- Scrutineer 1 :
- Scrutineer 2 :
- Scrutineer 3 :

Total Points:

TOTAL SCORE OF THE THREE SCRUTINEERS / /

Date _____ Place _____

Scrutineers Name and signature

1. - _____

2. - _____

3. - _____

600 - RECORDS AND PERFORMANCES

600.01 - GENERAL

All record trials and their control subscribe to the following rules.

The record belongs personally to the driver who has established or beaten it. The record is entered under the name and nationality of the driver.

Any race against the clock, and called kilometre or mile trials or any similar name is prohibited if not conducted in accordance with these rules.

Combining speed records with distance records is not allowed.

No direct or indirect aid is to be provided to the driver who attempts a record.

600.02

World speed records, hour records, distance records and competition records are all records open to all boats and aquabikes of international classes adopted by the U.I.M.

600.03

National records recognised by U.I.M. are the records established by boats of National classes duly recognised by the National authority (the rules of which have been approved by the U.I.M.). The Certificate differs from those issued for World Records.

600.04

The World's Unrestricted Water Speed Record is awarded to the fastest boat in the world piloted by the driver, irrespective of the class of boat; this record is established in conformity with the U.I.M. rules.

601 - CLASSES ELIGIBLE FOR RECORDS

601.01

World records may be attempted with the following craft :

- All U.I.M. series and classes;
- American Powerboat Association classes;
- Prototypes;

601.02

A boat can only establish a record in its own class.

601.03

When a class is modified so as to reduce the performance of the class, for example by :

- reducing the cylinder capacity;
- restricting the tuning permitted;
- restricting fuel permitted;
- introducing a minimum weight;
- increasing the minimum weight;
- or any other means;
- the records established before the modification comes into force will be frozen and a new series of records will be started;

All proposals for new rules, rule changes and U.I.M. General Assembly minutes shall state :

- whether or not records will be frozen;
- the classes to be frozen;
- the date(s) when the classes are to be frozen;.

601.04 - QUALIFICATION

For offshore records, a boat must have completed the course, taken the chequered flag and finished first, second or third, in a U.I.M. World or U.I.M. Continental Championship race in the class to be attempted, and after the date when the class was last frozen.

602 - DEFINITION OF UNITS

602.01 - TIME, DISTANCE AND SPEEDS

The hour is the twenty-fourth part of a terrestrial day. The symbol is letter "h".

The minute of time is the sixtieth part of one hour. The symbol is letters "mn";

The second of time is the sixtieth part of one minute. The symbol is the letter "s".

Nautical mile	=	nm
statute mile	=	st.m
kilometre	=	km
Nautical mile per hour	=	knot
statute mile per hour	=	mph
kilometre per hour	=	kmh

602.02 - STATUTE MILE, NAUTICAL MILE AND KILOMETRE

The statute mile is an Anglo-Saxon measure.

The Nautical mile is used at sea.

The knot is not a unit of length; it expresses the speed of one nautical mile per hour.

One statute mile (st.m) = 5280 feet = 0.8690 nm = 1,609.3 km

One nautical mile (nm) = 6080 feet = 1,852 km = 1.1508 st.m

One kilometre = 3280.844 feet = 1000 metres

604 - BASES (THE COURSE)

604.01

Records may only be established on bases recognised by National Authorities and notified to the U.I.M. in accordance with these rules.

604.02

The base is measured and certified by an official surveyor holding a certificate of competence from a recognised institute, in the presence of a delegate of the National Authority.

The base must be measured by triangulation or electronic distance measuring equipment (EDM), which must have a first class certificate delivered by an Observatory or similar authority. This certificate must not be older than two years.

The official surveyor provides a certificate in duplicate in which the strict adherence to the above requirements is certified.

The certificate is signed by the surveyor and countersigned by the delegate of the National Authority. One of the copies is for retention by the National Authority for its special National records files and the other is sent to U.I.M. for its special World record files.

The base for a record must be a fixed base, as laid out for a National Authority, or must be measured for a special occasion in waters designated by the driver. It must be defined by fixed marks ashore.

604.03

No distinction is made between records established on the sea, rivers or lakes. Records must not be attempted during the period between half an hour before and half an hour after a change in direction in flow of water over the course (change of tidal flow).

604.04

Any expenses incurred in the laying of a temporary course are defrayed by the entrant attempting the record.

604.05

For speed records there shall be only one attempt on the course at any one time. An attempt must not start until the Officer Of the Day of the trial is satisfied that effective rescue boats are on the course.

605 - CALCULATION OF SPEED**605.01**

The National Authority calculates the speed from the data provided by timekeepers. The speed is to be shown in kilometres per hour (kph) and statute miles per hour (mph), even when the attempt takes place on a course of one nautical mile.

The duration of each run is obtained by taking the arithmetical mean of the times recorded by two sets of timing devices, the mean time being given to one tenth of a second.

Should one of the timing devices stop or one of the signals fail and only one duration be registered, this duration will be accepted.

605.02

The only duration so obtained serve to calculate the speed of each run. Only two decimal places are to be used.

605.03

The speed of the trial is the arithmetical mean of the speeds, two decimal places being used.

605.04

For speed records established on a distance of one nautical mile (1 nm = 1.852 km = 1.1508 st.m) the following constants are to be used :

Where t is the time in seconds :

the speed in mph = $4142.9 \div t$

the speed in kph = $6667.2 \div t$

606 - MARGIN REQUIRED

A record will be valid only if its speed at least equals the speed of the previous record multiplied by 1.003.

607 - MOTORS

607.01

All accessories and/or elements required by the racing rules for each class of boat attempting to establish or improve a record must be on board during said attempt.

607.02

A boat which holds two certificates for two different classes is entitled to establish World records in those two classes, but shall make two distinct trials, one for each class.

607.03

Immediately after the record trial, the motor(s) or engine(s) must be sealed with a distinctive seal in the presence of the officials who have witnessed the trial.

607.04

Within 48 hours of the trial, two measurers must check that the hulls, motors and equipment are in conformity with the specifications of their class, and this inspection must be carried out in the presence of the Officer of the Day or an Official specifically appointed by the National Authority.

607.05

When attempting to break a U.I.M. class record, the noise level of the motor shall not exceed the authorised level (rule 735.14).

608 - REQUEST TO ORGANISE

608.01

All applications for records trials must be made in writing to the National Authority at least one week before the trial and must be accompanied by the fee stipulated by the National Authority.

The National Authority applies to the U.I.M. for homologation of records.

608.02

Only records established under the direct control of a National Authority affiliated to the U.I.M. can be recognised.

608.03

Each National Authority decides on the application for records to be established on its territory. The National Authority may refuse to consider a record application, but must then explain the reasons in writing.

The National Authority indicates the validity of the record permit.

608.04

It is the duty of the driver to accept responsibility for any safety measures deemed necessary for this attempt.

The Officials will not accept any responsibility, they will only record the results and ensure that the rules are complied with.

609 - OFFICIALS

609.01

The National Authority shall appoint an Official Observer and other duly qualified persons to hold the official functions.

An Official so nominated shall not hold any other office than that to which he was nominated. An Official shall not himself establish a record.

609.02

The Official Timekeepers and Officials may receive payment for their services at rates fixed by the National Authority.

609.03

Time keeping may only be carried out by Timekeepers and Officials who have been recognised and authorised by the appropriate National Authority.

The stop-watches or any other apparatus employed must have a first-class certificate delivered by an observatory or similar authority. This certificate must not be older than two years.

Each Timekeeper hands to the National Authority a signed report of the record attempt, countersigned by the Official Observer.

609.04

Timing from aboard anchored boats or aboard competing boats is not allowed.

610 - SPEED RECORDS**610.01**

There is only one World record per class. This record can be established over the nautical mile, Statute mile, or the kilometre. For aquabikes the distance may be 500 metres.

610.02

The course is covered once in each direction consecutively.

The course is to be marked at either end by two posts in solid material, put up on shore with approved distinguishing marks affixed to them. The two lines uniting the two pairs of posts are parallel to each other and at right angles to the line of the measured course. Steering marks may be placed wherever needed.

90° 1 nautical mile 90°
 1 kilometre
 1 statute mile

610.03

In wide rivers and lakes, the width of the course may be marked off at each end by two buoys, moored at a distance of one hundred metres from each other. The boats must run between these buoys.

610.04

Timing is to be one tenth of a second up to 200 kph. Above 200 kph timing is to be by photo-electrical or similar apparatus to one hundredth of a second.

There must be a Timekeeper and a Timekeeper Observer at each position.

Individual timers may be used at each position or a single timer electrically activated from each position.

Where two timers are used at a position the time is the average of the two timers.

The times of day of each attempt must be recorded.

The time elapsing between the two runs must not exceed 20 minutes except for jet attempts, when the permitted interval is one hour.

610.05

A driver may re-start an attempt because of breakdown or other reason and may stop the engine or boat between runs.

If the attempt has not been completed within four hours of a starting time fixed by the National Authority, the Officer Of the Day may stop the trial.

611 - DISTANCE RECORDS

611.01

Distance records are established on a total course of twelve nautical miles for the outboard series, twenty-four nautical miles for other series, and six nautical miles for aquabikes.

611.02

In distance records, the base is covered up and down successively in even numbers and the turns are not included in the distance.

The course for distance records may be :

- a course established for speed records;

- a base of three nautical miles maximum, in straight lines with buoys placed at the corners if necessary, no corner to be less than 135 degrees.

The buoys placed at the corners must always be rounded.

611.03

Any base must be defined by two posts set in concrete at either end.

611.04

Timing is to be to one tenth of a second.

The timing of distance records is done by a Timekeeper with two timing devices at the starting line. He is assisted by two Officials, one with him and one at the other end.

611.05

Stopping the motor or the boat during a distance record attempt is prohibited.

612 - HOUR RECORDS

612.01

The following International and National records are recognised by the U.I.M : 1, 2, 3, 4, 6, 9, 12 and 24 hours duration records for all the series recognised by the U.I.M.

The homologation of a record of lesser duration than that applied for may be granted, but only within such limits as provided for by the above rule.

612.02

The base must not exceed six nautical miles from station to station, that is to say twelve nautical miles up and down. For aquabikes only, the distance may be 3 nautical miles from station to station, that is to say 6 nautical miles up and down.

612.03

The base is defined by two posts set in concrete at either end.

612.04

The boat may be driven by more than one driver, but their name must be filed in the record application.

612.05

Repairs and adjustments which can be carried out on board may be effected on the course.

The same engine(s) and hull must be used throughout the trial.

On the other hand, repairs that cannot be effected on board and refuelling and changing of drivers must be effected at the control station and are to be supervised by an Official.

612.06

Any time during which a boat stops during an attempt is included in the elapsed time for the attempt.

Turns are included in the course and are consequently not excluded from the timing.

612.07

Timing is carried out by one or several Timekeepers with two timing devices stationed at the start. The Timekeeper is assisted by two Officials, one stationed with him and one stationed at the other end.

612.08

A driver can claim several different hour records during one trial by selecting the best series of consecutive laps.

612.09

The boat must cross the finishing line at the end of the lap (the last lap) during which the record time has elapsed.

The average speed of the last lap is calculated and the distance which corresponds to the time still to be run is added to the distance covered at the end of the penultimate lap.

The result is shown as the distance covered in the record time and the average speed for that distance.

612.10

Example for a two hour record on a base of six nautical miles (nm), twelve nautical miles per lap :

boat completes 8 laps	in 1 h 54 mn 40 s	= 110.48 st.m
ninth lap completed	in 13 mn 50 s	= 830 s
time balance	5 mn 20 s	= 320 s
distance balance	= 13,81 st.m ÷ 830x320	= 5.32 st.m
Total distance in 2 h	= 115.80 st.m	
Average speed	= 115.80 ÷ 2	= 57.90 mph

613 - WORLD COMPETITION RECORDS

613.01

The U.I.M. recognises World records of 5, 10 and 15 statute miles in races for all U.I.M. circuit classes.

No other records may be established during a race.

Unlimited hydroplanes only are entitled to statute mile records established on :

- a single lap course (two laps for APBA Gold Cup races) while qualifying or racing ;
- total heat average speed while racing;

614 - PERFORMANCE CERTIFICATES

614.01

The U.I.M. may issue Performance Certificates for the best heats made during races for the principal International trophies.

614.02

Application for these is to be made by the National Authority of the Country where the race took place and the accuracy of the results, timing, course and class is to be certified by them.

614.03

The U.I.M. only recognise the results which improve upon previous performances.

614.04

The amount of the fee for a Performance Certificate is decided upon annually by the General Assembly.

614.05

The U.I.M. may issue a Performance Certificate for speed trials made by boats not included in U.I.M. classes. These trials must, however, be made in accordance with U.I.M. rules.

The U.I.M. award Performance Certificates for boats of the Production Boat Series having established or broken speed, duration, distance or competition records according to the Record Rules.

614.06

For these performances, it is not permissible to remove any of the boat's accessories, or items of comfort, provided by the builder and these are compulsory for racing.

The U.I.M. may award Performance Certificates for any of the offshore classes, International and National, established at sea on properly measured runs.

615 - HOMOLOGATION OF RECORDS

615.01

Only records homologated by the U.I.M. are considered as World Records.

Only records established under the direct control of a National Authority affiliated to the U.I.M. can be recognised.

615.02

If a record has been established on the territory of two National Authorities, it will be entered as a record by the National Authority on whose territory the start was made.

As soon as a record trial has taken place, the National Authority shall communicate the results to the Secretary General of the U.I.M. by fax and confirm it by letter.

The information has no official character and is only transmitted to such National Authorities as apply for it, in order to avoid useless trials.

615.04

The Timekeepers and Measurers must complete the official U.I.M. record application form, which must be signed as correct by the Senior attempt Official (President, Chairman, Officer Of the Day etc.) and be sent to the National Authority within three days of the trial, (seven days for APBA controlled classes).

The National Authority transmits the application for homologation, together with other necessary papers, to the U.I.M. no later than four weeks after the trial, (sixteen weeks for APBA controlled classes).

615.05

For a request for homologation to be considered by the U.I.M., the National Authority must send :

- a plan of the course if not already in possession of the U.I.M.;
- the completed official U.I.M. application form which must include any National Authority calculations;
- a copy of the boat's measurement certificate;
- a photograph of the boat under way with the colours, inscriptions, etc. noted on the reverse ; also to be noted on the reverse, the driver's name, class and speed achieved;
- a list of the full names of the Officials with, at least, the names of the Timekeepers, Measurers, Rescue Officer, Officer Of the Day and Observer;
- offshore boats, a copy of the results in which the boat qualified, as per U.I.M. rule 601.04;
- for attempts on tidal waters, the calculation stating the times of the changes of direction of water along the course (changes in tidal flow) and the source of the information;
- a cheque for the amount due;

615.06

The recognition fee is fixed annually by the General Assembly.

615.07

Pending the official confirmation by the U.I.M., neither the driver, builder, nor any other party shall publish or cause to be published, circulate or cause to be circulated, the results of the trial, until the delegate of the National Authority has given the actual results in writing, after checking the calculations in the presence of Timekeepers.

Even when such authorisation has been received, results may only be published with the addition of the words "Subject to official ratification by the U.I.M. in as large and prominent type as that in which any part of the notification itself is printed.

Neglect to comply with the above regulations automatically entails the refusal to recognise the record. The National Authority has the right to penalise whoever is responsible.

615.08

The U.I.M. issues annually a list of World records, changes being made known, when they occur, by means of newsletters.

The U.I.M. keeps a registered list of all World records and delivers a certificate for each new record.

616 - U.I.M. RULES FOR LONG DISTANCE **OFFSHORE MOTORBOAT** **WORLD SPEED RECORDS**

General - This edition of the rules supersedes the existing rules.

616.01 - AUTHORITY

The U.I.M. through the relevant National Authority is the authority in all matters concerning speed records. It co-operates with National motorboat authorities as appropriate but is also able to examine and ratify record claims made by individuals and to approve attempts whether or not they fall within the jurisdiction of a national motorboat authority.

616.02 - RATIONAL

Records and races

Elapsed times achieved in motorboat races organised by clubs which are recognised by a National motorboat authority are accepted as correct by the U.I.M./relevant National Authority. The shortest time, without handicap, is the record for an event. A record achieved during a race is listed as a race record and an outright record. If, however, an individual performance over the same course betters a race and outright record, it will only be listed as an outright record.

Records will be recognised for passage between pre-determined points providing all relevant rules for independent observation are established, maintained and ratified.

Racing rules

The U.I.M. racing rules do not apply during individual (non-race) record attempts.

616.03 - GENERAL

The purpose of the rules is to enable attempts to be made on a variety of offshore records. They do not form the conditions of any prize or trophy, nor are they motorboat instructions.

The record for each passage shall stand until the elapsed time of that record has been reduced by at least one minute. Every request to ratify a record shall be considered in time and date order.

The time used for this purpose will be Greenwich Mean Time.(GMT)

The U.I.M./relevant National Authority cannot accept any responsibility for personal injury or loss of life, damage to or loss of property arising from any record attempt made under its rules whether supervised by its officials or not.

The official language is English.

616.04 - LONG DISTANCE OFFSHORE WORLD RECORDS

The World Record for the passages listed in rule 10 will be held by the motorboat and skipper which establishes the shortest elapsed time in accordance with these rules.

Any offshore record can be held in either of the following categories:

- Any vessel less than 50 m LOA, with any number of crew.
- It is not allowed to do any records single-handed that require duration longer than 6 hours.

All records are to be divided into three outright classes, up to 30ft/9.14 metres LOA, over 30ft/9.14 metres LOA and up to 50ft/15.24 metres LOA and over 50ft/15.24 metres LOA, to be measured using the method for Offshore Class I. Also U.I.M. Classes, when all class racing rules must be adhered to.

"Without assistance" - means that a vessel may not receive any kind of outside assistance or take on board any fuel supplies, materials or equipment during a record attempt. A craft may be anchored or beached during the record attempt but any repairs must be made entirely by the crew without outside resources or materials.

"With assistance" - means a vessel may enter harbour for fuel, repairs, alterations or stores as required. Crew, excepting the skipper, may be changed. During any stops, the timing of the voyage continues.

It is never permitted to take on board fuel stores or equipment or get any other kind of help from another vessel whilst under way (except as permitted by 616.05 6 Emergencies).

616.05 - MOTORBOAT RULES

Applies to all long distance offshore records and all types of vessel.

Propulsion - any method of mechanical force.

Steering - any type of automatic steering is allowed.

Navigation - any type of navigation equipment is allowed and there is no objection to any type of information or advice being transmitted to or from the vessel. However, the vessel must be driven and navigated by the onboard crew.

Emergencies:

In the event of the record attempt being diverted or compromised by the necessity to render assistance to a mariner in distress (excluding members of the record crew), time allowances may be made subject to the provision of adequate documentary and substantive evidence of time lost.

In the event of fouling another vessel or manned structure (e.g. an oil rig) it is permitted for the crew of the other vessel or structure to assist the vessel in getting clear.

Rounding - when a course calls for a mark, beacon or point of land to be 'rounded', this means that an imaginary string from the start to the finish points must enclose the feature specified when pulled tight.

Tidal stream or current - no allowance is made for tidal stream or current in considering any of these records.

Refuelling - must not be done by another vessel whilst at sea.
Recognised fuel barges within harbour confines are acceptable.

Responsibility - It is the sole and inescapable responsibility of the skipper of a motorboat engaged in any such record attempt to decide whether to start and, having done so whether to continue the voyage.

The Vessel - shall have communication equipment capable of maintaining contact throughout the attempt with monitoring stations along the route.

Life-raft - a certified life-raft or life-rafts suitable for the maximum number of persons on board shall be carried along with the full safety equipment appropriate for the planned passage.

Tow - when a tow is received, the attempt shall return to the last official recorded "own power" position before resuming. If a "without assistance" boat receives a tow the attempt then fails or becomes "with assistance."

616.06 - SAFETY RULES

It is not within the province of these rules to set safety standards. However, all vessels making record attempts must comply with the appropriate safety standards laid down by International Maritime Law and by their Country of origin or registration, and of any race or trophy in which they may be competing, or ratification will be refused.

In the case of a circumnavigation attempt, the challenge vessel shall be built to comply with the rules for classification of an International Maritime Organisation (I.M.O.) recognised Classification Society and as such, be of suitable class for the range and speed of the vessel. In addition, the vessel shall be certified by the Statutory Regulating Authority of the Country of registry, which, as a minimum, shall be equivalent to the UK Maritime & Coastguard Agency (M.S.A.) Code of Practice for Safety of Large Commercial Sailing and Motor Vessels.

Evidence of compliance must be provided to the relevant National Authority prior to departure.

616.07 - NOTICE

At least one month's notice be given of intended record attempts, accompanied by the appropriate fee, otherwise the relevant National Authority cannot guarantee to observe a record. Records cannot be recognised retrospectively.

616.08 - OBSERVATION / DECLARATION

When proper notice of a record attempt has been received, the relevant National Authority will appoint an observer to make arrangements for

the start and finish to be witnessed and timed. The observer or a person appointed by him, will also need to check that the documentary evidence provided relates to the vessel that has completed the record passage. The skipper and one other member of the crew will be asked to sign a declaration that all the rules have been followed. Where practicable, there is no objection to the observer being on board during the record attempt.

616.09 - TIMING

The moment of starting and finishing a record passage must be recorded to the nearest second using GMT. The accuracy of these must be checked within six hours of the start and finish of the attempt by reference to an official time signal by telephone or radio. Details of the timepiece used must be logged and signed for by the observer or his appointee.

Records will always be defined as an elapsed time in days, hours, minutes and seconds. Average speed may be shown for information only.

616.10 - RECORD COURSES

General

Only fixed points of land, fixed navigational beacons or other charted fixed objects can be used as reference points though they need not necessarily be sighted. In special cases, pre-approved by the Observer, such as positions derived from satellite navigation systems are acceptable.

All routes are reversible and the record time from A to B can be beaten by an attempt from B to A.

If a new route is requested then an application from a National Authority to the U.I.M. is required; provided that the new route does not conflict with a very similar existing route it will be approved. The application should detail any restrictions and the mileage.

The start and finish positions if A to B must be given.

When a tow or other outside assistance is obtained, the attempt shall return to the last officially recorded "own power" position before resuming.

Around the World (616.04 – second paragraph applies)

To make motorised passage around the World, eastbound or westbound, a vessel must start from and return to the same point, must cross all meridians of longitude, and must pass through the Suez and Panama Canals.

Further records may be applied for and set between ports of passage or recognised points if they are contained within an overall record for circumnavigation. Full details of the proposed passages must be submitted with the application.

Transatlantic - New York to the Lizard: (616.04 – second paragraph applies)

Start by crossing a line drawn due south (true) from the highest point of the Ambrose Light Tower (40 27.53N, 73 49.88W). The finish is a line due south (true) from the highest point of Lizard Point Lighthouse (4957.58N, 05 15.07W). Pass within 1.5 nautical miles of each of these points.

London - Monte Carlo: (616.04 – second paragraph applies)

The distance is 2392 statute miles/3850 kilometres.

The Start and Finish line at London to be between Royal Pier, Gravesend and Tilbury Fort and the Start and Finish line at Monte Carlo to be due east of the Yacht Club de Monte Carlo.

Round Great Britain: (616.04 – second paragraph applies)

The distance round the mainland to be used for calculating the speed, as determined by the Royal Yachting Association (R.Y.A.)/Admiralty, is 2524 kilometres/1568.3 statute miles.

The position of the Start and Finish Line to be determined by the applicant who will be responsible for making proper application to the R.Y.A. for the attempt.

Poole - Cherbourg:

The distance to be used for calculating the speed, as determined by the R.Y.A., is 115.806 kilometres/71.958 statute miles.

The Start/Finish line at Poole is between the Branksome Chine Outfall Buoy, 50° 42.21 'N, 01° 54.23 'W, to the brick building on the beach, all boats must pass to the north of the Branksome Chine Outfall buoy. The Start/Finish line at Cherbourg is the midpoint (49° 40.39 'N, 01° 39.42 'W) of the transit line between the breakwater ends of the Western entrance to Cherbourg outer harbour.

Round the Isle of Wight: (616.04. – 2nd paragraph) applies for over 5 laps and for 6 hours and over)

The distance round the island to be used for calculating the speed, as determined by the R.Y.A., is 88.512 kilometres/55 statute miles.

The Royal Yacht Squadron flagpole to the South Bramble buoy is the Start and Finish Line, or other line pre-approved by the R.Y.A..

Attempts must not clash with other sail or powerboat racing events and approval of timings must be obtained from the R.Y.A.

A R.Y.A. approved official shall be responsible for all Timekeeping arrangements, for keeping the Secretary of the Royal Yacht Squadron fully informed and shall receive and transmit all relevant papers. The Timekeeping details to be R.Y.A. approved seven (7) days before the start.

It is acceptable to do single lap, multiples of 5,10,15, 20 or so on laps. Records are acceptable for 6 hours, 12 hours 18 hours and 24 hours around the course, these will be timed as a fond record according to the calculations in the 611 series.

Marseilles to Carthage: (616.04 – second paragraph applies)

Start by crossing a line drawn 090 (true) from Pomegues Lighthouse (43 15.7N, 5 17.4E). The finish is a line drawn 090 (true) from Ras Quartajamah Lighthouse (Cape Carthage, near Sidi Bou Said, 36 52.3, 10 20.9E). Pass within 3 miles of each of these points.

Round Italy - Venice to Monte Carlo (616.04 – second paragraph applies)

Start by Lido di Venezia in front of the Excelsior hotel.

Finish line at Monte Carlo to be east of the Yacht Club de Monaco.

616.11 - FEES

The skipper or organisation wishing to make an attempt on a record is responsible for the costs of observing and ratifying it.

These must be paid in two parts as follows:

- On making an official application to the relevant National Authority for a record attempt to be observed, a fee of 762 € required.
- If attempt is unsuccessful then 305 € will be refunded, if successful the 305 € will be forwarded to the U.I.M. for the ratification fee.

616.12 - EXPENSES

The person or organisation making the record attempt is responsible for the legitimate expenses and remuneration of the official observer and any assistants. This must be paid before any record claim can be considered.

616.13 - RATIFICATION

Pending official confirmation by the U.I.M., neither the driver, builder nor any other shall publish or cause to be published, circulate or cause to be circulated, the results of the trial, until the delegate of the National Authority has given the actual results in writing, after checking the calculations in the presence of timekeepers.

Even when such authorisation has been received, results may only be published with the addition of the words "Subject to official ratification by the U.I.M." in as large and prominent type as that in which any part of the notification itself is printed.

Neglect to comply with the above regulations automatically entails the

refusal to recognise the record. The National Authority has the right to penalise whoever is responsible.

A request for ratification must reach the relevant National Authority within 14 days of the attempt being completed.

The following documents are to be submitted to the U.I.M./relevant National Authority; the submission may be by fax if followed up by the original documents.

- A statement by the skipper giving details of the vessel, the crew and the course followed. This must include a declaration that these rules have been followed, signed by the skipper and at least one other member of the crew.
- A report by the Official Observer giving details of the timing and verifying the accuracy of all observations.

616.14 - INTERPRETATION

In case of any dispute over any of these rules, the decision of the relevant National Authority is final.

GROUP 700

CUPS AND TROPHIES

700.01 - INTERNATIONAL CUPS

The National Authorities must send to the Secretary of the U.I.M. a copy of their international Cup Rules, Year Books, official Statutes and other publications. These documents will be held for reference by all National Authorities.

701 - PAVILLON D'OR

THE "PAVILLON D'OR" CRUISE

701.01 - ORIGINS

Following a proposal by the French Federation of Automobile Navigation, the International Union established, during its general assembly of 5, 6 and 7 October 1936, the International "Pavillon d'Or" cruise which takes place each year, according to the general rules of the U.I.M. and to those of the following cruise, valid for both cruises.

701.02 - ADMINISTRATION

Yearly cruise

The "Pavillon d'Or" cruise is inscribed yearly in the International calendar.

Organising Country

Each year, the organisation of the event is entrusted to one of National Authority affiliated to the U.I.M.

National Authorities wishing to organise this cruise shall make an application to the U.I.M.

The National Authority of the designed Country indicates to the U.I.M. the body affiliated to its National Federation, the club to which the organisation of the "Pavillon d'Or" cruise will be entrusted, together with the name of the person in charge.

Rules

Bodies entrusted with the organisation of the "Pavillon d'Or" cruise undertake to abide both by the rules specific to the cruise and to the U.I.M. rules relating to pleasure navigation.

Organisation costs

The cruise's organisation costs are borne by the organising body.

The said body may accept sponsors.

"Pavillon d'Or" Cups

The "Pavillon d'Or" shall be awarded to the yacht having left from the point furthest away from the final destination.

The yacht receiving the "Pavillon d'Or" may also be awarded cups or prizes according to the specific rules of the Cruise's programme.

The number of cups allotted on the occasion of the cruise is unlimited and each organising body may set the number as it sees fit. All cups and prizes remain the property of the winner.

Each participant in the "Pavillon d'Or" cruise receives a commemorative souvenir.

Prizes in cash

No cash prizes may be awarded on the occasion of the "Pavillon d'Or" cruise.

Failure to organise

If, for any reason recognised as being valid by the U.I.M., the National Authority designated to organise the cruise is unable to do so, the National Authority which received the second highest number of votes shall be entrusted with the said organisation, or the next best National Authority if the latter is unable to do so.

Dates

The cruise takes place each year between 1 June and 25 September.

The National Authority shall communicate to the U.I.M. the date of the "Pavillon d'Or" cruise, at the latest by September 30 of the year preceding the cruise, to enable the event to be published in the calendar of the U.I.M.

Program and specific rules

The program together with the specific rules, approved by the National Authority, shall be published at the latest 45 days before the event and communicated to the U.I.M.

Qualification

The cruise is open to all inhabitable motor yachts. Auxiliary sails may be used.

The owners shall belong to a National Authority.

No distinction is made between the various yacht categories. The number of engines and their power is not taken into consideration.

The "Pavillon d'Or" cruise is a regularity event. No sheer speed events may be organised.

The rules provide that the services of a pilot are acceptable for certain rivers. However, yachts shall navigate using their own means.

Registration fees

Registration fees set according to the advance program shall be sent to the organising club.

Registration fees shall reach the organising committee at the latest four weeks prior to the cruise's official date of arrival.

In the event of default, the entry fee remains in the hands of the organising committee.

The registration fee set by the organising committee may not be modified.

Yachtsmen not members of a club affiliated to a National Authority may take part in the cruise as long as they abide by the conditions set by the said authority.

Registration fees are split as follows :

80 % to the organising club;

10 % to the National Authority;

10 % to the U.I.M.;

In addition, the U.I.M. receives the inscription for the calendar;

701.03 - THE CRUISE

Departure

No departure shall take place more than three months prior to the cruise's official date of arrival.

Point of departure

Competitors are free to start from where they chose but shall cover at least one hundred nautical miles to reach the port of arrival. Such a distance shall not necessarily be calculated as to the route followed but may be covered as a circuit or loop provided always that the course is always in the general direction of the port of destination.

Ports of call

The organising body designates the ports and cities where the competitors shall stop for stopover controls.

Only such ports and cities designated by the organisers shall be considered to be official ports of call.

In the first port of departure, an official delegate enters in the log book the date and time of departure.

4. Stopovers

Any competitor whose stopover exceeds two weeks shall be disqualified.

Destination

The organising club shall fix in the rules that yachts the final date and time of arrival at the port of destination.

Any delay beyond the prescribed time shall lead to time penalties as stated by the specific rules of the race.

Ports of destination are :

- maritime for those yachts coming by sea;
- fluvial for those yachts coming via Inland routes;
- lacustrine for those yachts in inland waters;

Arrival

The organising club shall include in the specific rules all instructions relating to common routes.

Ports of concentration and ports of arrival shall be defined using polar co-ordinates Global Positioning System (GPS).

Route

Competitors are free to chose their route from the port of departure to the port of concentration. They may stop over in all ports excepting official ports of calls.

Log book

All participants receive a log book where the official delegates accredited by the organising committee enter :

- The place, date and time of departure;
- The places, dates and times of arrival for each port of call;
- The chosen port of concentration;
- The date and time of arrival at the port of arrival;
- Incidents during the race (breakdown), lock gates, etc.) and stopovers at unofficial ports of call;
- Navigation details;

Repairs

En route repairs are authorised, but the resulting time of immobilisation is not deducted. All yachts must reach the port arrival of the rally on the date specified in the program.

701.04 - THE CONTEST

Prizes and cups

Aside from the "Pavillon d'Or" itself, other prizes and cups may be awarded :

- to the 2nd and 3rd competitors having travelled the greatest distance;
- to the best log book;
- for any performance which may be calculated or measured, or defined by the specific rules of the organising committee;

Distance travelled

One point per nautical mile travelled is allotted, from the port of departure to the port of arrival. The distance is calculated as the direct route from start to finish using marine charts and official maps of lakes, rivers and canals.

Only the mileage travelled with the owner on board is taken into consideration.

Should two yachts cover the same distance, the "Pavillon d'Or" is awarded to the yacht having travelled the distance in the shortest time.

Other contests

Other contests or events may be decided by the organisers as they see fit.

All contests shall be clearly announced in the advance program.

Publicity

Press releases may only be issued by the National Authority or its delegate.

Jury

An international jury shall be appointed.

Definition of the "Pavillon d'Or"

A "Pavillon d'Or" shall be awarded to each competitor according to the attached model.

A log book shall be given to each competitor carrying the ensign of the organising club or the National Authority.

In the event that the arrival zone or port or arrival are located in an area common to several Countries, the National Authority of the organising committee may request the collaboration of the other Country(ies).

701.05 - CLAIMS

All claims shall be lodged in writing and submitted at the latest 12 hours after arrival at the port of destination.

Each claim shall be accompanied by a deposit in the national currency, the amount of which shall be set according to the Advance Program.

This amount shall be reimbursed if the claim is considered to be justified.

Decisions taken by the jury may not be appealed.

701 - PAVILLON D'OR FOR HISTORICAL BOATS

The U.I.M. Pavillon d'Or has existed for several years to encourage the development of events for Historical Boats, the U.I.M. has created the same trophy for historical boats.

This Trophy will be contested annually over a minimum of three events ; additional events may be added upon the request of the interested National Authorities up to a maximum of 6 events spread over a minimum of three different nations.

The rounds for the Pavillon d'Or Trophy must be inscribed on the U.I.M.

calendar, the organising clubs must be a member of a U.I.M. affiliated National Authority, except derogation as provided under article 3.1.1. of the U.I.M. By Laws.

A round of the U.I.M. Pavillon d'Or must contain following sections :

- Regularity test (100 points);
- Manoeuvring competition (140 points);
- Compliance with U.I.M./ASDEC Specifications (210 points);

Each round will establish a classification based on the sum of the points (450 points max) obtained for the three sections.

Additional items might be organised but they will not be taken into account for the award of the Pavillon d'Or.

The participating boats should be older than 25 years and comply with all legal rules of the Country where they are registered.

According to their classification in each event, the competitors will score points according to the U.I.M. scoring system (400, 300, 225, 169, 127, 95, 71, 53, 40, 30, 22, 17, 15, 13, 9, 7, 5, 4, 3, 2, 1).

The overall classification of the Pavillon d'Or shall be based on the total of the points scored in all events.

The general classification will be given in two sections :

- **CLASSIFICATION OF THE CREW** ; thus leaving any crew the possibility to crew several different boats in the course of any competition.

This facility will be agreed by the U.I.M. in respect of the type of boat and of its usual base (e.g. boats which are too large to be transported by sea towards another circuit and vice-versa)

- **CLASSIFICATION OF THE BOATS** ; in applying the system combination Crew/Boat which means that the relevant boat will need to be driven by the same crew.

The "boat" classification will be called the Pavillon d'Or - Constructors.

The "crew" classification will be called the Pavillon d'Or - Crew.

(= to be compared with the F1 Car racing classifications).

Each organiser must submit an advance-programme to the U.I.M. - Historical Boats Section - 60 days before the event will take place.

702 - TROPHY MARIO AGUSTA

702.01

The Trophy Mario Agusta is an award for drivers who use their vessels as pleasure craft and navigation during their holidays and leisure time.

702.02

The Trophy may be awarded annually at an event figuring in the U.I.M. International Pleasure Navigation Calendar.

702.03

The type of participating vessel is restricted to craft constructed for pleasure or touring, with either a single engine or several engines. All craft must be in conformity with the rules and regulations governing this type of navigation in their Country of origin.

702.04

The maximum overall length permitted is eight meters.

702.05

Three craft specifications are allowed :

Craft with cabin;

Craft without cabin;

Inflatables;

702.06

The first prize will be awarded to the driver, a member of a federated club, whose National Authority is itself affiliated to the U.I.M., who has covered the longest distance, measured in nautical miles, in one or multiple journeys, by sea, through rivers, deltas and / or canals, indifferently.

702.07

Only those nautical miles covered during effective navigation will be taken into account.

702.08

The log-book must be kept and duly updated with daily entries indicating miles covered, ports & towns visited. Official entry & departure documents will be required by the U.I.M. : documents such as visas and those issued by customs, port authorities, police, municipalities, tourist offices and lock operators, etc. so that the U.I.M. can verify accounts of voyages.

702.09

The required log-book is that based on a model established by the U.I.M. and must be purchased from the competitor's National Authority, as of February 01 each year. The log-book must be returned to the National Authority before October 15 of the same year. The National Authority will then transfer the log-book to the U.I.M. before November 30, of the same year.

702.10

Trailored boats may compete in the Trophy. However, the trailored part of each voyage will not count as nautical miles. The log-book will be kept only for those parts of the voyage effectively accomplished in the water. Both operations, that of launching & of recovery of a trailored craft must be duly noted and validated by an official agent.

702.11

During the entire duration of the voyage/s, the presence of paid crew aboard is strictly forbidden.

The presence of a pilot aboard is allowed if the master of the vessel considers it necessary for reasons of security. Similarly, estuary or port authorities may require the presence of a pilot aboard in certain circumstances, in which case the master of the vessel must conform to such instructions.

702.12

The Trophy Mario Agusta Jury is composed of members of the U.I.M. Pleasure Navigation Commission and the Secretary General of the U.I.M..

702.13

Decisions of the Jury are final and may not be appealed. The results of the Trophy competition are announced by the Pleasure Navigation Commission after satisfactory examinations of the Competitors' Log-books.

702.14

The present Rules and Regulations may be subject to modification or revision each year.

702.15

By the fact of registering as competitors for the Trophy Mario Agusta, those competing agree to the terms that neither the U.I.M. nor any National Authority may be held in any way responsible for any form or sort of damage of whatsoever nature, either involving persons or material, that might occur during voyages counting for the Trophy.

702.16

Medals will be awarded to the winners of the various categories during the U.I.M. General Assembly of the year following the Trophy competition.

703 - TROPHY SANZ PINAL

703.01

The Trophy Sanz Pinal is presented by the Federacion Espanola Motonautica as an annuity event. It is awarded annually to a member of a club, affiliated to a U.I.M. National Authority, who has covered the greatest number of miles during an individual long-distance cruise.

703.02

The National Authority procures for all its clubs all necessary forms and regulations and examines the results of those under its authority, selecting the best result and communicating it to the U.I.M. before the date of December 31 each year.

703.03

The basis of the contest is the total distance covered during an uninterrupted cruise of a maximum duration of 60 days, undertaken during a single year, and in so far as the parameters of the cruise are acceptable to the National Authority.

The log book of the national winner must be communicated to the U.I.M. by the National Authority.

No distinction is made between voyages in the open sea and those taking place on inland waters. To be considered a candidate for the Trophy Sanz Pinal, the competitor must have completed at least 300 nautical miles during the cruise in question.

703.04

A competitor's log book must contain the following :

- Arrival & departure hours and dates for all stopover ports comprising official notification from port authorities, lock-keepers, customs officials etc.
- The distance covered between the various stopover ports and all navigational details concerning the voyage.

If the National Authority considers that the log book has not been properly kept, a competitor may be disqualified.

703.05

Any stretch of water covered twice over shall be counted once only. Stretches of water covered while the owner of the craft is absent shall not count as part of the voyage miles. National Authority officials calculate distances according to direct passage routing for sea voyages and according to official inland charts concerning inland waters.

703.06

The calculation of points, certified by the National Authority, corresponds to one point per nautical mile, the total number of miles covered being equal to the total number of points.

703.07

National Authorities, presenting the Trophy Sanz Pinal to their affiliated clubs, may add supplementary rules to the present international rules should they judge it necessary. The supplementary rules may not contradict the international rules of the Trophy Sanz Pinal.

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RADIO CONTROLLED RULES

SPORTS TECHNICAL

AS FROM 2003

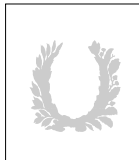
2002 EUROPEAN CHAMPIONS
RADIO CONTROLLED

E 3.5 CLASS



M. CORSINI
(ITALY)

E 7.5 CLASS



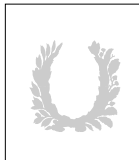
F. ROPSO
(ITALY)

E 15 CLASS



A. FORNELLI
(ITALY)

E 35 CLASS



G. BELLETTI
(ITALY)

GROUP 100

U.I.M. ADMINISTRATION

- 101 SPORTS CALENDAR**
- 102 OBSERVERS**
- 103-111 DIFFERENT TYPES OF RACES**
- 120 LICENCES**

GROUP 200

ORGANISATION OF RACES

- 200 ORGANISATION**
- 201 RACE COMMITTEE**
- 202 ADVANCE - PROGRAMME**
- 203 ENTRY**
- 204 PROGRAMME**
- 205 SAFETY**
- 206 DISTINCTIVE MARKS**

GROUP 300

RULES FOR CIRCUIT RACES

- 300 RULES FOR CIRCUIT RACES**
- 301 TIMING**
- 302 CIRCUIT**
- 303 MARKS OF THE COURSE**
- 305 STARTING SIGNALS**
- 306 START IN ENDURANCE SERIES**
- 307 START IN HEAT RACING SERIES**
- 308 RACE CANCELLED**
- 309 RACE POSTPONED**
- 311 RACE STOPPED**
- 312 RULES OF THE ROAD, ENDURANCE SERIES**
- 313 RULES OF THE ROAD, HEAT RACING
 SERIES**
- 318 CLASSIFICATION**
- 319 DEAD HEAT**

- 320 POSTING OF RESULTS**
- 321 INSPECTION OF HULLS AND ENGINES**
- 322 PRIZES**
- 323 AFTER THE MEETING (REPORTS)**

GROUP 400

JURISDICTION

See Pleasure Navigation Section

GROUP 500

- 500 SERIES AND CLASSES**
- 502 INSPECTIONS**
- 503 SPARE BOATS**
- 504 NOISE REDUCTION**
- 505 REPLACEMENT OF MECHANICAL PARTS**
- 510 CALLING TIME**
- 512 HULLS**
- 513 MACHINERY**
- 520 BREAKING OF RULES**
- 530 RETRIEVAL OF BOATS**
- 540 LEAVING THE PONTOONS**
- 550 FUEL**
- 580 TURBO-JET AND ROCKET PROPELLED
BOATS**

GROUP 600

RECORDS AND PERFORMANCES

See Pleasure Navigation Section

GROUP 700

ENVIRONMENTAL

- 701 NOISE LEVEL**
- 702 EMISSIONS**
- 703 RE-FUELLING**
- 704 ENVIRONMENTAL**

GROUP 800

OTHER COMPETITIONS

800.01 ELECTRIC POWERD BOATS

- A CATEGORIES**
- B ENGINES**
- C BATTERY CELLS**
- D HULLS**
- E PROPELLERS**
- F POWER DRIVER**
- G SPEED CONTROL**
- H ONBOARD ENGINE POWER BREAKER
DEVICE**
- J RACE NUMBERS**
- K CLASSES**
- L RACING COURSES**
- M NUMBER OF BOATS**
- N NUMBER OF HEATS**

- O STARTING PROCEDURES**
- P END OF RACES**
- Q PENALTIES**
- R OTHER APPLICABLE RULES**

GROUP 900

MISCELLANEOUS

- 901.01 ENDURANCE COURSE**
- 901.02 HEAT RACING COURSE**
- 901.03 SPEED RECORDS**
- 902 PLATES WITH RACING NUMBERS**
- 903 ANNEX C (REF. RULES 312 / 313)**

GROUP 100

U.I.M. ADMINISTRATION

Registration of international meetings and races on the annual calendar of the U.I.M. is compulsory and is subject to the payment of a fee, the amount of which is fixed annually by the General Assembly.

Payment of these fees must be made within three months after the fixing of the international calendar. It is compulsory even if the meeting does not take place.

Permission to organise these meetings is only delivered by the U.I.M. upon payment of these fees.

No permission is to be granted to National Authorities in arrears of payment.

The registration fee is to be doubled for any meeting requested after the deadline dates set by the rules.

All requests to organise an international championship must be sent to the U.I.M. Secretariat before the time set by the deadline in the rules.

When compiling the sports calendar due attention is to be paid to the confidential reports of the U.I.M. observer before granting the request to organise titled meetings.

The organisers must make known the series and classes which may participate and also mention the name and address of the organisers and/or the responsible person(s) who is/are in a position to supply information about the event. They must also name the type of races : titled, championship, etc.. the locality, the address of the race organisers as well as the date of the inscription. Such information must be provided as soon as possible.

101.02 - OTHER MEETINGS

It is forbidden to organise national championships for series and classes which are in the programme of a titles meeting taking place the same

day. The Sports Calendar also lists the registered Ordinary International Meetings.

The calendar of national and local events is not to be submitted to the U.I.M. The NA's concerned have responsibility for their own calendar.

The organisation of an event cannot be granted if the title, the locality and the date of said event are not given. U.I.M. does its utmost to avoid clashes between national and friendly meetings.

However, whether the interested parties agree or disagree, the U.I.M. lists all requests for dates, leaving it to the organisers to sort out their responsibilities.

The titled meetings have priority over all other meetings before the closing of the registrations for the sports calendar.

However, once the sports calendar has been finalised, all the remote-controlled motorboating meetings are on a equal footing.

A titled meeting which has been cancelled does not count any longer for the title. If the organisers decide to run the race in spite of the cancellation, the race must be considered as a friendly international race.

101.03 - DEADLINES

1 March : Deadline for forwarding requests for entries

1 April : Deadline for allotting international championships and races to the NA's

10 September : Deadline for forwarding dates and venues by NA's

Calendar meeting

October : Deadline for drawing-up the calendar by the Working Group on Remote-Controlled Power boating (WGRCP)

15 November : Publishing of the Calendar.

101.04 - CHANGE OF DATE

For any change of date of a World or Continental Championship requested after the 15th of December, U.I.M. must give its assent to the new date proposed.

Any request for a change of date should be received by the U.I.M. Secretariat at least 15 days before the date initially planned. The new date will be fixed at least 45 days after the date on which the change was announced.

The calendar fee will be doubled.

102 - U.I.M. OBSERVER-DELEGATES AND COMMISSIONERS

102.01 - FUNCTIONS

The presence of a U.I.M. Observer-Delegate or a U.I.M. Commissioner is required for any international titled meeting, except in case of force majeure.

The function of Observer-Delegate or Commissioner must be fulfilled most cautiously.

Members of the Council, which includes all U.I.M. Vice-Presidents, are entitled to be U.I.M. Observer-Delegates or Commissioners.

102.02 - U.I.M. OBSERVER-DELEGATE

He is not entitled to take part in any discussion.

He is not entitled to take sides as delegate of U.I.M., which he is not entitled to commit.

He is entitled to act as an adviser whenever his opinion is requested, but he is not entitled to act either as a referee or a mediator.

National Authorities are requested to send every year to the U.I.M. Secretariat a list of candidates observers with the following data :

Name and surname
 Full address
 Telephone - Telex - Fax
 Languages spoken
 Languages read and understood
 Is he free at all times ?
 If not, when is he ?
 Knowledge in powerboating
 Knowledge in sports matters
 Knowledge in technical matters.

As soon as he gets the Observer's agreement, the Secretary General of U.I.M. will send a letter to the National Authority informing them that the Observer has been designated for the events referred to in the letter.

The Observer-Delegate is entitled to take action with the race organising committee if safety conditions are not met. In some cases, he will even be entitled to request that a Continental or a World Championship be suspended if safety conditions are not improved.

102.03 - U.I.M. COMMISSIONER

The U.I.M. Commissioner :

- assists the local organiser and more particularly the Officer of the Day
- is a voting member of the international jury
- approves last minute amendments to the advance-programme
- supervises the signing of drivers' declarations
- takes part in important decision-making during the race with the Officer of the Day, the Safety Office and the 2nd Commissioner, if any takes part in urgent decision-making with the Officer of the Day and the organising promoter, in matters related to the overall organisation.

The U.I.M. Commissioner is entitled to take action with the race organising committee if safety conditions are not met. In some cases, he will even be entitled to request that the race be suspended if safety conditions are not improved.

102.04 - AFTER THE RACE

(applies to both the Observer-Delegate and the commissioner)

After the event, he sends his report to the U.I.M. Secretariat by using the official form, within ten days after the race.

The official results of the event must be enclosed with the report.

The reports from Observer-Delegates and Commissioners are to be considered as confidential matter and cannot be used for aims other than U.I.M.'s, not be communicated to the press or otherwise disclosed in any purpose whatsoever.

Should the Observer-Delegate or Commissioner fail to obtain all necessary data and results for transmission to U.I.M., he will note this fact in his report and will urge the organisers to send all required documents by themselves to the U.I.M. Secretariat, as soon as possible.

A copy of the Observer-Delegate or the Commissioner's report will be sent as soon as possible to the President and the Secretary of the U.I.M. Radio Controlled Sub-commission.

The National Authority must remind the organisers that the Observer-Delegate and the Commissioner must be accommodated in a good hotel for the whole duration of the event and as long as required by the fulfilment of their mission.

The costs of such accommodation are borne by the organisers.

102.05 - TRAVEL EXPENSES

The National Authority must also make it clear that travel expenses for the Observer-Delegate, from his town of residence to the place where the meetings are taking place, calculated on the basis of a return air ticket, are also borne by the organisers.

The expenses referred to above must be refunded to the Observer-Delegate during their stay by the organisers or to U.I.M. upon receipt of the relevant invoice.

103 - RACES RECOGNISED BY THE U.I.M.

The U.I.M. recognises the following events :

- international titled events
- international ordinary events
- national and local events
- club meetings.

104 - NON APPROVED RACES

All races not organised according to U.I.M. rules are not to be recognised and all officials who take any part in such races are suspended or excluded if offence recurs.

The same applies to motorboating events not approved by National Authorities.

Any driver having competed in a race not approved by the National Authority of that country or forbidden by the National Authority may be suspended for a period to be fixed by the National Authority.

In the case of an international meeting being organised by a non-affiliated club which has not asked permission to organise such a meeting, the National Authority must :

- notify the organising club that the drivers can be suspended
- notify the drivers that they will be suspended in their own country if they take part in international races not approved by the NA ; the latter may request U.I.M. to extend this suspension to other countries.

A "national" race or a race "by invitation" in which a foreign driver is invited to take part must be entered on the U.I.M. calendar, otherwise it becomes an unauthorised event. Invited drivers entering such unauthorised races may lose their international licences.

Remember that drivers having international licences are only allowed to enter a meeting outside their country provided their NA gives them the permission to do so.

Should a dispute arise, the "Radio Controlled Sub-commission" is the only body entitled to settle said dispute.

105 - TITLED INTERNATIONAL EVENTS

The word "international" defines the type of race. It is impossible to know beforehand the number of countries which will participate.

There are three categories of titled meetings :

- World Championships
- Continental and Hemispheric Championships
- International and National Grand Prix.

No meeting is allowed a title unless authorised to do so by the U.I.M. who draw-up the annual list in the official calendar.

The U.I.M. has the right to refuse a title if it thinks that the amount of prizes or the quality of the organisation are not acceptable.

Whenever possible, titled events should recur the same period every year.

Since U.I.M. aims at protecting titled meetings, its Radio Controlled Sub-commission may refuse to sanction any meeting which takes place at the same time. Such refusal is only notified at the specific request of the National Authority lodging a protest.

In order to promote new series and classes, new hulls, motors or engines, new ideas regarding propulsion and new concepts of racing, the Radio Controlled Sub-commission may, provided the request to organise be accompanied by an amount equal to double the fee of an ordinary meeting, approve the organisation of international contests for such boats, motors or engines, type of propulsion and races of a kind not yet known to the U.I.M., provided such events do not clash with other U.I.M. titled meetings at the sole discretion of U.I.M.. Safety conditions have to be met.

106 - INTERNATIONAL R.C. CHAMPIONSHIPS

106.01 - GENERAL RULES

No exception is to be allowed to this rule.

International Radio Controlled Championship are open to all U.I.M. Classes and should take place under U.I.M. rules.

The requests to organise these championships must be sent in due time. The Radio Controlled Sub-commission will allocate the Championships.

The U.I.M. delivers, following requests made by the champions of the National Authorities and transmitted to that National Authority to the U.I.M. Secretariat, a "Champion's Certificate".

The cost of such a certificate is fixed annually by the General Assembly and is to be paid by the National Authority to the U.I.M. when the request is made.

A list of champions is issued annually.

The U.I.M. rules and those of the National Authority are applicable to any point not foreseen in these rules.

A non-European citizen may be proclaimed European Champion provided he has had a European licence for more than two years. The same applies to all continents.

Immediately after the Championship, the name of the Champion must be transmitted by cable to the U.I.M. Secretariat.

Permission to organise a championship is granted only if the race is run on a course recognised by the National Authority and homologated by U.I.M..

All national authorities having homologated the courses where events are to take place send a list of them to the U.I.M. Secretariat, with a plan to scale.

WORLD AND CONTINENTAL CHAMPIONSHIPS

108.01 - NUMBER OF CONTINENTAL AND WORLD CHAMPIONSHIPS

There can be one championship every two years, per class per continent :

- World Championship starting in 1997, then 1999, 2001 and so on.
- Continental Championship in the alternate year.

108.02

A championship of any class can only be organised if the boats in that class exist in at least three countries.

108.03

The right to organise Continental and World Championships is annually rotated in the alphabetical order of the French names of the affiliated countries.

If a nation declines to organise a championship the next in rotation is taken into consideration.

If a country applies for the organisation of a championship for a certain year but fails to organise it, this country is considered as having held the championship as far as rotation is concerned.

108.04

Each country may designate 1 to 15 drivers according to its own rules. The defending champion participates additionally.

Drivers must be able to show their international licence.

108.05

Entries are to be sent by the National Authority to the organising National Authority before the deadline fixed by the latter.

108.06

Any equipment used in racing must be in conformity with U.I.M. rules.

108.07

After the race, the equipment of the three first classified drivers, hull and engine, are measured by the official measurer if required. The title of champion is only granted after satisfactory examination of the aforesaid equipment.

Repairs are permitted.

In those classes where engines are restricted, substitutions can only be effected with original parts.

Cylinder dimensions are measured when the engine is cold.

Allowance is made for the carbon and oil present in the combustion chamber.

108.08

A championship can only be held in international meetings.

The races for Championships are not to be combined with races for cups or trophies bearing another name.

108.09

The Champion receives a gold medal given by the organising club.

Other prizes may be awarded.

A driver who has entered for a World or Continental Championship cannot be compelled to enter other races programmed during the meeting.

108.10

As soon as the results have been verified, the National Authority forwards them to the U.I.M. Secretariat.

109 - CLASSIFICATION

For events with two or several races :

109.01

The final classification is given by adding the results of each driver in each race.

109.02

All races registered in the calendar are taken into account.

109.03

For each race, the classification will be as follows :

- 1 = 400 pts
- 2 = 300 pts
- 3 = 225 pts
- 4 = 169 pts
- 5 = 127 pts
- 6 = 95 pts
- 7 = 71 pts
- 8 = 53 pts
- 9 = 40 pts
- 10 = 30 pts
- 11 = 22 pts
- 12 = 17 pts

Drivers classified thirteenth onwards will be granted 10 points.

In case of dead-heat, the driver classified in the highest number of races is the winner.

In case of second dead-heat, the driver best classified in the last race is the winner.

110 - REMOTE CONTROLLED ENDURANCE **RACE**

110.01 - COURSE

The course provided for in Endurance racing is reproduced herein (see 901.01).

The buoys must be cylinder- or cone-shaped, with their axis placed vertically and the following dimensions : diameter 300 mm minimum and 500 mm maximum, 400 mm minimum height above water.

Their colour must be highly visible.

110.02 - RACING CONDITIONS

Races consist in covering as many laps as possible within a determined time, in competition with other boats.

They must rotate anti-clockwise.

Boats must always start from the starting pontoon.

The arrival line is located in front of the Jury.

110.03 - ACCESS TO FINAL HEAT

The selection of competitors to final heat is done through qualifying heats.

110.04 - NUMBER OF QUALIFYING HEATS

Each competitor can take part in three qualifying heats whose duration has been provided for in rule 110.07. The best heat is taken into consideration for the access to the final and/or semi-final heats.

110.05 - DIRECT FINAL

When the number of competitors is 12 or lower than 12, heats do not take place.

Should two competitors be transmitting on the same frequency, the first entry will come first.

110.06 - NUMBER OF COMPETITORS

The maximum number of competitors admissible for each heat is 12. The minimum number of entries is 3.

Should there be less than 30 competitors qualified at the end of qualifying heats, the first twelve qualified have a direct access to the final heat.

Should there be 30 competitors qualified or more, the first six qualified will have a direct access to the final heat.

Competitors classified 7th to 30th have access to two semi-final heats ; the first heat is composed of the odd positions (7, 9, 11, etc.) the second heat is composed of even positions (8, 10, 12, etc...) the first three qualified in each semi-final heat will be on the final starting grid.

In case of dead-heat the previous heats are taken into consideration. In case of second dead-heat, the name of the winner is drawn by lot.

110.07 - DURATION OF HEATS, SEMI-FINALS AND FINALS

- | | |
|--------------------|-----------------------|
| - Qualifying heats | 10' (ten minutes) |
| - Semi-finales | 15' (fifteen minutes) |
| - Finale | 30' (thirty minutes) |

110.08 - STARTING AND END-OF-RACE SIGNALS

The beginning and the end of each race are indicated by a sound signal.

110.09 - SUSPENSION OF THE FINAL

Finals must always take place within the period of time provided for in the rules governing the various series and classes. Should it be interrupted by the Jury, the final will be resumed and competitors will be given the positions they had just before the race was suspended.

The boats that were racing at the time the race was suspended have to come back immediately and must be controlled by the judge in charge of the starting procedure so that no action takes place that would involve an immediate disqualification.

110.10 - SUSPENSION OF A HEAT AND/OR A SEMI-FINAL

Should a qualifying heat and/or a semi-final be suspended, the latter can not be resumed but must be raced again.

110.11 - END OF RACE

For each competitor, the race ends after the end-of-race signal. The boat completes its last lap. This lap will be taken into account and the time required to complete the course will be deducted.

The competitor will however have to cover a lap at a reduced speed to come back to his own space on the pontoon in order to avoid disturbing other competitors still racing.

The boat crossing the arrival line more than twice after the end-of-race signal will be penalised by as many laps as the laps unnecessarily covered.

Competitors who were stopped when the end-of race signal was given are not allowed to resume the race and are penalised by two laps in case they do so.

110.12 - CLASSIFICATION

The classification is established on the basis of the number of laps completed by each boat, from the starting signal up to the end-of-race signal, penalties deducted if any.

110.13 - DEAD-HEATS

In case of dead-heats by the number of laps, the first boat completing the course within the shortest time will be classified first.

In case of a dead-heat, the results of the other eliminating heats will be taken into account to determinate admissibility to the final. Should there still be a dead-heat, the names would then be drawn by lots.

During the final, should there be several boats stopped with the same number of laps and should they fail to complete the lap in progress after the end-of-race signal, they will be classified in the order of their last pass in front of the time-keepers.

110.14 - CHANGING QUARTZ FOR THE FINAL

The competitor who covered the smallest number of laps in qualification has to change his quartz if required.

Should he be unable to do so, he will not start and will be classified last in final.

110.15 - SERIES STARTING GRID DRAWN BY LOTS

The series starting grid is publicly drawn by lots 30' before beginning of the race.

110.16 - RADIO TRANSMITTERS

Competitors will keep their radio transmitters during the whole competition.

They will be responsible for them.

Any competitor caught while manipulating his transmitter on the ON position outside the qualifying heat or the final in which he is racing will be disqualified for the competition and for all the categories in which he is involved.

Moreover, he will be reported to U.I.M. and his National Federation.

The radio transmitter frequencies must comply to local regulations and/or restrictions of the hosting country.

111 - REMOTE CONTROLLED HEAT RACING SERIES

111.01 - COURSE

The course provided for in Heat Racing series is reproduced herein 902.02 (Fig.2).

111.02 - BUOYS

The buoys must be cylinder or cone-shaped, with their axis placed vertically and the following dimensions: diameter 150 mm maximum, 300 minimum height above water.

They must be made of a material that does not damage hulls in case of a collision (i.e. expanded polystyrene) and must bear visible coloured stripes (red or orange).

111.03 - RACING CONDITIONS

Races consist in covering a pre-determined number of laps, in competition with other boats.

111.04 - NUMBER OF QUALIFYING HEATS

It is up to the organising committee to determine the number of qualifying heats. There must at least three of them.

111.05 - NUMBER OF COMPETITORS

The maximum number of competitors admissible for each heat is 8.

The minimum number of competitors is three.

When the number of competitors in a class is 8 or lower than 8, the qualifying procedure does not take place.

111.06 - DURATION OF HEATS AND OF THE FINAL

Qualifying heats, semi-finals and final:

6 (six) laps ALL CLASSES

111.07 - STARTING AND END OF RACE SIGNALS

The beginning and the end of each race are indicated by the clock.

111.08 - SUSPENSION OF THE HEAT

Should the heat be suspended, the whole racing procedure is restarted.

111.09 - END OF THE RACE

For each competitor, the race ends after all the laps provided for have been covered.

The competitor will however have to cover a lap at a reduced speed to come back to his own space on the pontoon in order to avoid disturbing other competitors still racing.

111.10 - CLASSIFICATION

The classification is established by adding the competitor's scores for each race, deducting penalties if any.

111.11 - GRANTING OF POINTS

For each race, points are granted to competitors as follows:

1 = 400 pts

2 = 300 pts

3 = 225 pts

4 = 169 pts

5 = 127 pts

6 = 95 pts

7 = 71 pts

8 = 53 pts

Not finished: 25 pts

Not started : 0 pt

Scores are given in the order of arrival of the race and are added to the scores of the previous races to make the final classification.

111.12 - QUALIFICATIONS

If at the end of qualifying heats there are less than 20 competitors qualified, the first 8 classified will be eligible for the final .

Should there be 20 competitors qualified or more, the first 4 would directly go to the final.

Competitors classified 5th to 20th form two semi-finals; the first one is composed of the odd positions (5, 7, 9...) and the second one is composed of the even positions (6, 8, 10...) that cover only one heat.

The first two classified in each semi-final will complete the final starting grid.

In case of dead-heat, the result of qualifying heats is taken into account. In case of second dead-heat, the name of the winner is drawn by lots.

111.13 - FINAL

8 competitors maximum take part in the final.

These competitors start three heats minimum.

The final classification is provided by adding the points scored in each heat.

111.14 - DEAD HEAT

At the end of the final, should there be a dead-heat for the first place, and in that case only, an additional race will take place between these two competitors in order to determine the winner.

111.15 - RADIO TRANSMITTERS

Competitors will keep their radio transmitters during the whole competition.

They will be responsible for them.

Any competitor caught while manipulating his transmitter on the ON position outside the qualifying heat or the final in which he is racing will be disqualified for the competition and for all the categories in which he is involved.

Moreover, he will be reported to U.I.M. and his National Federation.

The radio transmitter frequencies must comply to local regulations and/or restrictions of the hosting country.

120 - INTERNATIONAL RACING LICENCES

120.01 - COMPULSORY LICENCE

No one can take part in a race, establish or improve on a record in international class boats without being in possession of a licence from his National Authority.

120.02 - REGISTRATION

The National Authority establishes every year a list of people to whom licences have been delivered and this is send to the U.I.M. Secretariat.

The list must include : the name and surname, the address and the licence number.

These lists are sent to the National Authorities upon request.

Registration numbers are given by the NA and are "national" or "international" according to the criteria it has fixed.

Any National Authority has the right to deliver licences to:

- its nationals
- nationals of another country affiliated to the U.I.M. but with the agreement of the National Authority of that nation

With the approval of U.I.M., a National Authority may also deliver licences to persons belonging to a country which is not yet affiliated to the Union.

Drivers are only permitted to hold licences from one National Authority.

A National Authority may refuse to give a licence without having to state the reason. Notification to this is sent to all affiliated clubs and to the U.I.M..

120.03 - VALIDITY OF LICENCES

Licences are valid from the 1st of January until the 31st of December each year unless local circumstances require other dates, but validity should always be shown on the licence itself and must be for one year.

A licence is valid in any country affiliated to the Union and entitles the holder to enter or drive in all competitions organised in any affiliated country, provided he complies with what is stated in the U.I.M. general racing rules.

However, for any event held under special rules, the licence holder must abide by the rules in question.

120.04 - COST OF LICENCE

The cost of the yearly licence is to be fixed by the National Authority.

120.05 - EXAMINATION OF LICENCES

At any meeting, the holder must produce his licence at the request of any official of the meeting.

120.06 - CANCELLATION OF LICENCE

Anybody who takes part in an unauthorised meeting loses his licence.

However, if the unauthorised meeting is held in waters of a country other than the one which has delivered the licence, both National Authorities must agree as to the duration of the suspension.

The U.I.M. decides finally in case of a dispute.

200 - RACE ORGANISATION

200.01 - GENERAL

The following bodies have the right to organise any R.C. motorboating event : National Authorities, Affiliated Clubs, Corresponding Members.

Affiliation to the National Authority implies their acceptance of the national rules and those of the U.I.M..

A Club, member or driver who is suspended is not allowed to take part in any sporting event in his country or in any other country, neither as a driver nor as an official, as long as the suspension lasts.

A member or a driver who takes part in any race in a foreign country affiliated to the U.I.M. undertakes ipso facto to abide by the rules of the National Authority of that country.

A member if a driver belonging to a country which is not affiliated is to be considered as a national of the country where he takes part in the race.

All Clubs, race organisers, officials and drivers are obliged to be acquainted with the general rules of the U.I.M. as well as the rules related to races or series (local, national or international).

Two updated copies of the present rule-book must be available for every organisation.

All races organised by the National Authorities or their clubs are subject to the U.I.M. racing rules.

All rules and programmes must mention this.

The general racing rules and the rules related to records are applicable to all international series.

200.02 - INTERPRETATION OF THE RULES

In all cases not foreseen by the International rules or seemingly inaccurately defined, the National Authority, a race committee, race jury or an appeal board, is to judge bearing in mind the intention of the disputed rules.

Series and Class rules are a complement to general rules and are therefore to prevail.

The English text is the official one. In case of disagreement on the interpretation of these rules, the English text prevails.

201 - RACE COMMITTEE

201.01 - ORGANISING AUTHORITIES

All races are under control of the organising club who is to nominate :

- An organising Committee which organises the general and administrative aspects of the meeting.
- A Race Committee which organises and supervises the actual racing and decides whether competitors are eligible or not.
- The Race Committee has to deal with : safety, the race secretariat, buoys, technical problems, the starter and the recovery.
- A Jury in charge of setting all problems related to the rules.

The jury decides on any dispute submitted to them by the Race Committee, the names of the members are to be printed in the official programme.

The decisions made by the Race Committee and the Jury must be based on U.I.M. rules. The Jury has to see to it that meetings are held in conformity with them and with the spirit of the sport.

The Organising Committee appoints a Committee that verifies all the licences and all other documents required. It also verifies whether boats, engines and radio equipment's are in conformity with the rules.

No officer nor member of the Jury nor member of a national or international Committee or Commission is allowed to intervene in the judgement of a problem in which he is involved himself or is an interested party.

The following are the Race Officials :

- the members of the Race Committee

- the Officer of the Day
- the Safety Officer (deputy to the Officer of the Day)
- the Secretary of the Race Committee
- the Technical Officers of the Race Committee
- the Course and Buoy Officers
- the Time-keepers
- the Lap Scores
- the Members of the Jury
- those in charge of retrieving boats
- the radio and sound level meter controller.

201.02 - THE OFFICER OF THE DAY

The Officer of the Day must supervise the organisation of races for which he is responsible to the organising clubs.

He must maintain order on the course, ascertain that all services are working properly, see to it that boats are in conformity with the racing rules and that the race is proceeding in accordance with the official programme ; he has to bring together the Officers' and Timekeepers' reports as well as all the information required to enable the results to be compiled.

201.03 - THE SAFETY OFFICER

He is deputy Officer of the Day. He is in charge of managing and/or co-ordinating (with the agreement of the Officer of the Day) all safety services on shore (medical care and fire) as well as on the water (boats and people on board the latter, equipment's). Whenever necessary, he stands for the Officer of the Day.

201.04 - THE RACE SECRETARY

The Race Secretary reports to the Officer of the Day and is responsible for the physical organisation of the meeting.

He has to make sure that the various officials are aware of their respective duties and are equipped with the accessories required. He has to collect all the documents of the meeting.

201.05 - DUTIES OF RACE OFFICIALS

Race Official's duties are limited to the tasks they have been entrusted with.

201.06 - TECHNICAL OFFICIALS

They have to check that the hull, the engine and the radio are in conformity with the rules. They check the good functioning of throttle and rudder controls.

They check the synchronisation of the clock and the camera (heat racing).

After the race, they do all necessary checks.

201.07 - THE BUOY AND COURSE OFFICIALS

They must ensure that the drivers conform to the racing and course rules. At the end of the meeting, they draft and sign a report certifying that the drivers' conduct during the race and at the turn buoys was according to the rules.

The report is delivered to the Officer of the Day who takes all necessary measures regarding the application of rules.

201.08 - THE TIMEKEEPERS AND STARTER

Timekeepers will :

- calculate on completion the elapsed time for each driver - establish the finishing order after having made all necessary calculations
- hand their reports, with all documents required, to the Officer of the Day
- for international races, there must be at least two timekeepers.

Their times and results must only be handed to the Officer of the Day.
The Starter signals the start when ordered by the Officer of the Day.

201.09 - THE U.I.M. OFFICIALS

The U.I.M. Official :

- assists the local organiser, the Officer of the Day more particularly
- is a voting member of the international jury
- approves last minute modifications to the advance-programme
- supervises the signing of declarations by drivers
- takes part in important decision-making during the race with the Officer of the Day, the Safety Officer and the 2nd official if any- assists the Safety Officer in the organisation of rescue operations (more particularly in case the rescue team is external to the organisation)
- with the Officer of the Day and the race promoter, takes part in urgent decision-making regarding the general organisation.

202 - ADVANCE PROGRAMME

202.01 - FORWARDING DATE

45 days before any international race, the organising committee must forward at least one advance-programme to the U.I.M. Secretariat, to the National Authorities and to the invited clubs and drivers.

Should rule 202.01 not be complied with, the Council has the right to request the Sports Commission to apply suitable penalty against international events during the following year.

Advance-programme arriving after the event at the U.I.M. Secretariat are considered as not delivered. In this case, the fines laid down may be implemented.

202.02 - REQUIRED DETAILS

The above advance-programme must be written in the language of the organising country and in French and English and must contain the following information :

- 202.020 The name of the organiser
- 202.021 The series and the classes of boats catered for
- 202.022 The race or races which will be run
- 202.023 The venue and the date of the race (s)
- 202.024 The course of each race and plan drawn to scale
- 202.025 The sentence "the races are held under U.I.M. rules"
- 202.026 The special conditions which in accordance with the U.I.M. rules may be added
- 202.027 The time limit and place to which entries must be sent and whether written by telegram or by hand
- 202.028 The amount of the entrance fee
- 202.029 All information regarding the prizes for each race
- 202.030 Where and when the entry forms, programmes and racing instructions can be obtained from the organising committee
- 202.031 Transports facilities and concessions
- 202.032 Terms and conditions of a mandatory insurance for personal and material damage to third parties, competitors included
- 202.033 Minimum age of competitors required by law
- 202.034 Times for practice per class
- 202.035 Full instructions regarding noise regulations
- 202.036 Date provided, for the verification of the hull and the engine

- 202.037 Should no advertising be allowed during a race, this is to be mentioned in the advance-programme
- 202.038 The time limits for the late starters and finishers.
- 202.039 No modifications are to be made to any particular rule after the advance-programme is posted, except if decided by the Race Committee and approved by the Jury and for cases of force majeure only. The modifications must be communicated in writing to the officials and participants 1 hour before the first start at the latest.
- 202.040 Place and time of the drivers meeting.
- 202.041 Radio control equipment frequencies allowed and any other local regulations and restrictions

203 - ENTRIES

203.01 - RATIFICATION

The National Authorities must ratify in writing any entry for drivers wishing to race abroad.

Direct correspondence between the organising committee and foreign drivers is permitted but no entry is to be accepted unless approved by the driver's National Authority.

For Continental Championships the correspondence is held between the National Authorities.

The Organising Committee will keep blank entry forms at the disposal of drivers.

An entry is not valid unless it reaches the organising committee within the time prescribed in the advance-programme.

203.02 - ADMISSION

The Organising Committee has the right to decide whether the entrant is acceptable

No National Authority organising an international race has the right to refuse the entry of a foreign driver commissioned by his own National Authority provided said driver conforms to U.I.M. rules.

The closing date for inscriptions may be a fortnight before the event.

203.03 - ENTRY FEE

Each organising committee fixes the amount of entry fee for the races. Entry is valid only after payment of the entry fee.

The National Authorities have the right to establish a rule whereby higher fees are imposed on late entries.

203.04 - MINIMUM AGE AND IDENTITY

Each National Authority has the right to fix a minimum age for the drivers allowed to race on its water. Such age restriction is to be shown in the advance-programme.

The driver may only race under his name or pseudonym written on the licence.

203.05 - PUBLICITY ON THE BOATS

Advertisements may be freely applied on the entire surface of the boat.

The driver or mechanic can not be compelled to carry any advertising whatsoever on their person, clothes or upon the boat that is being races and his/their refusal to do so cannot in any way be an obstacle to their participation in any race.

Penalties may be applied to any driver who, in the course of any sporting event, has made any statement contrary to the truth or distorting facts.

Political publicity or immoral advertisements are not allowed.

Should no publicity be admitted for an event, this has to be announced in the advance-programme.

Any National Authority has the right to fix its own rules regarding publicity for its own boats and courses.

204 - RACE PROGRAMME **(RACING INSTRUCTIONS)**

204.01

The Organising Committee must clearly state the following points in their race programme (the official programme intended for officials and participants, not the programme to be circulated among spectators) :

- 204.011 Dates and Venues of races
- 204.012 Layout of course drawn to scale
 - Length of the course
 - Direction of the course
 - Starting line and its marks
 - Finishing line and its marks
 - Full details of buoys
 - Full details of fixed obstacles.
- 204.013 Starting time of each race
 Signals for each race
 Signals of postponement, cancellation, re-start and shortening of the race
- 204.014 Time when the control closes for each race
- 204.015 Full particulars of prizes for each race
- 204.016 Time limit and place for handing over the protests
- 204.017 Time and place of the distribution of prizes
- 204.018 Appeal procedure
- 204.019 Composition of the Race Committee, the Jury and the Technical Commission

- 204.020 List of the boats entered for each race and if possible names and clubs of drivers
- 204.021 Place and time at which written instructions will be issued. Where and when the official time is to be communicated and place and time of drivers' meeting
- 202.022 Only drivers whose entries have been accepted by the organising committee have their names printed in the programme.

205 - SAFETY

205.01 - PROTECTIONS

Adequate protection measures must be taken in order to avoid that a boat getting out of the water generate casualties or damage.

For the same safety reasons, it is recommended that boats be controlled from a point located at 1 meter minimum above water level.

The jury will have to determine whether safety conditions are fulfilled.

205.02 - RETRIEVING BOAT

The Organising Committee has to provide for a boat that will retrieve the boats stopped on the course.

One motorboat having all characteristics to guarantee the safety of people in charge of retrieving boats will be used in this purpose.

A spare boat with similar characteristics will be immediately available if required. The retrieving boat will be equipped with an equipment composed of a pole of an adequate length at the end of which a linen strip or cloth or any such device has been attached, up to about 150 cm.

This equipment is meant to hold the boats when the latter have run out of control after a breakdown.

205.03 - PEOPLE IN CHARGE OF RETRIEVING BOATS

People in charge of retrieving boats have to wear a life jacket when performing their duties.

These people are not to be selected among competitors, mechanics of people directly or indirectly concerned by the same competition.

The retrieving order is the order in which boats have stopped, whenever possible. In all cases, it is up to the Officer of the Day.

205.04 - INSURANCE

All drivers must be covered by an adequate insurance, either through an insurance policy subscribed by him and shown before the start or through an insurance policy subscribed by the National Authority.

Do not underestimate your responsibility in case of accidents during authorised official practice before the boats and the documents required are checked. Only this verification is the evidence that the driver is properly insured.

205.05 - TECHNICAL PROTECTION

An engine is not allowed to be on while the propeller is in the air, except when putting the boat immediately on the water.

206 - DISTINCTIVE MARKS

206.01

The national flag is painted on a 5 x 3 cm rectangle on the deck, the bonnet or on the vertical part of the planking. This flag must be visible on one side of the hull.

The boat must also bear, on the same side as the national flag, the boat registration number with the country's identification lettering.

206.02 - RACE NUMBER

The race number is determined by the Organising Committee. It may therefore vary at each competition.

Competitors have to equip their boats with their own plates, in conformity with the data provided in Fig. 4 and bear the number provided by the Organisation.

206.03

In case of loss or damage to the identification data, the competitor has to withdraw the boat immediately from the course. The boat is not allowed to resume the race if the conditions provided for previously have not been met. The laps covered without identification data are not taken into account.

300 - RULES FOR CIRCUIT RACES

300.01 - DEFINITIONS

French	English	Italian	German
Course	Race	Gara	Rennen
Manche	Heat	Prove	Lauf
Tour	Lap	Giro	Runde
Circuit	Circuit	Circuito	Strecke
Parcours	Course	Percorso	Rennstrecke
Base	Base	Base	Messtrecke

CIRCUIT

A circuit is a closed course defined by buoys where races are to be held.

BASE

A base is a course expressly measured for record attempts.

COURSE

Total distance to be covered per heat or race.

MARKS OF THE COURSE

Any objects specially indicated as such in the race instructions are to be considered as marks of the course.

OBSTACLE

Any non specified object is considered as being an obstacle to free navigation.

RACE

A race is a competition in speed between boats held in one or more heats.

301 - TIMING

In all international races and national championships, the timing is carried-out by officially appointed timekeepers using certified timing

devices graduated to one tenth of a second. The devices must have a split second system, one part of which must be capable of being stopped and restarted or any electronic device of the same or better accuracy.

302 - CIRCUIT

302.01

All circuits must be measured on the spot by the Officer of the Day.

302.02 - RACE CONTROL

Race control :

- a) should be situated so that the circuit can easily be controlled
- b) the race must be observed at least by three persons :
 - the Officer of the Day or his assistant
 - one or two officials or one or two members of the Jury.
- c) the officer of the day is the general observer of the race. He must be in contact with his assistant and the Safety Officer
- d) the other two supervisors share the circuit in observation.

303 - MARKS OF THE COURSE

303.01 - DEFINITIONS

The Marks of the Course are any objects (boat, buoy...) that are specifically designated as such in the racing instructions. The objects that are not designated specifically as such are to be considered as obstacles.

A buoy officer is posted near the turning marks. He must observe the turning of the marks and report to the Race Committee.

Should any mark be removed from its proper position, the Officer of the Day shall have it replaced.

Should it be impossible to replace the mark in time for boats to round it, the Officer of the Day shall decide whether the race is to be restarted or not (see art. 311).

Protection buoys may be placed on the circuit.

305 - THE START

305.01

Starts are :

- flying starts with clock or digital display
- dead engine starts

The clock must have a dial of no less than 1 meter.

In case of digital display, the height of each digit must be no less than 20 cm.

The clock is driven mechanically or electrically. It may indicate the seconds but should preferably be moving continuously.

The official start is given by the clock. In case of digital display, time elapses second after second.

305.02 - PLACE OF THE CLOCK

The axis of the clock or the digital display panel must be located on the starting line, no less than 1 meter above water level.

306 - STARTING PROCEDURE - ENDURANCE SERIES

306.01 - CALL TO THE PIT

After the call by the Race Officer, the competitor has two minutes to

come to the pontoon ; after this time, the latter will be considered as failing to start.

306.02 - RADIO TESTS

After these two minutes, the Start Officer carries-out a radio test ; competitors whose radio is transmitting on a frequency different from the frequency stated will be disqualified from the heat or the final in progress. Should the equipment be faulty, they will be allowed to start only after they have repaired their radio and only after being authorised to do so by the Start Officer while the race is proceeding normally.

306.03 - RADIO INTERFERENCE

Should there be radio interference between competitors, the Start Officer is allowed to modify the position of competitors on the pontoon. Should the problem persist, the Start Officer will draw by lots the competitor who will replace his quartz.

306.04 - ENGINE WARMING-UP

At the end of the radio test, the competitor has two minutes to warm-up the engine.

The hull can be placed on the water, provided it is still held. Should the competitor fail to do so, he would be disqualified.

After that time, signalled by the Start Officer, each competitor and mechanic goes to the space he has been assigned behind the boat and stay there until the starting signal, without touching anything on the hull or on its stand.

306.05 - SPACES ON THE STARTING GRID

Spaces on the starting grid must be marked-off and numbered 12 to 1 (see drawing on fig 1).

Number 1 must be closest to the first turning buoy.

They must be between 1.50 and 2 m wide and deep.

In the first series of eliminating heats, the space occupied by the competitor is determined by the Jury ; in the second series, spaces are reversed, with the same race numbers.

The space and race number for finals is based upon the classification established after the eliminating heats.

Space 1 is granted to the competitor who covered the highest number of laps in qualifying heats, and so on.

In case of a direct final, race and space numbers are allocated by the Jury.

306.06 - STARTING SIGNALS

The Start Officer does the countdown as follows :

30" - 15" - 10" - 5 " and starting signal.

After that signal only, competitors are allowed to come closer to the boat and touch it.

Should a competitor or a mechanic touch the hull or anything connected to it before the starting signal are penalised by one lap.

306.07 - EARLY START

In case of early start of the engine, the competitor is disqualified for the heat in progress.

306.08 - PENALTIES

Calls to order and penalties incurred by competitors during a competition are notified by the Jury to the competitor or his mechanic on a yellow card to indicate a blame or a red card for a disqualification.

306.09 - POSITION IN THE RACE

Whenever possible during the heats and the final, the Jury should provide "non official "information on positions in the classification.

307 - STARTING PROCEDURE - RC HEAT **RACING SERIES**

307.01 - CALL TO THE PIT

After the call by the Race Officer, the competitor has two minutes to come to the pontoon ; after this time, the latter will be considered as failing to start.

307.02 - RADIO TEST

After these two minutes, the Start Officer carries-out a radio test ; competitors whose radio is transmitting on a frequency different from the frequency stated will be disqualified from the heat or the final in progress.

Should the equipment be faulty, they will be allowed to start only after they have repaired their radio and only after being authorised to do so by the Start Officer while the race is proceeding normally.

307.03 - RADIO INTERFERENCE

Should there be radio interference between two competitors, the Start Officer is allowed to modify the position of competitors on the pontoon. Should the problem persist, the Start Officer will draw by lots the competitor who will replace his quartz.

307.04 - SPACES ON THE STARTING GRID

Spaces on the starting grid must be numbered 1 to 8 (see drawing on Fig.2), n°1 being closest to the first turning buoy.

307.05 - STARTING PROCEDURE

Each heat in Heat Racing is composed of three different stages in the starting procedure :

307.05.01 - STAGE 1

Stage 1 is so-called "Pit time" or preparation time. It lasts two minutes during which the competitor starts the engine and launches the boat. The Start Officer authorises the launching of the boat.

Failure to wait the authorisation by the Start Officer would lead to a disqualification from the heat in progress.

At the beginning of this stage the starting clock, located at the centre of the circuit, in line with the arrival line, is started and a whole round of it takes 30".

Four lights are simultaneously on, each of them being off every 30". Should all competitors fail to start the engine during Stage 1, the heat would be considered null and could not be repeated.

Competitors keep running on the racing course until the end of Stage 2.

In case of digital clock, time is displayed backwards.

307.05.02 - STAGE 2

Stage 2 is so-called "Clock time". Its duration is 30" and it starts at the end of Stage 1.

An orange light is on during the whole stage.

No boat can be put on the water during Stage 2.

During Stage 2, drivers have to drive their boats clockwise on the circuit in order to get to the starting line at the end of this Stage, which coincides with the beginning of Stage 3.

The end of Stage 2 coincides with the effective beginning of the heat, whatever the position of competitors on the course. It is signalled by a flash located on the clock and by the arm on the zero.

The heat is considered as effectively started when a boat crosses the starting line after the end of Stage 2.

Boats crossing the starting line before the end of Stage 2 are considered as having taken an early start and must therefore complete the lap and cross again the starting line.

307.05.3 - STAGE 3

Stage 3 is so-called "Course Time".

This stage consists in covering the number of laps required. The boat completing them first is the winner. Course is considered as completed only after laps penalties, if any, have been covered.

307.06 - PENALTIES

A driver infringing the rules is inflicted a penalty lap or disqualification for the heat in progress if he damages another competitor. The infringement and the penalties inflicted must be immediately notified by the Start Officer to the competitor.

A boat that rounds a buoy inside at any moment of the three stages incurs a 50-point penalty for each buoy so fouled.

Touching one or several buoys involves no penalty.

Should an infringement be of a serious nature likely to damage competitors' hulls, hurt spectators, etc. because of a wilful violation of rules, the offender would be excluded from the competition in progress as well as from all the other races that are to take place during the same event : this is subject to a final judgement by the jury.

307.07 - COLLIDING WITH HULLS STOPPED ON THE COURSE

Colliding with a hull stopped on the course involves disqualification for the heat in progress.

Should a driver collide with a hull stopped on the course twice on the same event, he would be disqualified for the competition in progress as well as for all the other races that are taking place during the same event.

308 - RACE CANCELLED

The Race Committee has the power to cancel any race should unfavourable weather or reasons of "force majeure" render this action necessary.

309 - RACE POSTPONED

309.01

The officer of the Day has the power to postpone the start even after the first signals have been given, but only in cases of "force majeure" or if an error in the starting signals has been made.

309.02

In both cases, signalling is to be recommenced. When the need for postponement has passed, the Race Committee signals the start according to the instructions contained in the programme.

New entries are not to be accepted for postponed races.

Only drivers who duly entered and were present when the race was postponed are admitted.

311 - RACE STOPPED

A stopped race is a race that has been interrupted by the Officer of the Day after the start.

Stopping the race is decided by the Officer of the Day for reasons of which he is the sole judge.

A boat that has committed an infringement to the rules (leading to a disqualification) in a stopped race is not allowed to start in the re-started race.

312 - RACING CONDUCTS - R.C. ENDURANCE SERIES

312.01 COURSE

During the race, competitors have to maintain their course, avoiding other boats. Should a competitor infringe this rule, he would be first blamed and in case he would do it again, he would be disqualified upon a decision by the Jury.

312.02 - BACK TO THE PIT

It is not allowed to drive a boat back to the pit by crossing the course. Infringing this rule involves an immediate disqualification of the competitor for the heat in progress.

312.03 - STARTING MODE

The boat starts from the pontoon on a line parallel to the direction of the race, leaving priority to those boats coming from the left.

312.04 - REVERSED NAVIGATION DIRECTION

U-turns are not allowed. Any infringement to this rule would lead to disqualification.

312.05 - ROUNDING AND FOULING MARKS

A competitor that does not cover the course regularly, missing one or several marks in the same lap, will be penalised by as many laps as the marks missed.

A mark that has been missed can be fouled again and then no penalty is incurred .

312.06 - RADIO FAILURES

The competitor who has remote control problems should raise his arms and state "RADIO FAILURE", enabling then the Judge to take note of the number of the boat, to advise other

competitors with the loudspeaker and to decide on its retrieval if required.

312.07 - RETRIEVAL

During retrieval procedures, competitors have to be most careful to avoid endangering the people in charge of retrieval.

Speed must be reduced near the retrieving boat operating on the circuit.

Those infringing these rules will be blamed and in case they do it again or in case of collision, they will be disqualified by a final judgement by the jury.

A boat retrieved can resume the race during a final only.

A boat retrieved during an eliminating heat and/or a semi-final is not allowed on the water for the whole heat in progress.

312.08

To better understand items 312.xx, see group 900 Annex C herein.

313 - RACING CONDUCT - R.C. HEAT RACING SERIES

313.01

During stage 2 of start procedure, competitors should navigate in such a way that they will cross the starting line at full speed.

However, during the last five seconds of Stage 2, they can neither slow down nor alter their course when they have passed Buoy n° 6.

313.02

As a general rule, it is forbidden to suddenly alter one's course and the competitor with the inside position has the priority.

313.03

When overtaking another boat, the competitor will have to provide for a length equal to the size of no less than three boats before cutting in, in order to avoid obstructing the course of the competitor overtaken.

313.04

Fast turns to the left (except intended to avoid a collision) are forbidden.

Slight corrections to the left are admissible, but only to overtake a competitor.

A turn to the left over 45° may be considered as an infringement.

313.05

Should a boat damaged be unable to complete the heat, the driver of the boat who has committed the infringement is disqualified for the heat in progress.

313.06 - RADIO FAILURES

The competitor having problems with his radio control must immediately raise his arms and state "RADIO FAILURE" enabling then the judge to take note of the number of the boat, to advise other competitors with the loudspeaker about the boat which is in trouble.

313.07

For safety reasons, the retrieving boat will not navigate while boats are racing.

Once the race is over, the retrieving boat can navigate and retrieve the boats that have been damaged or have had a breakdown.

During practice, the retrieving boat will navigate only if all boats are stopped.

The competitor putting his boat on the water while the retrieving boat is navigating will be excluded from any competition.

313.08

To better understand items 313.01 to 313.07, see group 903 (Annex C).

318 - SCORING SYSTEM

318.01

In Endurance competitions, a competitor has to complete no less than one validated lap to be scored.

318.02

The competitor who has had no valid result is not scored.

318.03

The official results must be posted on the notice board within an hour of the end of the races.

318.04

A general classification of different classes racing together is not permitted.

318.05

National classes may be raced at international meetings.

318.06

If boats of different series or classes start together, every boat must be classified in its own class.

When the programme for a meeting does not provide races for all classes, boats not provided for may race in the next higher class in the programme, subject to their acceptance by the Race Committee.

318.07

It is forbidden to take part in a race for a lower class than that to which a boat belongs, except with the written consent of all competitors.

The Race Committee may hold a race, as a separate class, for one design hulls equipped with identical engines.

319 - DEAD HEAT

319.01

Two boats are in dead-heat when :

- They cross the line together
- They obtain the same points in a points race
- They obtain the same ratings in a rating race

320 - POSTING OF THE RESULTS

The results of each race must be posted at the pits in clearly visible position.

The officer in charge must sign the results and state the hour of posting. The same officer has to be present during one hour so as to receive protests.

321 - INSPECTION OF HULLS AND ENGINES

321.01

Boats and engines taking part in a race which are subject to checking must not leave the boat park until an hour after the results have been posted.

321.02

For special cases (inspection of multi-cylinder engines and others involving the use of special tools and equipment) the Technical Officers postpone the inspection and stipulate the time and place where the engines must be available.

321.03

In the meantime, the engine must be sealed to prevent any alterations being made.

Refusing to produce the engine or the hull entails disqualification.

321.04

The scrutineers are requested to dismantle the engines according to the correct practice and limit their inspections to the necessary parts.

Engines and boats will be placed pending inspection in well defined place which remains under the care of the "boat park" official. This is called the "parc fermé".

321.05

After the pre-race technical inspection, the boats must not leave the boat park except for practising or racing.

321.06

If any repairs can only be effected outside the boat park, they must be carried out in the presence of the Technical Officer.

321.07

Officers allowed to attend the inspection :

- The Officer of the Day
- The delegate of the interested National Authority
- The measurers named by the Organising Committee
- The driver and a mechanic if necessary
- The U.I.M. Observer
- The U.I.M. Commissioners

The presence of other persons is only allowed if authorised by the Officer of the Day.

322 - PRIZES

322.01

A National Authority may ask to reduce the number of prizes according to the number of competitors. However, the value of the prizes must be maintained as provided for in the advance-programme. This possibility must be explained in the advance-programme.

322.02

Challenge to be competed for over several years :

if a competitor fulfils the conditions for winning the challenge definitely, he is awarded the challenge. If two or more competitors fulfil the conditions for winning the challenge, the holder shall be decided by drawing lots.

323 - AFTER THE MEETING (REPORTS)

323.01

At the end of the meeting, the Secretary of the Race Committee must send to the Radio Controlled Commission of his National Authority a report including :

323.01.1 Classification and distribution of prizes ;

323.01.2 Report by timekeepers

323.01.3 List of drivers

323.01.4 Penalties

323.01.5 Documents on any records broken in conformity with U.I.M. rules

323.01.6 List of protests and decisions relating to the meeting or the race

323.01.7 All necessary documents in order to evaluate the meeting or the race

323.02

These documents must be sent to the Radio Controlled Commission of the National Authority within 8 days of the end of the meeting or the race.

323.03

After each titled meeting, the organisers must send the results to the Secretariat of U.I.M..

323.04

If the results are taken into consideration for the granting of a trophy or a prize, the points must be sent to the U.I.M. by cable within 48 hours.

323.05

20 days after a competition, the organising club must send the final results to the National Authorities of all participants and to the Secretariat of the Union.

500 - SERIES AND CLASSES

500.01 - ENDURANCE

Class	E 3.5	up to 3.50 cc
	E 7.5	up to 7.50 cc
	E 15	up to 15.00 cc
	E 35	up to 35.00 cc with ignition only

500.02 - "HYDRO" HEAT RACING

Class	H 3.5	up to 3.50 cc
	H 7.5	up to 7.50 cc
	H 15	up to 15.00 cc

500.03 - "FREE HULLS" HEAT RACING

F 15	up to 15.00 cc
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502 - INSPECTIONS

502.01

Hulls, engines, fuel, accessories and equipment's, subject to restrictions regarding dimensions and other characteristics must be submitted for verification.

502.02

The competitor must report with his boat at the inspection jury who will stamp it.

In this purpose, the boats must be in good working condition and must be provided with a port of no less than 1.5 mm diameter on a non-eradicable side.

502.03

The driver must report with his boat at the inspection and must comply with the instructions given by the Race Committee.

502.04

Technical inspectors have the right, once races are over, to carry out all the checks they consider necessary even when the inspection has taken place before practice and they have the right to inspect as they think fit.

502.05

Drivers are at all times responsible for the condition of their boat.

502.06

Errors, if any on the part of the manufacturer, builder, mechanic or even the previous owner do not justify in any way non-conformity with the rules.

502.07

Any driver refusing to comply with the decisions of the Technical inspectors or who do not comply

with the conditions of the rules, is not allowed to take part in a race, or should he have raced cannot be classified and penalties can be applied.

503 - SPARE BOAT

There can not be spare boat

504 - NOISE REDUCTION**504.01**

The use of an efficient device to attenuate the noise is compulsory.

504.02

The noise emitted by the engine cannot exceed the maximum limit of 80 dB(A).

504.03

The noise measurement is done with the boat on the water while racing by means of suitable and freshly calibrated noise level meter placed at 22 metres from the running boat.

Standard noise level measurements techniques should be used along with the noise level meter manufacturer precautions given in order to minimise the influence of the environment.

504.04

Should the exhaust pipe or the silencer break-down, the competitor has to move back to the pits immediately, following a regular course and must have it repaired. Should this rule be infringed, the competitor would be disqualified for the heat in progress. In Heat Racing, the boat has to go back immediately to the pits. It will start again only if the race is still in Stage I.

504.5

However, should the laws of the organising countries be more drastic, the latter will apply.

505 - REPLACEMENT OF MECHANICAL PARTS

Mechanical parts as well as radio control parts can be replaced (without changing the frequency declared).

510 - CALLING TIME

Once the calling time is over, the competitor loses his right to take part in the heat for which he had been called.

512 - HULLS

512.01

512.01.1 - ENDURANCE CATEGORY

The hull and the bottom are free.

512.01.2 - "FREE HULLS" HEAT RACING CATEGORY

The hull and the bottom are free.

512.01.3 - "HYDRO" HEAT RACING CATEGORY

HYDRO means a hull that is born by two or more surfaces being separated under water while the boat is racing.

512.02 - WEIGHTS AND LENGTHS

There is no weights limit.

The total length, everything included, should not exceed 1800 mm.

513 - MACHINERY

513.01

The type and number of engine is free, cylinder capacities being defined under item 500.

513.02

One or more propulsion devices or engines are allowed and the pushing effort must be applied in the water. The class is defined by adding the engines cylinder capacities.

513.03

The transmission and the use of multipliers, reducers, differentials, clutches, etc. are free.

520 - BREAKING OF RULES

520.01

Should a competitor break the rules, he will be liable to the following :

- disqualification of the boat (for infringement of class rules; in that case, results obtained in races will be cancelled and the competitor is excluded from the classification);
- disqualification of the competitor (for infringement of the rules on courses, maximum time, class, noise level, misbehaviour during a race).

Should the competitor or the mechanics behave in way that goes against the sporting spirit, the competitor would be reported to the R.C. Commission by the Officer of the Day.

During the race, the competitor and his mechanic have to comply with the provisions taken by the Jury and to have a correct behaviour vis-à-vis all the race Officers, other competitors and the public. Should they fail to do so, they would be disqualified and reported to the R.C. Commission.

530 - RETRIEVAL OF BOATS

530.01

The driver and his mechanic may retrieve their boats only from their stand on the pontoon. In such a case, they are authorised to use a tool whose total length does not exceed 1.20 m. Any infringement of this rule would lead to the disqualification of the competitor for the heat in progress.

The retrieval of dead boats is to be done only by people in charge of the retrieval.

540 - LEAVING THE PONTOONS

The competitor who leaves the pontoon with the boat during a heat or a final in progress is considered as withdrawing from the heat or the final in progress.

550 - FUEL

Fuel has to be provided by the competitor who is the sole responsible for its composition.

According to class/category rules the basic components for fuel are lubricating oil and METHANOL or GASOLINE as appropriate.

The use of other substances aimed to increase engine performance is regulated by the Class/Category rules.

The race officials are entitled to make fuel tests at any time during the event.

Use of fuel not complying to Class/Category rules lead to disqualification of the competitor from the involved race.

580 - TURBO-JETS AND ROCKET PROPELLED BOATS

580.01

Turbo-jet and rocket propelled boats are accepted as international series.

580.02

These boats are not entitled to race, though they can attempt world speed records.

580.03

Propulsion is according to the physical law which states that action and reaction are equal and opposite.

580.04

A rocket is an engine which burns two fuels other than air. These two fuels are combined so as to obtain a mass of gas which is rejected at high speed.

700 - ENVIRONMENTAL

701 - NOISE LEVEL

Every driver is responsible that his engine(s) does not exceed established noise levels at all times. See rules 504

702 - EMISSIONS

To reduce emissions as much as possible, all drivers are recommended to use biodegradable lubrication oil.

703 - RE-FUELLING

Every care must be taken not to spill fuel or oil. An absorbent carpet to avoid any spillage on to the ground must be used.

704 - ENVIRONMENTAL CARE IN RACE AREAS

704.1

Every organiser must appoint a responsible person or persons to make necessary arrangements to control the environment of the whole race area including spectator areas.

704.2

There must be large containers in the pits where waste, such as cans, containers, oil waste can be placed.

One person must be responsible for this and to authorise the proper removal after the race meeting with the Local Authorities.

704.3

It is mandatory to inform the Drivers of all these environmental matters at the Drivers meeting (Rule 204.021).

800.01 - ELECTRIC COMPETITION

A - CATEGORIES

a.1 - Multi Racing (MR)

a.2 - Heat Racing (HR)

B - ENGINES

Any type of motors are allowed (including Cobalt magnets type).

More than one motor are allowed on a boat.

C - BATTERY CELLS

Only Sub-C type (43 mm x 23 mm) are allowed as motor supply.

No restrictions for Radio Control supply.

D - HULLS

d.1 - MR Classes : Any material, mono-hull only, submerged prop. Drive.

d.2 - HR Classes : No restrictions.

E - PROPELLERS

Only water propellers allowed.

No restrictions on propeller material, number of blades, etc.

F - POWER DRIVE

No restrictions on type of drive (direct drive and/or gear boxes allowed in all classes).

G - SPEED CONTROL

Must be remotely operated by the Radio Control device.

It must be able to completely stop the engine.

No restrictions on how the control is achieved.

H - ONBOARD ENGINE POWER BREAKER DEVICE

It must be fitted externally on all boats and clearly marked ON/OFF.

J - RACE NUMBERS

A vertical plate must be provided on all boats (80 mm X 80 mm.).

Number should be BLACK on WHITE background. See Rule 902 – Fig 4/B

Race number to be decided by the organiser committee and to be communicated to the competitor at time of entry.

K - CLASSES

k.1 - (MR) Formula 2

Race time 5 minutes

N. of Cells up to 7

Course M type (anti clock-wise turns)

k.2 - (HR) Oval 1

Race time 4 minutes

N. of Cells up to 20

Course Oval type (clock-wise turns)

k.3 - (HR) Oval 2

Race time 4 minutes

N. of Cells up to 12

Course Oval type (clock-wise turns)

k.4 - (HR) Oval 3

Race time 4 minutes

N. of Cells up to 7

Course Oval type (clock-wise turns)

L - RACING COURSES

I.1 - M type (for Multi Racing)

See Art. 901.01 (fig.1) for Course layout

Sizes A = 60 metres ; B = 30 metres ; C = up to 6 metres

I.2 - Oval type (for Heat Racing)

See Art. 901.02 (fig.2) for Course layout

- Sizes
- a) Straights = 44 metres ; Turns radius = 7,6 metres
(EMERGENCY ONLY)
 - b) Straights = 90 metres ; Turns radius = 14 metres
(PREFERRED)

M - NUMBER OF BOATS

A maximum of 6 boats are allowed in each heat.

Only one driver for each boat is permitted.

N - NUMBER OF HEATS

A minimum of three qualifying heats are run by every competitor in each class.

Per heat points are granted as follows :

1	400 pts
2	300 pts
3	225 pts
4	169 pts
5	127 pts
6	95 pts

Not started : 0 pt

The performance of each competitor is calculated by adding the scores gained in each heat. The worst heat results not to be considered for this calculation.

The best performance of each competitor is taken to qualify for the final in each class.

The best 6 results run the final.

If there are less than 6 qualified competitors in a given class, no final is run and the best qualifying results are taken as final scoring.

O - STARTING PROCEDURES

o.1 - Multi Racing

- a) call to the pits (2 minutes to complete the starting grid)
- b) Radio Controls check should be successfully performed
- c) 15 seconds to start is announced
- d) all the boats must be put on the water and kept in position by the pit mechanics
- e) 5 seconds to start is announced and then a second by second count down to GO
- f) boats must live the starting pits straight toward buoy n.2 for the first left turn

o.2 - Heat Racing

- a) call to the pits (2 minutes to complete the starting grid)
- b) Radio Control check should be successfully performed
- c) all the boats must be put on the water and kept in position by the pits mechanics
- d) at the START signal the boats may be launched
- e) boats must live the starting pits straight towards buoy n.4 for the first right turn
- f) START signal is immediately followed by a 15 seconds count down announced as follows :
10, 8, 6, 5, 4, 3, 2, 1, GO
- g) boats not launched before 5 seconds announce are considered retired for that heat

- h) once entering the course a boat must keep going
- i) launched boats should manage not to cross the start line before the GO.

P - END OF RACES

At the end of the Race Time the STOP is announced. All boats on the course shall complete the lap they begun prior the STOP announcement. The time to complete the lap after STOP is recorded for each boat and the lap is recorded as well.

The last lap is not scored if the time to complete if after STOP exceeds 60 seconds.

Q - PENALTIES

Early START, buoy jumping, stop the boat after launching it during H.R. starting procedure will lead to 1 lap penalty.

Hitting an identified dead boat on the course will lead to disqualification for the heat.

When this occur, the disqualified boat must leave immediately the course whenever possible.

R - OTHER APPLICABLE RULES

For every situation not specifically addressed in this set of rules, the U.I.M. Radio Controlled Rules are applicable as appropriate.

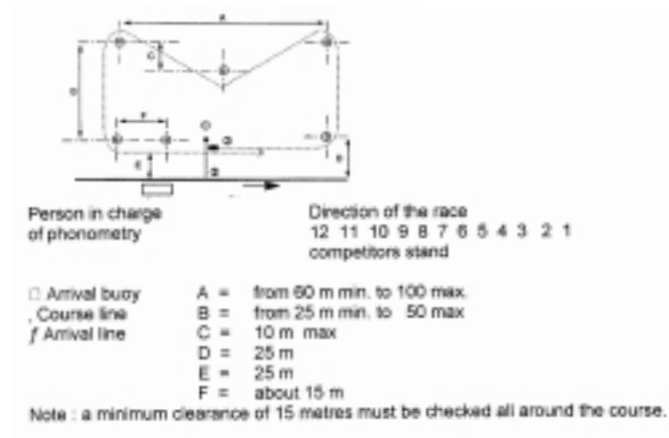
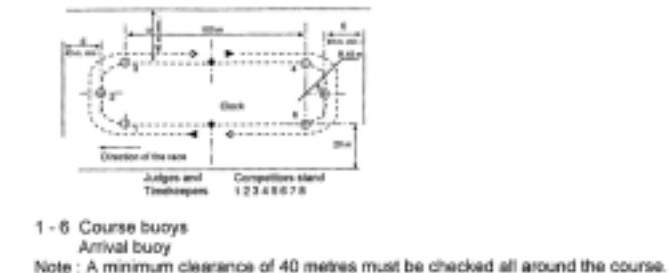
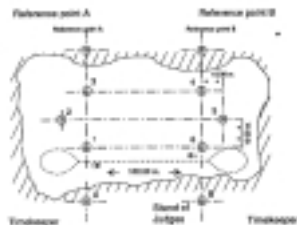
Fig. 1 901.01 ENDURANCE COURSE**Fig. 2 901.02 HEAT RACING COURSE**

Fig. 3 901.03 SPEED RECORD COURSE**Fig. 4 902 RACING NUMBER PLATES****A. Endurance**

Black digits on White background

Numbers Height : Minimum 75 mm Maximum 80 mm

Numbers Width : Minimum 30 mm Maximum 40 mm (except for N° 1).

B. Heat racing and electric boats

Black digits on White background

Numbers Height : Minimum 65mm Maximum 75 mm

Numbers Width : Minimum 20 mm Maximum 25 mm (except for N° 1).



ANNEX C 903 - REF. RULES 312 AND 313

BREACH OF THE RULES

Rules set forth hereunder are mandatory. Any failure to comply with them will be considered as a breach and will lead immediately to a reprimand, a lap penalty or disqualification for the race.

A) Turns

Sharp turns are not allowed, except to avoid a collision. A slight correction may be required to overtake a boat but a turn exceeding 45° during the race is considered as a breach of rules.

Should a sharp turn cause a competitor to overturn, the person having committed the fault would have one lap penalty.

Should the boat so overturned break down in such a way that it cannot resume the race, the person having committed the fault would be disqualified for the heat.

B) Rules of the road

1. A boat following its course on the circuit must be able to keep it without having its way obstructed by other competitors trying to interfere.

A boat wishing to overtake another boat and to race on its lane must cover a length corresponding to no less than three boats before cutting in.

2. During a race, the natural course is the one which the most similar to the course drawn.
3. The above-listed rules also apply to turns.
4. Driving and sporting rules require that water lanes or courses be kept. Zigzagging or any other action aimed at preventing or obstructing the way to an overtaking competitor is considered as an infringement to the rule by the said boat.
5. Any failure to comply with these rules are breaches and lead to penalty laps.

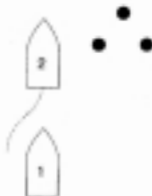
C) Overtaking

Overtaking is allowed anywhere on the circuit provided rules set forth in Chapter "a" (turns) and "b" (rules of the road) are being complied with. Examples are given hereafter.

Drawing n° 1 : Boat n° 1 has the right of way. Boat 2 and 3 try to incorrectly overtake it. Boat n° 2 cuts the course of boat n° 1. This is a breach of rules. Boat n° 3 tries to get closer from the inside, it has to turn left and cuts the course of boat n° 1 to avoid the budy. This is a breach of rules.



Drawing n° 2 : Boat n° 2 overtakes correctly. If a boat overtakes another and keeps its course in such a way that it covers the length of three boats before cutting in it acts correctly.



Drawing n° 3 : This procedure also applies to turns. N° 2 is to be penalised. Note that n° 1 has to cut in towards the buoy to avoid a foreseen collision. N° 2 breaks the rules.



Drawing n° 4 : N° 2 turns and provides much space between itself and the other boat. N° 1 does not have to cut in towards the buoy. No breach.



Drawing n° 5 : In this case, competitor 1 leaves its course by turning off far, making it possible for the other driver to take up its course by turning sharp. There is no breach. Now n° 2, at point « X », is on the right course.



[illegible]

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2004 ADDENDUM

RULE CHANGES TO THE 2003 PLEASURE NAVIGATION RULES

ART. 309 – 405 - 502

309 – World & Continental Endurance Championships

A. Introduction

The Title of the Championship shall be known as the “Powerboat Production 1 - UIM World Championship Endurance Racing ” (hereinafter “Powerboat Production 1”). A similar title is applicable to any Continental Championship to which these rules apply.

Powerboat Production 1 is the UIM Pleasure Navigation endurance classification comprising Group A monohull craft in a racing series including the following categories: Sport, Super Sport, and Evolution (formerly Tourism, Sport, Prototype and Super Yacht). The purpose of the rules set forth herein is (1) to ensure safe and competitive racing in the race series at reasonable cost to the participants, (2) to expand and broaden the general base of participation and (3) for the marine industry to develop and demonstrate the performance of its products.

This rule 309 concerns only the UIM World and Continental Endurance Championship. In the case of conflicting rules, these rules (Rule 309) shall prevail over the other relevant UIM rules applicable to Endurance Racing.

In cases seemingly not foreseen by the rules decisions shall be taken bearing in mind the spirit of the rules.

B. Championship and Race Format

The UIM Powerboat Production 1 World Championship is composed of a minimum of five races per season. The Title “World Champion” shall be awarded to the highest points winner over the course of the Championship for each category. To be eligible for the Championship, the boat and team combination must be present and participate in all Championship events subject to exceptional circumstances agreed by the Production 1 Management Committee. ***The term “team” shall be defined as the first and second pilot as identified by the team in its championship inscription.***

Championship points shall be awarded to the top six (6) finishers in each category with points awarded as follows: Winner 10 points, 2nd place 7 points, 3rd place 5 points, 4th place 3 points, 5th place 2 points and 6th place 1 point.

Each Powerboat Production 1 event shall be composed of two separate races, a Saturday race and a Sunday race. Championship points shall be awarded for both the Saturday and Sunday races.

1. Saturday (Rally) Race

This race shall be minimum 50 nautical miles in distance.

2. Sunday (Endurance) Race

This race shall be minimum 80 nautical miles in distance.

Subject to modifications by organizing committee.

The courses are subject to change or modification upon appropriate notice to teams due to safety reasons, bad weather, local authority restrictions, etc...

C. Boat Specifications

These boat specifications (taken together with the race format described above) seek to create a broad and fair competitive balance within each race category.

1. General Rules applicable to all categories

(a) All race boats shall be monohull craft.

(b) All race boats shall be limited to two identical inboard engines while racing (***outboard engines shall be permitted for a transition period***). Teams are limited to three engine blocks per race weekend; in other words, teams may only change one engine during the course of the race weekend. Spare parts are unlimited except that in Sport and Super sport after market replacement and/or modified parts that seek or have the effect of an increase in power are prohibited.

(c) There shall be no performance index.

(d) Boat canopies are not permitted, all boats shall be open top.

(e) There shall be no technical outside assistance during the race. Teams may carry a mechanic during the race and are allowed any assistance that the team in the boat may provide.

(f) Each team may carry up to 4 crew in the boat during the race.

(g) boats may run on fuel pursuant to rule 302.10. No performance increasing additives are permitted

(h) all participating boats are subject to manufacturer certification and certification of a technical body as per UIM rules. ***Participants shall provide proof of a manufacturer certification of all boats or the certified results of a dyno test not more than 6 months old to the UIM technical scrutineer upon demand at events.***

(i) boat race numbers will be provided by the Promoter upon inscription for the Championship.

(j) Boat name shall be that of primary sponsor or team owner.

(k) each boat shall carry the appropriate UIM logo identification ***and Championship or event title sponsor logo identification as required by the Promoter.***

(l) Attendance at all official race briefings is mandatory. Non-attendance may be punished by disqualification.

2. Category Sport (formerly Tourism)

Eligible boats shall:

(a) be Group A boats with minimum length from 9.00m having standard production model hulls and standard production model engines;

(b) have no modifications to hulls and engines, changes to accommodation fittings, etc... ;

(c) have a minimum commercial hull production of 25 units and have sold and titled them to pleasure boat owners;

(c) be engines listed in the manufacturer catalogue and have having a commercial production of 100 units;

(d) have the same props and drives as sold by manufacturer;

(e) have minimum cabin clearance and cabin size determined with reference to length of boat as follows:

Length	Min. Clearance	Min. cabin surface
9.00m -11.99m	1.75m	2m_
12.00m – 14.99m	1.90m	4m_
15.00m – 19.99m	1.90m	6m_
20.00m – open	1.90m	8m_

(f) have a maximum power to weight ratio of 1hp per 10kg (5% tolerance). There shall be no reference to maximum capacity of engine;

(g) have all other matters consistent with existing Group A rules (502.02).

3. Category Supersport (formerly Sport)

Eligible boats shall:

(a) be Group A boats with minimum length of 9.00m and a maximum length of 15.00m having standard production model hulls and standard production model engines that would otherwise qualify as Sport category but fall outside because they do not comply with one or more category Sport requirements;

(b) have a maximum power to weight ratio of 1hp per 5kg (5% tolerance). There shall be no reference to maximum capacity of engine;

(c) have all other matters consistent with existing Group A rules (502.02).

d) After market spare parts or special accessories available in the market, shall be permitted for the drive system only, including water pick up, extension boxes, nozzle, props, steering system, special flaps, and power lift.

4. Category Evolution (formerly Prototype)

Eligible boats shall:

(a) consist of boats not meeting the requirements of the Sport or Super sport categories. ***Additionally boats shall have a minimum length of 10.8 metres and a minimum weight of 4500 kg post race. Boat weight shall be verified by mandatory weighting of at least the top 4 finishing boats after each race.***

(b) have a power to weight ratio of 1hp per 3.5 kg. There shall be no reference to maximum capacity of engine.

c) For purposes of defining “boat and team combination”, each team shall consist of a minimum of three licensed pilots inscribed in the World Championship. The driver and throttleman shall be considered the first and second pilots and must be identified as such in the World Championship inscription. The third pilot shall act as alternate to the first and second pilot and, in the event of their absence, may substitute or replace them.

D. Interpretation of Rules / Changes to Rules

The race format and the boat specifications found herein seek to create a broad competitive balance within race category. The use of the term “standard” in these rules is meant to describe components or parts that are both manufactured and intended for sale to the public at commercially reasonable prices. For purposes of competitive balance, the Production 1 Management Committee is entitled to determine into which Powerboat Production 1 category a particular boat qualifies. It may require adjustments to boats, engines, drives, propellers, etc... to ensure a competitive balance within category.

It is the intent of these rules to limit top speeds to 90mph for safety reasons.

E. Enforcement

Consistent with the purpose and intent of the rules, the Production 1 Management Committee is entitled to take whatever action it deems necessary to enforce these rules. Any parts or components which give a boat

or team an unfair competitive advantage may be deemed illegal and result in disqualification. The Production 1 Management Committee, in its sole and absolute discretion, may decide if any part, component, or equipment change is in an effort to defeat or beat the rules, and it may disqualify an entry in violation of the spirit and intent of these rules.

The Production 1 Management Committee is composed by

- The UIM President
- The President of Pleasure Navigation Commission
- The promoter

405 – THE APPEALS PROCEDURE

405.01 – THE APPEAL BOARD

No changes to the 2003 rules.

405.02 – INTERESTED PARTIES

No changes to the 2003 rules.

405.03 – RIGHT OF APPEAL

A decision by a Jury may be appealed by the parties involved when they consider that an injustice has been made against them and/or fresh evidence has been made available that may alter the decision of the Jury.

A driver can also appeal against :

- a penalty, imposed by the U.I.M. Executive Committee, or imposed by the OOD for the reason of post race technical scrutinising
- ***new results, but only concerning the decisions, which lead to the changing of the results***

without the prior lodging of a protest, but only if this was not possible, because the race jury has had dissolved itself before or within the protest period after the finish of the event.

405.04 – NOTICE OF INTENTION TO APPEAL

The intention of appeal must be notified in writing to the Secretariat of the U.I.M. by the interested party within four days of the day following the jury decision ***or in the case of a decision against which a driver can appeal without the prior lodging of a protest within four days of the day following the notification of this decision to the NA of the driver or if earlier to the driver himself.***

405.05 – TIME LIMIT

No changes to the 2003 rules.

405.06 – LODGING OF AN APPEAL

No changes to the 2003 rules.

405.07 – NOTIFICATION OF THE PARTIES OF THE APPEAL

No changes to the 2003 rules.

405.08 – TIME LIMIT FOR DECISIONS ON APPEALS

No changes to the 2003 rules.

405.09 – APPEAL BOARD DECISIONS

The Appeal Board takes its own decision based on every information available. The decision of the Jury may be upheld, changed or not upheld. The decision of the Appeal Board is final.

405.10 – COSTS

The appeal fee is 1525 €. In addition the driver has to pay 2290 € to the U.I.M. for possible costs of the appeal committee (meetings, telephone or fax costs). The appeal fee for a single championship/invitation race (not a Formula race, or a WOC race or similar) is 760 €, in addition the driver has to pay 760 € to the U.I.M. for possible costs of the appeal committee.

The driver who appeals has to pay for the meeting costs anyway if the appeal is upheld or not. If the cost of the appeal committee is less than 2290 € then the difference will be paid back to the driver.

The appeal fee of 1525 € will be refunded if the appeal is upheld. ***If the original decision is changed by the appeal board (405.09), it can decide to refund a percentage of the appeal fee if appropriate.***

The driver may agree to forego an appeal committee meeting and for the appeal to be handled through telephone, Email and fax, if possible, in order to minimise the cost for the appeal.

502 - ENDURANCE COMPETITIONS

502.01 - GENERAL

In order to clarify the regulations of Endurance Championship it is pointed out that all boats certified CE entering the over mentioned Championship must conform to ISO-CE regulations currently in force, with particular attention to :

- ISO 10088 – 7840 (tanks installation)
- ISO/DIS 11592 : 1997 (determination max power for FB engines) relevant to boats. ***For outboard 4-stroke engines a further 5 % increase is allowed.***
- ISO/DIS 6185 – 3 : 1997 (determination max power for FB engines) relevant to inflatables. ***Four outboard 4-stroke engines a 5 % increase is allowed.***

Engines

The maximum engine capacity for the various categories and classes is referred to:

- the total of the engines installed in case of petrol aspirated engines;
- the allowed capacity must be the same used for petrol turbocharged engines and diesel turbocharged engines, while it is legitimate to allow the capacity doubling exclusively for diesel aspirated engines;
- for petrol and diesel engines the presence of supercharging causes a reduction of the maximum allowed capacity that shall be divided per coefficient 1,4.
- No tolerance is allowed to cubatures resulting from the computation.

Length Measurement

The criteria of length measurements for the purpose of craft classification and breadth calculation are illustrated below :

Length of a boat
(drawing)

Length of a RIB (Rigid Inflatable Boat)
(drawing)

Beam of a craft

The beam is to be measured on the deck, excluding any side appendices. For RIB (Rigid Inflatable Boat) the measurement shall include the overall size of the tubes; a tolerance of +/- 3% is allowed.

502.02 - GROUP “A”

RACE NUMBER

The race number shall be displayed on the topsides (port and starboard) of the hull and on the deck, within a dedicated area :

- The numbers on the topside (port and starboard) shall be displayed on the forward half of the boat.
- The number on the deck shall be correctly read from the driver seat position.
- Numbers and letter must be highlighted in black on a white background.
- The letter and number which identify the class shall be put in front of the race number.

The race number and its dedicated area shall have minimum dimensions as indicated below

- Numbers and letter must have a thickness of no less than 75mm.

CATEGORIES AND CLASSES

Tourism category

It includes cruising vessels regularly homologated for such a purpose including internal fittings complying with the specifications of the boat builders; these fittings must be suitable for normal life on board for the whole crew and provide accommodation overnight for at least two persons.

When accommodation fittings are removed, the boat is automatically reclassified into the Sport category.

Each craft must be fitted with two identical and independent motors capable of developing a speed of at least 30 knots, and have a minimum cruising range of 250 nautical miles.

No modification is allowed in respect of the standard production of the model used that must correspond to the one reported on the approval certificate.

Driving seats can be replaced.

The width of the craft must not be less than the product of the length measurement multiplied by a coefficient of 0.26 .

All engine models coming from the standard engine which have been modified in order to increase the performances for sporting activity, even though included in the boat builders catalogue, are not permitted.

For turbocharged engines the crankcase of the turbocharger must be original as supplied by the boat builder originally.

The gears-box must allow the following manoeuvres: forward, reverse and idle with the engines running.

Class T1:

- length (LOA): from 7.51m. to 8.99 m.
- minimum inside cabin ceiling: 1.40m. high, measured throughout a continuous surface of 0.50 m² at least.
- maximum capacity of the engine(s): 4,750cc Outboard / 8,600cc Inboard

Class T 2

- length (LOA): from 9.00 m. to 11.99 m.
- minimum inside cabin ceiling: 1.60m. high, measured throughout a continuous surface of 0.75 m² at least.
- maximum capacity of the engine(s) : 12,000cc.

Class T 3

- length (LOA): from 12.00 m. to 15.00 m.
- minimum inside cabin ceiling: 1.80m. high, measured throughout a continuous surface of 1.5 m² at least.
- maximum capacity of the engine(s) : 18,000cc.

Sport category

It includes boats with all the general characteristics provided for Tourism

category except for what specified below:

The beam width of the hull is not restricted.

Sport Category boats may also not be provided with cabin.

All engine models coming from the standard engine which have been modified in order to increase the performances for sporting activity, if included in the boat builder catalogue and reported in the craft approval certificate, are allowed in the sport category.

Class S 1

- length (LOA): from 7.51 m. to 8.99 m.
- maximum capacity of the engine(s) : Outboard 9,000cc. / Inboard 15,000cc.

Class S 2

- length (LOA): from 9.00 m. to 11.99 m.
- maximum capacity of the engine(s) : 21,000cc.

Class S 3

- length (LOA): from 12.00 m. to 15.00 m.
- maximum capacity of the engine(s) : 28,000cc.

Prototypes category

It includes all cruising vessels regularly homologated for such a purpose as a prototype with all the general characteristics provided for Sport category except for what is specified below:

The certified speed of the craft shall be of 50 knots minimum.

Craft can be fitted with a maximum of three engines.

Class P 1

- length (LOA) from 7.51 m. to 8 m.
- maximum capacity of the engine(s) : Outboard 11,000 cc. / Inboard 18,000cc.

Class P 2

- length (LOA) from 9.00 m. to 11.99 m.
- maximum capacity of the engine(s) : 25,000cc.

Class P 3

- length (LOA) from 12.00 m. to 15.00 m.
- maximum capacity of the engine(s) : 33,000cc.

Super Yacht Category

It includes cruising vessels regularly homologated for such a purpose including internal fittings complying with the specifications of the boat builders and with all the general characteristics provided for Tourism category except for what specified is below:

Belonging to Super Yacht category are all craft whose length is more than 15 m. and whose characteristics correspond to those provided by Tourism Category.

The number of engines is not restricted.

When accommodation fittings are removed, the boat cannot be classified in

this category.

Class Y

- length (LOA) : 15.01m. minimum
- minimum inside cabin ceiling: 1.80m. high, measured throughout a continuous surface of 1.50 m² at least.
- maximum capacity of the engine(s) : minimum 2 engines / maximum 4 engines
- The width of the craft must not be less than the product of the length measurement multiplied by a coefficient of 0.26

502.03 - GROUP “B”

RACE NUMBER

The race number shall be displayed on the topsides (port and starboard) of the hull within a dedicated area:

- The numbers on the topsides (port and starboard) shall be displayed on the forward half of the boat;
- Numbers and letter must be highlighted in black on a white background;
- The letter and number which identify the class shall be put in front of the race number;

The race number and its dedicated area shall have minimum dimensions as indicated below:

- Numbers and letter must have a thickness of no less than 50mm.

CATEGORIES AND CLASSES

4 Meters (Unique) Category

All pleasure boats with LOA from 4.00m. to 7.50m. fully conforming to the Boat Certification issued by an appropriate Technical Body, stating the dimensions of the craft and the installed engine capacities, may belong to this category according to the following:

Class U

- length (Lao): from 4.00m. to 7.50m.
- maximum engine capacity: 1,450cc. Outboard / 3,000cc. Inboard

Pleasure craft with engine capacities not less than 500cc. may participate in Endurance competitions of Group B.

If reported in the craft's certification, the powering can be made of two identical and independent engines.

Tourism Category

Production pleasure craft, including fittings, produced in series and in compliance with the specifications of the boat builders, belong to this category.

No modification is allowed in respect of the standard production of the model used that must correspond to the one reported on the approval certificate

Driving seats can be replaced

The beam width of the craft must not be less than the product of the length measurement multiplied by a coefficient of 0.26

Craft may be fitted with two engines if allowed by approval certificate.

All engine models coming from the standard engine which have been modified in order to increase the performances for sporting activity, even though included in the boat builders catalogue, are not permitted.

The gears-box must allow the following manoeuvres: forward, reverse and idle with the engine running.

The participation of boats certified/homologated as unique example is not allowed.

To apply fixed and/or removable decks (awnings and similar) to boats is not allowed.

Class T1

- length (LOA): up to 6.00 m.

- ***Weight not less than 750 kg for outboard and 1100 kg for inboard and in-outboard.***

- maximum engine capacity: 2,100cc. and 4 cylinders - Outboard
4,300cc. and 6 cylinders - Inboard

Class T 2

- length (LOA): up to 6.85 m.

- ***Weight not less than 980 kg for outboard and 1350 kg for inboard and in-outboard.***

- maximum engine capacity: 2,700cc. and 6 cylinders - Outboard
5,800cc. and 8 cylinders - Inboard

Class T 3

- length (LOA): up to 7.50 m.

- ***Weight not less than 1200 kg for outboard and 1650 kg for inboard and in-outboard.***

- maximum engine capacity: 4,000cc. and 8 cylinders - Outboard
7,500cc. and 8 cylinders - Inboard

TECHNICAL ATTACHMENT TO THE ENDURANCE COMPETITIONS RULES

GROUP B – BOAT TOURISM CATEGORY – UNIQUE

All engines must be strictly of current production.

This technical attachment foresees the allowed modifications.

All modifications not mentioned are not allowed.

1.Exclusively the spare parts provided by the engine manufacturer for that kind and model of engine are allowed, so as provided by the catalogue and the engine exposure.

2.All spare parts must be of the current selling production kind.

3. Elimination of the whole lubricating system separated in 2-stroke

- engines is allowed.*
- 4. *Cylinders can be bored up to the increase provided by the engine manufacturer for that model of engine. The increased pistons must be original spare parts as provided by point 1.***
 - 5. *In 2-stroke engines, resetting, after the boring, the rounding off of the cylinder openings is allowed; the height of the rounding off can not be more than 1,5 mm. and the angle can not be more than 30°.***
 - 6. *Substitution of the jets of the minimum and maximum in carburetted engines as per point 1 is allowed.***
 - 7. *Elimination of the revolution limiting device is allowed..***
 - 8. *Substitution with no original parts is allowed only in the following particulars : plugs, cables and pipes of plugs, electric wires, nuts and bolts, water pipes, clips, anodes, oil filters, propellers.***
 - 9. *Stiffener of the anchorage between the legging and the steering bar in outboard engines is allowed.***
 - 10. *In inboard and out/inboard engines, the use of direct exhausts is allowed. The exhaust pipes must go through the stern mirror. Modification of the exhausts is allowed only after the manifolds and the mixing kind exhaust gas-refrigerating water system that must remain original.***

A – *Transmissions like racing (Stern drive) and surface for drive transmission are allowed.*

B - *The use of multi ratio speed change gear is not allowed.*

Sport Category

It includes all the general characteristics provided for Tourism category except for what specified below:

The beam width of the hull is not restricted.

The units produced in series which have been modified in at least one of the following respects, belong to this category:

- in the deck structures through removal of parts, or by creation of a partial rigid bridge on prow;
- by removal of fittings (furniture);

All engine models coming from the standard engine which have been modified in order to increase the performances for sporting activity, if included in the boat builder catalogue and reported in the craft approval certificate, are allowed in the Sport category.

Tourism craft units equipped with engines exceeding the allowed capacity in their class, automatically enter Sport category.

Class S1

- length (LOA): up to 6.00 m.
- ***Weight not less than 750 kg for outboard and 1100 kg for inboard and in-outboard***
- maximum engine capacity: 2,100cc. and 6 cylinders - Outboard

5,800cc. and 8 cylinders - Inboard

Class S2

- length (LOA): up to 6.85 m.

- ***Weight not less than 980 kg for outboard and 1350 kg for inboard and in-outboard***

- maximum engine capacity: 2,700cc. and 6 cylinders - Outboard

7,500cc. and 8 cylinders - Inboard

Class S3

- length (LOA): up to 7.50 m.

- ***Weight not less than 1200 kg for outboard and 1650 kg for inboard and in-outboard***

- maximum engine capacity: 8,000cc. and 8 cylinders - Outboard

8,900cc. and 8 cylinders - Inboard

TECHNICAL ATTACHMENT TO THE ENDURANCE COMPETITIONS RULES

GROUP B – BOAT SPORT CATEGORY

All engines must be homologated and supplied by the sales network. The spare parts of the engine and the possible substituted or replaceable parts, as from the following points, must be supplied by the sales network as current production parts.

A supplier's catalogue of the part must be available during technical verifications.

All modifications not expressly allowed are forbidden.

- 1.The whole engine capacity can not be modified. The reboring of the cylinder with an increase not over 0,040 is allowed.***
- 2.Drive shaft, rods, flywheel, pistons and all the components of the “rotating group” are free. Rods and drive shaft must be made out of steel. The run of the piston must be unchanged.***
- 3.Mechanical workings inside the engine are allowed. The position and the number of the sparking plugs and of the valves can not be changed.***
- 4.Only the use of the engine head and the original carters for that kind of engine is allowed.
If supplied, substituting the head with another on catalogue for that kind of engine is allowed, remaining allowed on these parts what mentioned in point 3.***
- 5.The substitution of the cylinder liner is allowed.***
- 6.Valves, camshafts, lifters, springs, gearing system, chain or distribution belt and all distribution parts can be substituted.***
- 7.Substitution or elimination of the sealing washers of the engine is allowed.***
- 8.Substitution of the carburettors with others supplied by the engine manufacturer for that kind and model of engine is allowed. The fuel supply foreseen by the engine manufacturer for that kind and model***

of engine must be strictly of current production. Any kind of modification with exception of the substitution of jets in the carburettor engines and the recalibration of the power board of the electronic injection engines is not allowed.

- 9.The intake system foreseen by the engine manufacturer for that kind and model of engine must be strictly of current production. Any kind of modification is not allowed. The filtering element must be present as paper filter, sponge, metallic laminated pack or metallic net pack. The intake stacks and the dynamic conveyors directed to the intake system are not allowed.*
- 10.The external configuration of the engine must remain of current production as from exploded engine of the manufacturer, so as all external parts. The substitution of elements with others not foreseen by the engine manufacturer for that kind and model of engine is forbidden. As external configuration and elements are to be intended the visible parts without unassembled engine block, carter and head.*
- 11.The substitution with no original parts is allowed only in the following particulars: plugs, cable and pipes of the plugs, electric wires, nuts and bolts, water pipes, clips, anodes, oil filters, propellers.*
- 12.The elimination of the hydro-drive is allowed.*
- 13.The elimination of the whole lubricating system separated in the 2-stroke engines is allowed.*
- 14.The substitution of the whole exhaust system unit is allowed, provided it is of a mixing kind exhaust gas-refrigerating water, with component supplied in current production for that kind and model of engine, as from catalogue supplied by the manufacturer of the exhaust unit or the engine. A supplier's catalogue of the part must be available during technical verifications.*
- 15.The use of multi ratio speed change gear is not allowed.*
- 16.The elimination of the revolution limiting device is allowed.*
- 17.The stiffener of the anchorage between the legging and the steering bar in outboard engines is allowed.*
- 18.The use of commercial fuel is exclusively allowed, so as supplied by the fuel stations located in harbour and roads areas.*
- 19.Any kind of additive is excluded: liquid, solid, gaseous, increasers of power or antiknock.*
- 20.The verification fuel regulations are foreseen by the technical attachment "FUEL VERIFICATION" and are the only ones enforceable in conformity to point 18.*

**THIS ATTACHMENT IS INTEGRAL PART OF THE ENDURANCE RULES.
IF THESE REGULATIONS ARE NOT RESPECTED THE JURISDICTION
RULES WILL BE APPLIED (GROUP 400).**

Explanatory note

Within the Boat Sport category transmissions like racing (racing drives) and surface drive shafts are allowed.



Anti Doping Regulations

based upon

The World Anti-Doping Code

Date of enforcement: 01 July 2004

TABLE OF CONTENTS

INTRODUCTION

PREFACE	.....
Fundamental Rationale for the Code and U.I.M's Anti-Doping Rules	
Scope.....	
ARTICLE 1	DEFINITION OF DOPING
ARTICLE 2	ANTI-DOPING RULE VIOLATIONS
ARTICLE 3	PROOF OF DOPING
ARTICLE 4	THE PROHIBITED LIST.....
ARTICLE 5	TESTING
ARTICLE 6	ANALYSIS OF SAMPLES
ARTICLE 7	RESULTS MANAGEMENT
ARTICLE 8	RIGHT TO A FAIR HEARING.....
ARTICLE 9	AUTOMATIC DISQUALIFICATION OF INDIVIDUAL RESULTS.....
ARTICLE 10	SANCTIONS ON INDIVIDUALS
ARTICLE 11	CONSEQUENCES TO TEAMS
ARTICLE 12	SANCTIONS AND COSTS ASSESSED AGAINST NATIONAL AUTHORITIES
ARTICLE 13	APPEALS.....
ARTICLE 14	NATIONAL AUTHORITIES INCORPORATION OF IF RULES, REPORTING AND RECOGNITION
ARTICLE 15	RECOGNITION OF DECISIONS BY OTHER ORGANISATIONS
ARTICLE 16	STATUTE OF LIMITATIONS
ARTICLE 17	U.I.M. COMPLIANCE REPORTS TO WADA
ARTICLE 18	AMENDMENT AND INTERPRETATION OF ANTI-DOPING RULES....

U.I.M. ANTI-DOPING RULES

INTRODUCTION

Preface

At the U.I.M. General Assembly held on 21. October 01 in Monaco, the U.I.M. accepted the World Anti-Doping Code (the "Code") and created accordingly the U.I.M. Anti Doping Code in replacement of its own already existing Anti Doping regulations. Due to modifications of the World Anti Doping Code it became necessary to rewrite the U.I.M Anti Doping Code. These Anti-Doping Rules are adopted and implemented in conformance with the U.I.M.'s responsibilities under the Code, and are in furtherance of the U.I.M.'s continuing efforts to eradicate doping in the sport of powerboating.

Anti-Doping Rules, like Competition rules, are sport rules governing the conditions under which sport is played. Drivers accept these rules as a condition of participation. Anti-Doping Rules are not intended to be subject to or limited by the requirements and legal standards applicable to criminal proceedings or employment matters. The policies and minimum standards set forth in the Code and implemented in these Anti-Doping Rules represent the consensus of a broad spectrum of stakeholders with an interest in fair sport and should be respected by all courts and adjudicating bodies.

Fundamental Rationale for the Code and U.I.M.'s Anti-Doping Rules

Anti-doping programs seek to preserve what is intrinsically valuable about sport. This intrinsic value is often referred to as "the spirit of sport"; it is the essence of Olympism; it is how we play true. The spirit of sport is the celebration of the human spirit, body and mind, and is characterized by the following values:

- Ethics, fair play and honesty
- Health
- Excellence in performance
- Character and education
- Fun and joy
- Teamwork
- Dedication and commitment
- Respect for rules and laws
- Respect for self and other participants
- Courage
- Community and solidarity

Doping is fundamentally contrary to the spirit of sport.

Scope

These Anti-Doping Rules shall apply to the U.I.M., each National Authority of the U.I.M., and each Driver in the activities of the U.I.M. or any of its National Authorities by virtue of the Driver's accreditation, or participation in the U.I.M., its National Authorities, or their activities or Meetings.

To be eligible for participation in the U.I.M. meetings, a competitor must have an international licence issued by his National Authority. This international licence will only be issued to Drivers who have personally signed the Appendix 2, in the actual form approved by the U.I.M. All forms from under-age applicants must be counter-signed by their legal guardians.

The National Authority must guarantee that all Drivers registered for a international licence accept the Rules of the U.I.M., including these U.I.M. Anti-Doping Rules compiled in accordance with the World Anti-Doping Code

It is the responsibility of each National Authority to ensure that all national-level Testing on the National Authority's Drivers complies with these Anti-Doping Rules. In some cases, the National Authority itself will be conducting the Doping Control described in these Anti-Doping Rules. In other countries, many of the Doping Control responsibilities of the National Authority have been delegated or assigned by statute to a National Anti-Doping Organisation. In those countries, references in these Anti-Doping Rules to the National Authority shall apply, as applicable, to the National Authorities' National Anti-Doping Organisation.

These Anti-Doping Rules shall apply to all Doping Controls over which the U.I.M. and its National Authorities have jurisdiction.

ARTICLE 1 DEFINITION OF DOPING

Doping is defined as the occurrence of one or more of the anti-doping rule violations set forth in Article 2.1 through Article 2.8 of these Anti-Doping Rules.

ARTICLE 2 ANTI-DOPING RULE VIOLATIONS

The following constitute anti-doping rule violations:

2.1 The presence of a Prohibited Substance or its Metabolites or Markers in a driver's bodily Specimen

2.1.1 It is each Driver's personal duty to ensure that no Prohibited Substance enters his or her body. Drivers are responsible for any Prohibited Substance or its Metabolites or Markers found to be present in their bodily Specimens. Accordingly, it is not necessary that intent,

fault, negligence or knowing Use on the Drivers' part be demonstrated in order to establish an anti-doping violation under Article 2.1.

2.1.2 Excepting those substances for which a quantitative reporting threshold is specifically identified in the Prohibited List, the detected presence of any quantity of a Prohibited Substance or its Metabolites or Markers in a Driver's Sample shall constitute an anti-doping rule violation.

2.1.3 As an exception to the general rule of Article 2.1, the Prohibited List may establish special criteria for the evaluation of Prohibited Substances that can also be produced endogenously.

2.2 Use or Attempted Use of a Prohibited Substance or a Prohibited Method

2.2.1 The success or failure of the Use of a Prohibited Substance or Prohibited Method is not material. It is sufficient that the Prohibited Substance or Prohibited Method was Used or Attempted to be Used for an anti-doping rule violation to be committed.

2.3 Refusing, or failing without compelling justification, to submit to Sample collection after notification as authorized in these Anti-Doping Rules or otherwise evading Sample collection.

2.4 Violation of the requirements regarding Driver availability for Out-of-Competition Testing including failure to provide required whereabouts information set forth in Article 5.5 (Driver whereabouts requirements) and missed tests which are declared based on reasonable rules.

2.5 Tampering, or Attempting to tamper, with any part of Doping Control.

2.6 Possession of Prohibited Substances and Methods

2.6.1 Possession by a Driver at any time or place of a substance that is prohibited in Out-of-Competition Testing or a Prohibited Method unless the Driver establishes that the Possession is pursuant to a therapeutic use exemption granted in accordance with Article 4.4 (Therapeutic Use) or other acceptable justification.

2.6.2 Possession of a Prohibited Substance that is prohibited in Out-of-Competition Testing or a Prohibited Method by Driver Support Personnel in connection with a Driver, Event or training, unless the Driver Support Personnel establishes that the Possession is pursuant to a therapeutic use exemption granted to a Driver in accordance with Article 4.4 (Therapeutic Use) or other acceptable justification.

2.7 Trafficking in any Prohibited Substance or Prohibited Method.

2.8 Administration or Attempted administration of a Prohibited Substance or Prohibited Method to any Driver, or assisting, encouraging, aiding, abetting, covering up or any other type of complicity involving an anti-doping rule violation or any Attempted violation.

ARTICLE 3 PROOF OF DOPING

3.1 Burdens and Standards of Proof

The U.I.M. and its National Authorities shall have the burden of establishing that an anti-doping rule violation has occurred. The standard of proof shall be whether U.I.M. or its National Authorities have established an anti-doping rule violation to the comfortable satisfaction of the hearing body bearing in mind the seriousness of the allegation which is made. This standard of proof in all cases is greater than a mere balance of probability but less than proof beyond a reasonable doubt. Where these Rules place the burden of proof upon the Driver or other Person alleged to have committed an anti-doping rule violation to rebut a presumption or establish specified facts or circumstances, the standard of proof shall be by a balance of probability.

3.2 Methods of Establishing Facts and Presumptions

Facts related to anti-doping rule violations may be established by any reliable means, including admissions. The following rules of proof shall be applicable in doping cases:

3.2.1 WADA-accredited laboratories are presumed to have conducted Sample analysis and custodial procedures in accordance with the International Standard for laboratory analysis. The Driver may rebut this presumption by establishing that a departure from the International Standard occurred.

If the Driver rebuts the preceding presumption by showing that a departure from the International Standard occurred, then the U.I.M. or its National Authority shall have the burden to establish that such departure did not cause the Adverse Analytical Finding.

3.2.2 Departures from the International Standard for Testing which did not cause an Adverse Analytical Finding or other anti-doping rule violation shall not invalidate such results. If the Driver establishes that departures from the International Standard occurred during Testing then the U.I.M. or its National Authority shall have the burden to establish that such departures did not cause the Adverse Analytical Finding or the factual basis for the anti-doping rule violation.

ARTICLE 4 THE PROHIBITED LIST

4.1 Incorporation of the Prohibited List

These Anti-Doping Rules incorporate the Prohibited List which is published and revised by WADA as described in Article 4.1 of the Code. The U.I.M. will make the current Prohibited List available to each National Authority, and each National Authority shall ensure that the current Prohibited List is available to its members and constituents.¹

4.2 Prohibited Substances and Prohibited Methods Identified on the Prohibited List

Unless provided otherwise in the Prohibited List and/or a revision, the Prohibited List and revisions shall go into effect under these Anti-Doping Rules three months after publication of the Prohibited List by WADA without requiring any further action by the U.I.M. As described in Article 4.2 of the Code, the U.I.M. may upon the recommendation of its Medical Commission request that WADA expand the Prohibited List for the sport of Powerboating or certain disciplines within the sport of Powerboating. The U.I.M. may also upon the recommendation of its Medical Commission request that WADA include additional substances or methods, which have the potential for abuse in the sport of Powerboating in the monitoring program described in Article 4.5 of the Code. As provided in the Code, WADA shall make the final decision on such requests by the U.I.M.

4.3 Criteria for Including Substances and Methods on the Prohibited List

As provided in Article 4.4.3 of the Code, WADA's determination of the Prohibited Substances and Prohibited Methods that will be included on the Prohibited List shall be final and shall not be subject to challenge by a Driver or other Person.

4.4 Therapeutic Use

4.4.1 Drivers with a documented medical condition requiring the use of a Prohibited Substance or a Prohibited Method must first obtain a Therapeutic Use Exemption ("TUE").

4.4.2 Drivers included by U.I.M. in its Registered Testing Pool and other Drivers prior to their participation in any International Event must obtain a TUE from U.I.M. (regardless of whether the Driver previously has received a TUE at the national level). TUE's granted by the U.I.M. shall be reported to the Driver's National Authority and to

(1) *The Prohibited List in force is available on WADA's website at www.wada-ama.org.*

WADA. Other Drivers subject to Testing may obtain a TUE from their National Anti-Doping Organization or other body designated by their National Authority. National Authorities shall promptly report any such TUE's to the U.I.M. and WADA.

4.4.3 The U.I.M. Council shall appoint a Committee of physicians to consider requests for TUE's (the "TUE Committee"). Upon U.I.M.'s receipt of a TUE request, the Chair of the TUE Panel shall appoint three members of the TUE Panel (which may include the Chair) to consider such request. The TUE Committee member(s) so designated shall promptly evaluate such request in accordance with the International Standard for Therapeutic Use Exemptions and render a decision on such request, which shall be the final decision of the U.I.M.

4.4.3.1 International-Level Drivers should apply to the U.I.M for the TUE at the same time the Driver first provides whereabouts information to the U.I.M. and, except in emergency situations, no later than 21 days before the Driver's participation at an International Event.

4.4.4 WADA, at the request of a Driver or on its own initiation, may review the granting or denial of any TUE to an International Level Driver or a national level Driver that is included in a Registered Testing Pool. If WADA determines that the granting or denial of a TUE did not comply with the International Standard for Therapeutic Use Exemptions in force at the time then WADA may reverse that decision. Decisions on TUE's are subject to further appeal as provided in Article 13.

ARTICLE 5 TESTING

5.1 Authority to Test

All Drivers affiliated with a National Authority shall be subject to In-Competition Testing by the U.I.M., the Driver's National Authority, the relevant National Authority of the country where the event takes place and any other Anti-Doping Organization responsible for Testing at a Competition or Event in which they participate. All Drivers affiliated with a National Authority shall also be subject to Out-of-Competition Testing at any time or place, with or without advance notice, by the U.I.M, WADA and the Driver's National Authority.

5.2 Responsibility for U.I.M. Testing

The U.I.M. Medical Commission shall be responsible for overseeing all Testing conducted by the U.I.M.. Testing may be conducted by members of the U.I.M. Medical Commission or by other qualified persons so authorised by this Commission.

5.3 Testing Standards

Testing conducted by the U.I.M. and its National Authorities shall be in substantial conformity with the International Standard for Testing in force at the time of Testing.

5.3.1 Blood (or other non-urine) Samples may be used either to detect Prohibited Substances or Prohibited Methods or for screening procedure purposes only. If the blood is collected for screening only, it will have no other consequences for the Driver other than to identify him/her for a urine test under these anti-doping rules. In these circumstances, the U.I.M. Medical Commission may decide at its own discretion which blood parameters are to be measured in the screening Sample and what levels of those parameters will be used to indicate that a Driver should be selected for a urine test.

5.4 Coordination of Testing

The U.I.M. and National Authorities shall promptly report completed tests through the WADA clearinghouse to avoid unnecessary duplication in Testing.

5.5 Driver Whereabouts Requirements

5.5.1 The U.I.M. shall identify a Registered Testing Pool of those Drivers who are required to provide up-to-date whereabouts information to U.I.M. The U.I.M. may revise its Registered Testing Pool from time to time as appropriate³. Each Driver in the Registered

(3) The purpose of the U.I.M. Registered Testing Pool is to identify top-level International Drivers to facilitate Out-of-Competition Testing by U.I.M. and WADA. The criteria for inclusion of Drivers on this list may vary and for

Testing Pool shall file semi-annual reports with the UIM on forms provided by the U.I.M. which specify the locations and times where the Driver will be residing, training and competing and a telephone number under which information about the actual whereabouts can be obtained at any time and under which the driver can be reached. Drivers shall update this information as necessary so that it is current at all times. The ultimate responsibility for providing whereabouts information rests with each Driver, however, it shall be the responsibility of each National Authority to use its best efforts to assist the U.I.M in obtaining whereabouts information as requested by the U.I.M.

5.5.2 Any Driver in the U.I.M. Registered Testing Pool who is unavailable for Testing on three attempts during any period of 18 consecutive months shall be considered to have committed an anti-doping rule violation pursuant to Article 2.4. For each attempt, the Doping Control Officer shall call the aforementioned telephone number at least 6 to 12 hours prior to the intended visit to make an appointment with the driver for testing. Unavailability is considered, if the driver could not be reached after 3 telephone calls with a minimum of one hour delay after each call or if the driver does not attend the appointment with a maximum delay of 30 minutes. Notification shall be sent to the Driver between each attempt which is to be counted as an unavailable test.

5.5.3 Any Driver in the U.I.M. Registered Testing Pool who fails to timely submit a required semi-annual whereabouts report after receipt of two formal written warnings from the U.I.M or a National Authority to do so in the preceding 18 months shall be considered to have committed an anti-doping rule violation pursuant to Article 2.4.

5.5.4 Each National Authority shall also assist their National Anti-Doping Organization in establishing a national level Registered Testing Pool of top level national Drivers who are not already included in IF's Registered Testing Pool. The National Authority/National Anti-Doping Organization may establish its own whereabouts reporting requirements and criteria for Article 2.4 violations applicable to those Drivers.

5.5.5 Whereabouts information provided pursuant to Articles 5.5.1 and 5.5.4 shall be shared with WADA and other Anti-Doping

Organizations having jurisdiction to test a Driver on the strict condition that it be used only for Doping Control purposes.

example, criteria for inclusion might be based on: current or past racing results, established performance standards). The registered testing pool should aim at the best drivers.

5.6 Retirement and Return to Competition

5.6.1 A Driver who has been identified by the U.I.M. for inclusion in the U.I.M.'s Registered Testing Pool shall continue to be subject to these Anti-Doping Rules, unless and until the Driver gives written notice to the U.I.M. that he or she has retired or until he or she no longer satisfies the criteria for inclusion in the U.I.M's Registered Testing Pool and has been so informed by the U.I.M..

5.6.2 A Driver who has given notice of retirement to the U.I.M. may not resume competing unless he or she notifies the U.I.M. at least six months before he or she expects to return to competition and is available for Out-of-Competition Testing, at any time during the period before actual return to competition.

5.7 Selection of Drivers to be Tested

5.7.1 At International Events, a minimum of three drivers should be tested in each class, i.e. the winning Driver plus two other Drivers selected at random. In the case of more than one crew members per boat a minimum of three crews (six Drivers) are to be tested according to this regulation.

5.7.2 At National Events, each National Authority shall determine the number of Drivers selected for Testing in each Competition and the procedures for selecting the Drivers for Testing.

5.7.3 In addition to the selection procedures set forth in Articles 5.7.1 and 5.7.2 above, the U.I.M. Medical Commission at International Events, and the National Authority at National Events, may also select Drivers or teams for Target Testing so long as such Target Testing is not used for any purpose other than legitimate Doping Control purposes.

5.7.4 Drivers shall be selected for Out-of-Competition Testing by the U.I.M. Medical Commission and by National Authorities through a process that substantially complies with the International Standard for Testing in force at the time of selection.

5.8 National Authorities and the organizing committees for International Events shall provide access to Independent Observers as directed by the U.I.M.

ARTICLE 6 ANALYSIS OF SAMPLES

Doping Control Samples collected under these Anti-Doping Rules shall be analyzed in accordance with the following principles:

6.1 Use of Approved Laboratories

The U.I.M. shall send Doping Control Samples for analysis only to WADA-accredited laboratories or as otherwise approved by WADA. The choice of the WADA-accredited laboratory (or other method approved by WADA) used for the Sample analysis shall be determined exclusively by the U.I.M..

6.2 Substances Subject to Detection

Doping Control Samples shall be analyzed to detect Prohibited Substances and Prohibited Methods identified on the Prohibited List and other substances as may be directed by WADA pursuant to the Monitoring Program described in Article 4.5 of the Code.

6.3 Research on Samples

No Sample may be used for any purpose other than the detection of substances (or classes of substances) or methods on the Prohibited List, or as otherwise identified by WADA pursuant to its Monitoring Program, without the Driver's written consent.

6.4 Standards for Sample Analysis and Reporting

Laboratories shall analyze Doping Control Samples and report results in conformity with the International Standard for Laboratory Analysis.

ARTICLE 7 RESULTS MANAGEMENT

7.1 Results Management for Tests Initiated by the U.I.M.

Results management for Tests initiated by the U.I.M. (including Tests performed by WADA pursuant to agreement with the U.I.M.) shall proceed as set forth below:

7.1.1 The results from all analyses must be sent to the U.I.M. in encoded form, in a report signed by an authorised representative of the laboratory. All communication must be conducted in such a way that the results of the analyses are confidential.

7.1.2 Upon receipt of an A Sample Adverse Analytical Finding, the U.I.M. Medical Commission shall conduct a review to determine whether: (a) an applicable therapeutic use exemption has been granted, or (b) there is any apparent departure from the International Standards for Testing or Laboratory Analysis that undermines the validity of the Adverse Analytical Finding.

7.1.3 If the initial review under Article 7.1.2 does not reveal an applicable therapeutic use exemption or departure from the International Standard for Testing or the International Standard for laboratory

analysis in force at the time of Testing or analysis that undermines the validity of the Adverse Analytical Finding, the U.I.M. shall promptly notify the Driver of: (a) the Adverse Analytical Finding; (b) the anti-doping rule violated, or, in a case under Articles 7.1.8 or 7.1.9, a description of the additional investigation that will be conducted as to whether there is an anti-doping rule violation; (c) the Driver's right to promptly request the analysis of the B Sample or, failing such request, that the B Sample analysis may be deemed waived; (d) the right of the Driver and/or the Driver's representative to attend the B Sample opening and analysis if such analysis is requested; and (e) the Driver's right to request copies of the A and B Sample laboratory documentation package which includes information as required by the International Standard for Laboratory Analysis.

7.1.4 Arrangements shall be made for Testing the B Sample within three weeks of the notification described in Article 7.1.3. A Driver may accept the A Sample analytical results by waiving the requirement for B Sample analysis. The U.I.M. may nonetheless elect to proceed with the B Sample analysis.

7.1.5 The Driver and/or his representative shall be allowed to be present at the analysis of the B Sample. Also a representative of the Drivers's National Authority as well as a representative of the U.I.M. shall be allowed to be present.

7.1.6 If the B Sample proves negative, the entire test shall be considered negative and the Driver, his National Authority, and the U.I.M. shall be so informed.

7.1.7 If a Prohibited Substance or the Use of a Prohibited Method is identified, the findings shall be reported to the Driver, his National Authority, the U.I.M., and to WADA.

7.1.8 The U.I.M. Medical Commission shall conduct any follow-up investigation as may be required by the Prohibited List. Upon completion of such follow-up investigation, the U.I.M. shall promptly notify the Driver regarding the results of the follow-up investigation and whether or not the U.I.M. asserts that an anti-doping rule was violated.

7.1.9 For apparent anti-doping rule violations that do not involve Adverse Analytical Findings, the U.I.M. shall conduct any necessary follow-up investigation and shall then promptly notify the Driver of the anti-doping rule which appears to have been violated, and the basis of the violation.

7.2 Results Management for Tests initiated by National Authorities

Results management conducted by National Authorities shall be consistent with the general principles for effective and fair results management which underlie the detailed provisions set forth in Article 7.1. Results of all Doping Controls

shall be reported to the U.I.M. within 14 days of the conclusion of the National Authority's results management process. Any apparent anti-doping rule violation by a Driver who is a member of that National Authority shall be promptly referred to an appropriate hearing panel established pursuant to the rules of the National Authority or national law. Apparent anti-doping rule violations by Drivers who are members of another National Authority shall be referred to the Driver's National Authority for hearing.

ARTICLE 8 RIGHT TO A FAIR HEARING

8.1 Hearings arising out of U.I.M. Testing or Tests at International Events

8.1.1 The U.I.M. Executive Committee shall appoint a standing panel consisting of a Chair with legal knowledge or equivalent experience/qualification and two experts with experience in anti-doping ("U.I.M. Anti Doping Hearing Board").

8.1.2 When it appears, following the Results Management process described in Article 7, that these Anti-Doping Rules have been violated in connection with U.I.M. Testing or Testing at an International Event then the case shall be assigned to the U.I.M. Anti Doping Hearing Board for adjudication.

8.1.3 The appointed members shall have had no prior involvement with the case and shall not have the same nationality as the Driver or other Person alleged to have violated these Anti-Doping Rules.

8.1.4 Hearings pursuant to this Article shall be completed expeditiously following the completion of the results management process described in Article 7. Hearings held in connection with Events may be conducted on an expedited basis.

8.1.5 The National Authority of the Driver or other Person alleged to have violated these Anti-Doping Rules may attend the hearing as an observer.

8.1.6 The U.I.M. shall keep WADA fully apprised as to the status of pending cases and the result of all hearings.

8.1.7 A Driver or other Person may forego a hearing by acknowledging the Anti-Doping Rule violation and accepting Consequences consistent with Articles 9 and 10 as proposed by the U.I.M.

8.1.8 Decisions of the U.I.M. Anti Doping Hearing Board may be appealed to the U.I.M. Appeal Board and afterwards to the Court of Arbitration for Sport as provided in Article 13.

8.2 Principles for a Fair Hearing All hearings pursuant to Article 8. shall respect the following principles:

- a timely hearing;
- fair and impartial hearing body;
- the right to be represented by counsel at the Person's own expense;
- the right to be fairly and timely informed of the asserted anti-doping rule violation;
- the right to respond to the asserted anti-doping rule violation and resulting Consequences;
- the right of each party to present evidence, including the right to call and question witnesses (subject to the hearing body's discretion to accept testimony by telephone or written submission);
- the Person's right to an interpreter at the hearing, with the Doping Panel to determine the identity, and responsibility for the cost of the interpreter; and
- a timely, written, reasoned decision.

ARTICLE 9 AUTOMATIC DISQUALIFICATION OF INDIVIDUAL RESULTS

A violation of these Anti-Doping Rules in connection with an In-Competition test automatically leads to Disqualification of the individual result obtained in that Competition with all resulting consequences, including forfeiture of any medals, points and prizes.

ARTICLE 10 SANCTIONS ON INDIVIDUALS

10.1 Disqualification of Results in Event During which an Anti-Doping Rule Violation Occurs

An Anti-Doping Rule violation occurring during or in connection with an Event may lead to Disqualification of all of the Drivers's individual results obtained in that Event with all consequences, including forfeiture of all medals, points and prizes, except as provided in Article 10.1.1.

10.1.1 If the Driver establishes that he or she bears No Fault or Negligence for the violation, the Driver's individual results in the other Competition shall not be Disqualified unless the Driver's results in Competition other than the Competition in which the anti-doping rule violation occurred were likely to have been affected by the Driver's anti-doping rule violation.

10.2 Imposition of Ineligibility for Prohibited Substances and Prohibited Methods

Except for the specified substances identified in Article 10.3, the period of Ineligibility imposed for a violation of Article 2.1 (presence of Prohibited Substance or its Metabolites or Markers), Article 2.2 (Use or Attempted Use of Prohibited Substance or Prohibited Method) and Article 2.6 (Possession of Prohibited Substances and Methods) shall be:

First violation: Two (2) years' Ineligibility.

Second violation: Lifetime Ineligibility.

However, the Driver or other Person shall have the opportunity in each case, before a period of Ineligibility is imposed, to establish the basis for eliminating or reducing this sanction as provided in Article 10.5.

10.3 Specified Substances

The Prohibited List may identify specified substances which are particularly susceptible to unintentional anti-doping rules violations because of their general availability in medicinal products or which are less likely to be successfully abused as doping agents. Where a Driver can establish that the Use of such a specified substance was not intended to enhance sport performance, the period of Ineligibility found in Article 10.2 shall be replaced with the following:

First violation: At a minimum, a warning and reprimand and no period of Ineligibility from future Events, and at a maximum, one (1) year's Ineligibility.

Second violation: Two (2) years' Ineligibility.

Third violation: Lifetime Ineligibility.

However, the Driver or other Person shall have the opportunity in each case, before a period of Ineligibility is imposed, to establish the basis for eliminating or reducing (in the case of a second or third violation) this sanction as provided in Article 10.5.

10.4 Ineligibility for Other Anti-Doping Rule Violations

The period of Ineligibility for other violations of these Anti-Doping Rules shall be:

10.4.1 For violations of Article 2.3 (refusing or failing to submit to Sample collection) or Article 2.5 (Tampering with Doping Control), the Ineligibility periods set forth in Article 10.2 shall apply.

10.4.2 For violations of Article 2.7 (Trafficking) or Article 2.8 (administration of Prohibited Substance or Prohibited Method), the period of Ineligibility imposed shall be a minimum of four (4) years up to lifetime Ineligibility. An anti-doping rule violation involving a Minor shall be considered a particularly serious violation, and, if committed by Driver Support Personnel for violations other than specified substances referenced in Article 10.3, shall result in lifetime Ineligibility for such Driver Support Personnel. In addition, violations of such Articles which also violate non-sporting laws and regulations, may be reported to the competent administrative, professional or judicial authorities.

10.4.3 For violations of Article 2.4 (Whereabouts Violations or Missed Tests), the period of Ineligibility shall be:

First violation: Three (3) months to one (1) year Ineligibility.

Second and subsequent violations: Two (2) years' Ineligibility.

10.5 Elimination or Reduction of Period of Ineligibility Based on Exceptional Circumstances

10.5.1 If the Driver establishes in an individual case involving an anti-doping rule violation under Article 2.1 (presence of Prohibited Substance or its Metabolites or Markers) or Use of a Prohibited Substance or Prohibited Method under Article 2.2 that he or she bears No Fault or Negligence for the violation, the otherwise applicable period of Ineligibility shall be eliminated. When a Prohibited Substance or its Markers or Metabolites is detected in a Driver's Specimen in violation of Article 2.1 (presence of Prohibited Substance), the Driver must also establish how the Prohibited Substance entered his or her system in order to have the period of Ineligibility eliminated. In the event this Article is applied and the period of Ineligibility otherwise applicable is eliminated, the anti-doping rule violation shall not be considered a violation for the limited purpose of determining the period of Ineligibility for multiple violations under Article 10.2, 10.3 and 10.6.

10.5.2 This Article 10.5.2 applies only to anti-doping rule violations involving Article 2.1 (presence of Prohibited Substance or its Metabolites or Markers), Use of a Prohibited Substance or Prohibited Method under Article 2.2, failing to submit to Sample collection under Article 2.3, or administration of a Prohibited Substance or Prohibited Method under Article 2.8. If a Driver establishes in an individual case involving such violations that he or she bears No Significant Fault or Negligence, then the period of Ineligibility may be reduced, but the reduced period of Ineligibility may not be less than one-half of the minimum period of Ineligibility otherwise applicable. If the otherwise applicable period of Ineligibility is a lifetime, the reduced period under this section may be no less than 8 years. When a Prohibited

Substance or its Markers or Metabolites is detected in a Driver's Specimen in violation of Article 2.1 (presence of Prohibited Substance), the Driver must also establish how the Prohibited Substance entered his or her system in order to have the period of Ineligibility reduced.

10.5.3 The U.I.M. Anti Doping Hearing Board may also reduce the period of Ineligibility in an individual case where the Driver has provided substantial assistance to the U.I.M. which results in the U.I.M. discovering or establishing an anti-doping rule violation by another Person involving Possession under Article 2.6.2 (Possession by Driver Support Personnel), Article 2.7 (Trafficking), or Article 2.8 (administration to a Driver). The reduced period of Ineligibility may not, however, be less than one-half of the minimum period of Ineligibility otherwise applicable. If the otherwise applicable period of Ineligibility is a lifetime, the reduced period under this Article may be no less than 8 years.

10.6 Rules for Certain Potential Multiple Violations

10.6.1 For purposes of imposing sanctions under Article 10.2, 10.3 and 10.4, a second anti-doping rule violation may be considered for purposes of imposing sanctions only if the U.I.M. (or its National Authority) can establish that the Driver or other Person committed the second anti-doping rule violation after the Driver or other Person received notice, or after the U.I.M. (or its National Authority) made a reasonable attempt to give notice, of the first anti-doping rule violation; if the U.I.M. (or its National Authority) cannot establish this, the violations shall be considered as one single first violation, and the sanction imposed shall be based on the violation that carries the more severe sanction.

10.6.2 Where a Driver, based on the same Doping Control, is found to have committed an anti-doping rule violation involving both a specified substance under Article 10.3 and another Prohibited Substance or Prohibited Method, the Driver shall be considered to have committed a single anti-doping rule violation, but the sanction imposed shall be based on the Prohibited Substance or Prohibited Method that carries the most severe sanction.

10.6.3 Where a Driver is found to have committed two separate anti-doping rule violations, one involving a specified substance governed by the sanctions set forth in Article 10.3 (specified substances) and the other involving a Prohibited Substance or Prohibited Method governed by the sanctions set forth in Article 10.2 or a violation governed by the sanctions in Article 10.4.1, the period of Ineligibility imposed for the second offense shall be at a minimum two years' Ineligibility and at a maximum three years' Ineligibility. Any Driver found to have committed a third anti-doping rule violation involving any combination of specified substances under Article 10.3

and any other anti-doping rule violation under Article 10.2 or 10.4.1 shall receive a sanction of lifetime Ineligibility.

10.7 Disqualification of Results in Competitions Subsequent to Sample Collection

In addition to the automatic Disqualification of the results in the event which produced the positive Sample under Article 9 (Automatic Disqualification of Individual Results), all other competitive results obtained from the date a positive Sample was collected (whether In-Competition or Out-of-Competition), or other doping violation occurred, through the commencement of any Provisional Suspension or Ineligibility period, shall, unless fairness requires otherwise, be Disqualified with all of the resulting consequences including forfeiture of any medals, points and prizes.

10.8 Commencement of Ineligibility Period

The period of Ineligibility shall start on the date of the hearing decision providing for Ineligibility or, if the hearing is waived, on the date Ineligibility is accepted or otherwise imposed. Any period of Provisional Suspension (whether imposed or voluntarily accepted) shall be credited against the total period of Ineligibility to be served. Where required by fairness, such as delays in the hearing process or other aspects of Doping Control not attributable to the Driver, the U.I.M. or Anti-Doping Organization imposing the sanction may start the period of Ineligibility at an earlier date commencing as early as the date of Sample collection.

10.9 Status During Ineligibility

No Person who has been declared Ineligible may, during the period of Ineligibility, participate in any capacity in an Event or activity (other than authorized anti-doping education or rehabilitation programs) authorized or organized by the U.I.M. or any National Authority. In addition, for any anti-doping rule violation not involving specified substances described in Article 10.3, some or all sport-related financial support or other sport-related benefits received by such Person will be withheld by the U.I.M. and its National Authorities. A Person subject to a period of Ineligibility longer than four years may, after completing four years of the period of Ineligibility, participate in local sport events in a sport other than sports subject to the jurisdictions of the U.I.M. and its National Authority, but only so long as the local sport event is not at a level that could otherwise qualify such Person directly or indirectly to compete in (or accumulate points toward) a national championship or International Event.

10.10 Reinstatement Testing

As a condition to regaining eligibility at the end of a specified period of Ineligibility, a Driver must, during any period of Provisional Suspension or Ineligibility, make him or herself available for Out-of-Competition Testing by the U.I.M., the applicable National Authority, and any other Anti-Doping Organization having Testing jurisdiction, and must provide current and accurate whereabouts information as provided in Article 5.5. If a Driver subject to a period of Ineligibility retires from sport and is removed from Out-of-Competition Testing pools and later seeks reinstatement, the Driver shall not be eligible for reinstatement until the Driver has notified the U.I.M. and the applicable National Authority and has been subject to Out-of-Competition Testing for a period of time equal to the longer of the period set forth in Article 5.6 or the period of Ineligibility remaining as of the date the Driver had retired. During such remaining period of Ineligibility, a minimum of 2 tests must be conducted on the Driver with at least three months between each test. The National Authority shall be responsible for conducting the necessary tests, but tests by any Anti-Doping Organization may be used to satisfy the requirement. The results of such tests shall be reported to the U.I.M.. In addition, immediately prior to the end of the suspension period, a Driver must undergo Testing by the U.I.M. for the Prohibited Substances and Methods for Out-of-Competition Testing. Once the period of a Driver's suspension has expired, and the Driver has fulfilled the conditions of reinstatement, then the Driver will become automatically re-eligible and no application by the Driver or by the Driver's National Authority will then be necessary.

ARTICLE 11 CONSEQUENCES TO TEAMS

If a member of a team is found to have committed a violation of these Anti-Doping Rules during an Event, the team shall be Disqualified from the Event.

ARTICLE 12 SANCTIONS AND COSTS ASSESSED AGAINST NATIONAL Authorities

ARTICLE 13 APPEALS

13.1 Decisions Subject to Appeal

Decisions made under these Anti-Doping Rules may be appealed as set forth below in Article 13.2 through 13.4. Such decisions shall remain in effect while under appeal unless the appellate body orders otherwise.

13.2 Appeals from Decisions Regarding Anti-Doping Rule Violations, Consequences, and Provisional Suspensions

A decision that an anti-doping rule violation was committed, a decision imposing Consequences for an anti-doping rule violation, a decision that no

anti-doping rule violation was committed, a decision that the U.I.M. or its National Authority lacks jurisdiction to rule on an alleged anti-doping rule violation or its Consequences, may be appealed exclusively as provided in this Article 13.2.

13.2.1 In cases arising from competition in an International Event or in cases involving International-Level Drivers, the decision may be appealed to the U.I.M. Appeal Board and in last instance to the Court of Arbitration for Sport ("CAS") in accordance with the provisions applicable before such court.

13.2.2 In cases involving Drivers that do not have a right to appeal under Article 13.2.1, each National Authority shall have in place an appeal procedure that respects the following principles: a timely hearing, a fair and impartial hearing body; the right to be represented by a counsel at the person's expense; and a timely, written, reasoned decision.

13.2.3 In cases under Article 13.2.1, the following parties shall have the right to appeal to CAS: (a) the Driver or other Person who is the subject of the decision being appealed; (b) the other party to the case in which the decision was rendered; (c) the U.I.M. and any other Anti-Doping Organization under whose rules a sanction could have been imposed; and (d) WADA.

13.3 Appeals from Decisions Granting or Denying a Therapeutic Use Exemption

Decisions by WADA reversing the grant or denial of a TUE exemption may be appealed exclusively to CAS by the Driver, the U.I.M., or National Anti-Doping Organization or other body designated by a National Authority which granted or denied the exemption. Decisions to deny therapeutic use exemptions, and which are not reversed by WADA, may be appealed by International-Level Drivers to the U.I.M. Appeal Board and in last instance to CAS.

13.4 Appeal from Decisions Pursuant to Article 12

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13.5 Time for Filing Appeals

The time to file an appeal shall be twenty-one (21) days from the date of receipt of the decision by the appealing party. The above notwithstanding, the following shall apply in connection with appeals filed by a party entitled to appeal but which was not a party to the proceedings having lead to the decision subject to appeal:

a) Within ten (10) days from notice of the decision, such party/ies shall have the right to request from the body having issued the decision a copy of the file on which such body relied;

b) If such a request is made within the ten-day period, then the party making such request shall have twenty-one (21) days from receipt of the file to file an appeal to CAS.

**ARTICLE 14 NATIONAL Authorities INCORPORATION OF IF RULES,
REPORTING AND RECOGNITION**

14.1 Incorporation of U.I.M. Anti-Doping Rules

All National Authorities shall comply with these Anti-Doping Rules. These Anti-Doping Rules shall also be incorporated either directly or by reference into each National Authorities Rules. All National Authorities shall include in their regulations the procedural rules necessary to effectively implement these Anti-Doping Rules. Each National Authority shall obtain the written acknowledgement and agreement, in the form attached as Appendix 1, of all Drivers subject to Doping Control and Driver Support Personnel for such Drivers. Notwithstanding whether or not the required form has been signed, the Rules of each National Authority shall specifically provide that all Drivers, Driver Support Personnel and other Persons under the jurisdiction of the National Authority shall be bound by these Anti-Doping Rules.

14.2 Statistical Reporting

National Authorities shall report to the U.I.M. at the end of every year results of all Doping Controls within their jurisdiction sorted by Driver and identifying each date on which the Driver was tested, the entity conducting the test, and whether the test was In-Competition or Out-of-Competition. The U.I.M. may periodically publish Testing data received from National Authorities as well as comparable data from Testing under the U.I.M.'s jurisdiction.

14.3 Public Disclosure

Neither the U.I.M. nor its National Authority shall publicly identify Drivers whose Samples have resulted in Adverse Analytical Findings, or who were alleged to have violated other Articles of these Anti-Doping Rules until it has been determined in a hearing in accordance with Article 8 that an anti-doping rule violation has occurred, or such hearing has been waived, or the assertion of an anti-doping rule violation has not been timely. Once a violation of these Anti-Doping Rules has been established, it shall be publicly reported within 20 days.

14.4 Recognition of Decisions by U.I.M. and National Authorities

Any decision of the U.I.M. or a National Authority regarding a violation of these Anti-Doping Rules shall be recognized by all National Authorities, which shall take all necessary action to render such results effective.

ARTICLE 15 RECOGNITION OF DECISIONS BY OTHER ORGANISATIONS

Subject to the right to appeal provided in Article 13, the Testing, therapeutic use exemptions and hearing results or other final adjudications of any Signatory to the Code which are consistent with the Code and are within the Signatory's authority, shall be recognised and respected by U.I.M. and its National Authorities. The U.I.M. and its National Authorities may recognize the same actions of other bodies which have not accepted the Code if the rules of those bodies are otherwise consistent with the Code.

ARTICLE 16 STATUTE OF LIMITATIONS

No action may be commenced under these Anti-Doping Rules against a Driver or other Person for a violation of an anti-doping rule contained in these Anti-Doping Rules unless such action is commenced within eight years from the date the violation occurred.

ARTICLE 17 U.I.M. COMPLIANCE REPORTS TO WADA

The U.I.M. will report to WADA on the U.I.M.'s compliance with the Code every

second year and shall explain reasons for any noncompliance.

ARTICLE 18 AMENDMENT AND INTERPRETATION OF ANTI-DOPING RULES

18.1 These Anti-Doping Rules may be amended from time to time by the U.I.M. General Assembly on Proposal of the U.I.M. Medical Commission or Council

18.2 Except as provided in Article 18.5, these Anti-Doping Rules shall be interpreted as an independent and autonomous text and not by reference to existing law or statutes.

18.3 The headings used for the various Parts and Articles of these Anti-Doping Rules are for convenience only and shall not be deemed part of the substance of these Anti-Doping Rules or to affect in any way the language of the provisions to which they refer.

18.4 The INTRODUCTION and the APPENDIX I DEFINITIONS shall be considered integral parts of these Anti-Doping Rules.

18.5 These Anti-Doping Rules have been adopted pursuant to the applicable provisions of the Code and shall be interpreted in a manner that is consistent with applicable provisions of the Code. The comments annotating various provisions of the Code may, where applicable, assist in the understanding and interpretation of these Anti-Doping Rules.

18.6 Notice to a Driver or other Person who is a member of a National Authority may be accomplished by delivery of the notice to the National Authority.

18.7 These Anti-Doping Rules shall not apply retrospectively to matters pending before the date these Anti-Doping Rules came into effect.

APPENDIX 1 - DEFINITIONS

Adverse Analytical Finding. A report from a laboratory or other approved Testing entity that identifies in a Specimen the presence of a Prohibited Substance or its Metabolites or Markers (including elevated quantities of endogenous substances) or evidence of the Use of a Prohibited Method.

Anti-Doping Organization. A Signatory that is responsible for adopting rules for initiating, implementing or enforcing any part of the Doping Control process. This includes, for example, the International Olympic Committee, the International Paralympic Committee, other Major Event Organizations that conduct Testing at their Events, WADA, International Authorities, and National Anti-Doping Organizations.

Driver Support Personnel. Any coach, trainer, manager, agent, team staff, official, medical or para-medical personnel working with or treating Drivers participating in or preparing for sports competition.

Attempt. Purposely engaging in conduct that constitutes a substantial step in a course of conduct planned to culminate in the commission of an anti-doping rule violation. Provided, however, there shall be no anti-doping rule violation based solely on an Attempt to commit a violation if the Person renounces the attempt prior to it being discovered by a third party not involved in the Attempt.

Code. The World Anti-Doping Code.

Competition. A single race, match, game or singular athletic contest. For example, the finals of the Olympic 100-meter dash. For stage races and other athletic contests where prizes are awarded on a daily or other interim basis the distinction between a Competition and an Event will be as provided in the rules of the applicable International Federation.

Consequences of Anti-Doping Rules Violations. An Driver's or other Person's violation of an anti-doping rule may result in one or more of the following: (a) Disqualification means the Driver's results in a particular Competition or Event are invalidated, with all resulting consequences including forfeiture of any medals, points and prizes; (b) Ineligibility means the Driver or other Person is barred for a specified period of time from participating in any Competition or other activity or funding as provided in Article 10.9; [and (c) Provisional Suspension means the Driver or other Person is barred temporarily from participating in any Competition prior to the final decision at a hearing conducted under Article 8 (Right to a Fair Hearing).]

Disqualification. See Consequences of Anti-Doping Rules Violations above.

Doping Control. The process including test distribution planning, Sample collection and handling, laboratory analysis, results management, hearings and appeals.

Driver. For purposes of Doping Control, any crew Member of a race boat; i.e. helmsman, throttle man, navigator, technician or other crew member being present on board during the race or other competition/ record attempt.

In-Competition. For purposes of differentiating between In-Competition and Out-of-Competition Testing, unless provided otherwise in the rules of an International Federation or other relevant Anti-Doping Organization, an In-Competition test is a test where an Driver is selected for testing in connection with a specific Competition.

Independent Observer Program. A team of observers, under the supervision of WADA, who observe the Doping Control process at certain Events and report on observations. If WADA is testing In-Competition at an Event, the observers shall be supervised by an independent organization.

Ineligibility. See Consequences of Anti-Doping Rules Violations above.

International Event. An Event where the International Olympic Committee, the International Paralympic Committee, an International Federation, a Major Event Organization, or another international sport organization is the ruling body for the Event or appoints the technical officials for the Event.

International-Level Driver. Drivers designated by one or more International Federations as being within the Registered Testing Pool for an International Federation.

International Standard. A standard adopted by WADA in support of the Code. Compliance with an International Standard (as opposed to another alternative standard, practice or procedure) shall be sufficient to conclude that the procedures addressed by the International Standard were performed properly.

Major Event Organizations. This term refers to the continental associations of National Olympic Committees and other international multi-sport organizations that function as the ruling body for any continental, regional or other International Event.

Marker. A compound, group of compounds or biological parameters that indicates the Use of a Prohibited Substance or Prohibited Method.

Metabolite. Any substance produced by a biotransformation process.

Minor. A natural Person who has not reached the age of majority as established by the applicable laws of his or her country of residence.

National Anti-Doping Organization. The entity(ies) designated by each country as possessing the primary authority and responsibility to adopt and implement anti-doping rules, direct the collection of Samples, the management of test results, and

the conduct of hearings, all at the national level. If this designation has not been made by the competent public authority(ies), the entity shall be the country's National Olympic Committee or its designee.

National Event. A sport Event involving international or national-level Drivers that is not an International Event.

National Authority. A national or regional entity which is a member of or is recognized by the U.I.M. as the entity governing the U.I.M.'s sport in that nation or region.

National Olympic Committee. The organization recognized by the International Olympic Committee. The term National Olympic Committee shall also include the National Sport Confederation in those countries where the National Sport Confederation assumes typical National Olympic Committee responsibilities in the anti-doping area.

No Advance Notice. A Doping Control which takes place with no advance warning to the Driver and where the Driver is continuously chaperoned from the moment of notification through Sample provision.

No Fault or Negligence. The Driver's establishing that he or she did not know or suspect, and could not reasonably have known or suspected even with the exercise of utmost caution, that he or she had Used or been administered the Prohibited Substance or Prohibited Method.

No Significant Fault or Negligence. The Driver's establishing that his or her fault or negligence, when viewed in the totality of the circumstances and taking into account the criteria for No Fault or Negligence, was not significant in relationship to the anti-doping rule violation.

Out-of-Competition. Any Doping Control which is not In-Competition.

Participant. Any Driver or Driver Support Personnel.

Person. A natural Person or an organization or other entity.

Possession. The actual, physical possession, or the constructive possession (which shall be found only if the person has exclusive control over the Prohibited Substance/Method or the premises in which a Prohibited Substance/Method exists); provided, however, that if the person does not have exclusive control over the Prohibited Substance/Method or the premises in which a Prohibited Substance/Method exists, constructive possession shall only be found if the person knew about the presence of the Prohibited Substance/Method and intended to exercise control over it. Provided, however, there shall be no anti-doping rule violation based solely on possession if, prior to receiving notification of any kind that the Person has committed an anti-doping rule violation, the Person has taken

concrete action demonstrating that the Person no longer intends to have Possession and has renounced the Person's previous Possession.

Prohibited List. The List identifying the Prohibited Substances and Prohibited Methods.

Prohibited Method. Any method so described on the Prohibited List.

Prohibited Substance. Any substance so described on the Prohibited List.

Provisional Hearing. For purposes of Article 7.5, an expedited abbreviated hearing occurring prior to a hearing under Article 8 (Right to a Fair Hearing) that provides the Driver with notice and an opportunity to be heard in either written or oral form.

[Provisional Suspension. See Consequences above.]

Publicly Disclose or Publicly Report. To disseminate or distribute information to the general public or persons beyond those persons entitled to earlier notification in accordance with Article 14.

Registered Testing Pool. The pool of top level Drivers established separately by each International Federation and National Anti-Doping Organization who are subject to both In-Competition and Out-of-Competition Testing as part of that International Federation's or Organization's test distribution plan.

Sample/Specimen. Any biological material collected for the purposes of Doping Control.

Signatories. Those entities signing the Code and agreeing to comply with the Code, including the International Olympic Committee, International Federations, International Paralympic Committee, National Olympic Committees, National Paralympic Committees, Major Event Organizations, National Anti-Doping Organizations, and WADA.

Tampering. Altering for an improper purpose or in an improper way; bringing improper influence to bear; interfering improperly to alter results or prevent normal procedures from occurring.

Target Testing. Selection of Drivers for Testing where specific Drivers or groups of Drivers are selected on a non-random basis for Testing at a specified time.

Team Sport. A sport in which the substitution of players is permitted during a Competition.

Testing. The parts of the Doping Control process involving test distribution planning, Sample collection, Sample handling, and Sample transport to the laboratory.

Trafficking. To sell, give, administer, transport, send, deliver or distribute a Prohibited Substance or Prohibited Method to an Driver either directly or through one or more third parties, but excluding the sale or distribution (by medical personnel or by Persons other than an Driver's Support Personnel) of a Prohibited Substance for genuine and legal therapeutic purposes.

Use. The application, ingestion, injection or consumption by any means whatsoever of any Prohibited Substance or Prohibited Method.

WADA. The World Anti-Doping Agency.



APPENDIX 2 - Acknowledgment and Agreement

I, as a member of [National Authority] and/or a participant in a [National Authority or U.I.M.] authorized or recognized event, hereby acknowledge and agree as follows:

- 1.** I have received and had an opportunity to review the [U.I.M.] Anti-Doping Rules.
- 2.** I consent and agree to comply with and be bound by all of the provisions of the [U.I.M.] Anti-Doping Rules, including but not limited to, all amendments to the Anti-Doping Rules and all International Standards incorporated in the Anti-Doping Rules.
- 3.** I acknowledge and agree that National Authorities and the U.I.M. have jurisdiction to impose sanctions as provided in the U.I.M. Anti-Doping Rules.
- 4.** I also acknowledge and agree that any dispute arising out of a decision made pursuant to the U.I.M. Anti-Doping Rules, after exhaustion of the process expressly provided for in the U.I.M. Anti-Doping Rules, may be appealed finally as provided in Article 13 of the U.I.M. Anti-Doping Rules to an appellate body for final and binding arbitration, which in the case of International-Level Drivers is the Court of Arbitration for Sport.
- 5.** I acknowledge and agree that the decisions of the arbitral appellate body referenced above shall be final and enforceable, and that I will not bring any claim, arbitration, lawsuit or litigation in any other court or tribunal.
- 6.** I have read and understand this Acknowledgement and Agreement.

Date

Print Name (Last Name, First Name)

Date of Birth
(Day/Month/Year)

Signature (or, if a minor, signature of
legal guardian)